



STRATEGIC PLANNING AND DEVELOPMENT COMMITTEE MEETING

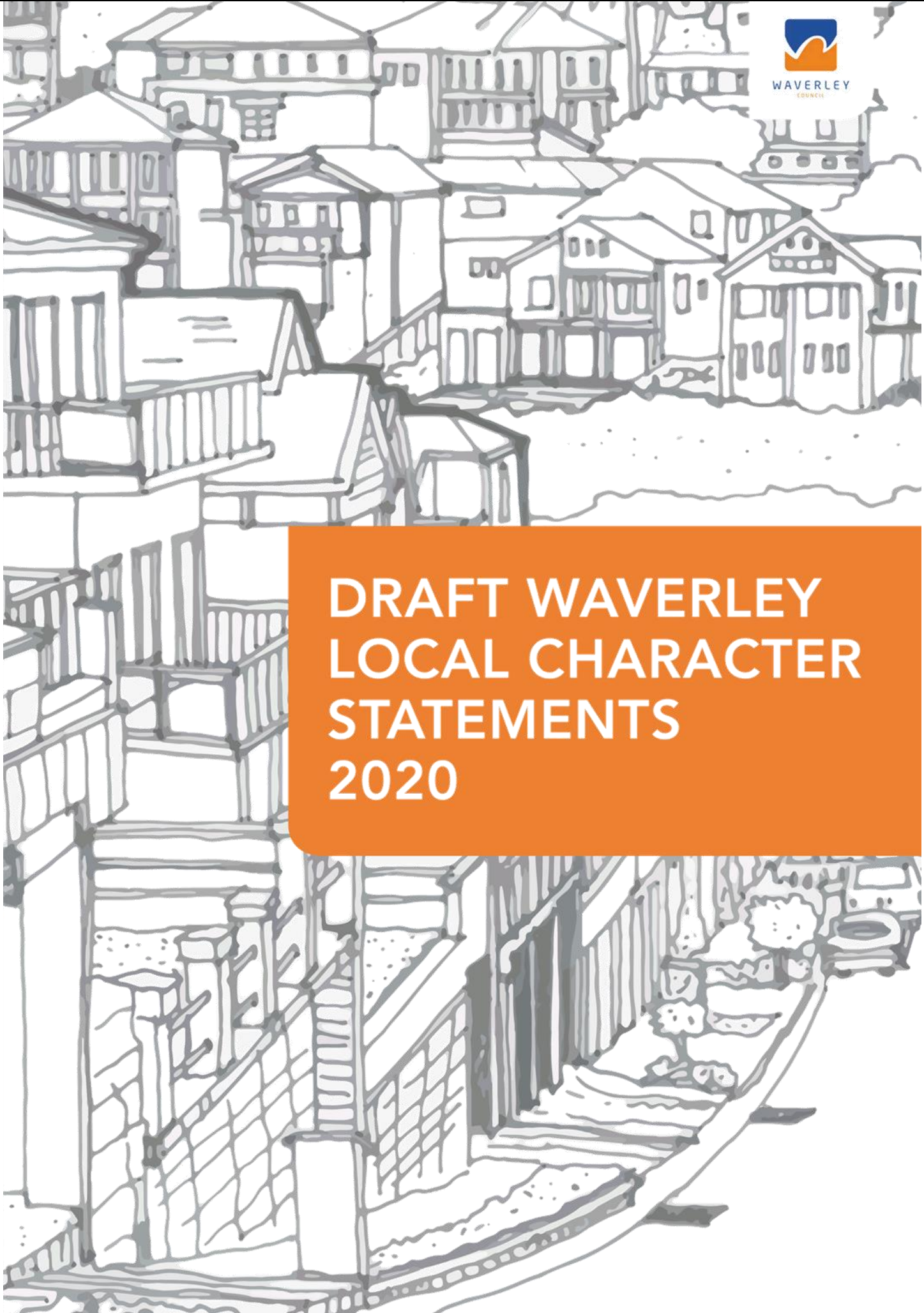
ATTACHMENTS UNDER SEPARATE COVER

7.30 PM, TUESDAY 1 SEPTEMBER 2020

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ATTACHMENTS UNDER SEPARATE COVER

PD/5.2/20.09	Draft Waverley Local Character Statements – Exhibition	
1	Draft Waverley Local Character Statements 2020	2
PD/5.5/20.09	Bondi Junction Urban Design Review Update - Adoption	
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A detailed line drawing of a residential street scene. It shows a row of houses with various rooflines, balconies, and fences. In the foreground, there's a stone wall and a path leading towards the houses. The style is a sketchy, hand-drawn illustration.

DRAFT WAVERLEY LOCAL CHARACTER STATEMENTS 2020

ACKNOWLEDGEMENT

We acknowledge the Bidjigal and Gadigal people who
traditionally occupied the Sydney coast.

We also acknowledge Aboriginal Elders past, present and
emerging.

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NOTE: Unless stated all photographs and graphics are authored by Waverley Council.



1 EXECUTIVE SUMMARY

“Waverley: connecting the city and the sea. A welcoming and cohesive community that celebrates and enhances our spectacular coastline, vibrant places, and rich cultural heritage” (Waverley Community Strategic Plan vision)

1.1 What is local character?

Local character is what makes one area distinctive from another. It is the way an area ‘looks’ and ‘feels’. Character is created through the interrelation of distinctive natural and built elements in the public and private domains, including topography, vegetation, streetscape, built form, activity types, as well as the emotional and cultural experience of a place.

All areas in the Waverley local government area (LGA) have character, however in some, the character may be more identifiable, more unusual, or more attractive and what is important in one area might be different in another – from vibrant local centres, leafy streets, consistent dwelling typologies and architectural styles, to areas and items of heritage significance, and access to coastal views and open space.

This character makes the Waverley LGA an attractive place to live, work and visit. Consequently, the area is subject to ongoing redevelopment pressures and change, such as:

- Alterations and additions to existing dwellings, including second-story and parking structure (e.g. enclosed garage) additions, rooftop additions comprising apartment dwellings or consolidation of apartments,
- Replacement of existing dwellings with ‘higher-value’ dwellings and densities, such as residential flat buildings (RFBs) or dual-occupancies, and
- Exempt and Complying Development undertaken in accordance with State Environmental Planning Policies (SEPPs), for example the *State Environmental Planning Policy (Exempt and Complying Development Codes) 2008* (Codes SEPP).

If not managed, areas may be impacted by new development, which may be insensitive to the area’s existing character, in terms of building height, landscaped areas and car parking provision etc.

Respecting local character does not mean that change cannot occur, rather, it means that good planning and design needs to be implemented to ensure new development is well-considered and complements the characteristics of an area. Built form, bulk, scale and height as well as landscaping and materiality all play a part in ensuring the character of an area is maintained and / or enhanced whilst still allowing for new development to occur. Accordingly, it is important that Council and the community create a shared vision for an area and how it will change over time, including identifying existing characteristics to be maintained and / or enhanced.

1.2 Local character study and statements

The *draft Waverley Local Character Statements 2020* (draft Local Character Statements) form part of Phase 4 of the Comprehensive LEP Review process and deliver on Action 7.3 of the Waverley Local Strategic Planning Statement (LSPS) and Action 5.1 of the Waverley Local Housing Strategy (LHS). The local character work will also deliver on the NSW Government’s direction (following 2016 planning reforms) to elevate the consideration of local character in planning and decision-making.

The draft Local Character Statements form part of a suite of environmental studies that provide an evidence base to strengthen the integration of local character considerations into the Waverley planning framework. This will occur first as part of the strategic

6 planning process and then through its translation into statutory controls, in the comprehensive review of the Waverley Local Environmental Plan 2012 (WLEP) and the Waverley Development Control Plan 2012 (WDCP). Specifically, this work will help Council prioritise place-based planning and local character when assessing Planning Proposals (PPs) and Development Applications (DAs).

Elevating the consideration of local character in planning and decision-making requires a comprehensive examination of the character of an area through qualitative and quantitative assessment. Synthesising information from a variety of State and local sources, the local character study identifies and describes the high-level social, environmental and economic characteristics of 23 defined areas across the Waverley LGA (with the exception of Bondi Junction). Characteristics include housing type and architectural style, density and height, heritage and culture, parking, public / private domain interface, open space, topography and tree canopy. The 23 areas provide a narrowed focus for the local character

study (see Figure 1). The local character study has informed the preparation of a written and graphic statement for the 23 areas. Each statement comprises a high-level description of the area's existing characteristics and key character attributes as well as desired future character objectives.

This is the first time that Council has undertaken a local character study for the whole LGA. The study seeks to complement the work on local character already undertaken in the WDCP, and other strategic studies including, the Waverley LHS, *draft Waverley Village Centres Strategy (VCS)*, *draft Waverley Council Heritage Assessment 2020* and the Bondi Junction Urban Design Review Update 2020. It does not seek to duplicate the detailed description and actions for areas outlined within these studies, for example Waverley's village centres. The study has been informed by the DPIE's 'Character Assessment Toolkit' and 'Local Character Wheel' documented in the *Local Character and Place Guideline* (2019).

1.3 Community influence in identifying local character

'The community' is any individual, group or organisation that identifies or has an interest in a place. It includes, residents, landowners, business owners, community organisations, visitors and workers, government agencies and statutory bodies.

The character of an area is subjective in nature. The community use and experience a place differently, and what are important characteristics to some may be different to others, making the planning for the future of an area more difficult. Effective consultation with the community is required to hear how places across the Waverley LGA are experienced and valued and to set an agreed vision for how an area may change or evolve over time.

Council undertook extensive community consultation for the preparation of the Waverley Community Strategic Plan (CSP), Waverley LSPS, Waverley Local Housing Discussion Paper and draft Waverley VCS. The feedback received as part of these consultations, including that documented in the Community Strategic Plan Engagement Report 2018 and Village Centres Strategy Consultation Summary Report – Engagement August-September 2019, have informed the preparation of the local character study and draft Waverley Local Character Statements 2020.

The character study and statements are a starting point to identify and describe the existing and future character of an area. Community feedback will be used to refine these documents and inform potential changes to the WLEP and WDCP. Where changes are proposed, the community will have further opportunities to provide feedback.



Have Your Say!

You can provide your feedback by making a formal submission to the General Manager of Waverley Council by emailing info@waverley.com.au or connecting directly with Council staff working on this project via the Waverley Have Your Say webpage.



Figure 1 - Draft Local Character Areas map

1.4 Local character and heritage

“Heritage and local character are often intrinsically linked, and individual places can be heavily influenced by their existing built or natural environment, creating a rich tapestry from which to build. Rather than create a distinction between past and present, heritage can be used as a foundation of local character” (Justin Hewitt, Local Character and Place Collection, 2019, p.19).

The intrinsic relationship between heritage and character is acknowledged, however there remain important differences between areas or items of heritage significance and areas of local character, in terms of the assessment process and current legislative status, within the planning system.

A Heritage Conservation Area (HCA) is an umbrella term with specific Conservation Areas sitting below them. For example, an ‘Urban Conservation Area’ (a HCA with special architectural or historic interest that includes built form) and a ‘Landscape Conservation Area’ (a HCA that contains significant landscape / parklands etc.) Heritage items are buildings, archaeological remains, trees etc. that is an exemplar of its type.

HCAs or heritage items are assessed against the criteria set out in the NSW Heritage Manual, with reference to the national best-practice standards contained in the Burra Charter (Australia ICOMOS, 2013), to determine whether it has heritage significance and therefore warrants listing. Areas and items of heritage significance within the Waverley LGA are identified in Schedule 5 of the WLEP.

If a building is included within a HCA or if it is individually heritage listed, it does not mean that works proposed to the building are not possible. What it does mean is that any proposed changes to the building may require a heritage exemption or a Development Application (DA) to Council with a Heritage Impact Statement included, as specified in cl.5.10 of the WLEP and Part B9 of the WDCP. It also means that Exempt and Complying Development, for example under the Codes SEPP, cannot be undertaken on a building within a HCA or that is individually listed.

Local character is much broader than heritage. Each Council is free to choose how to undertake a local character study, using the DPIE’s Local Character and Place Guideline as a guide. Local character considerations go beyond historic and built form considerations to include all elements that contribute to the look and feel of an area including the public and private domains, including topography, vegetation, streetscape and activity types. Character elements of an area could be wide verges, leafy streets, dwelling typologies, styles or materials, consistent setbacks, significant views, etc.

Further, each Council can choose how to integrate the consideration of local character into the planning framework. Currently, character areas may be included within a standalone local character statement or within a Council’s DCP. A DA to Council for new development, if located in a character area, would be required to consider the DCP objectives and controls relating to that area. The opportunity for inclusion of a local character area within a Council’s LEP, as a local character overlay (LCO) is still being determined by the DPIE (refer to Part 3 for further information). Further, the opportunity for the exclusion or local exemption of a character area from Exempt and Complying Development would be subject to agreement by the DPIE (refer to Part 3 for further information). Therefore, a character area does not have the same legislative weight as a HCA or heritage item.

Identifying an area of local character is not an alternative form of heritage listing. The local character study has drawn from the detailed body of work undertaken as part of the draft Waverley Council Heritage Assessment 2020. It does not seek to duplicate the detail of this assessment, however, utilises the historic narrative to inform the local character statements and to identify areas of character.

1.5 Local character and Exempt and Complying Development

State Environmental Planning Policies (SEPPs) are state-legislated plans prepared by the NSW Government. They specify planning controls for certain areas and / or types of development. SEPPs have greater weight than Council's plans and policies. Common SEPPs that apply to the Waverley LGA are the Codes SEPP, Seniors SEPP, ARHSEPP and the *State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017* (Education SEPP).

Some SEPPs, where they apply, enable Exempt Development, being development that can be undertaken without approval, and Complying Development, being development that requires a Complying Development Certificate approved by an accredited Certifier under a fast-track assessment process. Types of development can include:

- Construction of a new building (e.g. a dwelling, secondary dwelling, school)
- Alterations and additions to an existing building
- Demolition of a building
- Internal alterations (fit-out) of commercial / retail premises.

Exempt and Complying Development does not require a Development Application to Council. Therefore, local design objectives and controls in Council's LEP and DCP do not apply. This includes local character considerations.

Whilst Exempt and Complying Development allows 'minor impact development', the application of some design controls is seen to create a potential conflict with the character of an area, in terms of building height, landscaped areas, car parking provision, materiality etc. For example, many streets in Waverley are characterised by their lack of driveways and on-site car parking, providing for larger landscaped frontages and mature street tree planting. Application of generic design controls in these areas could be detrimental to the character of these streetscapes.

Where an area's character has been identified as being highly valued in the local character study and through community consultation, there may be the opportunity to, in limited circumstances, introduce local variations to or seek a local exclusion from part of a SEPP, subject to the agreement of the Department of Planning, Industry and Environment (DPIE). It is noted that this approach will not be warranted for all character areas, or even the entirety of a character area.

For certain character areas, the introduction of a local variation would enable for certain character areas, the ability to introduce alternative controls within a SEPP for attributes that strongly affect the character of the area. For example, requiring a certain roof design (gable in Federation suburb), or no on-site car parking or driveway crossover. A local exclusion would exclude an area, which requires variation to the controls beyond the scope of a local variation, from part(s) of a SEPP, for example the LRMDHC. Local variations and exclusions are existing mechanisms within the state policy.

Further investigation and analysis are required to determine where the application of SEPPs, particularly the Codes SEPP, may result in development that is incompatible with an area's existing character, document the design controls that would result in this incompatibility, and propose alternative controls. This analysis would inform any request to the DPIE for local variations or exclusions. Requests would need to address and satisfy a state-wide set of clear criteria. Any variations or exclusions would be subject to agreement by the DPIE. Further, Council will continue to advocate for greater consideration of local character within State policy.

10 1.6 Recommendations

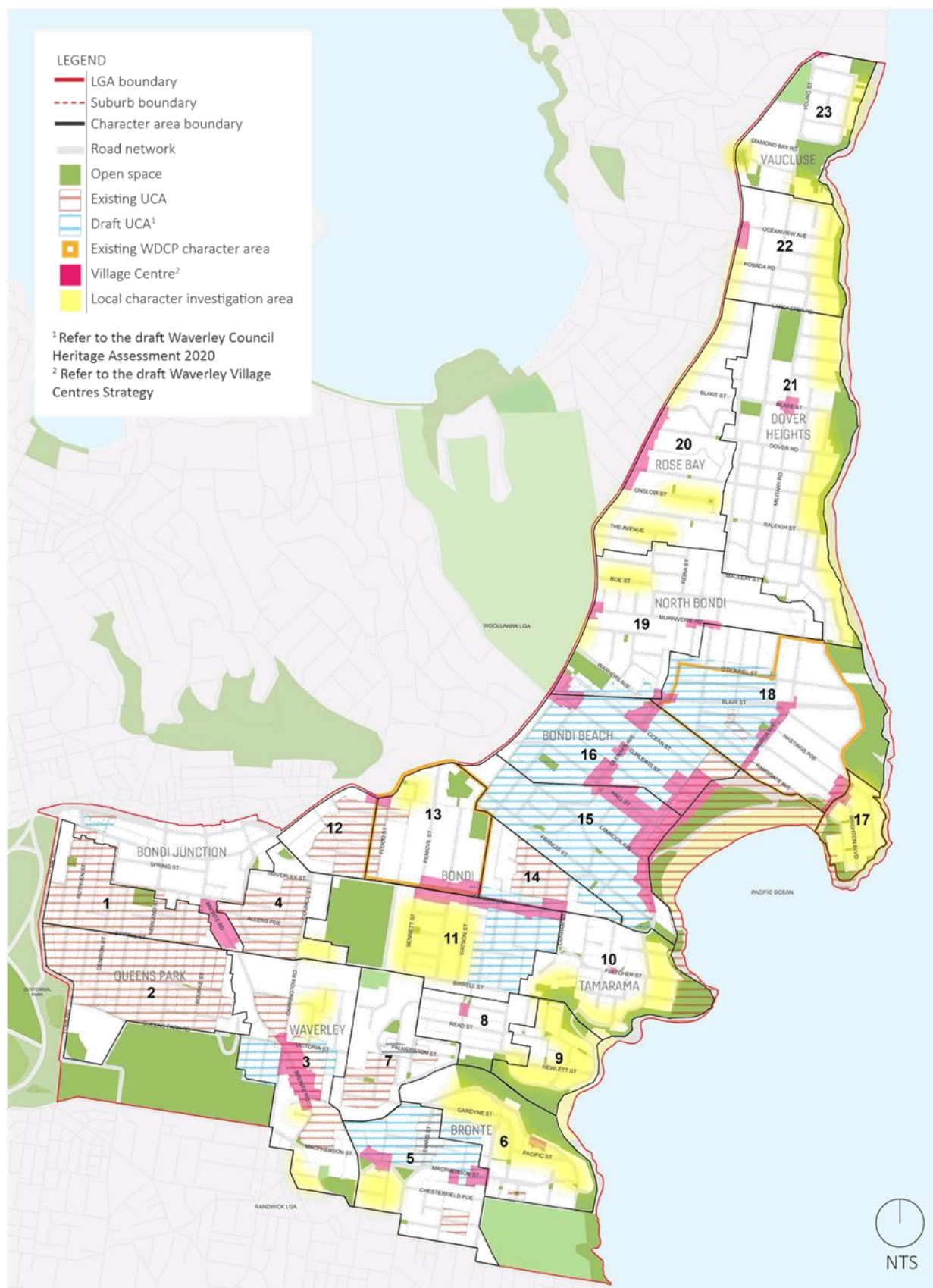
The local character study and statements have identified a number of recommendations for further investigation (see Table 1). These recommendations may support priorities and actions from other strategic studies including the Waverley LSPS and Waverley LHS.

Figure 2 identifies the investigation areas that warrant further review.

It is noted that the draft Local Character Areas include Waverley's village centres. These centres contribute to the social and economic character of each area. However, the draft Local Character Statements do not seek to duplicate the detailed description and recommendations of the draft VCS, including the desired future character of each centre. The draft Statements seek to complement the draft VCS, rather than duplicate significant work already undertaken. Both documents will inform the review of the Waverley LEP and DCP.

Table 1 - Key recommendations

Recommendation
1 Research and analysis
1.1. Undertake a further review of preliminary local character investigation areas (see Figure 2) to identify areas of high character value that may warrant more tailored planning objectives and controls in the WLEP and / or WDCP, or exemption from the Codes SEPP.
1.2. Consider policy development for particular types of development or areas, for example alterations and additions to Inter-War RFBs, local centres.
2 Review of local planning policy
2.1. Undertake a review of WLEP to identify inconsistencies (if they exist) between existing character and desired future character in respect of the zone objectives and development standards, including height of building and floor space ratio.
2.2. Undertake a review of the WDCP to ensure current development objectives and controls align with the local character study outcomes, for example protection of view corridors, alterations and additions to existing dwellings, adaptive reuse, vegetation coverage etc.
2.3. Undertake a review of existing WDCP to determine relevancy of existing character areas and investigate the inclusion of amended or new character areas, with associated planning objectives and controls (refer to recommendation 1.1)
3 Review of State planning policy
3.1. Undertake a review of the applicability of the Codes SEPP, including the LRMDHC, within the Waverley LGA to determine: <ul style="list-style-type: none"> • Application of the Code across the LGA (this may involve mapping of Complying Development hotspots, or mapping where the LRMDHC could apply), • Inconsistencies between the Code and Council's controls, that may result in an erosion of local character, • Whether Council's controls need to be updated to reflect the LRMDHC to promote a Development Assessment pathway, and • Whether local variations or exemptions from the Code is warranted.
3.2. Undertake a review of the applicability of the Seniors SEPP within the Waverley LGA to: <ul style="list-style-type: none"> • Identify opportunity sites that may warrant tailored planning objectives and controls, and • Investigate the application of new controls including zone objectives in the WLEP to encourage seniors housing that is consistent with desired future character.
4 Advocacy
4.1. Continue to work with the DPIE and advocate for greater consideration of local character within State policy, including local variations and exemptions where warranted.





2 DRAFT LOCAL CHARACTER STATEMENTS

“Through these descriptions of place, we can get a sense of the layering of social, natural and built elements that make that place distinct from any other. It is this layering of elements that creates a place’s individual local character” (Local Character and Place Collection, 2019)

2.1 Methodology

Synthesising information from a variety of State and local sources, the local character study identifies and describes the social, environmental and economic characteristics of 23 defined areas across the Waverley LGA (see Figure 3). Characteristics include housing type and style, density and height, heritage and culture, parking, public / private domain interface, open space, topography and tree canopy.

The boundaries of each area have been informed by natural features, open space, primary roads, change in land use of building typologies, existing and former character areas and / or suburb boundaries. The 23 areas provide a narrowed focus for the local character study.

The Bondi Junction Strategic Centre has been excluded from the character study. A number of studies and strategies have been completed for Bondi Junction in the past decade or so, including the Bondi Junction Complete Streets Project (2013), Bondi Junction Commercial Centre Review (2017) and the Bondi Junction Urban Design Review (2020). It is considered that the existing documents holistically and sufficiently address the existing and desired future character of the Bondi Junction centre. To avoid duplication of significant work, the centre has been excluded from this local character study.

The local character study has informed the preparation of a written and graphic statement for the 23 areas. Each statement comprises:

- A high-level description of the area’s existing characteristics, using text, photos and maps. The existing characteristics include the history, configuration and connectivity, built form, public and private interface, natural environment and recent development within an area,
- A rating of the area’s key existing character attributes (see Table 2), and
- A desired future character statement and objectives for the area.

2.2 Limitations

The local character study was undertaken using qualitative and quantitative data sources, each with their own limitations. Review and interpretation is subject to a degree of personal bias, as the character of an area is subjective in nature. Consultation with the community and other stakeholders will be important in validating the character statements and desired future character objectives.

Further, the study was undertaken at a relatively high-level across each area. Therefore, the statements, including the desired future character objectives, comprise a level of generality, focusing on types / groups of development, trends etc. as opposed to individual sites or dwellings.



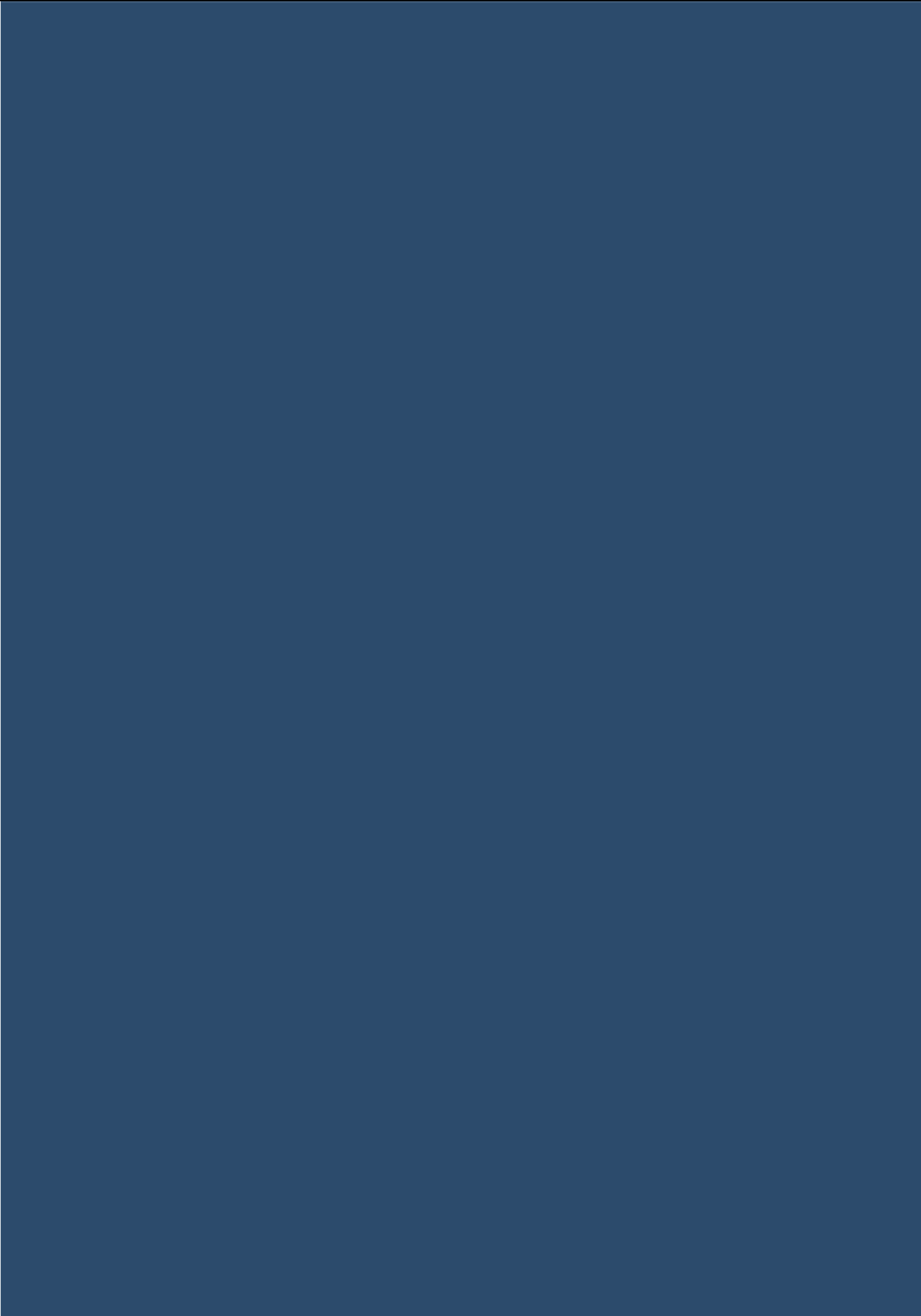
Figure 3 - Draft Local Character Areas map

Table 2 - Existing character attributes

Attribute	Description	Indicator
Built form	Refers to the typology and architectural style of buildings within an area	Inconsistent - Consistent
Height of buildings	Refers to the prevailing building height within an area	Low-rise (1-2 storeys)- high-rise (8+ storeys)
Subdivision pattern	Refers to the interaction between the public and private domain, connecting building and street within an area i.e. width of street corridor and verges, drive-way crossovers, sense of enclosure or openness, front setback depth (i.e. the distance between property boundary and front building line), visibility of building façade, fence treatment, front landscaping, parking structures	Small lots - large lots
Public and private domain interface	Refers to the level of vegetation coverage within an area i.e. mature canopy, street trees, density of vegetation, type of vegetation	Inconsistent - Consistent
Vegetation coverage	Refers to quantum of open space within an area i.e. beaches, parks, playgrounds and significant landscape features within an area i.e. topographical changes, coastline, cliffscapes	Low coverage (not leafy) - High coverage (leafy)
Open space and landscape features	Refers to quantum of open space within an area i.e. beaches, parks, playgrounds and significant landscape features within an area i.e. topographical changes, coastline, cliffscapes	Low - High
Views	Refers to the availability of views to and from an area	Low - High
Access and connectivity	Refers to the level of access and connectivity to and from an area by the road network, public and active transport	Poorly connected - well connected
Heritage significance	Refers to urban and landscape conservation areas and / or general, landscape, archaeological and aboriginal items within an area	Low - High
Diversity of uses	Refers to the variety of land uses, for example residential, commercial, retail, educational, religious, open space within an area	Low - High

Existing character attributes example...





CHARACTER AREA

1



CHARACTER AREA – 1

“The area is defined by its historic streetscapes, arranged in a linear north-south grid, comprising predominantly low-rise, closely-set, late-Victorian and Federation-style terraces, detached and semi-detached dwellings; limited driveways, regular street trees; proximity to Bondi Junction and significant public open space”.

The study area is generally bound by the rear of properties fronting Oxford and Ebley streets to the north, the rear of properties fronting Bronte Road to the east, Birrell Street to the south and York Road to the west.



Figure 1-1 - Local character area #01 map



Figure 1-2 - Heritage-listed terrace dwellings, St James Road



Figure 1-4 - Heritage-listed terrace dwellings, Ruthven Street

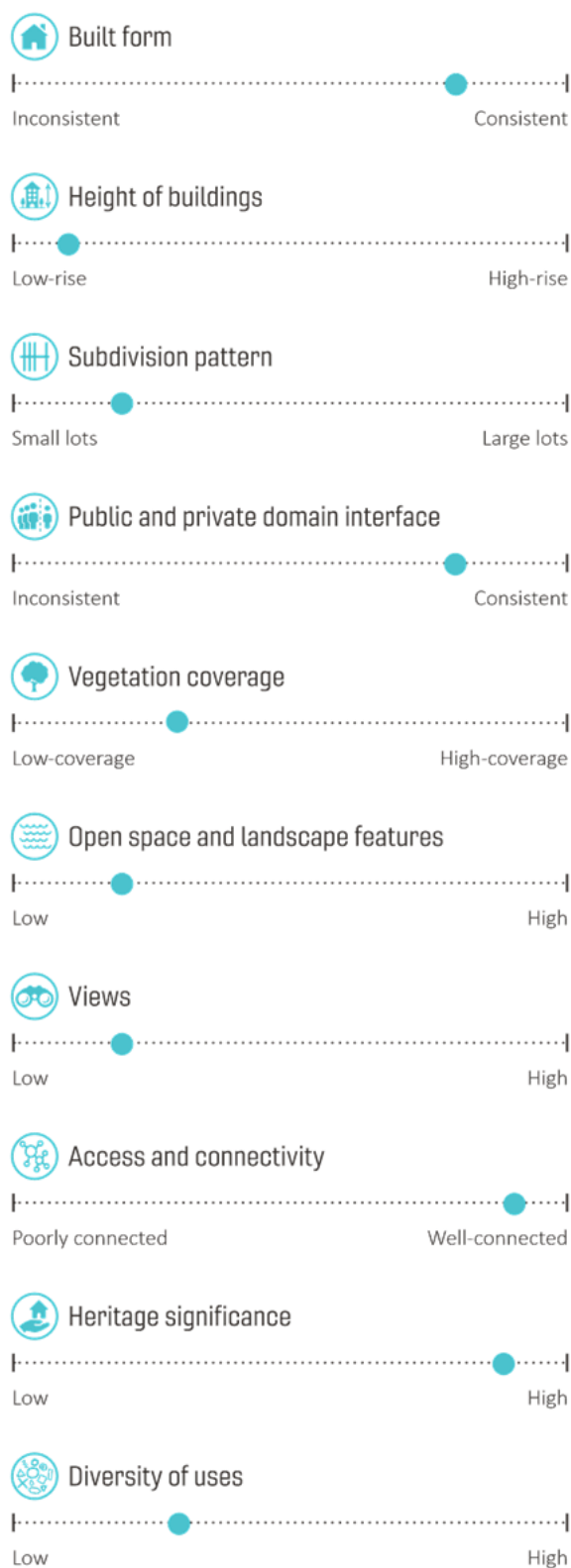


Figure 1-3 - Modern terrace dwellings, Hough Street



Figure 1-5 - Clemenston Park

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by well-maintained historic streetscapes of Federation-style dwellings, amidst avenues of leafy trees and vegetation.

Objectives

The objectives for development on land identified within the character area are as follows:

- To ensure an appropriate physical and visual curtilage is provided for Centennial Park, including its skyline, and nearby historic streetscapes / areas,
- To preserve the heritage items and the integrity of the Urban Conservation Area,
- To maintain the historically distinctive fine-grain pattern of subdivision and associated terrace housing typology,
- To maintain the predominant 1-2 storey height character of the area,
- To discourage demolition and promote sympathetic additions that retain the scale and massing of front elevations and the original roof form as viewed from the primary street frontage,
- To ensure new development respects the historic patterns, original built form, architectural styles, materials and details of the area,
- To ensure a transition between the Bondi Junction commercial core and residential dwellings within the area, for example Denison Street,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To limit new driveway crossovers and car parking within the front setback, by retaining vehicle access from rear lanes,
- To maintain and enhance the quality of urban plazas and parks, particularly solar access,
- To maintain and grow the canopy and provide increased opportunities for urban greening.

20 Existing character description

History

The area’s character is derived from the forms, streetscapes and social relationship of industry, worker housing and commerce in the early years of the Waverley municipality. Initial land grants of the 1830s-1840s were used for dairying, animal husbandry, quarrying and flour milling. The grid pattern of the early grants and initial streets established along their boundaries provided the basis for later subdivisions, which are reflected in the configuration of the area today. Improvements in transport and the rapid expansion of suburban Sydney between 1880-1915 saw the consolidation of Bondi Junction as a commuter suburb resulting in the development of the area for worker housing. Linear blocks were divided into deep, narrow-fronted allotments. The area remains predominantly residential with limited commercial, retail, educational and transport uses throughout.

Key uses / landmarks

Clemenston Park forms part of an important local community hub, which includes the Mill Hill Early

Education Centre, The GRACE Child Care Centre and Waverley Community Garden. The area also benefits from close proximity to Waverley Library and Mill Hill Centre in Bondi Junction. These places provide social and cultural programs and services supporting the wider Waverley community.

Configuration and connectivity

Blocks are typically arranged in a linear north-south alignment, across the area’s topography which slopes from the natural ridgeline along Oxford Street, south towards Queens Park. A well-defined and connected street network comprises primary through-streets, inner streets and rear laneways. York Road, Birrell, Newland, Denison and Ruthven streets provide access to, from and through the area. The area benefits from its close proximity to Bondi Junction, where heavy rail and bus services provide access to the coast, Sydney CBD and surrounding areas.

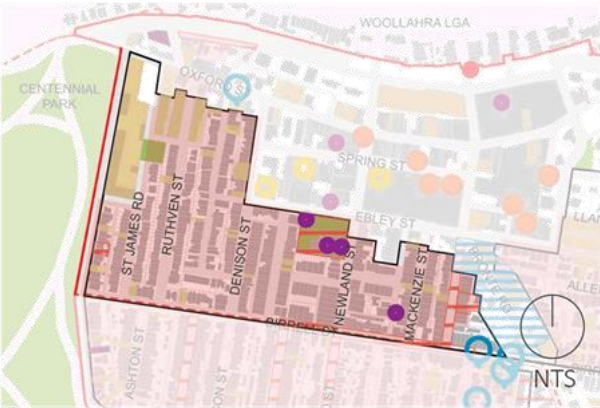


Figure 1-6 - Built form and uses area #01

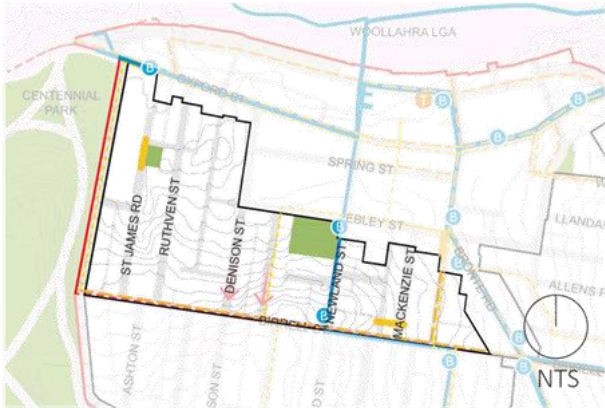


Figure 1-7 - Road network and movement area #01

Built form

Substantial development of the area between 1880-1915 has resulted in consistent streetscapes characterised by predominantly Late-Victorian and Federation-style terraces, semi-detached and detached dwellings of 1-2 storeys. Terraces are typically of face brick, stone and rendered masonry with chimneys, decorative plaster, timber and iron filigree detailing. Upper balconies are common. Notable terrace groupings include heritage listed 2-36 St James Road, 12-42 Ruthven Street and 43-55 Denison Street (see Figure 1-2, Figure 1-4 and Figure 1-8). Federation-style detached, and semi-detached bungalows are characterised by low gabled terracotta tiled roofs, with timber verandah posts, decorated timber or ironwork gable trim, window awnings and chimneys. An example is the western side of St James Road. Grander detached Federation dwellings characterise the western edge of the area, fronting York Road, overlooking Centennial Park. Historical shopfronts are set at street corners and within housing rows with post-supported and cantilevered verandahs. Ecclesiastical buildings including churches and manses are also evident throughout the area. Heritage listing of the area as the Mill Hill Conservation Area has retained the character of these early streetscapes, however alterations including second-storey additions, as well as later infill development, has impacted their integrity. Later development of Mid-century, late 20th century and 21st century styles, is prominent along St James Road, Ruthven Street and Hough Street. These dwelling typologies result in a population density of approximately 87 persons/ha.



Figure 1-8 - Heritage-listed terrace dwellings, Ruthven Street

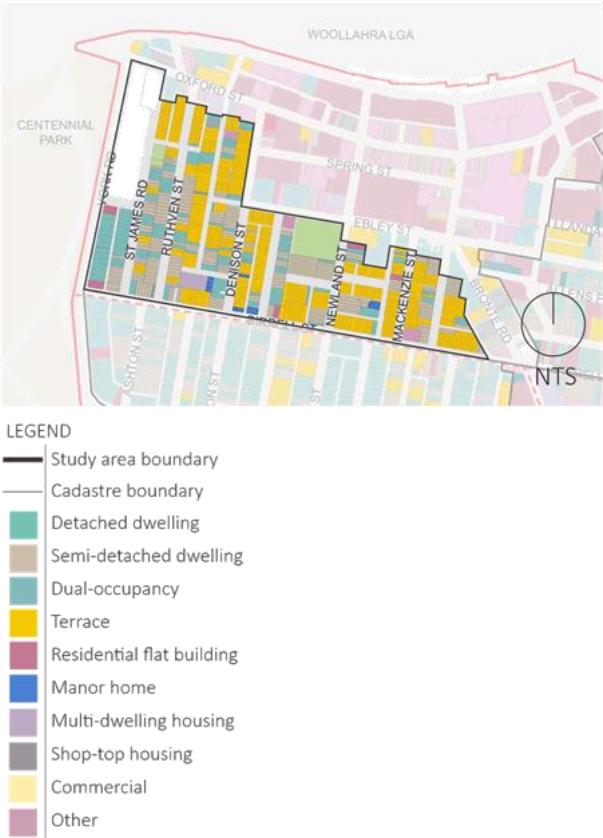


Figure 1-9 - Dwelling typologies area #01

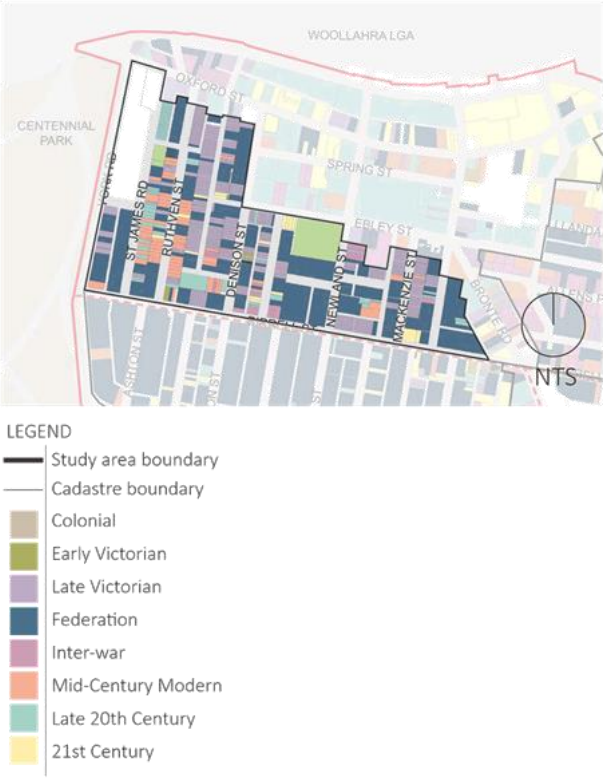


Figure 1-10 - Architectural styles area #01

22 Existing character description cont.

Public and private domain interface

The interface between the public and private domains varies across the area, dependent on street and dwelling typologies. Through-streets are characterised by a wider carriageway, relatively narrow verges and smaller, less dense street plantings. Inner streets typically have wider verges, accommodating more street trees. Brisbane Street is characterised by an avenue of significant fig trees. The narrow, corridor-like rear lanes are dominated by garage doors, high fences and walls, landscape screening and a variety of building setbacks. Shallow setbacks and low cast iron or timber fences, result in dwellings being highly visible from the public domain. In some cases, higher rendered brick fences reduce visibility. Front landscaping is generally limited. Continuous built form creates a sense of enclosure. Setbacks and landscaping increase on larger lots.

Most dwellings within the area are not serviced by off-street parking due to the prevalence of narrow front setbacks and limited rear lanes. This creates a consistent streetscape not interrupted by driveway crossovers, for example Walter and Kieran streets (see Figure 1-14). Where deeper front setbacks or sloping topography allows, car-parking is integrated within the dwelling or accommodated within hard-stand, carports or garages at the boundary. This occurs predominantly in newer infill development, for example Ruthven Street (see Figure 1-15).



Figure 1-11 - Terrace dwellings, Ruthven Street



Figure 1-12 - Rear garages and fences, Thompson Lane



Figure 1-13 - Garage frontages, Birrell Street



Figure 1-14 - Wide-grassed verges, Kieran Street



Figure 1-15 - Semi-detached dwellings, Ruthven Street

Natural environment

Open space is limited within the study area comprising of St James Reserve and Clemenston Park (see Figure 1-16 and Figure 1-5). Notwithstanding, the area adjoins regionally significant Centennial Park and benefits from easy access to nearby Queens Park, which offer passive and active recreation opportunities. Green links aid connectivity. Private open space is also limited due to narrow allotments with built form extending almost the full length. Vegetation coverage is predominantly low across the area due to densely set built form, narrow setbacks and varying street tree planting. These conditions result in a low-medium heat vulnerability.

Recent development

Recent development history comprises alterations and additions to existing dwellings, including second-storey additions, carports / garages, attic conversions; limited construction of secondary dwellings, such as loft / studio over rear garages; and limited construction of new 2-storey detached dwellings. Indicative of a trend toward maintaining the existing built form, increasing floor space and car-parking, whilst respecting the character of the Mill Hill Conservation Area.



Figure 1-16 - St James Reserve



Figure 1-17 - Kieran Street, looking west

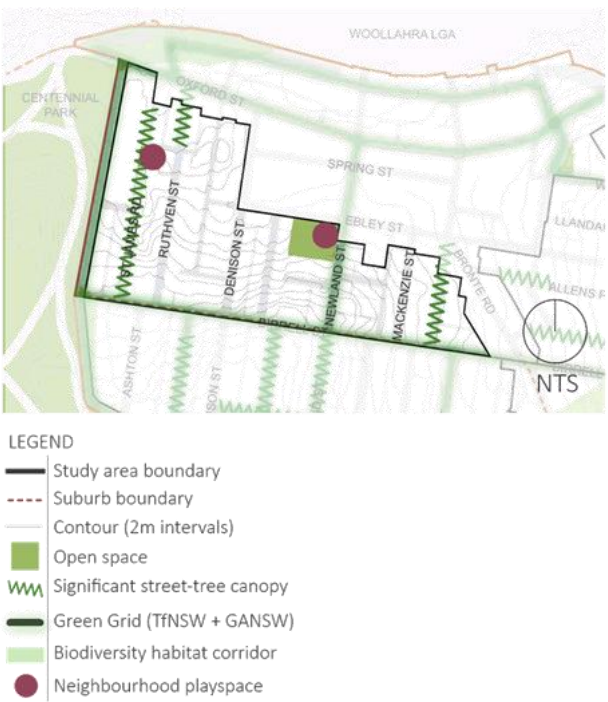


Figure 1-18 - Open space and vegetation area #01

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

2



CHARACTER AREA - 2

"The area is defined by its historic streetscapes, arranged in a linear north-south grid, comprising predominantly low-rise, Federation-style terraces, detached and semi-detached dwellings; limited driveway cross-overs; avenues of green, leafy tree, contrasting the openness of adjacent Queens Park and Centennial Park".

The study area is generally bound by Birrell Street to the north, the rear of properties fronting Bronte Road to the east, Queens Park Road to the south and York Road to the west.



Figure 2-1 - Local character area #02 map



Figure 2-4 - Newland Street streetscape



Figure 2-2 - Detached Federation-style dwellings, Alt Street



Figure 2-5 - Terrace dwellings, Cuthbert Street



Figure 2-3 - Rear garages, Newland Lane



Figure 2-6 - Rawson Avenue streetscape

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by well-maintained historic streetscapes of Federation-style dwellings, amidst avenues of leafy trees and vegetation.

Objectives

The objectives for development on land identified within the character area are as follows:

- To ensure an appropriate physical and visual curtilage is provided for Centennial Park, including its skyline, and nearby historic streetscapes / areas,
- To preserve the heritage items and the integrity of the Urban Conservation Area,
- To maintain the historically distinctive pattern of subdivision and associated terrace, semi-detached and detached dwelling typologies,
- To maintain the predominant 1-2 storey height character of the area,
- To reinforce the consistent and unified Federation-style dwelling elevations and frontages to the streets, including pitched roofscape and front setbacks,
- To promote sympathetic additions and new development that respects the historic patterns, architectural styles, heights, materials and details of existing dwellings,
- To ensure secondary dwellings and ancillary development enhance rear laneways and maintain a high standard of residential amenity for surrounding dwellings,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To promote landscaping at the rear of dwellings, to soften transition to rear laneways,
- To maintain and enhance the significant landscape features of the area, including the large street tree canopy,
- To limit new driveway crossovers and car parking within the front setback, by retaining vehicle access from rear lanes.

28 Existing character description

History

The area has evolved from the subdivision and development of 1840s land grants located at the edge of two early service villages, being Tea Gardens, now known as Bondi Junction and Maddens Corner, now known as Charing Cross. Early estates within the area included the Fitzgerald and Pearce estates. Improvements in transport and the rapid expansion of suburban Sydney between 1880-1915 saw the consolidation of these villages and the area developed for housing. The grid pattern of the early grants and initial streets established along their boundaries and remanent built form are reflected in the configuration of the area today. The area remains predominantly residential with limited commercial, retail and educational uses.

Configuration and connectivity

Blocks are arranged in a linear north-south alignment, across the prevailing slope, which falls from the north and north-east. The natural topography formed the upper catchment of the Lachlan / Botany Aquifer. Extensive sand deposits set above sandstone and shaped by former stream flows underlie the current built environment. A well-defined and connected street network comprises primary through streets, inner streets and rear laneways, extending north-south, with limited east-west connections. Birrell Street, York Road and Queens Park Road provide the primary access to and from the area. The area benefits from its close proximity to Bondi Junction and Bronte Road, where heavy rail and bus services provide access to the coast, Sydney CBD and surrounding areas.



Figure 2-7 - Built form and uses area #02



Figure 2-8 - Road network and movement area #02

Built form

Progressive subdivision and development of the area has resulted in three categories of lot size, reflecting the dominant dwelling typologies in the area. Small sized lots (typically 100m² to 250m²) dominate the north-eastern part of the area. These lots typically contain 1-2 storey terrace dwellings. In the central and southern parts of the area, lots tend to be larger (typically 200m²-400m²) comprising 1-2 storey semi-detached or detached style dwellings. The largest lots (500m²-800m²) located on the western and southern edges of the area, fronting York and Queens Park roads, comprise 1-2 storey detached dwellings and a small number of 3-4 storey residential flat buildings (RFBs). These larger lots are a result of the Centennial Parklands sales in 1904-1905 and the influence of the Garden Suburb Movement. Lots are typically aligned east-west.

The area is characterised by a variety of architectural styles reflective of the periods of development, including the predominant Federation period (1890-1915), with early-late Victorian terraces, Federation terraces and semi-detached dwellings as well as larger Federation and later Inter-War bungalows evident in the area. Exemplar dwellings include the late 19th century stone terraces at 1-2 Fitzgerald Street, workers cottages at 93 Birrell Street, Victorian Italianate semi-detached dwellings at 2-8 Fitzgerald Street, Federation terraces at 164-166 Denison Street, Federation bungalows at 41 York Road, 55 and 63 Alt Street and Inter-War bungalows at 1-7 and 2-12 Yenda Avenue. Building design was largely speculative with builder rather than architects designed dwellings, which resulted in streetscapes characterised by consistent groupings of styles and typologies. Distinctive property features that contribute to the

character of the area include tuck-point face brick, terra cotta Marseilles tile, timber fretwork, casement windows, gable fronted bays, rough cast chimneys and low fence lines.

Heritage listing of the area as the Queens Park and Yenda Avenue conservation areas has retained the character of these early streetscapes, however alterations including second-storey additions, as well as later infill development, has increased the vocabulary of the area. Later development of Mid-century, late 20th century and 21st century styles, can be seen along York Road, Queens Park Road, Rawson Avenue, Denison Street and Blenheim Street. These dwelling typologies result in a population density of approximately 64 persons/ha.



Figure 2-10 - Heritage-listed workers cottage, Blenheim Street

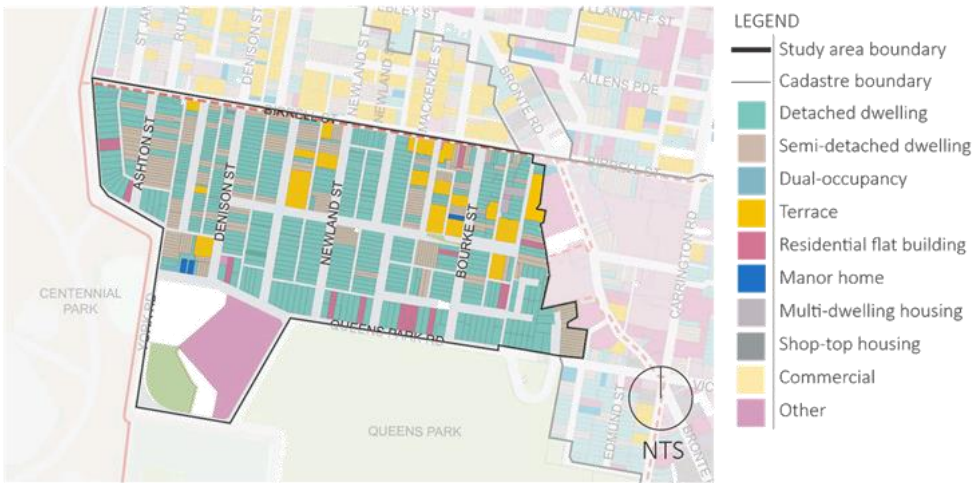


Figure 2-9 - Dwelling typologies area #02

30 Existing character description cont.



Figure 2-11 - Architectural styles area #02



Figure 2-12 - Heritage area #02

Public and private domain interface

The interface between the public and private domains varies across the area, dependent on street and dwelling typology. Through-streets are characterised by a wider carriageway, relatively narrow verges and smaller, less dense street plantings. Inner streets typically have wider verges, accommodating more street trees. The narrow, corridor-like rear lanes are dominated by garage doors, high fences and walls and landscape screening (see Figure 2-3). Shallow setbacks and low cast iron fences are characteristics of terrace dwellings, resulting in high visibility of the dwellings from the public domain. In some cases, higher brick fences reduce visibility. Front landscaping is generally limited. Semi-detached and detached dwellings typically have deeper front setbacks, with low brick or timber picket fences (see Figure 2-2). Visibility of the

dwelling remains high, however screening by front landscaping is common. Nil to narrow side setbacks result in continuous built form which creates a sense of enclosure.

Most dwellings within the area are serviced by off-street parking due to the prevalence of rear lanes, originally conceived for garbage collection, with later additions of hard-stand, carport or garage parking. This results in a consistent streetscape uninterrupted by driveway crossovers (see Figure 2-4). Whilst not common, where deeper front setbacks or sloping topography allows, car-parking is integrated within the dwelling or accommodated within hard-stand, carports or garages at the boundary.

Natural environment

The area benefits from access to significant public open space including the adjoining Centennial and Queens parks. The parks are of landscape and archaeological significance. Distinctive natural sandstone outcrops form part of the eastern edge of Queens Park and also appear in Cuthbert and Arnold streets. Former quarry workings are also evident in Arnold, Cuthbert and Stanley streets. North-south view axes provide views south through the area to Queens Park. The upper-eastern area benefits from views west over the parklands to the city.

Vegetation is an important element, giving the area an attractive, cool, green and leafy character. Formal plantings of mature figs are a distinguishing characteristic of the inner streets, for example Newland, Manning and Cuthbert (see Figure 2-13). Many of these streets are heritage-listed Landscape Conservation Areas. The avenues of mature trees create a sense of enclosure. Remnant heath vegetation is in Queens Park and the Moriah College grounds. Vegetation coverage reduces in the private domain due to narrow setbacks and high site coverage. These conditions result in a low-medium heat vulnerability.

Recent development

Recent development history comprises alterations and additions to existing dwellings, including second-storey additions, carports / garages, attic conversions; construction of secondary dwellings, such as loft / studio over rear garages; and limited construction of new 2-storey detached dwellings. Indicative of a trend toward maintaining the existing built form, however increasing floor space and car parking, whilst respecting the character of the Queens Park Conservation Area.



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?

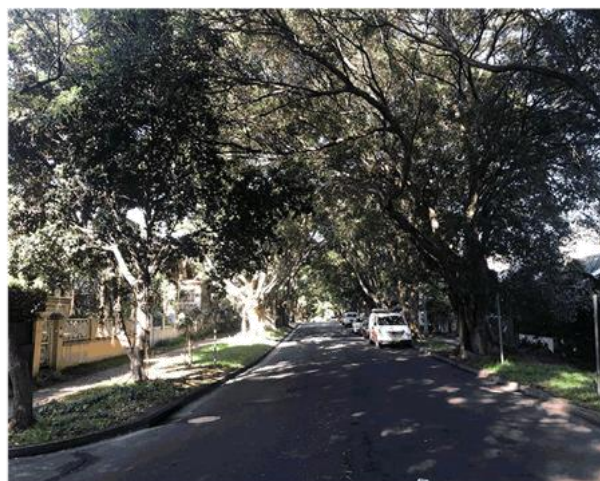
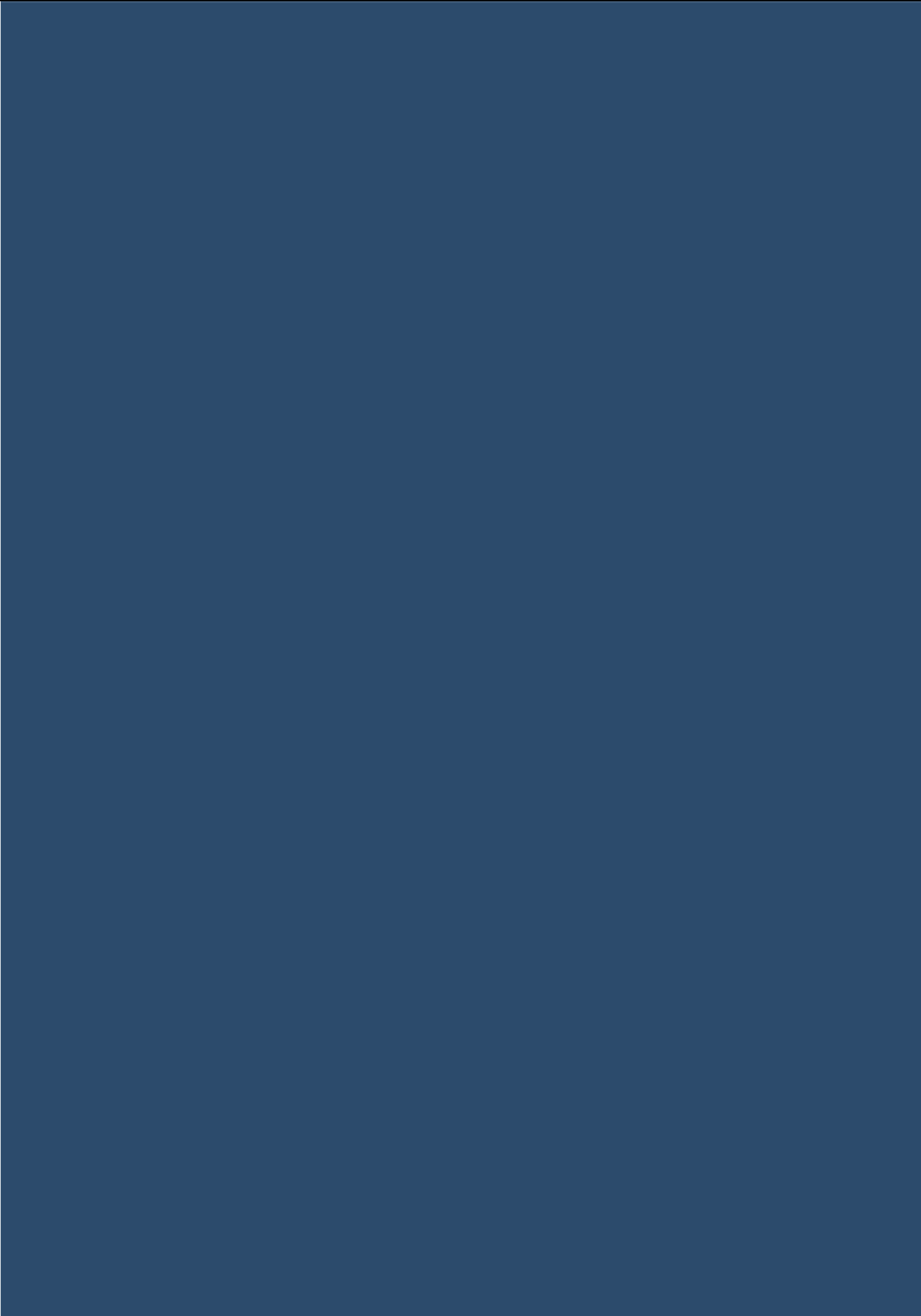


Figure 2-13 - Tree canopy, Newland Street



Figure 2-14 - Open space and vegetation area #02



CHARACTER AREA

3



CHARACTER AREA - 3

"The area is characterised by an irregular street grid, comprising a patchwork of dwelling typologies and styles, with low-rise dwellings within the Botany Conservation Area, transitioning to medium- high rise dwellings at the northern boundary, creating an inconsistent streetscape of varying styles, materials and vegetation coverage".

The study area is generally bound by Bondi Road to the north, Paul Street and St Marys Avenue to the east, Birrell Street to the south and Adams Lane and Hollywood Avenue to the west.

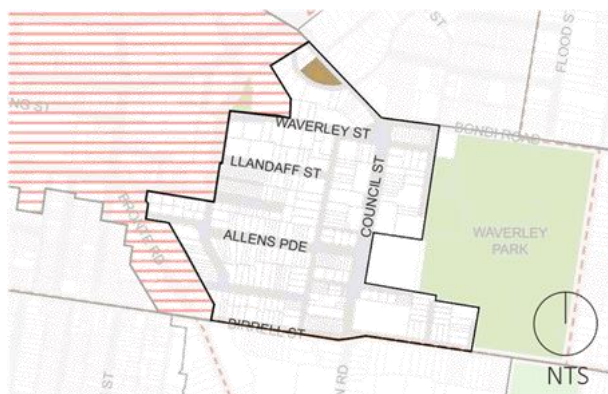


Figure 3-1 - Local character area #03 map



Figure 3-4 - Bondi Road built form, looking south-west



Figure 3-2 - RFBs, Waverley Street

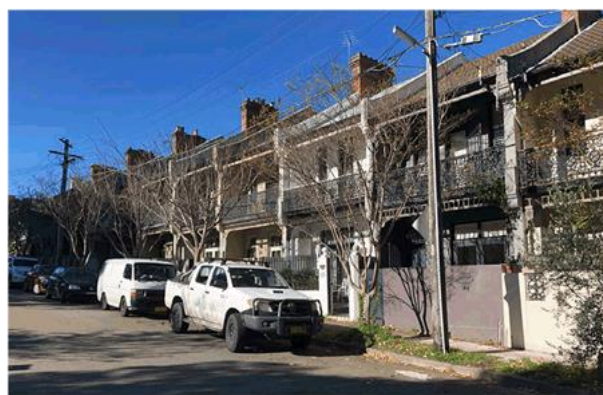


Figure 3-5 - Terrace rows, Llandaff Street

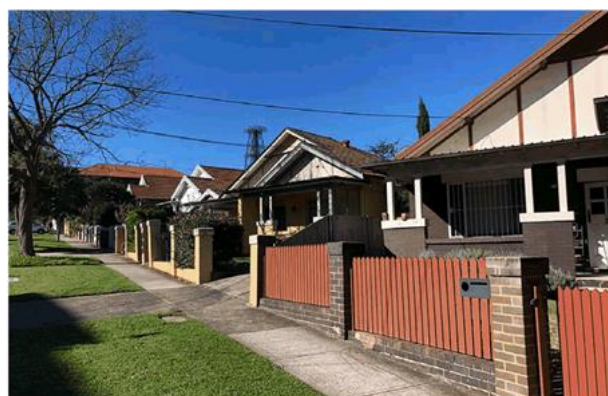
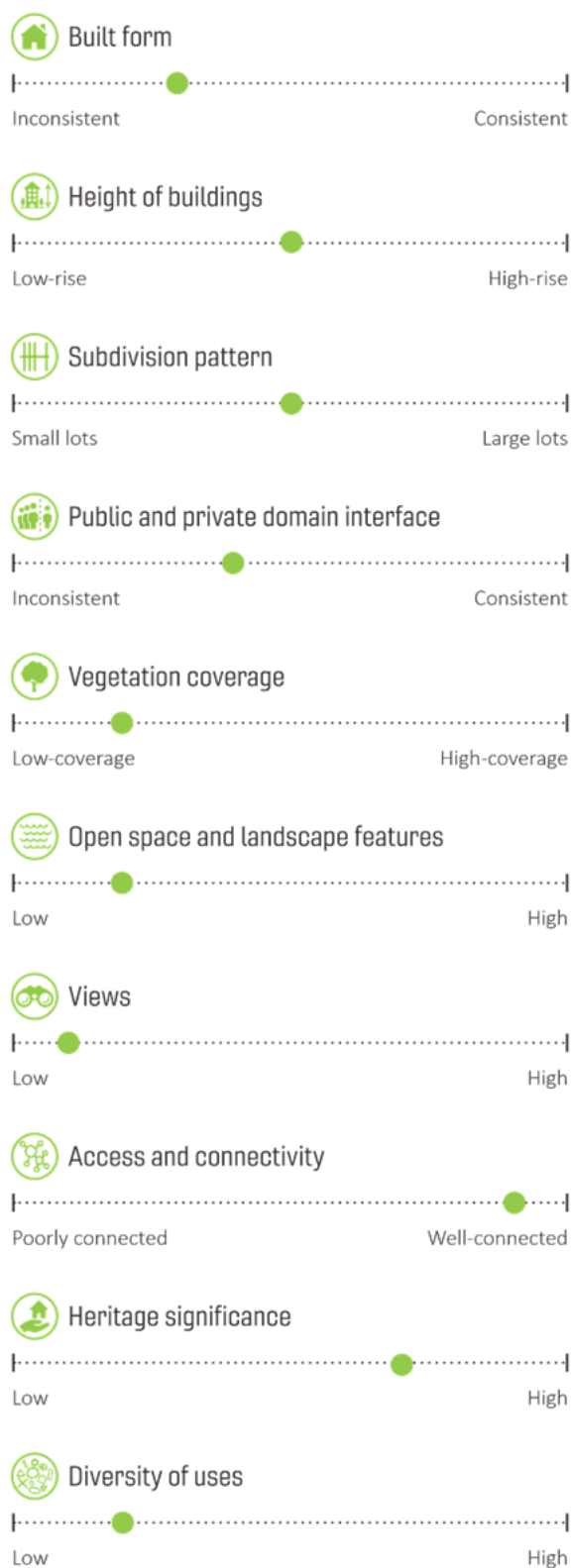


Figure 3-3 - Detached dwellings, Allens Parade



Figure 3-6 - RFBs, Hollywood Avenue

Existing character attributes



Desired future character

Vision

The desired future character of the area is a pleasant residential neighbourhood that provides a pleasant and interesting transition in scale from Bondi Junction to the Botany Conservation Area. The key walking streets are safe and easy to find, attractive and well-shaded.

Objectives

The objectives for development on land identified within the character area are as follows:

- To provide a transition in built form from the Bondi Junction Strategic Centre to the Urban Conservation Area,
- To preserve the heritage items and the integrity of the Urban Conservation Area,
- To maintain the predominant 1-4 storey height character within the Urban Conservation Area,
- To preserve the diversity of dwelling typologies, architectural periods and heights in the area by retaining buildings from the Late Victorian, Federation and Inter-war periods,
- To promote sympathetic additions and new development that respects the historic patterns, architectural styles, heights, materials and details of existing dwellings,
- To ensure an integrated approach and consistent treatment of groupings of dwellings, including terraces and semi-detached dwellings,
- To maintain the large street tree canopy over Porter Street, Hollywood Avenue, Allens Parade and Dalley Street,
- To grow the urban tree canopy and provide increased opportunities for urban greening,
- To promote landscaping at the rear and side of larger dwellings to provide screening between buildings,
- To ensure that carparking structures or entries do not dominate or adversely impact on the streetscape,
- To reduce conflicts between pedestrians and vehicles,
- To recognise Waverley Street as a key walking street between Bondi Beach and Bondi Junction and to improve the amenity for pedestrians.

36 Existing character description

History

The area has evolved from the subdivision and development of early land holdings along Old South Head Road, at the periphery of two service villages, being Tea Gardens, now known as Bondi Junction and Maddens Corner, now known as Charing Cross. The primary land holding was Barnett Levey's 60-acres granted in 1826. Levey was the third grantee of land in the Waverley municipality. Levey's Waverley Crescent Subdivision was advertised on 30 January 1828 in *The Australian*, comprising 68 allotments. Waverley House, formerly located near Waverley Crescent, was the earliest Villa in Waverley completed in 1828 and demolished in 1906. Improvements in transport and the rapid expansion of suburban Sydney between 1880-1915 saw the consolidation of Bondi Junction and the area developed for housing. The area remains predominantly residential with limited commercial, retail and educational uses.

Configuration and connectivity

An irregular street grid reflects progressive subdivision of the area up until the early 1920s. Internal streets are typically aligned east-west, connected by north-south streets, Hollywood Avenue, Botany Street and Council Street. Lots of varying sizes are predominantly aligned north-south. No-through streets result in varying inter-block connectivity. Pedestrian access and connectivity are maintained by multiple through-block links. Bondi Road, Birrell Street and Council Street provide the primary access to and from the area. The area benefits from its close proximity to Bondi Junction and Bronte Road, where heavy rail and bus services provide access to the coast, Sydney CBD and surrounding areas. The area also benefits from the commercial, retail and social services offered at these locations.



Figure 3-7 - Ebley Street, looking west

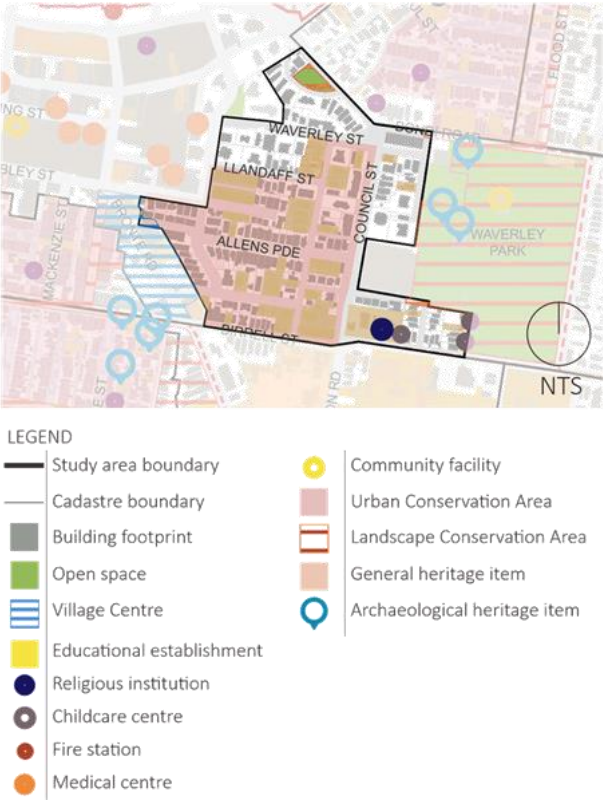


Figure 3-8 - Built form, uses and heritage area #03

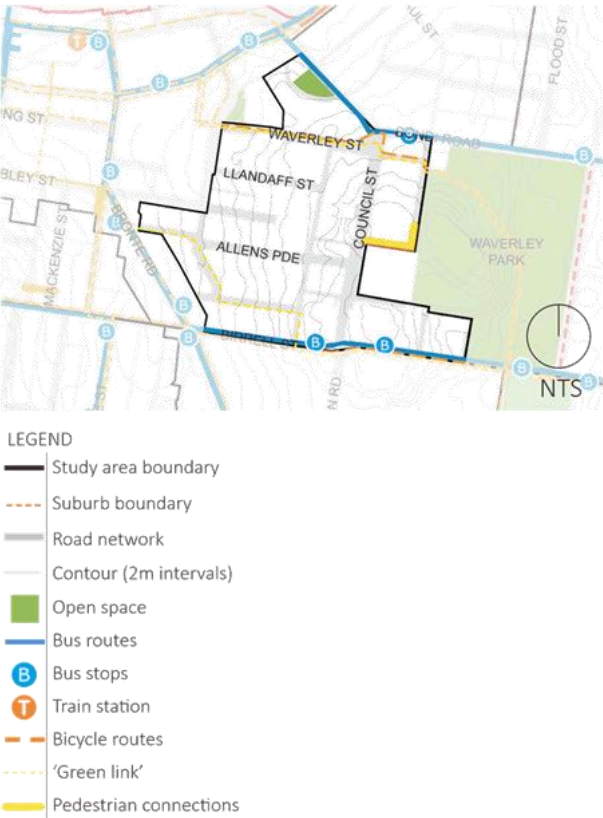


Figure 3-9 - Road network and movement area #03

Built form

The area is characterised by a patchwork of dwelling typologies and styles reflective of its progressive development. Clusters of late Victorian-style terraces and semi-detached dwellings, Federation-style terraces, semi-detached and detached dwellings, Inter-War style detached dwellings and residential flat buildings (RFBs) as well as Mid-century, late 20th century and 21st century style RFBs are evident in the area. Exemplar dwellings include the heritage-listed terrace rows at 208-236 Birrell Street, 3-39 Llandaff Street and 126-162 Ebley Street, RFBs at 9-15 Botany Street and 27 Allens Parade, detached and semi-detached dwellings in Porter Street and St Marys Anglican Church at 240 Birrell Street. Most of the area is heritage-listed as the Botany Street Conservation Area. Dwellings are predominantly 1-4 storeys in height, however, increase to 7-10 storeys in the north-east and north-west of the area. Many streets reflect a sudden transition between low-rise and high-rise dwellings, for example Llandaff Street. These dwelling typologies result in a population density of approximately 88 persons/ha, with the dominant tenure type being private rental.

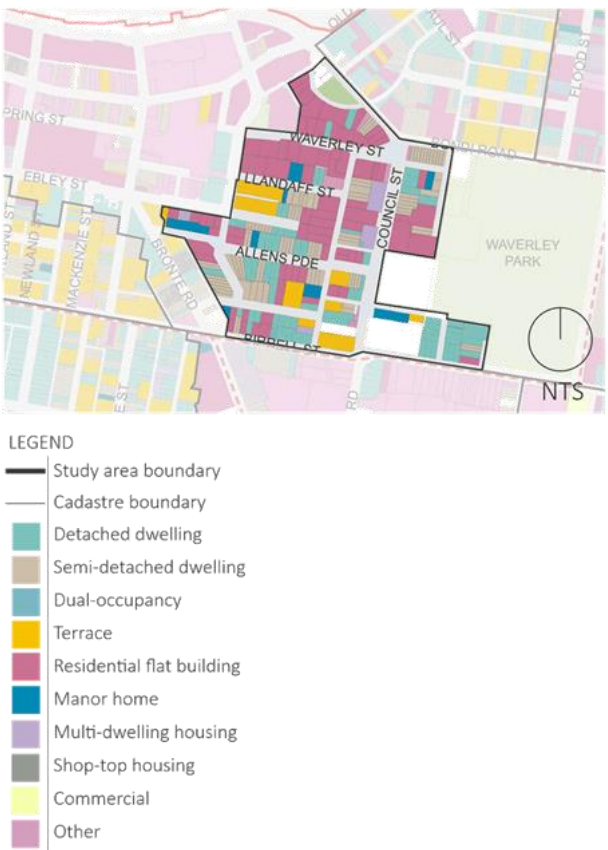


Figure 3-11 - Dwelling typologies area #03

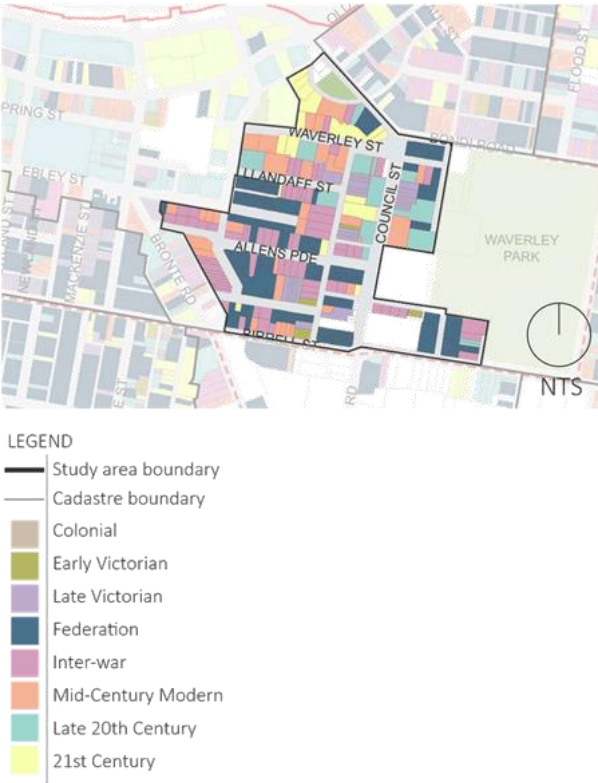


Figure 3-10 - Architectural styles area #03

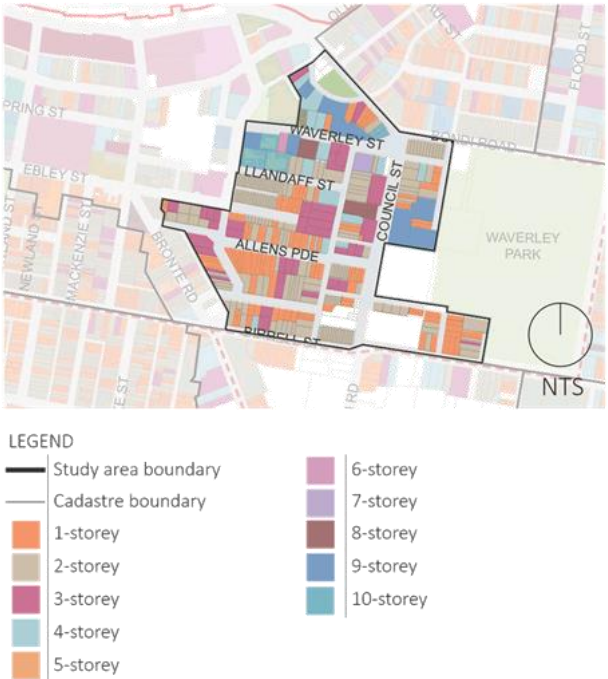


Figure 3-12 - Building height in storeys area #03

38 Existing character description cont.

Public and private domain

The streetscape reflects a variety of styles, materials and distribution of built form. The interface between the public and private domains varies as a result. Terraces, detached, and semi-detached dwellings are typically closely set, with narrow front setbacks. Front landscaping and fence materiality and height vary, influencing the visibility of the dwelling from the public domain, for example Ebley Street (see Figure 3-13). Later additions of a carport or enclosed garage at the boundary detract from the streetscape and further obscure the dwelling. Terraces typically have upper-level balconies which overlook the public domain. Rear lanes are dominated by garage doors, high fences and walls and landscape screening. Inter-War RFBs also typically comprise narrow setbacks and limited landscaping, with a solid brick façade (facades vary), small window openings and entry presenting to the street, for example Allens Parade (see Figure 3-15). Nil to narrow side setbacks result in continuous built form which creates a sense of enclosure. Mid-late 20th century RFBs comprise greater setbacks and are typically raised above street level, for example Waverley Street. Upper balconies overlook the public domain. Dwellings are predominantly serviced by off-street parking, through a combination of hard-stand parking in the front setback, ground-level enclosed or open garages and rear lane garages. Where carparking and servicing is provided below street-level, large openings dominant the streetscape.



Figure 3-13 - Terrace rows, Ebley Street



Figure 3-14 - Semi-detached dwellings, Ebley Street



Figure 3-15 - RFBs, Allens Parade

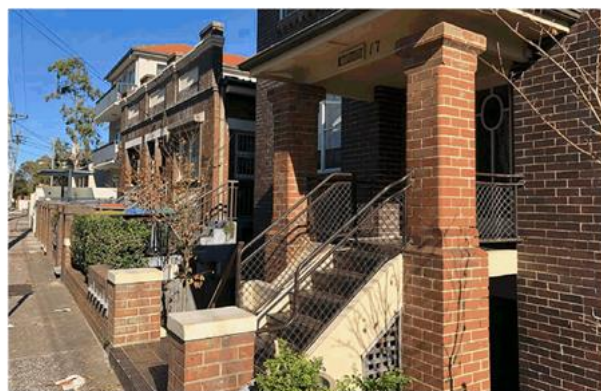


Figure 3-17 - RFBs, Botany Street



Figure 3-16 - RFB and terrace row, Porter Street

Natural environment

The area benefits from access to significant public open space including Waverley Park, which offer passive and active recreation opportunities. The area’s relatively flat topography, sloping gradually to the west, and through-block links, promote walkability. Vegetation coverage is predominantly low across the area, as a result of closely set development which limits the opportunity for significant private open space in the front or rear yards as well as the varying capacity of the street verge to accommodate planting. Mature fig trees characterise Porter Street (see Figure 3-19) and Allens Parade (see Figure 3-20). These conditions result in a low-medium heat vulnerability in the eastern portion of the area, increasing the medium-high in the western portion of the area.

Recent development

Recent development includes alterations and additions to existing dwellings such as second or third storey additions, hard-stand parking, carports, limited demolition of existing detached and semi-detached dwellings and construction of new 2-storey dwellings, and limited demolition and amalgamation of land for the construction of new 3+ storey RFBs. This indicates a trend toward maintaining existing built form within the Botany Street Conservation Area and intensification of valuable land.



Figure 3-18 - Open space and vegetation area #03

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



Figure 3-19 - Tree canopy, Porter Street, looking east



Figure 3-20 - Cnr Allens Parade and Hollywood Avenue



CHARACTER AREA

4



CHARACTER AREA - 4

"The area is defined by its civic character, comprising a variety of uses, including educational establishments and the distinctive Charing Cross village centre, peripheral streets extend from historic Bronte Road creating long north-south blocks of inconsistent streetscapes comprising layered built form, materiality and limited vegetation".

The study area is generally bound by Birrell Street to the north, Henrietta and Leichhardt Streets to the east, Varna, Wallace and Albion Streets to the south and Carrington Road and Henry Street to the west.



Figure 4-1 - Local character area #04 map



Figure 4-3 - Terrace row, Bronte Road



Figure 4-4 - Semi-detached dwellings, Zarita Avenue

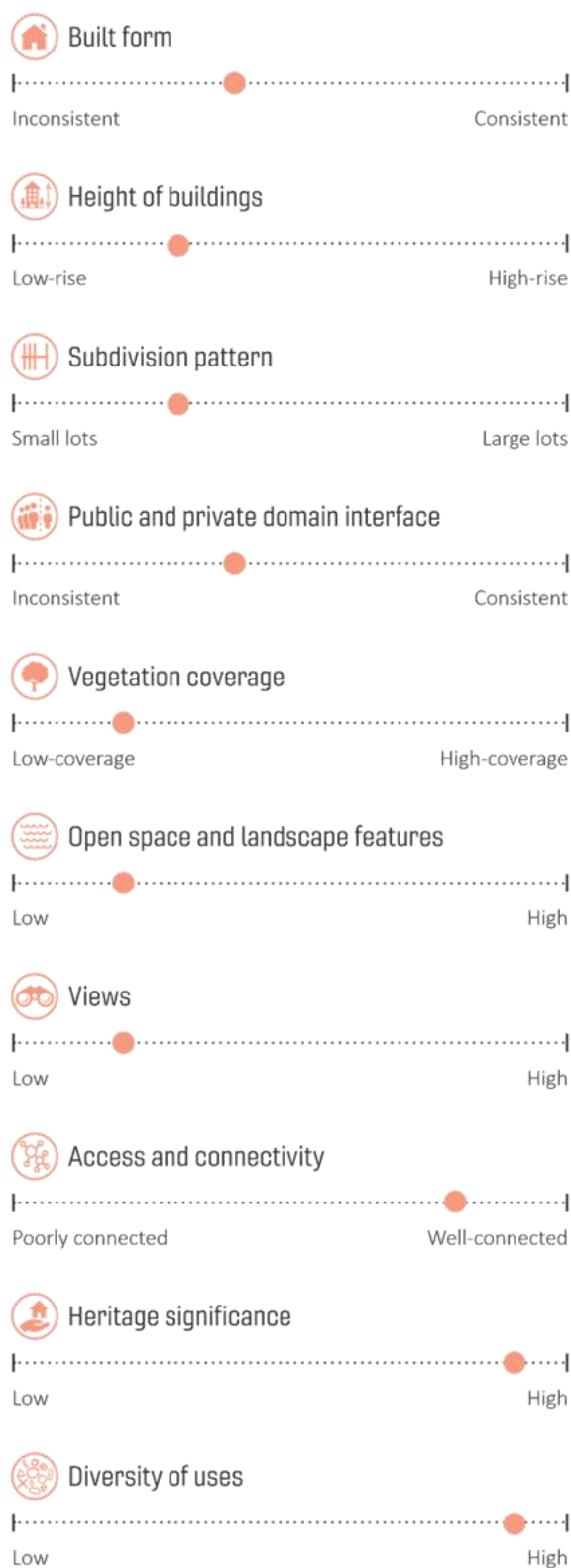


Figure 4-2 - Terrace shopfronts, Bronte Road



Figure 4-5 - Wiley Street, looking south

Existing character attributes



Desired future character

Vision

The desired future character of the area is an attractive and well-maintained historic centre, with a range of civic, commercial and residential uses that exist harmoniously.

Objectives

The objectives for development on land identified within the character area are as follows:

- To promote the significant contribution of the area during the early years of the Waverley municipality,
- To celebrate the history of the area through urban design and activations that promote local heritage,
- To recognise and maintain the diversity of uses in the area,
- To maintain the visual prominence of landmark buildings, including the Robin Hood and Charing Cross Hotels, Mary Immaculate and Grahame Memorial churches and War Memorial Hospital,
- To protect regional and local views from and within the public domain,
- To protect the curtilage of institutional heritage buildings including churches and school to enable visual appreciation of buildings in their campus-style setting,
- To maintain the predominant 1-3 storey height character of the area,
- To protect the high-street nature of Bronte Road, maintain and enhance distinctive pattern of continuous terrace and semi-detached shopfronts, prioritise active street frontages and provide no new vehicle crossovers,
- To preserve the diversity of dwelling typologies, architectural periods and heights in the area by retaining buildings from the Late Victorian, Federation and Inter-war periods,
- To promote sympathetic additions and new development that respects the historic patterns, architectural styles, heights, materials and details of existing dwellings,
- To ensure an integrated approach and consistent treatment of groupings of dwellings, including terraces and semi-detached dwellings, for example pitched roofscape and front setbacks,
- To increase pedestrian wayfinding and accessibility to nearby open space,
- To maintain and grow the canopy coverage as a defining feature when viewed from Centennial Parklands.

44 Existing character description

History

The area’s character is predominantly derived from the forms, streetscapes and social relationship of industry, worker housing and commerce in the early years of the Waverley municipality. Development began in the 1800s, with subdivision of early land grants providing the site of Maddens Corner, now known as Charing Cross, which served as a rural village on the route from Old South Head Road to Coogee and La Perouse (now Bronte Road). Further subdivision and configuration of the area was influenced by the establishment of the Vickery’s Tannery (1866), religious / educational institutions on large allotments, introduction of horse drawn omnibuses (1860s) and later steam, then electric trams along Bronte Road. This results in an irregular block pattern and complex road network comprised of primary arterial roads, secondary local streets and service laneways.

Configuration and connectivity

Following the natural ridgeline, Bronte Road provides the primary north-south connection through the study area. The topography slopes gradually to the west at Queens Park and south-east at Varna Park. District views are available to the west across Centennial Park and the CBD. Local roads extend from primary roads following the topography resulting in long north-south blocks of residential development, with limited east-west connections. The area is well-served by multiple bus routes along Bronte Road, Birrell Street and Carrington Road, which provide access to surrounding centres including Bondi Junction, to the north, and the Randwick health and education centre, to the south-west.

The area was, and remains, defined by its civic character, comprising religious institutions, educational establishments, small industries, community services, and retail and commercial uses, with surrounding residences. Church schools emerged from the 1850s, with St Catherine’s School Sydney, originally known as Clergy Daughter’s School, opening in 1856. The study area now includes some 5 schools and multiple childcare centres. The War Memorial Hospital (1922), located on the former Victorian ‘Edina Estate’ represents the early provision of medical services to the community. It continues to provide accommodation, medical and social services. Today, the Charing Cross village centre accommodates a diverse range of commercial and retail tenancies.

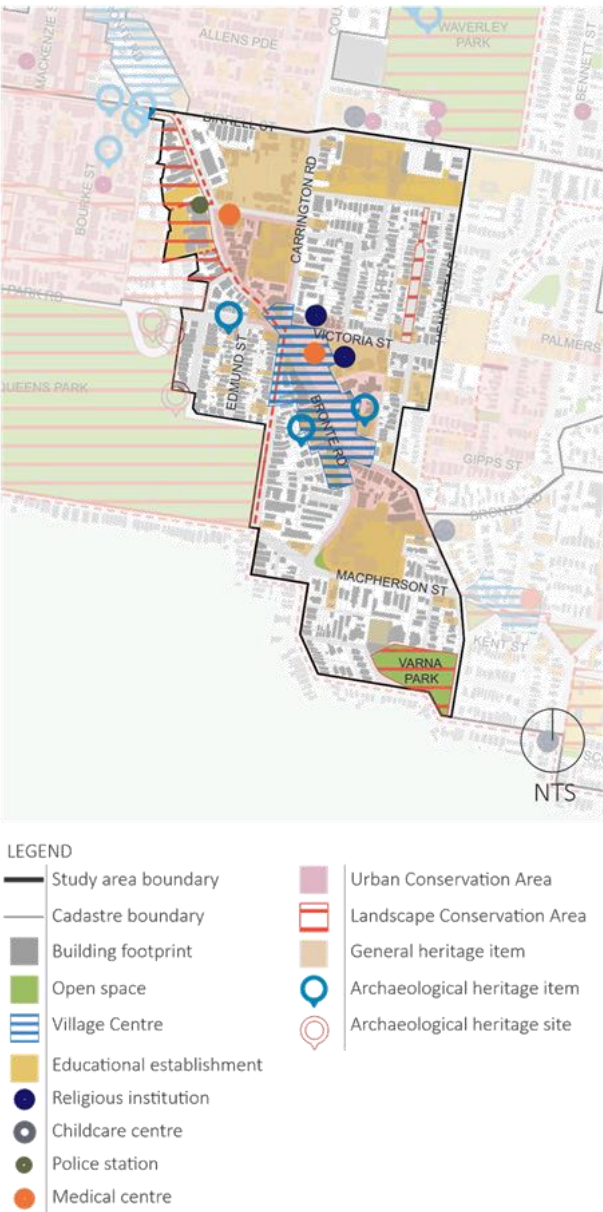


Figure 4-6 - Built form, uses and heritage area #04



Figure 4-7 - Intersection Bronte Road and Carrington Road

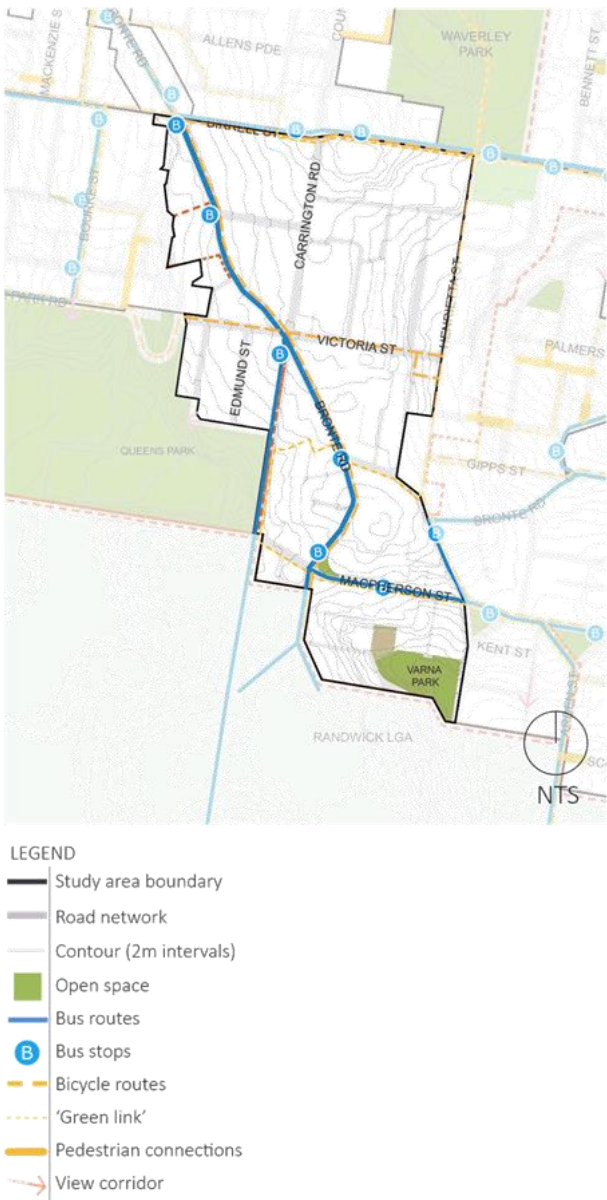


Figure 4-8 - Road network and movement area #04

Built form

Layered built form is indicative of the area’s early and continued development. The village centre is characterised by a distinctive ‘high street’ of 1-2 storey brick-fronted retail facades of late Victorian, Federation and Inter-War styles interspersed with mid-late 20th century development and modern additions (see Figure 4-2). Significant buildings include the Robin Hood and Charing Cross Hotels, former Waverley Post Office, Mary Immaculate and Grahame Memorial churches. Peripheral residential streets exhibit a predominantly 1-2 storey character with near-complete rows of late Victorian and Federation-style terraces, detached and semi-detached bungalows. Buildings are typically of face brick, stone and rendered masonry with decorative plaster, timber and iron filigree detailing. A number of large freestanding Victorian Villas remain generally unaltered, for example 74 Victoria Street and 348 Bronte Road, or within educational campuses. The ‘Glenrock Terraces’ on High Street provide a lasting example of mid-Victorian sandstone workers cottages in timber and stone, formerly part of the early Vickery Tannery. Inter-War development replaced earlier construction. Notable groupings of 1-3 storey Inter-War residences include the distinctive setting of Santa Marina Avenue (see Figure 4-10), 352-358 Bronte Road, Wallace Street (see Figure 4-9) fronting Varna park and Wills Ave. Heritage listing of much of the study area as the Charing Cross Conservation Area has retained the character of these early settings, however alterations, including second-storey and car-parking additions, have impacted their integrity. Mid-late 20th century and 21st century residential flat buildings (RFBs) are dispersed throughout the study area, however, are primarily located adjacent main roads including Albion, Macpherson and Birrell Street. These dwelling typologies account for a medium-high population density, with approx. 74 persons/ha.



Figure 4-9 - Semi-detached dwellings, Wallace Street



Figure 4-10 - RFBs, Santa Marina Avenue

46 Existing character description cont.

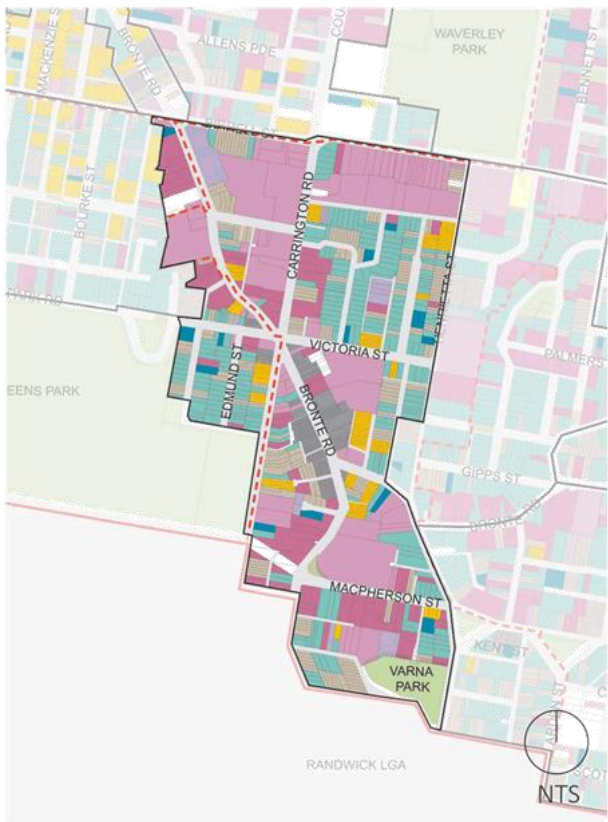


Figure 4-11 - Dwelling typologies area #04

Public and private interface

The public and private interface varies throughout the area, dependent on use, dwelling typology and street frontage. Terraces, detached and semi-detached bungalows are typically closely set, with narrow front setbacks and limited landscaping (see Figure 4-3 and Figure 4-4). The dwelling is highly visible from the street. Varying fence height and materiality creates an inconsistent streetscape character. The later addition of car-parking within the front setback, either hard-stand, carport or enclosed garage at the boundary, detracts from the streetscape and can obscure the dwelling. Inter-War RFBs also typically

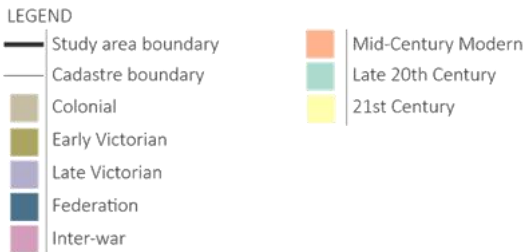
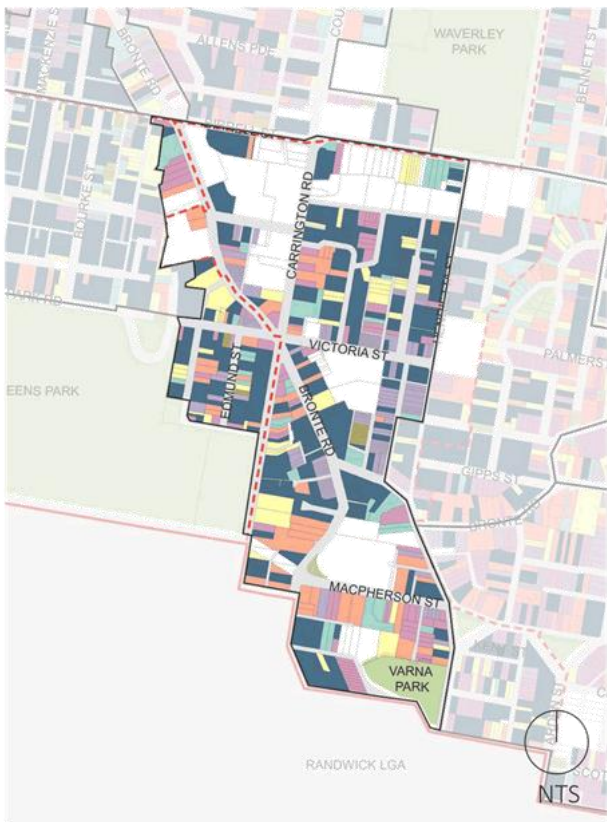


Figure 4-12 - Architectural styles area #04

comprise narrow setbacks and limited landscaping, with a solid brick façade (facades vary), small window openings and entry presenting to the street. Mid-late 20th century RFBs are raised above street level incorporating ground-floor carparking. Upper balconies overlook the public domain. Educational / hospital campuses comprise clusters of buildings with varied setbacks, often obscured from the public domain by buildings at the frontage, mature vegetation and landscaping. Through the village centre, fine-grain shopfronts abut the public domain, creating a highly visible, active frontage. Laneways provide off-street parking, typically with high fence lines and enclosed garages.

Natural environment

The area is located in close proximity to local and regional open space including Varna Park in the south, Queens Park to the west (see Figure 4-14), Waverley Park to the north and Bronte Beach to the east, which provide active and passive recreation opportunities. ‘Green Links’ aid with connectivity. The closely set nature of development reduces the opportunity for significant private open space in front or backyards. Vegetation coverage varies throughout the area, as a result of dense urban development and the varying capacity of the street verge to accommodate planting. Coverage is particularly low along Bronte Road through the village centre, contributing to heat vulnerability and a high Urban Heat Island effect. Coverage improves adjacent Varna Park and along

vegetated streetscapes and properties including Wiley and Blenheim Streets (both Landscape Conservation Areas) and the War Memorial Hospital site.

47

Recent development

Recent development history includes alterations and additions to existing terraces, detached and semi-detached dwellings and commercial premises such as, second or third storey additions, hard-stand parking, carports; limited construction of new 2-storey dwellings. Indicative of a trend toward maintaining existing built form and respecting the Charing Cross Conservation Area, whilst achieving more floor space and on-site car-parking.



Figure 4-13 - Open space and vegetation area #04

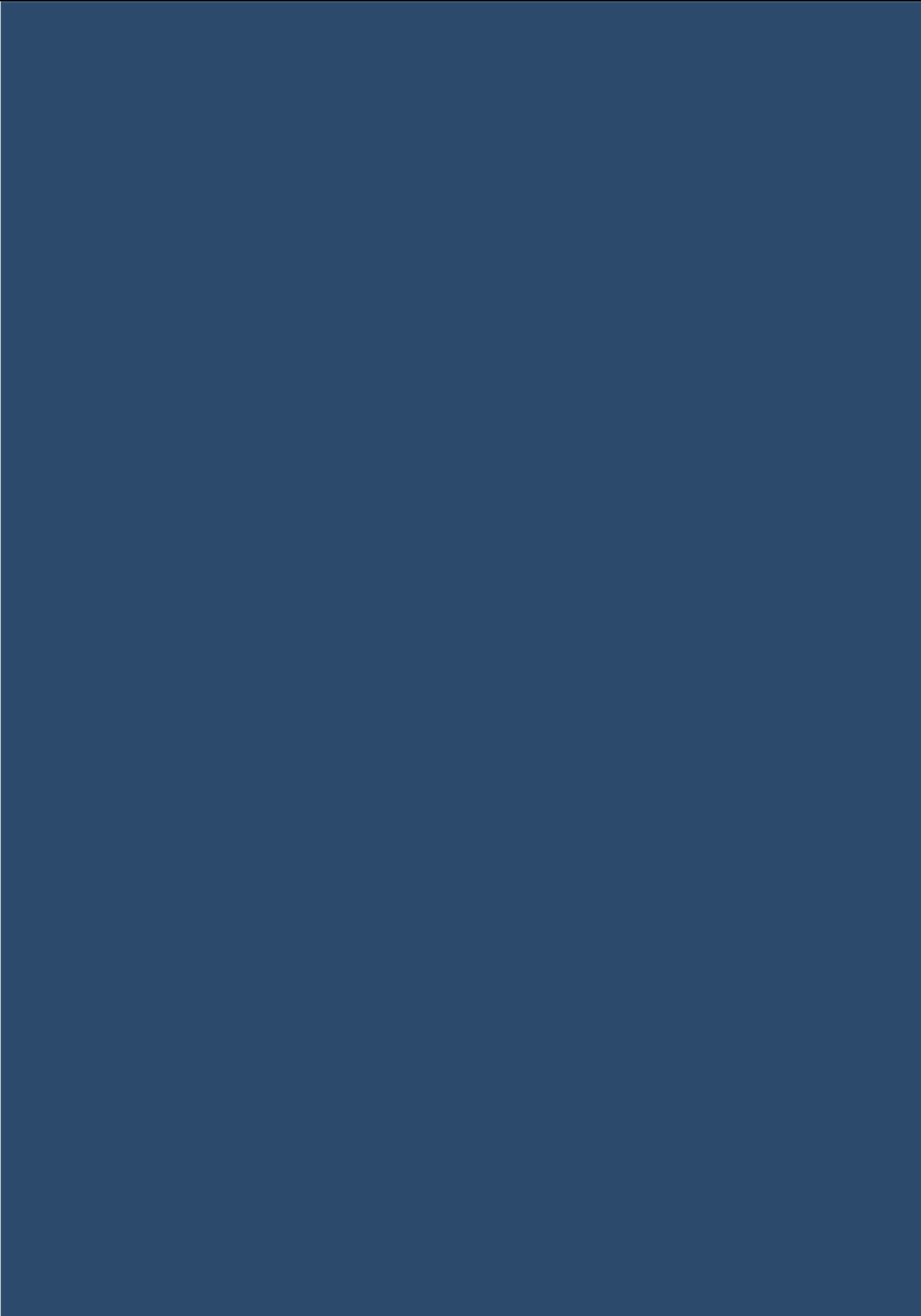


Figure 4-14 - Views west across Queens Park from Henry Street



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

5



CHARACTER AREA - 5

"The area is characterised by distinct pockets of development, set in an irregular grid pattern; predominantly low-rise detached and semi-detached dwellings with medium-rise residential flat buildings fronting primary roads; irregularly placed street-trees and driveway cross overs; and views to the coast and surrounding areas".

The study area is generally bound by Bronte Road and Murray Street to the north, St Thomas Street to the east, Boundary and Varna Street to the south and Leichhardt Street to the west.



Figure 5-1 - Local character area #05 map



Figure 5-4 - Detached dwellings, Evans Street



Figure 5-2 - Detached / semi-detached dwellings, Yanko Avenue



Figure 5-5 - Detached dwellings, Barclay Street

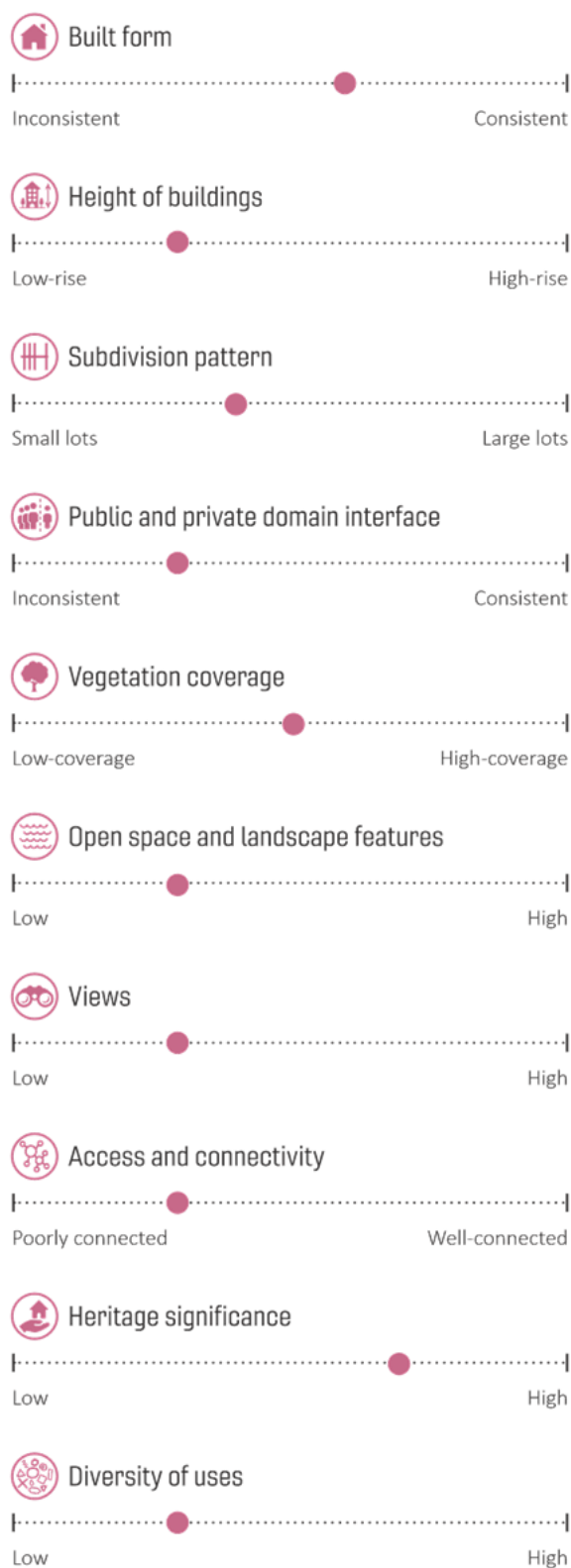


Figure 5-3 - RFBs, Macpherson Street



Figure 5-6 - RFB, Lugar Street

Existing character attributes



Desired future character

Vision

The desired future character of the area is a diverse residential area that is safe and walkable with access to attractive boutique neighbourhood shops.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise Macpherson Street as a neighbourhood shopping street, and maintain a low scale built form with regular shopfronts,
- To recognise and maintain the diversity of uses within the Macpherson Street local centres,
- To maintain the historically distinctive pattern of subdivision and associated dwelling typologies,
- To maintain the predominant 2-3 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography and avoid excessive excavation,
- To preserve the diversity of dwelling typologies, architectural periods and heights in the area by retaining buildings from the Late Victorian, Federation and Inter-war periods,
- To promote sympathetic additions and new development that respects the historic patterns, architectural styles, heights, materials and details of existing dwellings,
- To ensure the roofscape maintains the streetscape character of the area. Flat roof forms are permitted to minimise bulk and where the visual impact to the street and adjoining dwelling is minimised,
- To ensure secondary dwellings and ancillary development enhance rear laneways and maintain a high standard of residential amenity for surrounding dwellings,
- To maintain the large street tree canopy over Chesterfield Parade,
- To reduce vehicle conflicts on Chesterfield Parade and Chesterfield Lane, and improve pedestrian safety,
- To retain and increase street tree plantings on all streets to enhance the streetscape character,
- To maintain views and vistas from the public domain.

52 Existing character description

History

Development of the area evolved from a setting of large Marine Villas, with established grounds in the 1850-1880s, Prominent estates included Lugar Brae, Yanko and Chesterfield. Subdivisions during the land boom of 1875-1890 saw the grounds of the Villas reduced, with most Villas demolished by the 1920s and the land further subdivided for new housing. Introduction of tram services via Charing Cross to Waverley Cemetery in 1890 and Bronte Beach in 1911 saw village centres develop around tram stops along Macpherson Street, and further supported the delivery of housing. The prevailing character of the area can still be traced to the extant subdivision patterns, street names and built form of these periods.

Configuration and connectivity

Following the natural ridgeline, Macpherson Street provides the primary east-west connection through the study area. Bronte Road and Arden Street provide the primary north-south connection to and from, however internal north-south connections are limited. Buses service these routes. Local streets and laneways extend from the through-roads creating distinct pockets of development, set in an irregular grid pattern. No-through streets result in varying inter-block connectivity. North of Macpherson Street an elevated plateau offers easterly views to the coast. The topography slopes to the south of Macpherson Street. View openings and vistas exist from the public and private domains including St Thomas Street, Marroo Street, Busby Parade, Macpherson and Simpson Parks.

The area is characterised by residential development with supporting clusters of retail and commercial uses along Macpherson Street, childcare centres, Clovelly Public School and aged care provider St Vincents Care Services Bronte. The Macpherson Street village centres are characterised by a diversity of uses, independent businesses, community services and retail offerings catering to local and visitor needs.

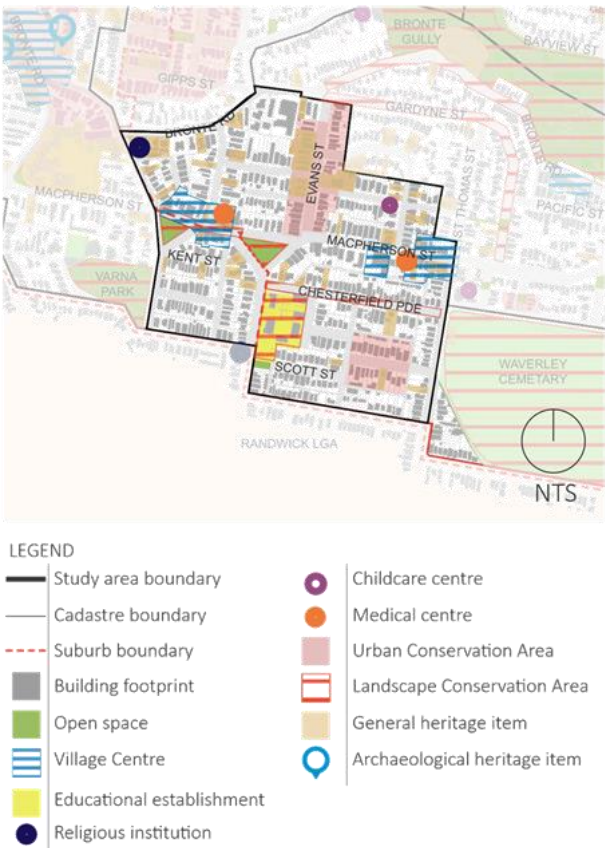


Figure 5-7 - Built form, uses and heritage area #05

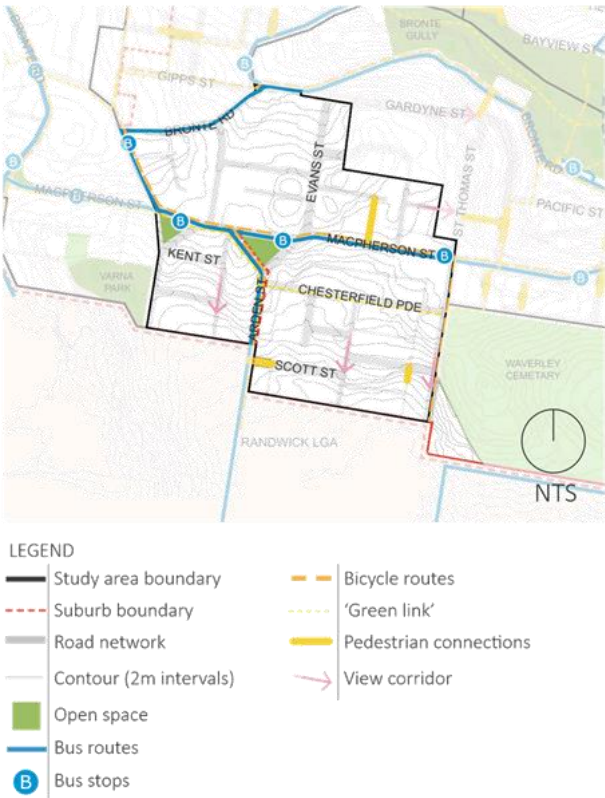


Figure 5-8 - Road network and movement area #05

Built form

The area exhibits a predominantly 1-2 storey character with detached or semi-detached dwellings of Victorian, Federation or Inter-War styles, reflecting the prominent periods of development. The styles are interspersed, resulting in a general lack of consistency across the area, however notable groupings and small consistent streetscapes remain evident. To the north streetscapes of Victorian villas and cottages influenced by the Victorian Italianate and Gothic styles and Federation bungalows are evident on Evans Street, Brae Street and Violet Street, located within the draft Yanko-Lugar Brae Urban Conservation Area. A notable grouping of Victorian Classical-style terraces is located at 6-24 Brae Street. Buildings are typically of face brick, stone and rendered masonry with decorative plaster, timber and iron filigree detailing. Inter-War bungalows are prominent throughout the area with notable clusters fronting Lugar Brae Avenue, Barclay Street, Macpherson Street, Inverness Street, Scott Street and Boundary Street. Bungalows of this period are characterised by low gabled roofs, thick masonry verandah posts and timber decorated gable trim. Alterations to early dwellings including second-storey and car-parking additions, are common. Mid-late 20th century and 21st century 3-4 storey residential flat buildings (RFBs), on large lots, replaced earlier construction and are predominantly located fronting Bronte Road, Lugar Street and Macpherson Street. Buildings of four or more storeys are outliers and are inconsistent with the prevailing height character of the area, for example the Oceanview tower on Macpherson Street. These dwelling typologies account for a low-medium density, with approx. 70 persons/ha.



Figure 5-9 - RFBs, Brae Street

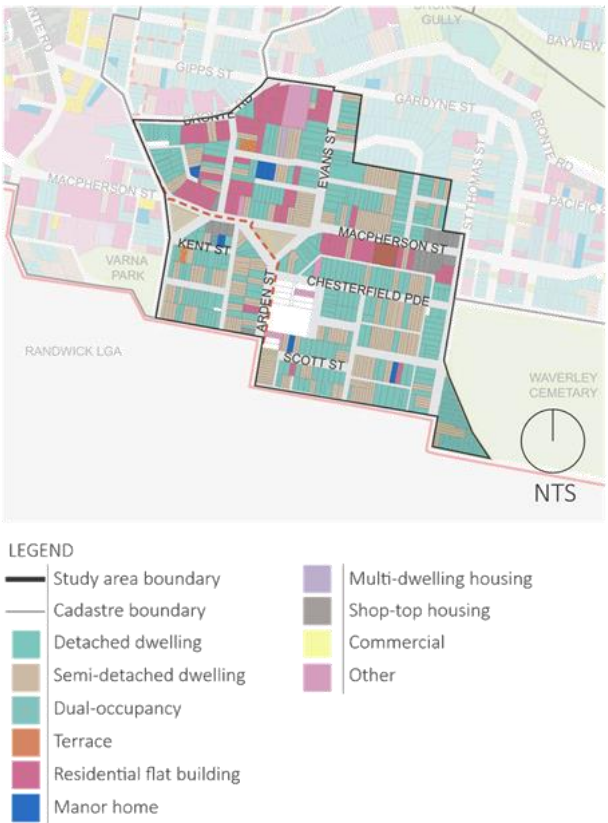


Figure 5-10 - Dwelling typologies area #05

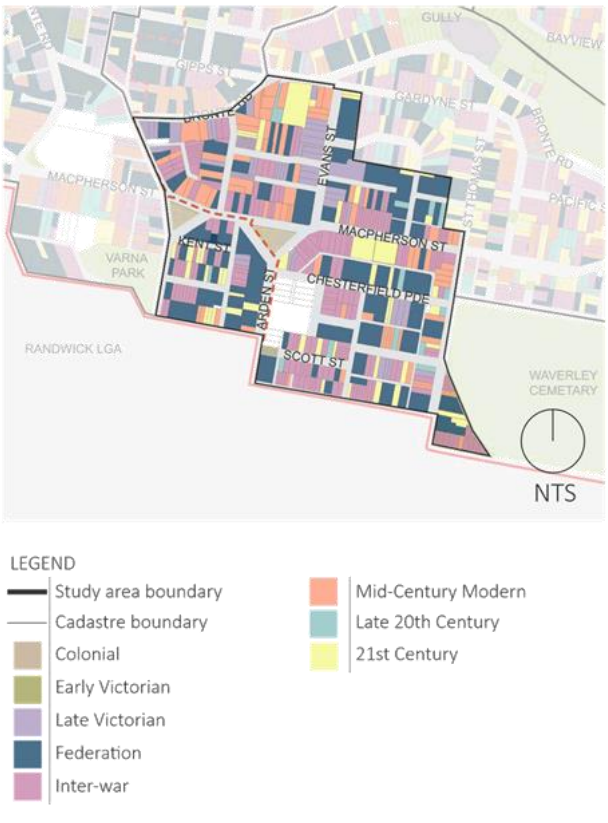


Figure 5-11 - Architectural styles area #05

54 Existing character description cont.

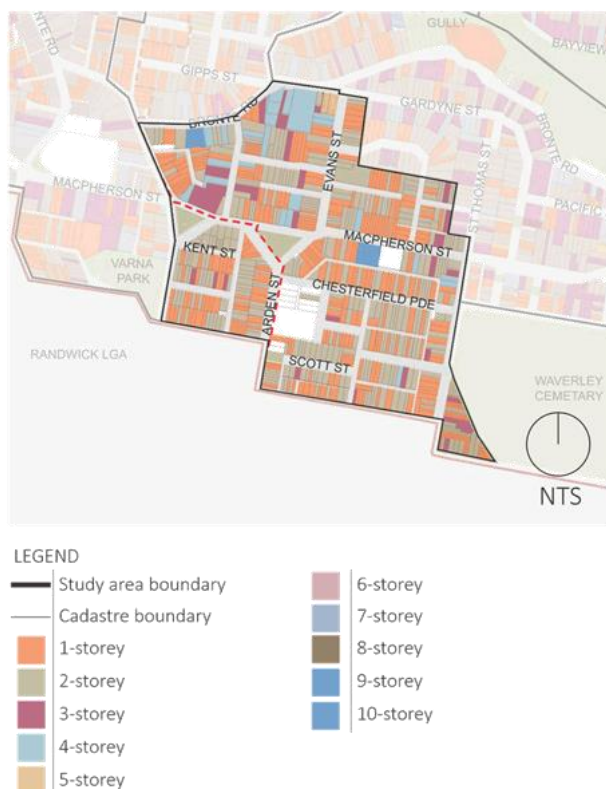


Figure 5-12 - Building height in storeys area #05

Public and private interface

The public and private interface differs considerably throughout the area as a result of varying street widths and depths of front setbacks, for example Lugar Brae Avenue compared to Evans Street. Lots typically have a narrow front setback resulting in proximity of the dwelling to the public domain. Varying fence height and materiality, for example low iron or high brick and front landscaping, creates a varying streetscape character and alters the visibility of the dwelling façade, for example Brae Street. Sloping topography south of Macpherson Street typically results in a high and low side of street. Dwellings on the high side are typically raised overlooking the public domain, with integrated garages at street level, for example Scott Street. Dwellings on the low side are partially obscured, for example Kent Street. Mid-late 20th century RFBs are raised above street level incorporating ground-floor carparking, for example Evans Street (see Figure 5-16). Upper balconies overlook the public domain. The topography and built form bring a sense of enclosure to the streets. At the time of construction, many dwellings would not have comprised on-site parking, a result of the dwelling typology, style and the close proximity to tram stops. Later additions of off-street parking in the form of rear laneway garages and hard-stand carport or enclosed garage within the front setback now result in the majority of dwellings in the study area having off-street parking.



Figure 5-13 - Semi-detached dwellings, Brae Street



Figure 5-15 - Carlton Street, looking south



Figure 5-14 - Detached dwelling, Evans Street



Figure 5-16 - RFB, Evans Street

Natural environment

The study area comprises limited open space, the triangular Simpson and Macpherson Parks (both heritage-listed Landscape Conservation Areas) and Scott Street Reserve, however benefits from surrounding local and regional open space including Queens Park to the west and Bronte Park and Beach to the north-east. 'Green Links' aid with connectivity. Vegetation coverage is typically low-medium across the study area, contributing to a medium heat vulnerability. Cover is particularly low along Macpherson Street, however, improves in surrounding streets dependent on the varying capacity of the street verge to accommodate planting, for example Busby Parade compared to Brae Street. Chesterfield Parade (see Figure 5-18), a Landscape Conservation Area, is distinguished by a colonnade of fig trees and supports a biodiversity habitat corridor. Private open space and landscaping varies depending of lot size, dwelling positioning, inclusion of rear parking, pool etc.



Figure 5-17 - Open space and vegetation area #05



Have Your Say!

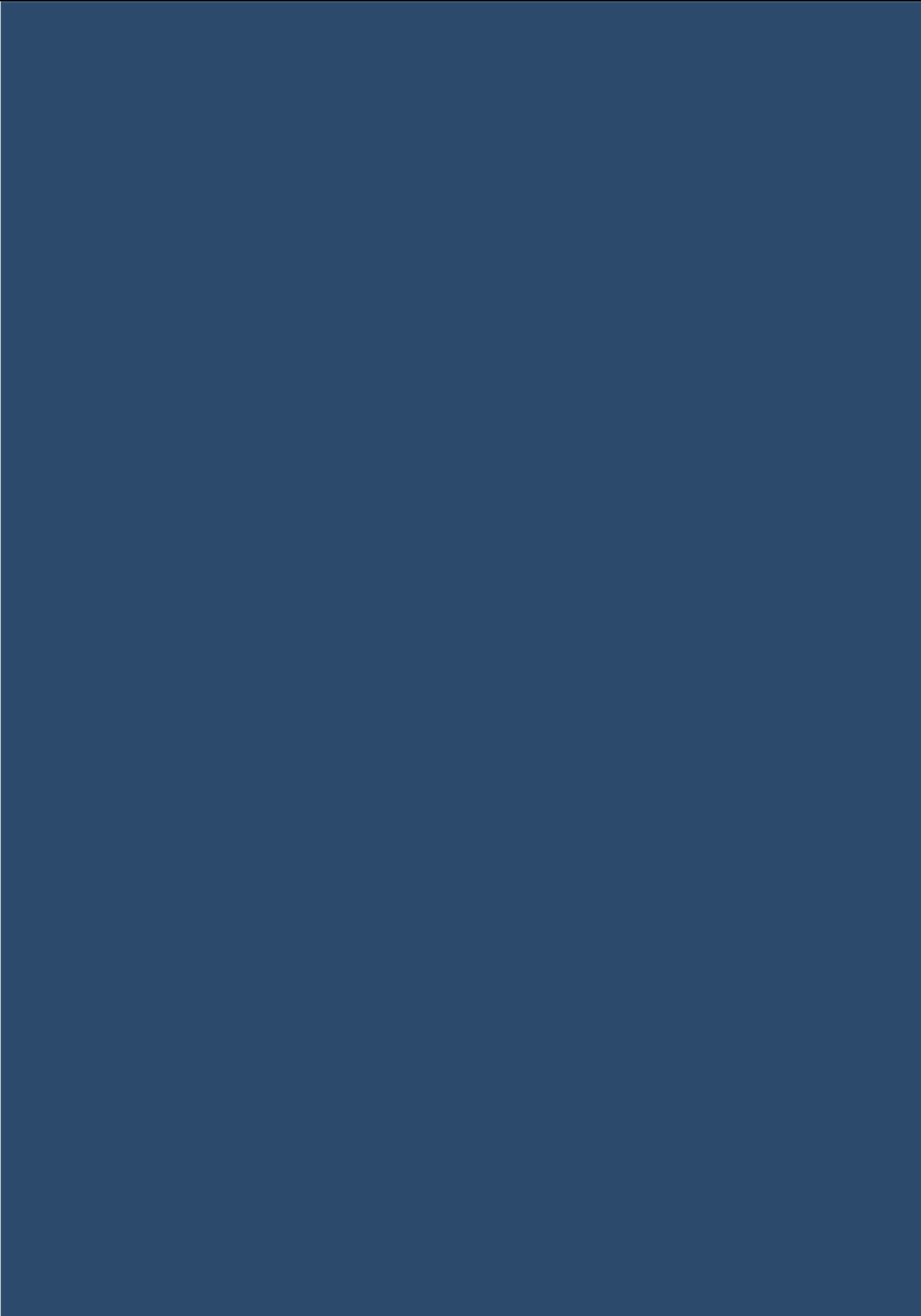
1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?

Recent development

Recent development includes alterations and additions to existing detached and semi-detached dwellings, such as second-storey additions, double garages; demolition of existing dwellings and construction of new 2-3 storey detached or semi-detached dwellings (including dual occupancy), with integrated garages; limited secondary dwellings including studios above rear garage; limited construction of new RFBs, particularly along Macpherson Street. This indicates a demand for additional floor space and car-parking within the area. It is also observed that Bronte has the fourth highest proportion of dwellings by suburb listed on Airbnb within Australia, indicating a trend of use of private dwellings for short-term rental accommodation.



Figure 5-18 - Tree canopy, Chesterfield Parade



CHARACTER AREA

6



CHARACTER AREA - 6

"The area is characterised by its headland location which forms a prominent corridor of coastal open space; panoramic views to the ocean; curvilinear street network, with irregular blocks comprising predominantly low to medium-rise dwelling typologies and styles; and streetscapes of varying materiality, landscaping and garages".

The study area is generally bound by St Thomas Street through to Gardyne Street to the east, Murray Street and Bronte Park to the north, the Pacific Ocean to the east and Trafalgar Street and Waverley Cemetery to the south.



Figure 6-1 - Local character area #06 map



Figure 6-4 - Headland viewed from Bronte Marine Drive



Figure 6-2 - Detached dwellings, Gardyne Street



Figure 6-5 - Gardyne Street, looking north

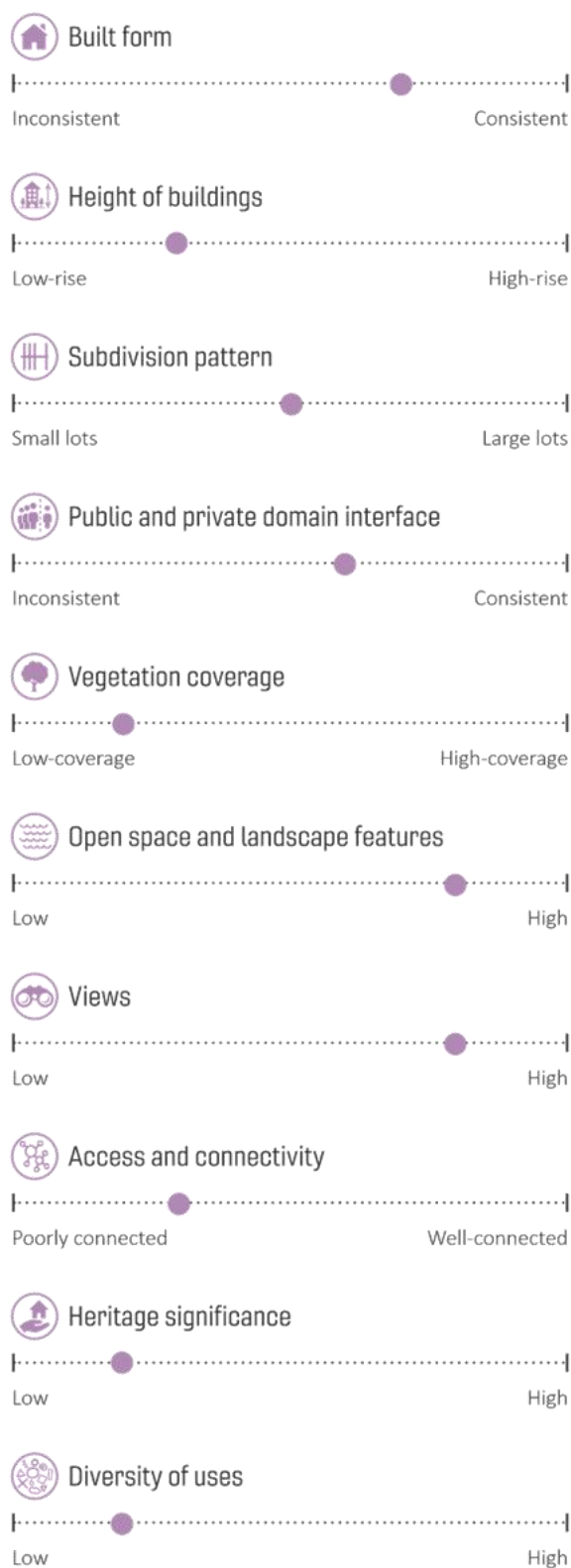


Figure 6-3 - Bronte Road village centre



Figure 6-6 - Pacific Street, looking west

Existing character attributes



Desired future character

Vision

The desired future character of the area is a celebrated iconic coastline and nature reserve, with a small strip of historic shopfronts, and sensitive residential development that respects the terrain.

Objectives

The objectives for development on land identified within the character area are as follows:

- To maintain the predominant 2-3 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography, reinforcing the layering of built form when viewed from the coastline,
- To avoid excessive excavation,
- To retain and enhance the historic shopfronts and manage amenity impacts to the surrounding residential dwellings,
- To ensure high-quality design of new contemporary detached and semi-detached dwellings, with appropriate façade and material articulation. Flat roofs are supported for view sharing,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality,
- To ensure that carparking structures are of a size and materiality that integrate with existing landscape features and do not dominate the streetscape,
- To retain and enhance natural landscape features including the sloping topography, sandstone or bedrock walls,
- To maintain effective landscape buffers between properties abutting Bronte Gully,
- To promote the reduction of hard surfaces and an increase of public and private landscaping,
- To maintain views and vistas from the public domain,
- To minimise the impact on existing views and vistas from the private domain and maintain residential amenity in terms of overlooking and noise,
- To reduce pedestrian and vehicle conflicts and increase pedestrian accessibility.

60 Existing character description

History

Development of the area began following the original land grant to the Colonial Architect Mortimer Lewis (1836-1843) and the construction of Robert and Georgiana Lowe’s Bronte House and Estate in 1840-1845. It was soon followed by other Marine Villa estates in the 1850s. These were subdivided and sold in the building boom of the late 1800s. These land sales continued through the Federation period of the early 1900s, supported by the introduction of tram services via Charing Cross and Macpherson Street to Bronte Beach in 1911. Bronte House remains a fine example of Regency-style architecture, with a large manicured garden merging into Bronte Gully. Whilst the area is predominantly residential, the Bronte Road village centre (see Figure 6-3), fronting Bronte Park at the former tram, now bus terminus, provides an active cluster of restaurants, cafes and retail tenancies catering to local and visitor needs. Another small cluster of commercial premises is located at the intersection of Murray Street and Bronte Road.

Configuration and connectivity

The topography slopes steeply to the north-east from the coastal plateau and headland to Bronte Gully, creating an “amphitheatre” offering wide, panoramic views of the coastline, ocean and surrounding areas. Subdivision and development of the area’s steep topography resulted in a curvilinear street network and irregular blocks and lots of varying size. Lot alignment is typically north-south, east-west despite the irregular layout. Topographical “ameliorations” and interventions provide suitable grades for streets and tram corridors result in terraced sites and streets, stone retaining walls and exposed sandstone bedrock.

Bronte Road and Macpherson Street provide the primary connection to and from and through the study area. Secondary east-west streets, Albert, Pacific and Trafalgar Streets, support internal vehicle and pedestrian connectivity. North-south streets are limited to St Thomas, Gardyne Streets and Yanko Avenue. Connectivity is offset by multiple through-block pedestrian links, for example Bronte Road to Gardyne Street, Pacific Street to Tipper Avenue and Macpherson to Trafalgar Street. North-south vehicular connectivity to and from the study area is further impeded by the Bronte Gully and Waverley Cemetery. Public transport connectivity is also limited, with the area serviced by a single bus route along Macpherson Street and terminating at Bronte Beach.

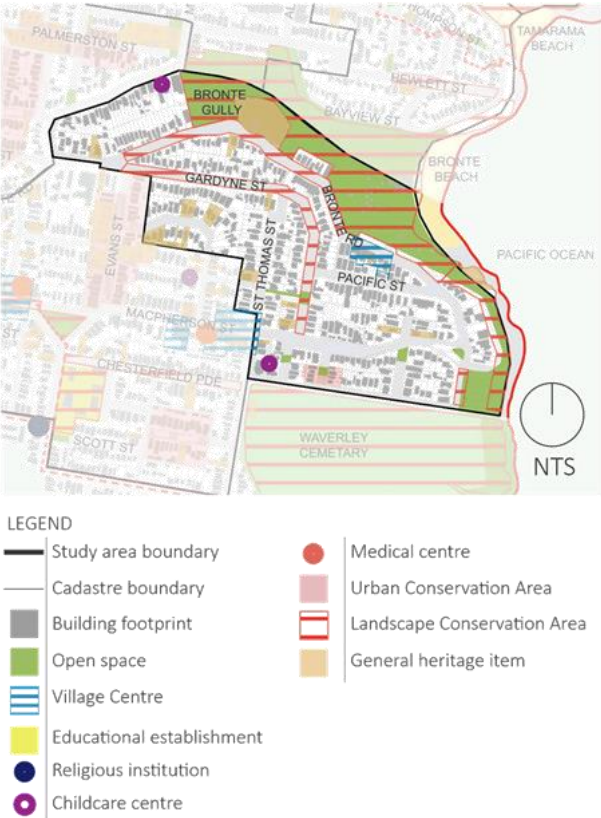


Figure 6-7 - Built form, uses and heritage area #06

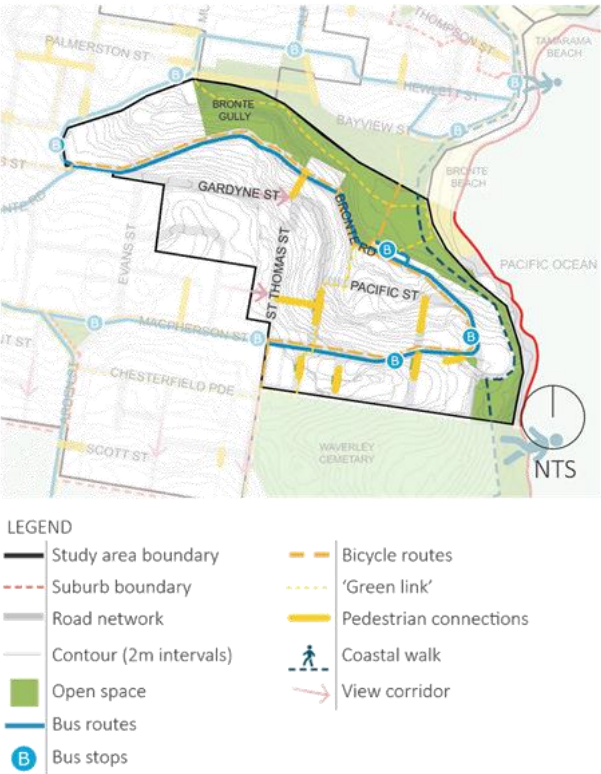


Figure 6-8 - Road network and movement area #06

Public and private domain interface

Streetscapes are characterised by upper and lower sites, each with different features. Dwellings on the upper sites are raised above street level, with built retaining walls, high fences or garage doors fronting the street (Figure 6-10). Front landscaping varies. Car access is difficult with excavation and cutting into the stone walls often required to accommodate garages, for example Gardyne Street (see Figure 6-11). Unsympathetic designs and materiality have in parts altered the street character. Upper balconies typically overlook the public domain and provide views to the ocean. Dwellings on the lower sites are often partially or completely obscured from street level as a result of the sloping topography, vegetation and carports and garages at the boundary, for example Gardyne Street (see Figure 6-13). Where a site abuts two street frontages, for example Gardyne Street and Bronte Road, the rear of the site has typical lower site characteristics with garage frontages and / or dense landscaping. On flatter sites, for example Yanko Ave (see Figure 6-12), Trafalgar Street and Pacific Street, dwelling visibility increases, however streetscapes remain characterised by varying fence height and materiality, front landscaping and garages built to boundary.

Built form

The study area comprises a variety of dwelling typologies and styles, from detached and semi-detached Federation and Inter-War bungalows to Inter-War and Mid-century Modern residential flat buildings (RFBs). Tipper Avenue, Collingwood, Pembroke and Murray Streets comprise early bungalows. Despite notable architectural styles, early dwellings have been significantly modified over time. Modifications include, second-storey and garage additions.



Figure 6-9 - RFB, Pacific Street



Figure 6-10 - Detached dwellings, Gardyne Street



Figure 6-11 - Detached dwellings, Gardyne Street



Figure 6-12 - Detached dwellings, Yanko Avenue



Figure 6-13 - Detached dwellings, Gardyne Street

62 Existing character description cont.

Inter-War and modern RFBs dominant Pacific Street (see Figure 6-9) and Bronte Road. Progressive change of the area has resulted in the replacement of earlier dwellings with large detached dwellings of late 20th century and 21st century styles, particularly along the coastline. These dwelling typologies account for a low density of approx. 50 persons/ha.

Dwellings are predominantly 1-3 storeys in height. The topography has resulted in the modulation of built form, with the dwelling base (retaining wall or garage) at street level and floors stacked above on upper sites or behind and below for lower sites. The exception being the 3-5 storey RFBs distributed through the area. The height of the building can appear increased due to the raised topography above street level, for example Macpherson Street. Flat roof-scapes are becoming more prominent for new dwellings and alterations to existing dwellings.

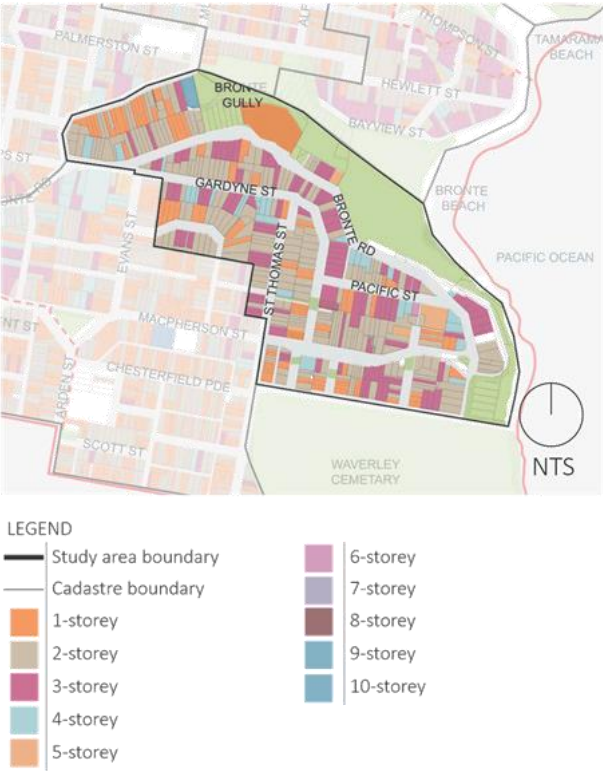


Figure 6-15 - Building height in storeys area #06

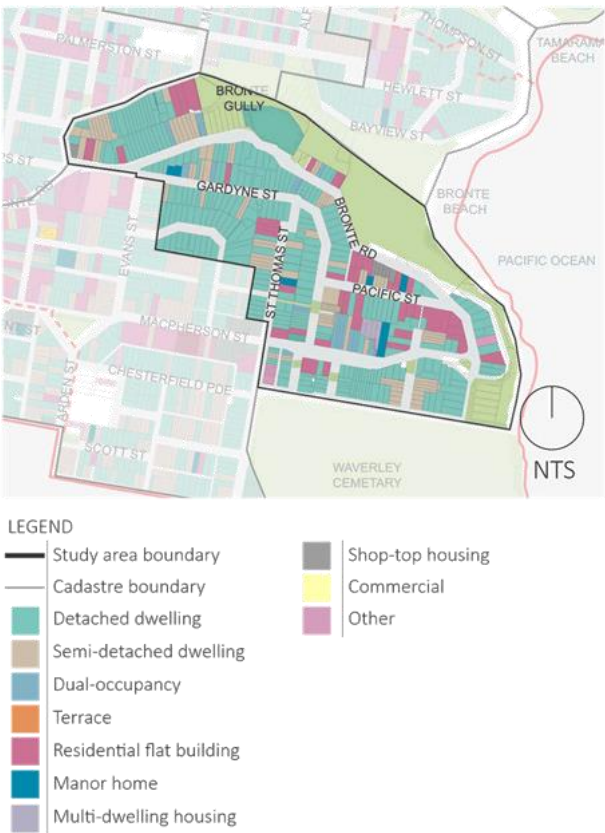


Figure 6-14 - Dwelling typologies area #06

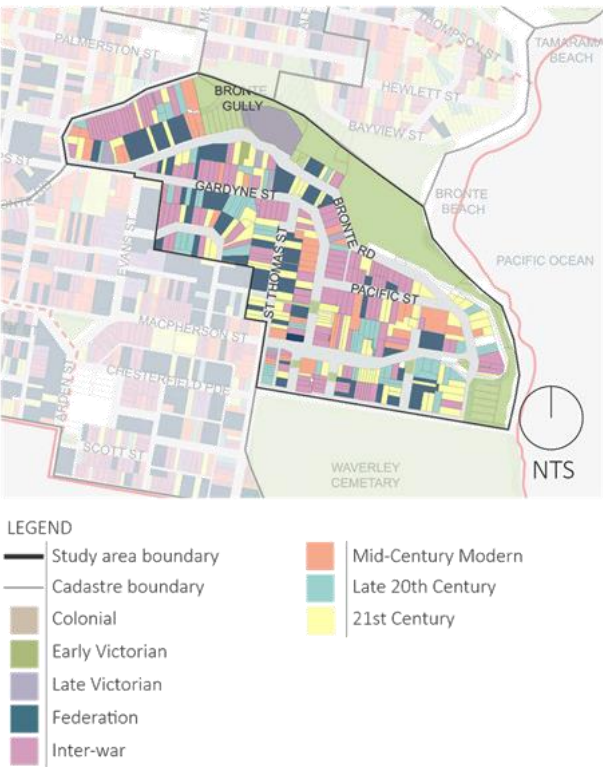


Figure 6-16 - Architectural styles area #06

Natural environment

The character of the study area is defined by its natural landscape setting. The headland, including the Calga Cutting, and the adjacent Waverley Cemetery and Bronte Park and Beach (all heritage-listed as Landscape Conservation Areas) create a prominent corridor of coastal public open space, having significant aesthetic and archaeological value. The corridor is reflective of the area’s long history of seaside leisure, offering passive and active recreation opportunities including the Bronte Ocean Pool and the Bondi-Coojee Walk. Bronte Gully and Park (see Figure 6-18) provide a visual and spatial interface with surrounding properties. The area supports a biodiversity corridor. Vegetation coverage in the public and private domains varies across the area, being predominantly low-medium on the upper sites, characteristic of the elevated position and need to maintain views. The area’s location provides for prevailing coastal winds from the east, which combined with the open character results in low heat vulnerability. Seen from a distance the area juxtaposes the layers of built form, roofscape and vegetation.



Figure 6-17 - Open space and vegetation area #06

Recent development

Recent development includes alterations and additions to existing detached and semi-detached dwellings, such as second-storey additions, double garages; alterations and additions to RFBs; demolition of existing dwellings and construction of new 2-3 storey detached or semi-detached dwellings (including dual occupancy), with integrated garages; limited secondary dwellings including studios above rear garage. This indicates a demand for additional floor space and car-parking within the area. It is also observed that Bronte has the fourth highest proportion of dwellings by suburb listed on Airbnb within Australia, indicating a trend of use of private dwellings for short-term rental accommodation.



Figure 6-18 - Bronte Gully and Park

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

7



CHARACTER AREA - 7

"The area is neither a main thoroughfare nor a destination, characterised by an irregular block pattern; sloping topography; inconsistent built form comprising low-rise detached dwellings, semi-detached dwellings and medium-rise residential flat buildings; and tree-lined streets and open space".

The study area is generally bound by Birrell Street to the north, Dickson and Murray Streets to the east, Bronte Road to the south and Henrietta Street to the west.



Figure 7-1 - Local character area #07 map



Figure 7-4 - Dwellings fronting Seaview Street



Figure 7-2 - Layered built form viewed from Brown Street



Figure 7-5 - RFB, corner Langlee Avenue and Birrell Street

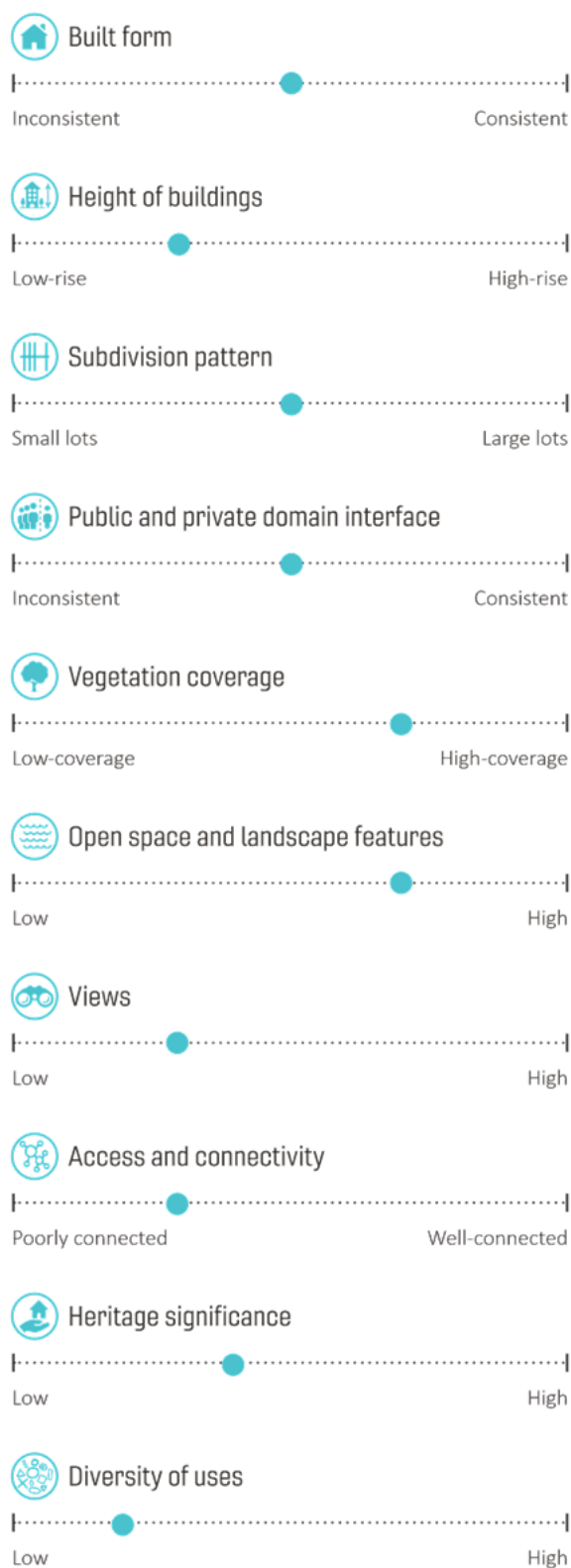


Figure 7-3 - Dwellings fronting Brown Street



Figure 7-6 - Brown Street, looking south

Existing character attributes



Desired future character

Vision

The desired future character is a leafy and diverse residential area with easy and safe access to open space.

Objectives

The objectives for development on land identified within the character area are as follows:

- To retain the natural landscape setting of the area, by maintaining low site coverage, enhancing landscaping in the front, rear and side of properties and limiting hard surfaces,
- To retain and extend street tree plantings on all streets to enhance the streetscape character,
- To retain and enhance natural landscape features including the sloping topography, sandstone or bedrock walls,
- To avoid excessive excavation,
- To preserve the diversity of dwelling typologies, architectural periods and heights in the area by retaining buildings from the Late Victorian, Federation, Inter-war periods and Mid-Century periods,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and reinforce the established form, style and materiality of the dwelling,
- To ensure new development responds to the existing built form and predominant street frontages in terms of height, setbacks and street alignment,
- To ensure that car parking does not dominate or adversely impact on the existing landscape or built character of the area,
- To discourage new driveway crossovers,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To maintain views and vistas from the public and private domains.

68 Existing character description

History

Development of the area has resulted from the subdivision of many original land holdings between the 1890s and 1940s, including the Palmerston, Malborough and Langlee Estates. Vacant land to the west of the Palmerston Estate, in the location of Malborough Reserve to Palmerston Avenue, was utilised as a quarry until the 1940s. The original ‘Palmerston House’ (1855), an early Victorian Villa, remains (albeit modified) at 18 Blandford Avenue. Housing shortages during WWI resulted in many large Federation homes being converted into flats, with flat development continuing through the Inter-War and Post-War periods. The area comprises predominantly residential uses, including aged care facilities, with educational establishments – Waverley College Junior School and Bronte Public School – at the periphery and small pockets of open space throughout. It is in close proximity to other commercial centres including Charing Cross and Bondi Junction.

Configuration and connectivity

The topography slopes from the north-west at Birrell Street, to the south-east of the study area (towards the Bronte Gully). A steep change occurs to the west of the area, at the rear of dwellings fronting Blandford Avenue and Brown Street, leading into a localised depression at the former quarry site. Dwellings on the eastern side of Langlee Avenue and Seaview Street (see Figure 7-2) overlook the lower areas towards the coastline. Views are also available from high points on Birrell Street.

The progressive subdivision of early estates combined with topographical changes have resulted in an irregular street and block pattern, with lots of varying size. Birrell Street, Murray Street and Bronte Road, at the area’s boundaries, provide the primary connections to and from the area. Buses service these routes. Internal roads limit circulation, being typically one-way or no-through, for example Dickson, Carter and Gibson Streets. The area is neither a main thoroughfare nor a destination, catering to predominantly local traffic only. Pedestrian access and connectivity are maintained by multiple through-block links (see Figure 7-8). Walkability varies along these pedestrian connections and throughout the area, with pedestrian links often consisting of stairs only.

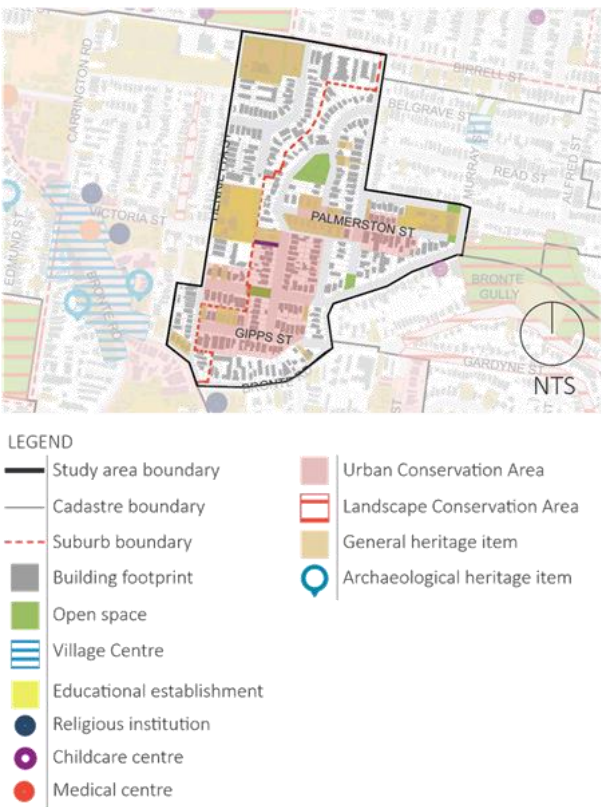


Figure 7-7 - Built form, uses and heritage area #07

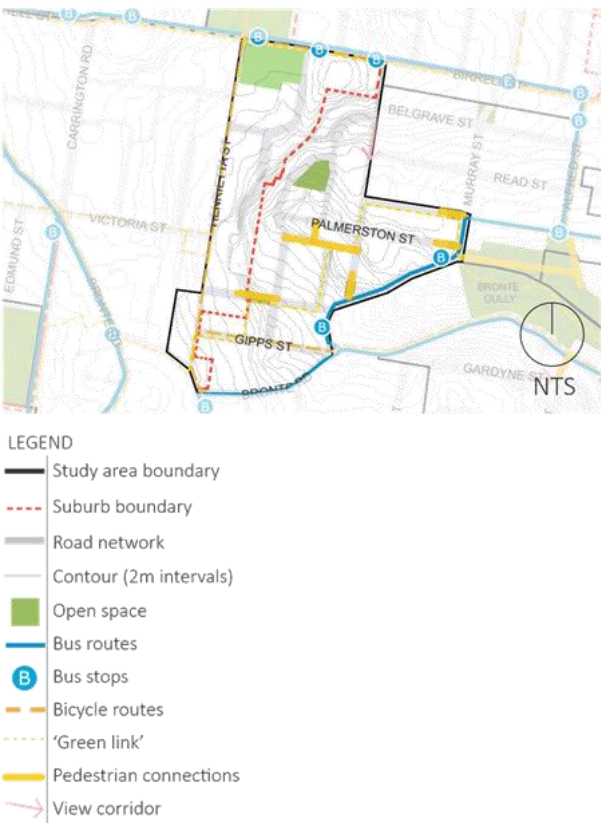


Figure 7-8 - Road network and movement area #07

Built form

The area is characterised by predominantly detached dwellings, interspersed with semi-detached dwellings and residential flat buildings (RFBs), of various architectural styles from Federation and Inter-War to 21st century. Clusters of detached and semi-detached simple Federation dwellings are evident within Langlee Avenue, Henrietta Street, Dickson Street, and Gibson, Gipps and Brown Streets (heritage-listed as the Brown Street Conservation Area). The bungalows are of brick or weatherboard construction with low gabled roofs, timber verandah posts and decorated timber gable trim. Simple Inter-War bungalows are also evident on Gibson Street (see Figure 7-13) and Blandford Avenue. Bungalows of this period are characterised by low gabled roofs, thick masonry verandah posts and timber decorated gable trim. Despite noticeable architectural styles, early dwellings have been significantly modified overtime. Modifications include, second-storey and garage additions. Cohesive groupings of Inter-War and early Post-war RFBs are evident on Henrietta Street and Palmerston Avenue (heritage-listed as the Palmerston Street Conservation Area) displaying curved or flat facades with decorative stepped brick, characteristic of the Art Deco and Free Classical styles. Mid-century detached dwellings and RFBs, for example on Blandford Avenue, Birrell Street and Bronte Road, reflect the periodic development and redevelopment of the area. Sporadic modern infill development of late 20th century and 21st century styles has also occurred throughout the area. These dwelling typologies result in a population density of approx. 76 persons /ha.



Figure 7-9 - Detached dwellings, Brown Street

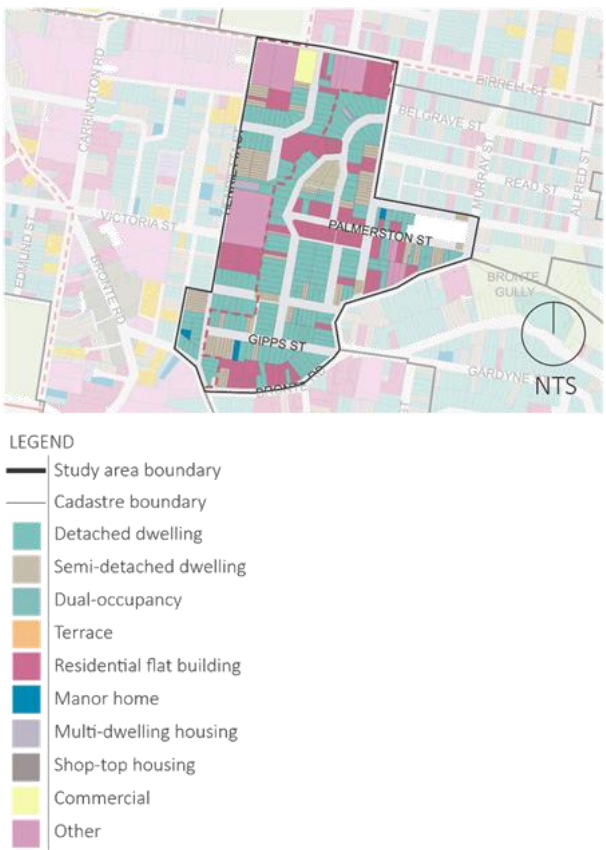


Figure 7-10 - Dwelling typologies area #07

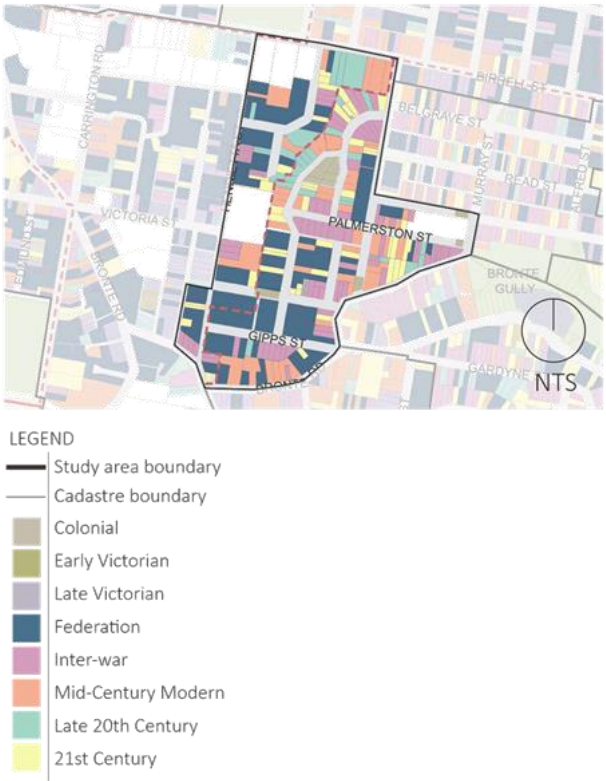


Figure 7-11 - Architectural styles area #07

70 Existing character description cont.

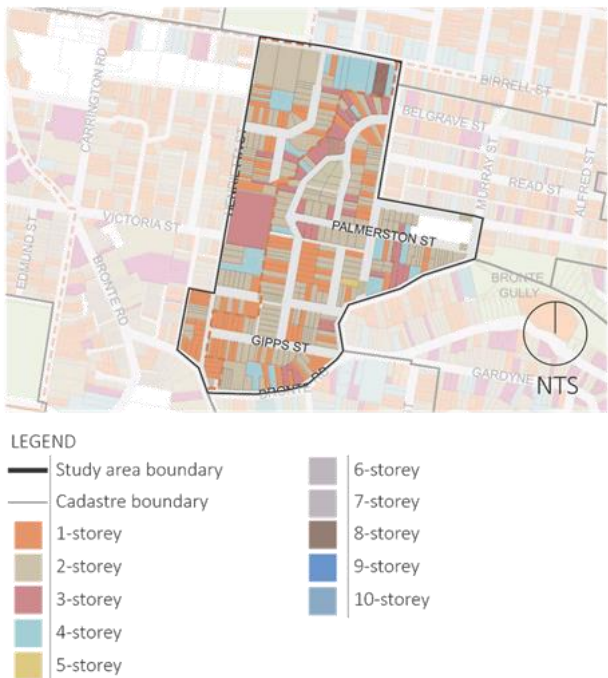


Figure 7-12 - Building height in storeys area #07

Dwellings are predominantly 1-2 storey in height, with the exception of 3-4 storey RFBs or dwellings on sloping sites. Transition in building height is largely influenced by topographical change. The height of a building may appear increased due to the raised topography with the dwelling sitting above a street-level garage, for example Blandford Avenue (see Figure 7-14). Buildings of four or more storeys, for example the 8-storey point-block-tower on Birrell Street (see Figure 7-16), are outliers and are inconsistent with the prevailing height character of the area.

Public and private domain interface

The public and private interface differs considerably as a result of varying street widths, and periodic development of different typologies and styles. Depths of front setbacks are generally consistent, however encroachment by parking additions including enclosed garages is common. On sloping sites excavation is required to accommodate garages. This can limit front landscaping, obscure the building façade and detract from the public-private transition, in instances creating an undesirable solid wall at street level. Mid-late 20th century RFBs are raised above street level incorporating ground-floor car-parking. Upper balconies overlook the public domain.



Figure 7-13 - Detached dwellings, Gibson Street



Figure 7-15 - RFBs, Palmerston Avenue



Figure 7-14 - Semi-detached dwellings, Blandford Avenue



Figure 7-16 - RFBs, Birrell Street

Natural environment

Vegetation coverage varies from medium-high in the south of the study area reducing to low-medium in the north. Many streets comprise significant tree planting on street or within the front setback, which softens the transition from the street to dwelling but can also obscure the dwelling façade. For example, terraced sites fronting Blandford Avenue are characterised by consistent front landscaping, which has been deteriorated by new modern infill. Irregular lots, with deep rear yards support private landscaping, including the dense vegetation, known as “the Jungle”, to the rear of Waverley College Junior School. Marlborough Reserve, Gibson Street Reserve (see Figure 7-18) and other pedestrian links also support vegetation. These factors contribute to a low-medium heat vulnerability.

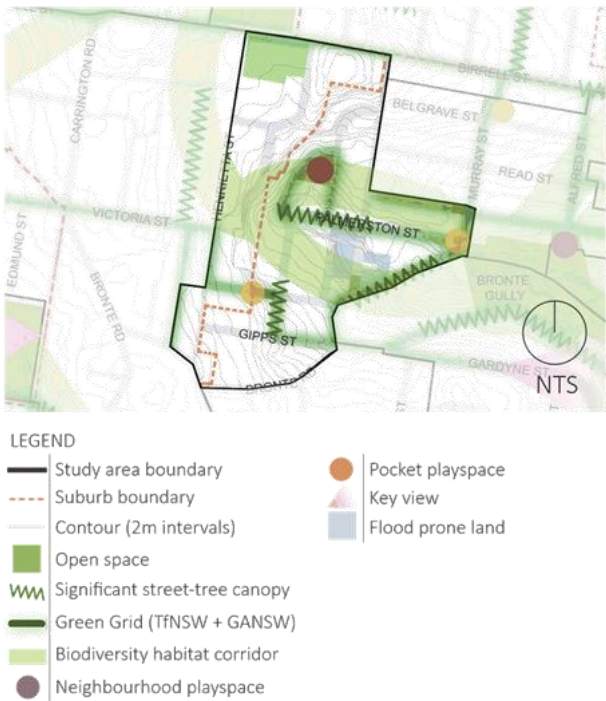


Figure 7-17 - Open space and vegetation area #07




Figure 7-18 - Gibson Street Reserve, looking east

Recent development

Recent development includes alterations and additions to existing detached and semi-detached dwellings, such as second-storey additions, double garages, carports, demolition of existing dwellings and construction of new 2-3 storey detached or semi-detached dwellings (including dual occupancy), with integrated garages, and the partial redevelopment of the Waverley Bowling Club for seniors housing. Indicative of a trend toward maintaining existing built form whilst achieving more floor space and on-site car-parking and redevelopment of sites for the highest and best use.

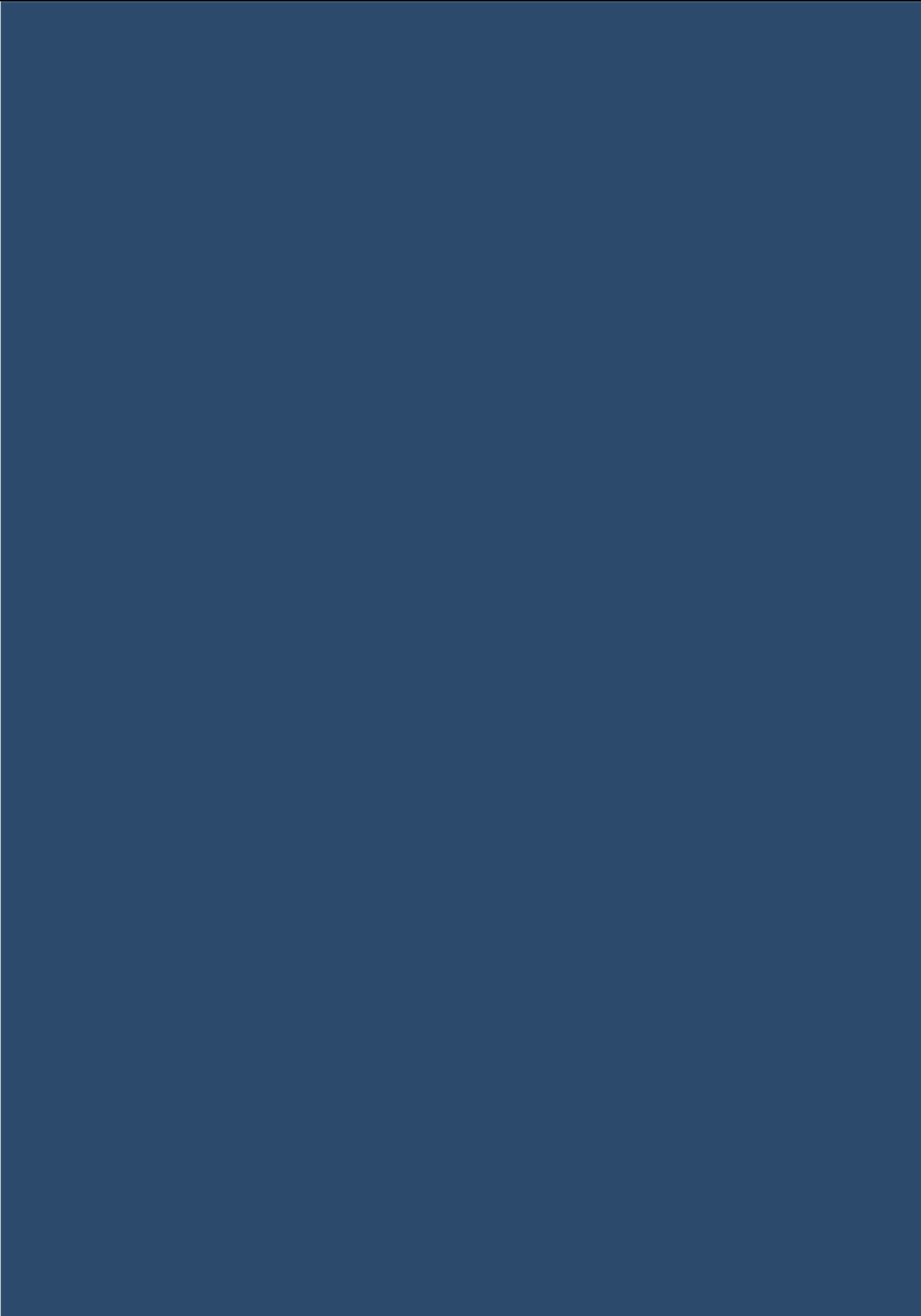


Figure 7-19 - Langlee Lane, looking east



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

8



CHARACTER AREA - 8

"The area is characterised by well-defined, regular blocks comprising detached, and semi-detached dwellings interspersed with residential flat buildings, of varying heights, architectural styles, materiality and finishes; narrow rear laneways; and wide-street verges with limited drive-way crossovers and sporadic street trees".

The study area is generally bound by Dickson Lane to the north, Cross Street to Hewlett Street to the east, Bronte Gully and Hewlett Street to the south and Dickson Street to the west.



Figure 8-1 - Local character area #08 map



Figure 8-4 - Semi-detached dwelling / RFB, Alfred Street



Figure 8-2 - RFB, Belgrave Street



Figure 8-5 - Streetscape, Belgrave Street



Figure 8-3 - RFB, cnr Alfred Street and Hewlett Street



Figure 8-6 - Detached dwellings, Read Street

Existing character attributes



Desired future character

Vision

The desired future character is a leafy and varied residential area with easy and safe access to open space.

Objectives

The objectives for development on land identified within the character area are as follows:

- To promote the diversity of dwelling styles and typologies, including semi-detached (dual occupancy),
- To encourage the restoration of traditional architectural forms and details of Federation and Inter-War-style bungalows,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and reinforce the established form, style and materiality of the dwelling,
- To ensure new development responds to the adjacent built form and street frontage in terms of height, setbacks and street alignment,
- To maintain the 1-4 storey height character of the area,
- To ensure secondary dwellings and ancillary development enhance rear laneways and maintain a high standard of residential amenity for surrounding dwellings,
- To discourage new driveway crossovers and car parking within the front setback, by retaining vehicle access from rear lanes,
- To retain and extend street tree plantings on all streets,
- To promote landscaping at the rear of dwellings, to soften transition to rear laneways.

76 Existing character description

History

Development of the area has resulted from the subdivision of various land holdings including the Dickson, Palmerston, Kenilworth, Mandeville and Pacific View Estates, in the Late Victorian to early Federation era, between 1880-1900. These holdings were part of 16 blocks of land of varying sizes on the elevated plateau, on the southern side of Birrell Street that were sold in 1855 to various purchasers. The topography slopes gently from the natural ridgeline along Belgrave Street, south-east towards Bronte Gully. The area was developed for predominantly residential use, with the subdivision sale notices advertising “commanding grand views, convenient access to Waverley and Bondi trams...” and a “healthy location”. The area remains predominantly residential except for the Belgrave local centre which comprises a small cluster of shops.

Configuration and connectivity

Successive subdivision of the estates combined with the area’s relatively flat topography has resulted in a well-defined, regular street network, of generally north-south and east-west alignment. The network consists of a primary through-street, secondary local streets and rear laneways. Murray Street provides the primary north-south connection to and from the area. The regular block and street network results in a high degree of permeability. Wide street corridors, with wide verges improve visibility. Relatively flat topography promotes walkability. Public transport is limited with only a few bus routes servicing the area via Birrell, Alfred, Hewlett and Murray streets, which provide connection to surrounding areas including Bondi Junction.



Figure 8-7 - Alfred Street, looking south

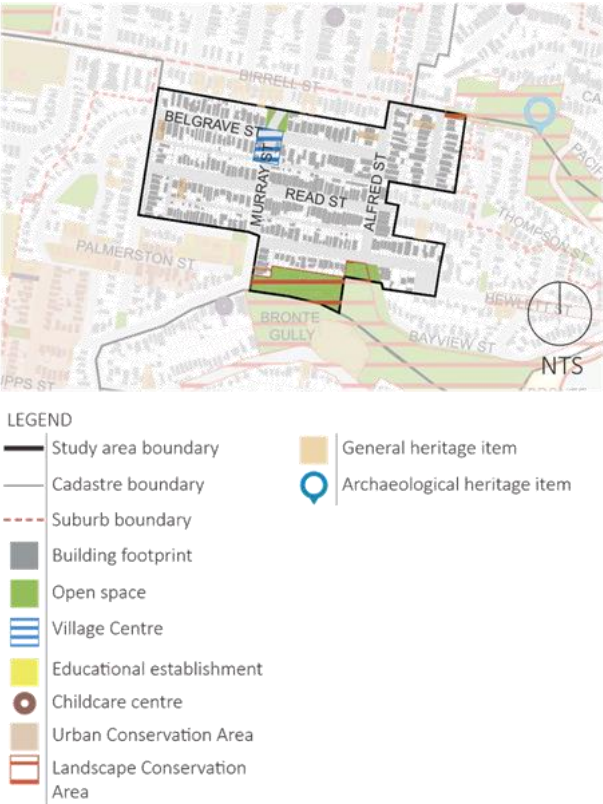


Figure 8-8 - Built form, uses and heritage area #08

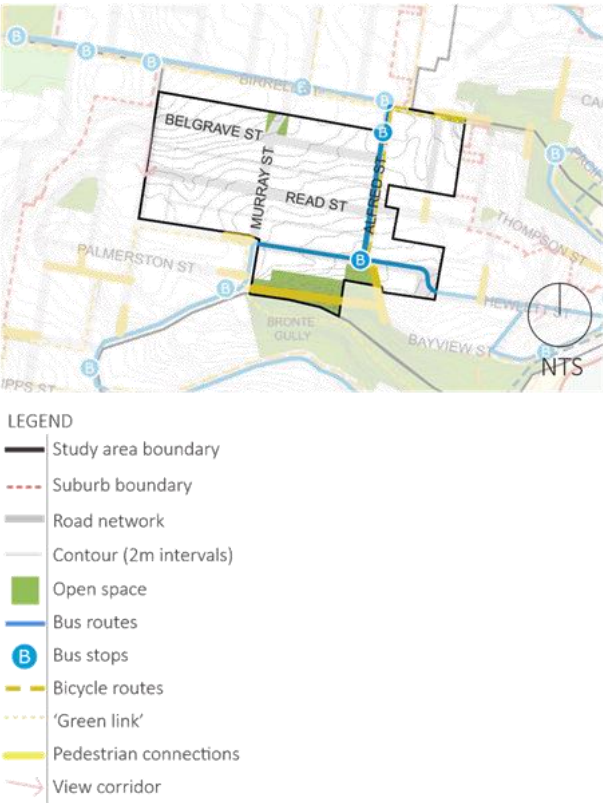


Figure 8-9 - Road network and movement area #08

Built form

The area is predominantly characterised by 1-2 storey detached and semi-detached dwellings interspersed with 3-4 storey residential flat buildings (RFBs), of varying architectural styles. The predominant dwelling typologies are Federation and Inter-War bungalows. These dwellings are often characterised by brick, rendered brick or weatherboard construction, low gabled terracotta tiled roofs, with thick masonry, timber or ironwork verandah posts and decorated timber gable trim. Window awnings and chimneys are additional features. Exemplar buildings include heritage-listed 2 Belgrave Street and 3 Alfred Street, as well as 13 Belgrave Street and 2 Read Street. A cluster of Federation bungalows are evident on the western side of Cross Street. Examples of earlier Late-Victorian dwellings are evident to the west of the area on Read, Dickson and Belgrave streets. These styles reflect the evolution of housing as the area changed from a district of rural worker housing and isolated villas to a setting of close subdivisions. However, despite noticeable architectural styles, early dwellings have been significantly modified overtime. Modifications include second-storey and garage additions. Replacement of early dwellings has occurred since the 1950s, with the Mid-century Modern style RFBs and late 20th century and 21st century detached and semi-detached dwellings evident throughout the area. The interspersed of these dwelling results in a varied tapestry of building heights, architectural styles, materiality and finishes. The area has a population density of about 70 persons/ha.



Figure 8-10 - Detached dwelling, Belgrave Street

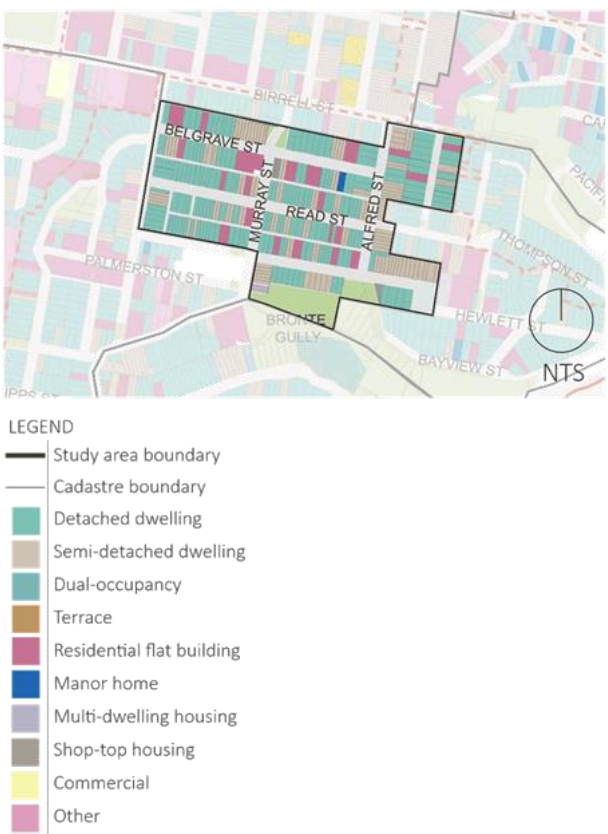


Figure 8-11 - Dwelling typologies area #08

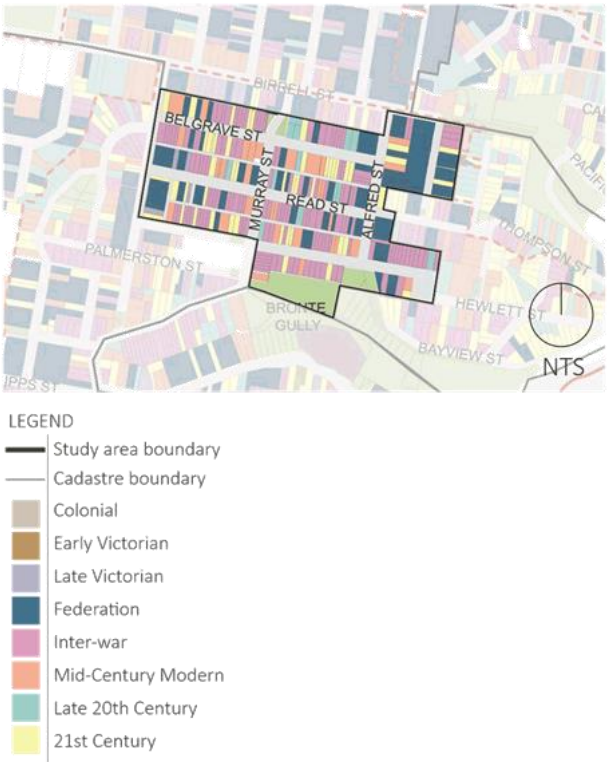


Figure 8-12 - Architectural styles

78 Existing character description cont.

Public and private interface

The public and private domain interface varies depending on street hierarchy. Murray Street, the primary through-street, is characterised by narrow verges with limited street trees. Side boundaries typically abut the street, resulting in inactive frontages, screened by high fences (see Figure 8-14). Where a front boundary abuts the street, it is typically characterised by street-level garages. For local streets, wide verges provide for additional street trees, softening the transition between the public and private domains. Dwelling setbacks are typically consistent and of a narrow depth, resulting in the dwelling being in close proximity to the street. However, visibility of the dwelling, however varies, due to carparking and landscaping of the front setback and the style of the front fence, for example timber or brick of low-medium height. The exception being Mid-century Modern RFBs, which are characterised by street-level garages with upper balconies overlooking the public domain (see Figure 8-16). Rear lanes are characterised by garages, hard-stand parking or high fences at the boundary (see Figure 8-17). Dwellings are predominantly serviced by off-street carparking at the rear of the property, accessed by the adjoining lane, hard stand / carport parking in the front setback or integrated garages for dwellings without rear lane access. Laneway parking reduces the need for driveway cross overs, resulting in a more consistent streetscape and greater on-street carparking.



Figure 8-13 - Street frontage, Read Street



Figure 8-14 - Street frontage, Hewlett Street



Figure 8-15 - RFB, Belgrave Street



Figure 8-16 - RFB, Murray Street



Figure 8-17 - Garage frontages, Read Lane

Natural environment

Open space is limited within the study area, comprising of small pockets in Jessie Street Reserve, Belgrave Street Reserve and Hewlett Street Park (see Figure 8-18). Notwithstanding, the area abuts Bronte Gully and is in close proximity to the coastal open space network and Tamarama and Bronte beach / park to the east, and Waverley Park to the north-west. A biodiversity habitat corridor traverses the area, connecting these open spaces. Vegetation coverage across the area is generally low. In the public domain, wide verges support increased planting, however rear lanes are typically void of vegetation. In the private domain, planting is low due to narrow front setbacks and rear lane structures, however planting increases where rear boundaries adjoin. These conditions result in a medium-high urban heat island affect and low-medium heat vulnerability.

Recent development

Recent development history includes alterations and additions to existing detached and semi-detached dwellings, such as second-storey additions, double garages, carport replacements; demolition of existing dwellings and construction of new 2-3 storey detached or semi-detached dwellings (including dual occupancy), with integrated garages; limited secondary dwellings including studios above rear garage. This indicates a demand for additional floor space and car-parking within the area. It is also observed that the suburb of Bronte has the fourth highest proportion of dwellings by suburb listed on Airbnb within Australia, indicating a trend of use of private dwellings for short-term rental accommodation.



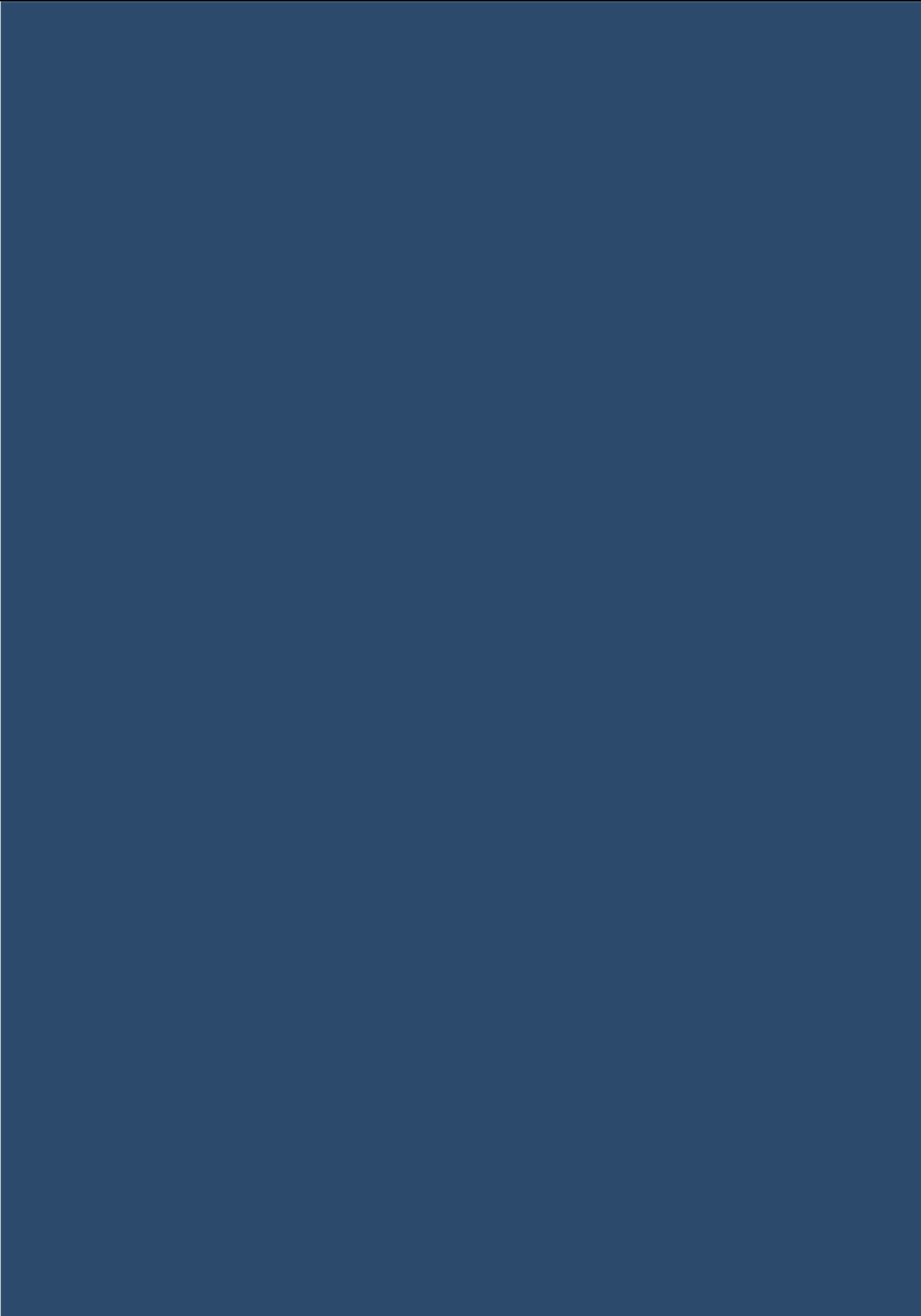
Figure 8-18 - Hewlett Street Park Community Garden

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



Figure 8-19 - Open space and vegetation area #08



CHARACTER AREA

9



CHARACTER AREA - 9

"The area is defined by its prominent headland location and adjoining gullies which form a prominent corridor of coastal open space; panoramic views of the ocean; irregular blocks, with terraced streets and steep lots comprising layered low-rise built form, with built retaining walls, fences, garages and limited vegetation fronting the street".

The study area is generally bound by Tamarama Gully and Park to the north, the Pacific Ocean to the east, Bronte Gully and Park to the south and Ashley Street to Hewlett Street to the west.



Figure 9-1 - Local character area #09 map



Figure 9-4 - Street frontage, Pacific Avenue



Figure 9-2 - Tamarama Beach and Park from Pacific Avenue



Figure 9-5 - Detached dwellings, Thompson Street



Figure 9-3 - Natural coastal setting from Wolaroi Crescent



Figure 9-6 - Tamarama Gully from Birrell Street pedestrian link

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by high-quality, contemporary detached and semi-detached dwellings with appropriate façade and material articulation. Dwellings are integrated with the natural sloping topography, buffered by vegetation that enhances the natural setting of the coastal headland.

Objectives

The objectives for development on land identified within the character area are as follows:

- To maintain the predominant 2-3 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography,
- To avoid excessive excavation,
- To ensure high-quality design of new contemporary detached and semi-detached dwellings, with appropriate façade and material articulation. Flat roofs are supported for view sharing,
- To ensure that carparking structures are of a size and materiality that integrate with existing landscape features and do not dominate the streetscape,
- To retain and enhance natural landscape features including the sloping topography, sandstone or bedrock walls,
- To maintain the significant landscape buffer between Thompson Street and Tamarama Marine Drive, and promote development that retains the landscape character,
- To promote the reduction of hard surfaces and an increase of public and private landscaping,
- To maintain views and vistas from the public domain,
- To minimise the impact on existing views and vistas from the private domain and maintain residential amenity in terms of overlooking and noise.

84 Existing character description

History

The character of the area is defined by its prominent headland location, offering panoramic views of the coastline, ocean and surrounding areas. The topography slopes steeply towards the coast and adjoining gullies. Progressive subdivision and development of the area from the late 1880s has resulted in irregular blocks, with terraced streets and steep lots of varying size. Stone retaining walls and exposed sandstone bedrock are common features, for example on Thompson Street. Lot alignment is typically north-south, east-west, except for the blocks closest to the south, which remained only partially developed until the 1940s.

Configuration and connectivity

Topographical changes, combined with an unclear street hierarchy, restricts access to, from and within the area. Hewlett (see Figure 9-7) and Darling streets provide the primary east-west connections. The coastal road, Tamarama Marine Drive and Bronte Marine Drive, provides connectivity to the north. Bronte Gully and Park impedes connectivity to the south. Limited buses service these routes. The prevalence of no-through local streets, for example Bayview Street, Wolaroi Crescent and Harlowe Place, further impede access and circulation within the area. Pedestrian connectivity is maintained through multiple through-block pedestrian links, for example Thompson Street, Wolaroi Crescent to Tamarama Park and Andrew Street to Hewlett Street.



Figure 9-7 - Hewlett Street, looking east

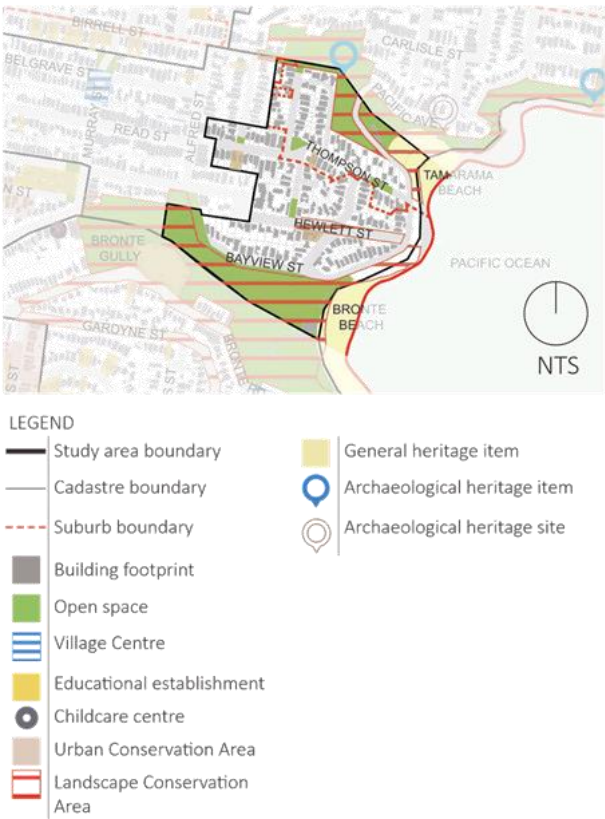


Figure 9-8 - Built form, uses and heritage area #09

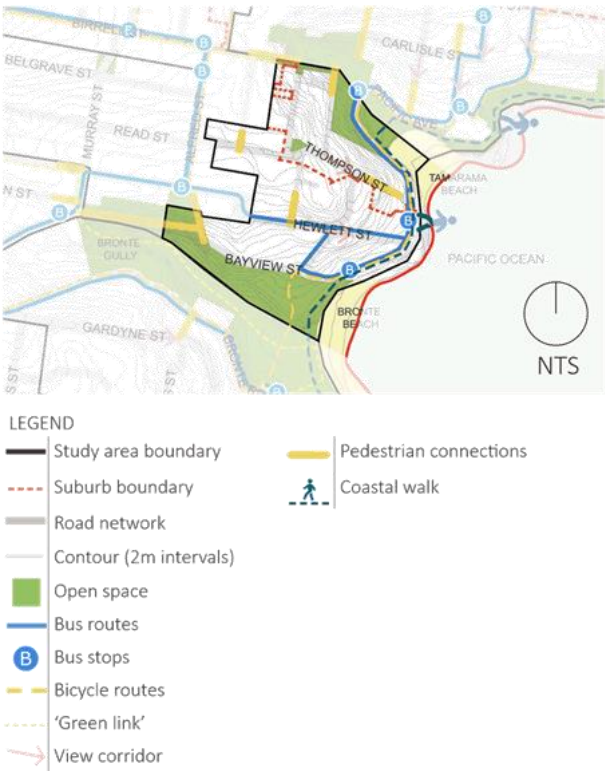


Figure 9-9 - Road network and movement area #09

Public and private domain interface

Streetscapes are typically characterised by upper and lower sites, each with different features. Variable verge widths and front setbacks result in built form (dwellings, garages) being situated at varying distances from the public domain. Dwellings on the upper sites are raised above the public domain, with built retaining walls, high fences and / or garage doors of varying materials and finishes, fronting the street. A solid wall presenting to the street is common, for example Thompson (see Figure 9-10) and Darling streets. Front landscaping also varies, with deep soil often limited due to garage construction, for example Bayview Street. Upper balconies typically overlook the public domain and are oriented to provide expansive views to the ocean. Dwellings on the lower sites often have their ground level partially or completely obscured from the street as a result of the sloping topography, vegetation, fencing and / or garages at the boundary, for example Ashley Street and Hewlett Street (see Figure 9-11). Again, materiality and finishes vary, contributing to the area's inconsistent character. On flatter sites, for example Andrew Street, dwelling visibility increases, however the streetscape remains characterised by parking structures and varying front landscaping. The combination of terraced upper and lower sites creates a layering of built form when viewed from a distance.

Built form

The area comprises predominantly detached dwellings, interspersed with semi-detached dwellings and clusters of residential flat buildings (RFBs) in Andrew and Bayview streets (see Figure 9-12). These dwelling typologies result in a low population density. Architectural styles vary, however Mid-century, late 20th century and 21st century styles dominant, highlighting the progressive redevelopment of the area since the 1950s. Limited examples of earlier Federation or Inter-War dwellings remain. Those that do remain, particularly detached or semi-detached bungalows, are characterised by brick, rendered brick or weatherboard construction, low gabled terracotta tiled roofs, with thick masonry or timber verandah posts and decorated timber gable trim. These have been substantially modified over time, with second-storey and garage additions. Notable examples of Inter-War RFBs include 99 Hewlett Street (heritage-listed) and 2 Bayview Street. Modern dwellings are typically 3-storey flat-roofed cascading buildings with wider modulation, wide integrated garages, large balconies and areas of transparent glass for doors, windows and balcony balustrades (see Figure 9-13).



Figure 9-10 - Detached dwellings, Thompson Street



Figure 9-11 - Obscured detached dwellings, Hewlett Street



Figure 9-12 - RFB, Bayview Street



Figure 9-13 - Detached dwellings, Mirimar Avenue

86 Existing character description cont.

These dwellings seek to maximise floor space and height and capture available views.

Dwellings are predominantly 2-3 storeys in height. The topography has resulted in the modulation of built form, with the dwelling base (retaining wall, fence or garage) at street level and floors stacked above on upper sites or behind and below for lower sites. The height of the building can appear increased due to the raised topography above street level. Further, dwellings sloping up / down sites are irregularly splayed, with varying front and rear setbacks, resulting in varied transitions between dwellings.

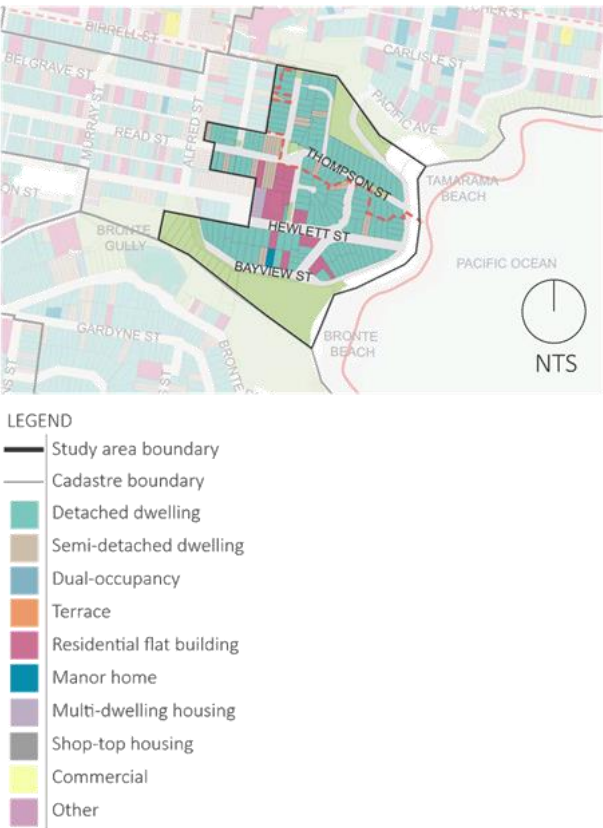


Figure 9-15 - Dwelling typologies area #09

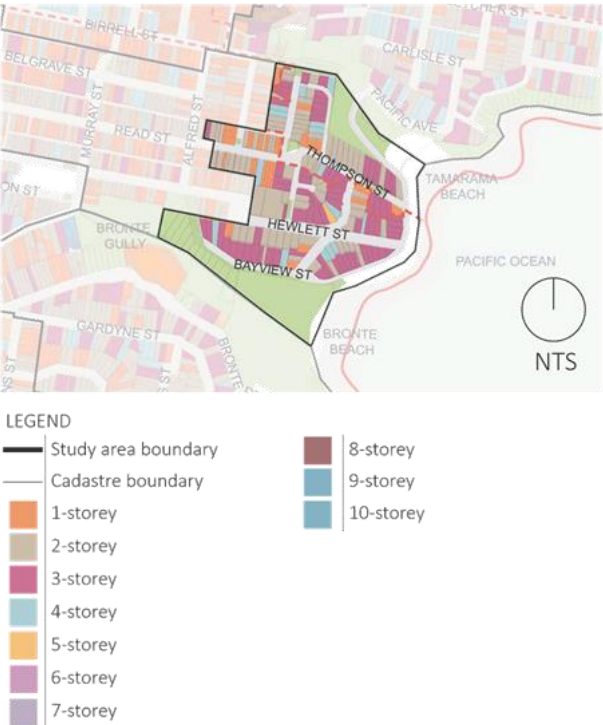


Figure 9-14 - Building height in storeys area #09

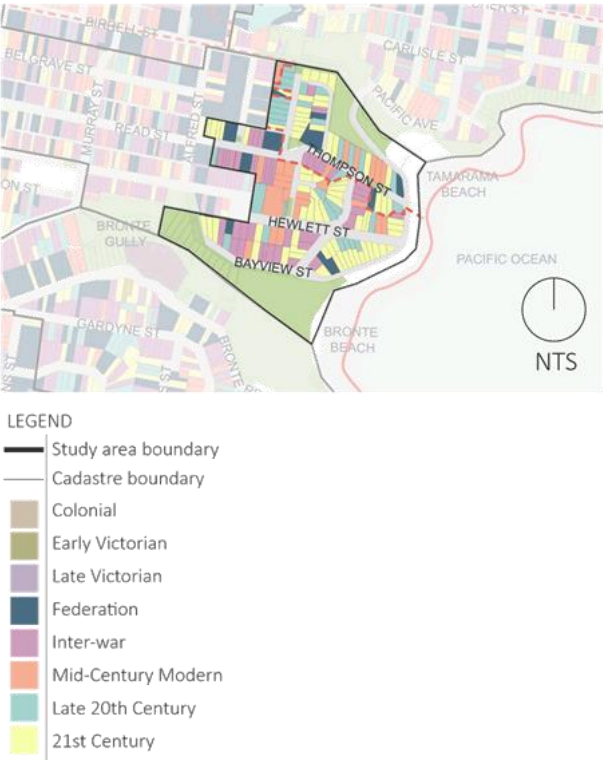


Figure 9-16 - Architectural styles area #09

Natural environment

The headland and adjoining gullies contribute to an important corridor of coastal public open space, having significant aesthetic, landscape and recreational value. Tamarama Beach and Park (see Figure 9-6), Marine Drive, Hewlett Street and Bronte Beach and Park are heritage-listed Landscape Conservation Areas. These areas support a biodiversity corridor. Vegetation coverage is typically low across the area. In the public domain, varying verge widths limit the ability to accommodate consistent planting. In the private domain, landscaping is often limited due to high site coverage and the desire to maintain views. Notwithstanding, the slope between Thompson Street / Wolaroi Crescent and Tamarama Marine Drive (see Figure 9-3), has identifiable landscape character. Dwellings are integrated within the landscape, immersed in vegetation, softening the impact of built form on the coastline. These factors contribute to a low-medium heat vulnerability across the area.

The area is recognised for its long history of seaside bathing and leisure and for its association with the beginnings of the surf lifesaving movement. Bronte Surf Life Saving Club (SLSC) and Tamarama SLSC were formed soon after ocean bathing was legalised in 1902. The iconic beaches, parks and Bondi-Coogee coastal walk offer passive and active recreation opportunities and support public gathering and events.

Recent development

Recent development includes alterations and additions to existing detached and semi-detached dwellings, such as second-storey additions, double garages, carport replacements; demolition of existing dwellings and construction of new 2-3 storey detached or semi-detached dwellings (including dual occupancy), with integrated garages; and limited secondary dwellings including studios above rear garage. This indicates a demand for additional floor space and car-parking within the area. It is also noted that at a suburb level, Tamarama has the highest percentage of dwellings listed on Airbnb in Australia, with one in five dwellings listed, Bronte has the fourth highest, indicating a trend of use of private dwellings as short-term rental accommodation.

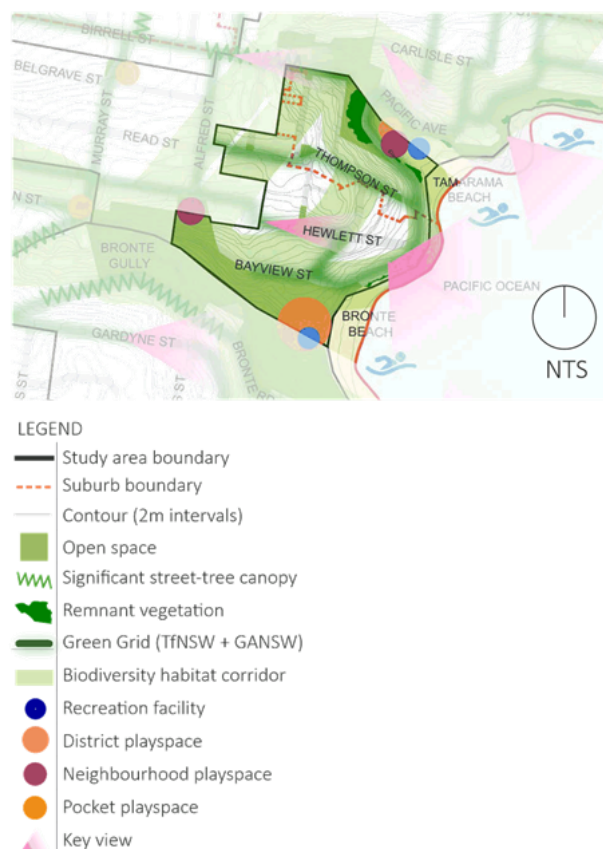


Figure 9-17 - Architectural styles area #09



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

10



CHARACTER AREA – 10

“The area is defined by highly irregular blocks comprising a patchwork of densely set, low-medium rise dwellings of various typologies and styles; streetscapes of varying materiality, limited landscaping and garages; coastline and ocean views; headland parks and gully that contribute to an important corridor of coastal open space”.

The study area is generally bound by Bondi Road and Hunter Park to the north, the Pacific Ocean to the east, Tamarama Gully and Park to the south and Denham Street to Tamarama Street to the west.



Figure 10-1 - Local character area #10 map



Figure 10-4 - Character area viewed from Birrell Street



Figure 10-2 - Dwellings fronting Pacific Avenue



Figure 10-5 - Marks Park, looking east

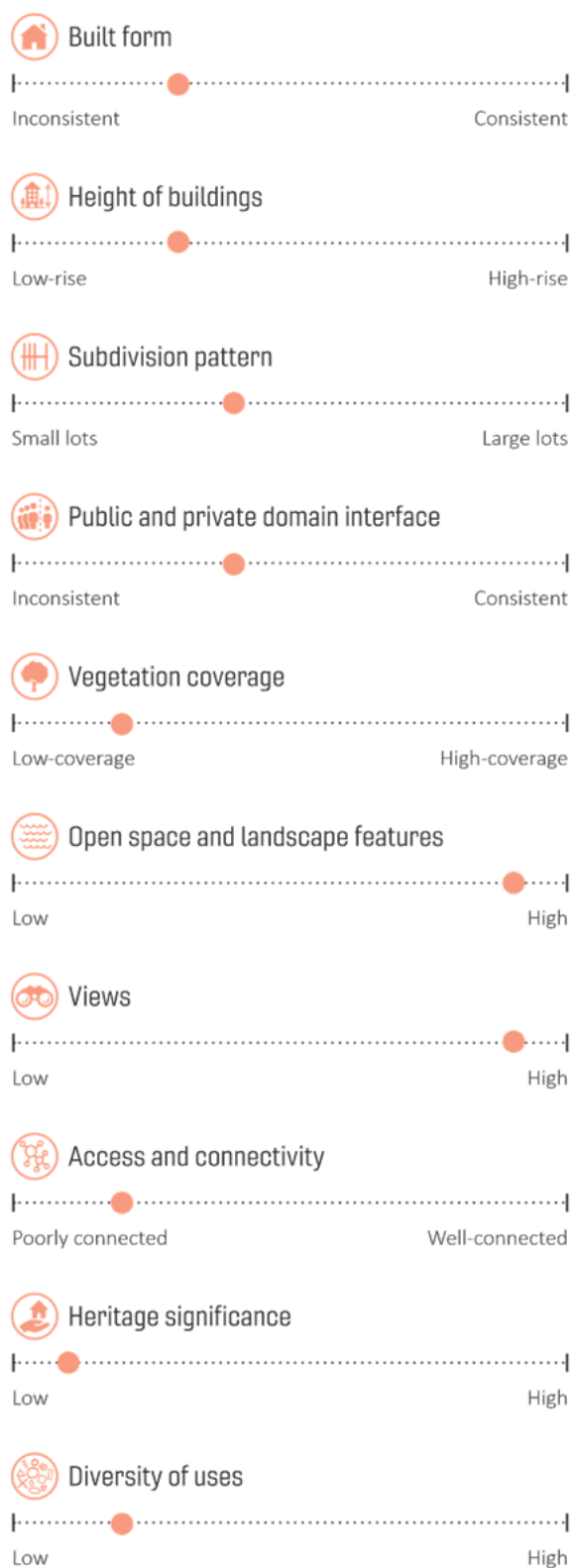


Figure 10-3 - Semi-detached dwellings, Dellview Street



Figure 10-6 - Semi-detached dwellings, Rowland Street

Existing character attributes



Desired future character

Vision

The desired future character of the area celebrates the iconic coastline, with a diverse range of residential development integrated with the natural sloping topography, buffered by vegetation that enhances the natural setting.

Objectives

The objectives for development on land identified within the character area are as follows:

- To encourage a diversity of dwelling styles and typologies, including semi-detached dwellings and low-rise RFBs (where compatible with adjacent built form),
- To ensure high-quality design of new contemporary detached and semi-detached dwellings, with well-articulated form and materiality. Flat roofs are supported for view sharing,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality, particularly for existing semi-detached dwellings,
- To maintain the predominant 2-4 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Integrate carparking within new dwellings,
- To promote the reduction of hard surfaces and an increase of public and private landscaping,
- To maintain views and vistas from the public domain,
- To minimise the impact on existing views and vistas from the private domain and maintain residential amenity in terms of overlooking and noise.

92 Existing character description

History

Development of the area has evolved from an early land holding of 10 acres, covering Tamarama bay, purchased by John Roby Hatfield in 1839. In addition to early housing, the land was used for grazing, dairy herding, market gardens and entertainment / leisure venues until the early 1900s. The Tamarama Aquarium was opened in 1887 and was a destination for dancing, bowling, skating and a shooting gallery. It closed in 1889. Wonderland City, an antipodean Coney Island later opened on the abandoned aquarium site in 1906. It closed in 1911. Popularity of the area for housing grew, driven by access to public transport, with the Sydney tram service reaching the Aquarium in 1887 and Bondi Beach in 1894, as well as the rising popularity of seaside daytrips and bathing. Tamarama Surf Life Saving Club (SLSC) was opened in 1906. Today, the area comprises predominantly residential uses, with supporting retail and commercial uses along Bondi Road and within the Fletcher Street village centre.

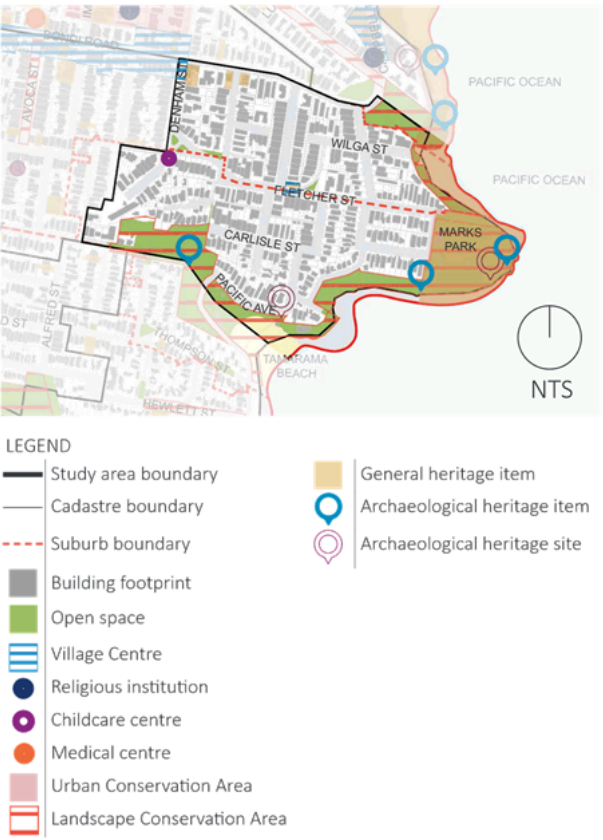


Figure 10-7 - Built form and uses area #10

Configuration and connectivity

Progressive subdivision and development of the area, combined with topographical constraints, tramline alignment, environmental damage from the former Aquarium and Wonderland City and neglect of Tamarama Gully, has resulted in a highly irregular block and street configuration. Lots are predominantly aligned east-west or north-south. The street hierarchy is unclear and hard to define, made up of multiple connected street segments, which impede clear navigation and circulation. The pattern of curved streets reflects the tramline alignment along Fletcher Street, responding to the tram's inability to take tight corners, as well as the poor road formation through the gully and coastline. The primary east-west connection through the area is Fletcher Street, with other east-west streets limited to no-through roads and laneways. Multiple north-south streets link Bondi Road and Pacific Avenue, which provide the primary access to and from the area. Multiple bus routes service the area providing connections to surrounding suburbs, Bondi Junction and Sydney CBD. Pedestrian connectivity is maintained along the street network, coastline and through-block links, for example Silva Street to Pacific Avenue.

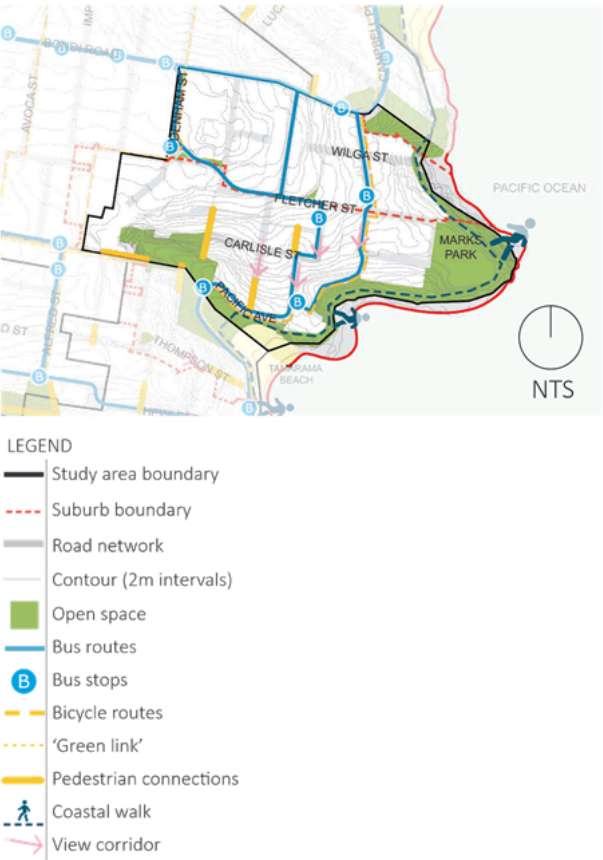


Figure 10-8 - Road network and movement area #10

Built form

The area is characterised by patchwork of dwelling typologies and styles, from detached and semi-detached Federation and Inter-War style bungalows, Inter-War and Mid-century Modern style RFBs, to late 20th century and 21st style detached, semi-detached dwellings and residential flat buildings (RFBs). This variety is reflective of the area's progressive development. Notable clusters of Federation and Inter-War style bungalows are located on Dudley Street, Sandridge Street and Rowland Avenue. Bungalows of these styles are characterised by brick construction, low gabled terracotta tiled roofs, with thick masonry or timber verandah posts and decorated timber gable trim. Notable examples of Inter-War style RFBs are located on Fletcher Street and Wonderland Avenue. Modifications to early dwellings are common including second-storey, garage and balcony additions. Mid-century RFBs are interspersed throughout the area. RFBs of this style are typically red or blonde brick, raised above ground-level parking. Contemporary detached, semi-detached and RFB development is interspersed throughout the area, but is particularly evident on sites with views to the coast or gully, for example Kenneth Street. Modern dwellings are typically flat-roofed cascading buildings with wider modulation, wide integrated garages, large balconies and areas of transparent glass for doors, windows and balcony balustrades. These dwellings seek to maximise floor space, height and capture available views. The population density of the area is approximately 81 persons/ha. The dominant tenure type is private rental.



Figure 10-9 - RFB, Dellview Street

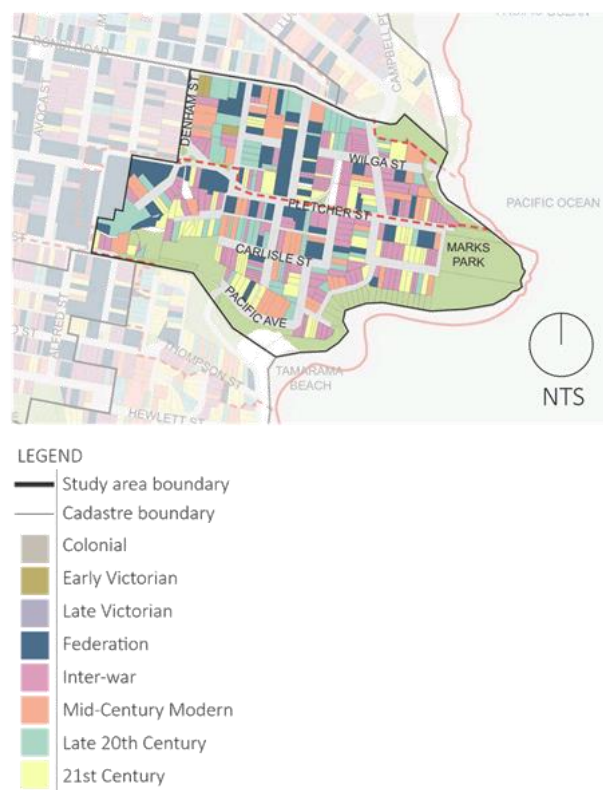


Figure 10-10 - Architectural styles area #10

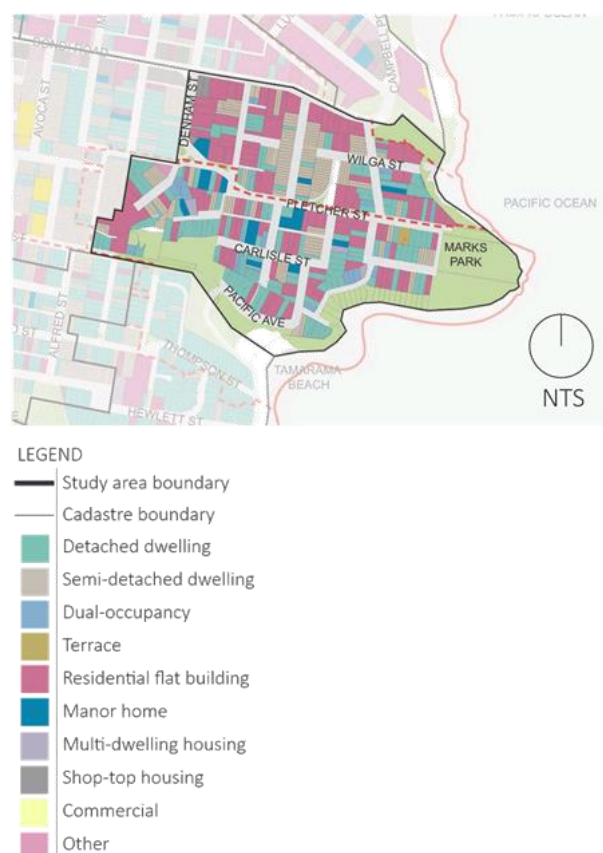


Figure 10-11 - Dwelling typologies area #10

94 Existing character description cont.

Dwellings are predominantly 2-4 storeys in height. Sloping topography has resulted in the modulation of built form, with the dwelling base (retaining wall, fence or garage) at street level and floors stacked above on upper sites or behind and below for lower sites. The height of the building can appear increased due to the raised topography above street level. Dwellings of 5+ storeys are evident within the area, including the 8-storey tower on Illawong Avenue, but are inconsistent with the prevailing height character.

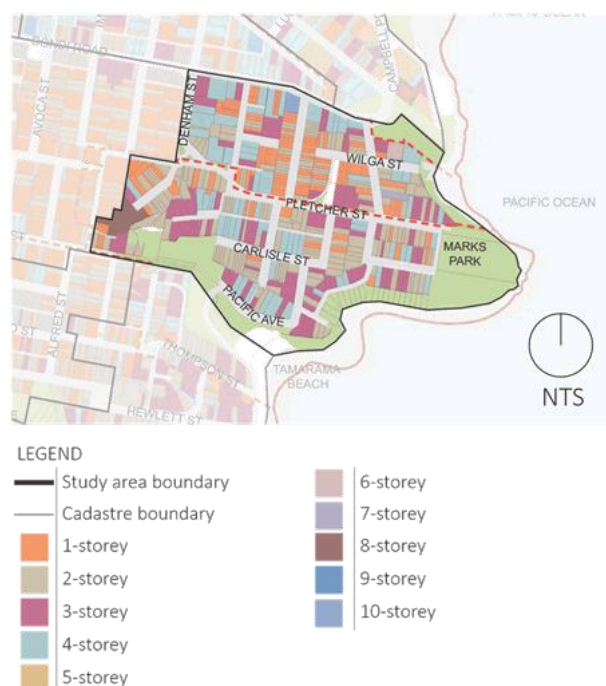


Figure 10-12 - Building height in storeys area #10

Public and private domain interface

The interface between the public and private domains differs due to street configuration, topographical changes, and dwelling typology. Inconsistent street corridor, verge and footpath widths vary the physical and visual transition between the domains, for example Sandridge Street versus Carlisle Street. The varying capacity of the verge to accommodate trees also influences the visual transition, altering the level of visibility between the street and dwelling. Laneways provide rear access and off-street parking, typically with high fence lines and enclosed garages at the boundary. Topographical changes result in a lower and upper side of the street. Dwellings on the upper sites are raised above the public domain, with built retaining walls, high fences and

/ or garage doors of varying materials and finishes, fronting the street. A solid wall presenting to the street is common, for example Kenneth Street. Front landscaping also varies, with deep soil often limited due to garage construction, for example Pacific Avenue. Upper balconies typically overlook the public domain. Dwellings on the lower sites often have their ground level partially or completely obscured from the street as a result of the sloping topography, vegetation, fencing and / or garages at the boundary, for example Carlisle Street (see Figure 10-14). Again, materiality and finishes vary, contributing to the area's inconsistent character. Front setbacks are generally consistent, however are difficult to read due to encroachment by parking structures. Inter-War RFBs also typically comprise narrow setbacks and limited landscaping, with a solid brick façade (facades vary), small window openings and entry presenting to the street (see Figure 10-9). Mid-late 20th century RFBs are raised above street level incorporating ground-floor carparking. Upper balconies overlook the public domain. Viewed from the coastline the area presents as a dense wall of layered built form (see Figure 10-4).



Figure 10-13 - Dwellings fronting Pacific Avenue



Figure 10-14 - Partially obscured dwellings, Carlisle Street

Natural environment

The headland parks and adjoining gully contribute to an important corridor of coastal public open space, having significant aesthetic, landscape and recreational value. Heritage-listed for their significance, the areas comprise European and Indigenous archaeological sites, remnant vegetation and a striking natural landscape that has drawn the attention of many artists. Vegetation coverage is predominantly low across the area, due to sparse street trees and high site coverage. Coverage improves along the coastline, with the area supporting a biodiversity corridor. These factors contribute to a generally low heat vulnerability. The area’s elevated coastal location offers views to the ocean and surrounding areas from Fletcher Street, the headland parks and private domain. The iconic beaches, parks and Bondi-Coogee coastal walk (see Figure 10-16) offer passive and active recreation opportunities and support public gathering and events.



Figure 10-15 - Open space and vegetation area #10



Figure 10-16 - Coastline and coastal walk, looking north-east

Recent development

Recent development comprises alterations and additions to existing detached and semi-detached dwellings and RFBs, including second-storey additions, attic extensions, garage extensions; construction of new detached and semi-detached dwellings; and limited consolidation of semi-detached and apartment dwellings. This indicates a demand for more floor space. It is also noted that at a suburb level, Tamarama has the highest percentage of dwellings listed on Airbnb in Australia, with one in five dwellings listed, indicating a trend of use of private dwellings for short-term rental accommodation.

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

11



CHARACTER AREA - 11

"The area is defined by an irregular grid pattern of long north-south blocks with limited east-west connections; closely set, low-rise built form on internal streets transitioning to medium-rise on peripheral streets, inconsistent streetscapes of varying styles, materials and vegetation coverage, including tree-lined avenues and open space".

The study area is generally bound by Bondi Road to the north, Denham Street to Tamarama Street to the east, Birrell Street to the south and St Marys Avenue to Paul Street to the west.



Figure 11-1 - Local character area #11 map



Figure 11-4 - Inter-War RFB, Bennett Street



Figure 11-2 - Terrace shopfronts, Bondi Road



Figure 11-5 - Semi-detached dwellings, Park Parade

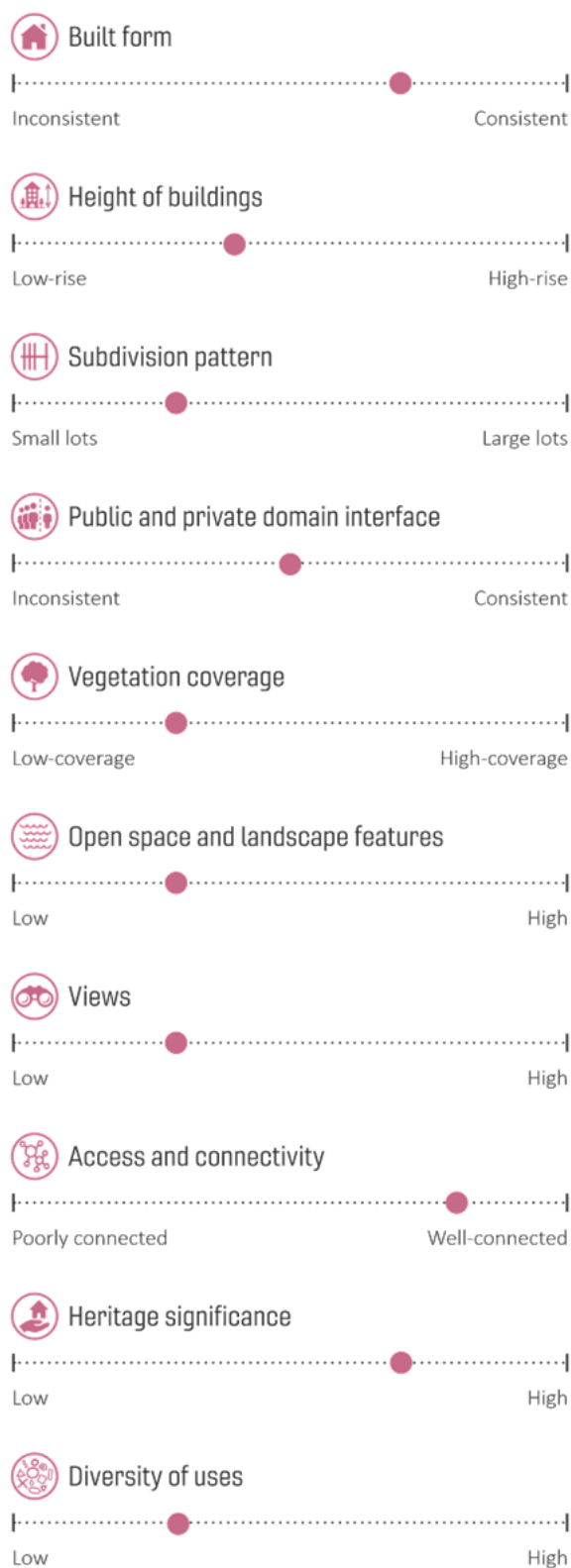


Figure 11-3 - Semi-detached dwellings, Avoca Street



Figure 11-6 - RFBs, Birrell Street

Existing character attributes



Desired future character

Vision

The desired future character is a densely-set residential area within easy walking distance of local shops and services, open space and the iconic coastline. The residential area is defined by a regular grid and street frontage pattern with a diversity of built form and canopy.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area, specifically along Bondi Road,
- To recognise and promote Bondi Road as a gateway to Bondi Beach,
- To celebrate the heritage and maintain the fine-grain nature of the historic shopfronts,
- To effectively manage the retail / commercial and residential interface,
- To reinforce the consistent groupings of Federation and Inter-War style dwellings, including pitched roofscape and front setbacks,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality, particularly for existing terraces and semi-detached dwellings,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To promote new detached, semi-detached dwellings and RFBs that complement the existing built form in terms of height, setbacks and predominant horizontal and vertical proportions,
- To maintain the 1-2 storey height character of internal streets,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Locate carparking structures behind the building line,
- To retain and increase tree canopy in the public and private domains,
- To reduce pedestrian, cyclist and vehicle conflicts, and to improve pedestrian and cyclist amenity on Bondi Road.

100 Existing character description

History

Development of the area has evolved from the progressive subdivision of a number of large gentlemen’s estates, including the Dickson, Avoca, Braylesford and Boonara estates originally constructed in the mid-late 19th century. In addition to early housing, the land was used for grazing, dairy herding and market gardens. Waverley Park was established in 1880. The linear north-south pattern of the estates and initial streets established along their boundaries are reflected in the configuration of the area today. Increased development progressively extended along Bondi Road and into the area following subdivision of the estates from the late 19th century. Housing development was driven by access to public transport with the Sydney tram service reaching Tamarama in 1887 and Bondi Beach in 1894. Bondi Road became a major transport route, with businesses servicing the growing local population and tourist trade. Today, the area comprises predominantly residential uses with supporting commercial and retail uses within the Bondi Road local centre (see Figure 11-2).

Configuration and connectivity

Blocks are arranged in an irregular grid pattern across the area’s topography, which slopes gradually to the east. Internal streets are predominantly aligned north-south. To the west of Watson Street, parallel through-streets (Park Parade, Bennett, Ocean and Watson streets) link Bondi Road and Birrell Street which provide the primary access to and from the area. To the east of Watson Street, north-south connectivity is less defined. Permeability of the area is reduced due to long north-south blocks and limited east-west connectivity resulting from the prevalence of no-through roads and laneways. Multiple bus services connect the area to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD.

Built form

The area is characterised by predominantly 1-2 storey terraces, detached and semi-detached dwellings along internal streets, with shop-top housing fronting Bondi Road and 3-4 storey residential flat buildings (RFBs) fronting Bennett and Birrell streets (see Figure 11-6).

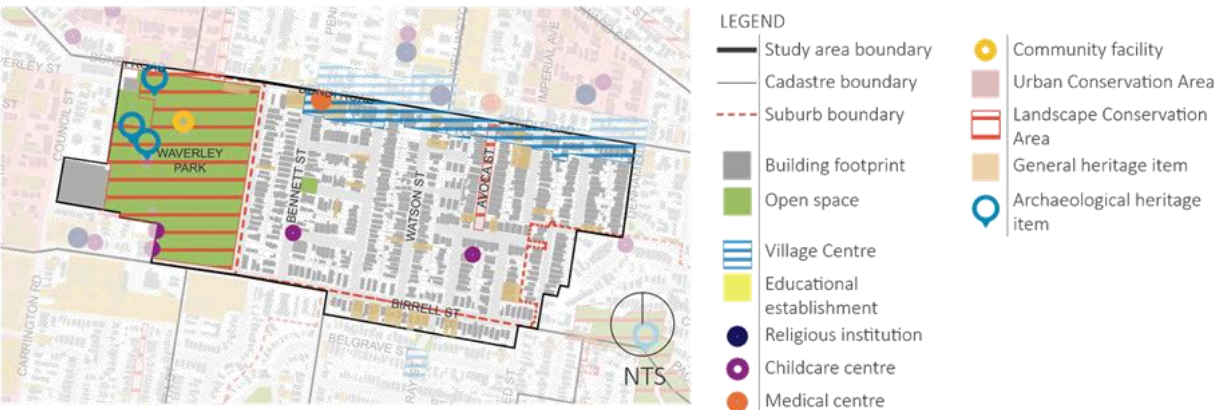


Figure 11-7 - Built form, uses and heritage area #11

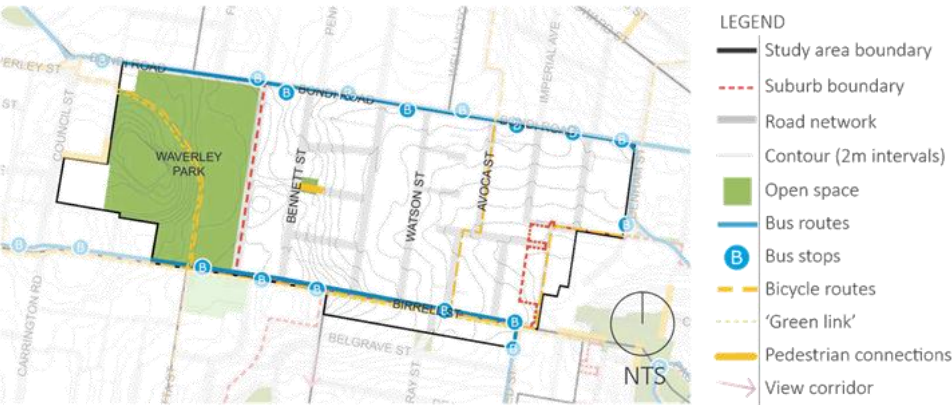


Figure 11-8 - Road network and movement area #11

The area is characterised by a variety of architectural styles, the dominant style being Federation which reflects the first wave of significant development of the former estates. Consistent streetscapes of this style are evident on Avoca, Phillip, Tamarama, King and Ewell streets. Federation-style shopfronts are a feature on Bondi Road. The Inter-War style reflects the second wave of development. Clusters of Inter-War detached, and semi-detached dwellings are located on Imperial and Boonara avenues and Denham, Avoca and Ocean streets. These styles are characterised by brick construction, low gabled terracotta tiled roofs, with thick masonry or timber verandah posts and decorated timber gable trim. Inter-War RFBs are located on Bennett Street. Alterations including materiality changes and second-storey and parking additions, are common. Later development, including Mid-century Modern style RFBs eroded the earlier setting, capitalising on views to the ocean and availability of land around earlier dwellings or through property amalgamation. RFBs of this style are typically red or blonde brick. Infill development of late 20th century and 21st style development has continued sporadically throughout the area. The population density of the area is approximately 92 persons/ha.



Figure 11-11 - Semi-detached dwellings, Coulton Street



Figure 11-12 - RFB, Watson Street



Figure 11-9 - Dwelling typologies area #11

- LEGEND
- Study area boundary
 - Cadastre boundary
 - Detached dwelling
 - Semi-detached dwelling
 - Dual-occupancy
 - Terrace
 - Residential flat building
 - Manor home
 - Multi-dwelling housing
 - Shop-top housing
 - Commercial
 - Other



Figure 11-10 - Architectural styles area #11

- LEGEND
- Study area boundary
 - Cadastre boundary
 - Colonial
 - Early Victorian
 - Late Victorian
 - Federation
 - Inter-war
 - Mid-Century Modern
 - Late 20th Century
 - 21st Century

102 Existing character description cont.

Public and private domain interface

The interface between the public and private domain varies across the area as a result of different street and dwelling typologies. Differing widths of the street corridor, verge and footpath varies the physical and visual transition between the domains, for example Tamarama Street compared with Imperial Avenue. The narrow, corridor-like rear lanes are dominated by garage doors, high fences and walls and landscape screening. Shallow front setbacks, low fencing and limited front landscaping result in high visibility of the dwelling from the public domain (see Figure 11-16). The later addition of car-parking within the front setback, either hard-stand, carport or enclosed garage at the boundary, detracts from the streetscape and the dwelling form. A number of corner sites present blank side walls or fences to the public domain. Inter-War RFBs also typically comprise narrow setbacks and limited landscaping, with a solid brick façade (facades vary), small window openings and entry presenting to the street (see Figure 11-17). Mid-late 20th century RFBs are raised above street level incorporating ground-floor carparking (see Figure 11-13). Upper balconies overlook the public domain. Through the Bondi Road centre, fine-grain shopfronts abut the public domain, creating a highly visible, active frontage.



Figure 11-13 - RFBs, Bennett Street



Figure 11-15 - Semi-detached dwellings, Watson Street



Figure 11-16 - Detached dwellings, Boonara Avenue



Figure 11-17 - RFB, Imperial Avenue



Figure 11-14 - Semi-detached dwellings, Avoca Street

Natural environment

The area benefits from access to Waverley Park, Stephen Street Reserve as well as the coastal open space network to the east. 'Green links' aid connectivity. Waverley Park is a major recreational facility which supports a number of sporting and community groups. The area's sloping topography provides views out from Waverley Park (see Figure 11-19) and Bondi Road towards the coast. Internal views are limited. Vegetation coverage is predominantly low across the area, as a result of closely set development which limits the opportunity for significant private open space in the front or rear yards as well as the varying capacity of the street verge to accommodate planting. Varying on-street planting contributes to a sense of openness or enclosure, for example King Street compared to Tamarama Street. Mature fig trees line King, Stephen, Ewell and Avoca streets. The area has a medium-high heat vulnerability.

Recent development

Recent development comprises alterations and additions to existing dwellings, including second-storey additions, attic conversions, carport / garages; limited construction of secondary dwellings, such as loft / studio over rear garages; and limited construction of new infill detached and semi-detached dwellings. Indicative of a trend toward maintaining existing built form whilst achieving more floor space and on-site car-parking.

103

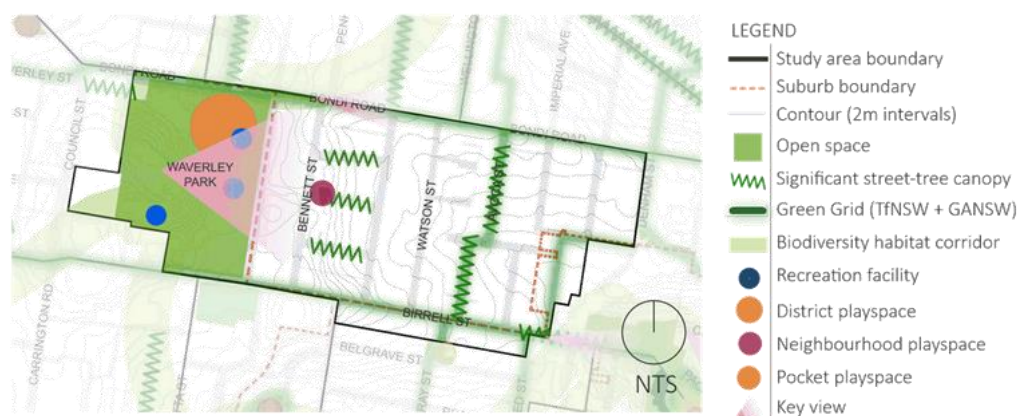


Figure 11-18 - Open space and vegetation area #11

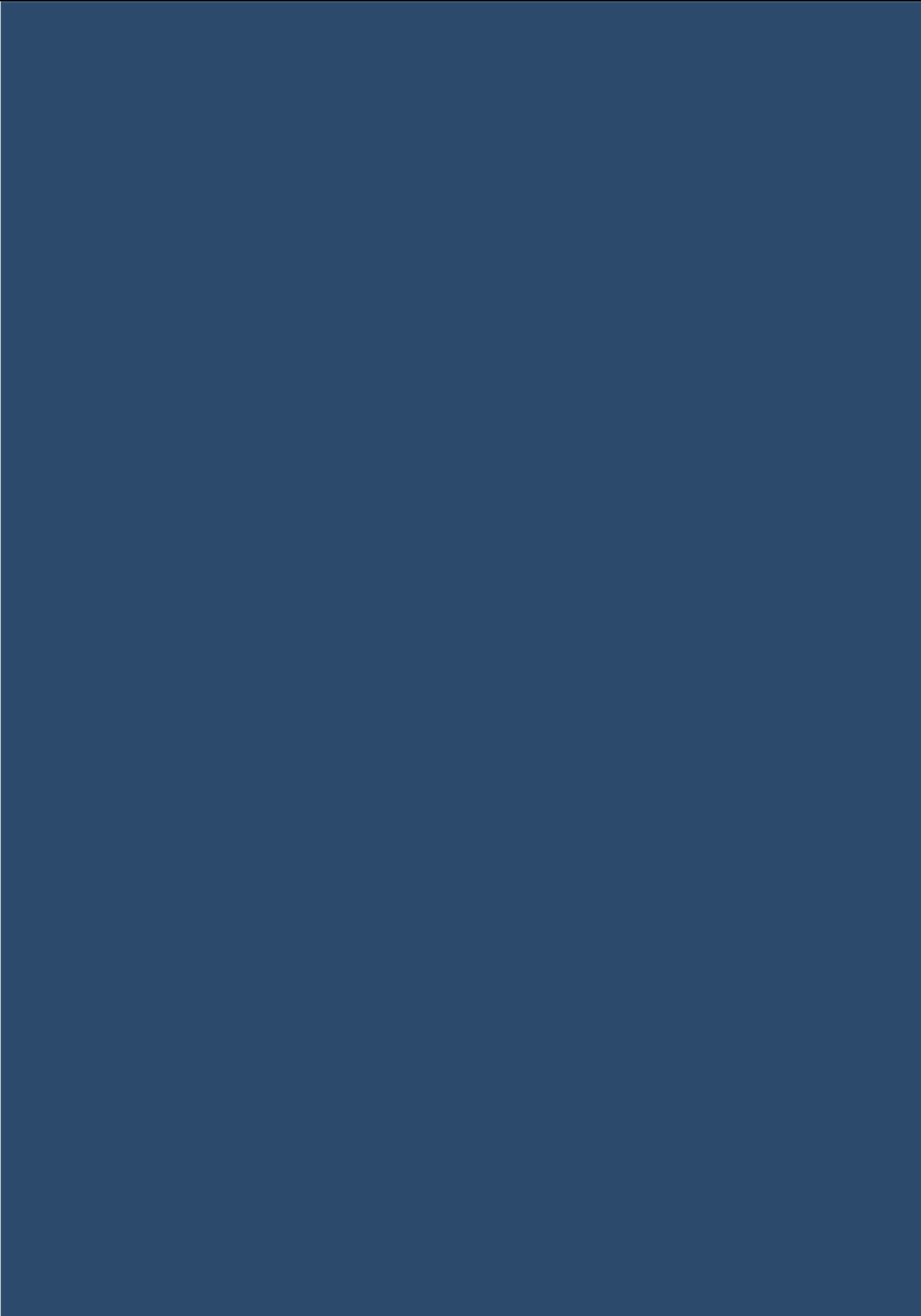


Figure 11-19 - Waverley Oval, looking east



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

12



CHARACTER AREA - 12

"The area is characterised by established streetscapes of closely set, low-rise terraces, detached and semi-detached dwellings residential flat buildings of predominantly Late Victorian, Federation and Inter-War styles, narrow rear laneways; and narrow-street verges, limited drive-ways and irregularly spaced street-trees".

The study area is generally bound by Old South Head Road (OSHR) to the north, Flood Lane to the east, and Bondi Road to the south and west.

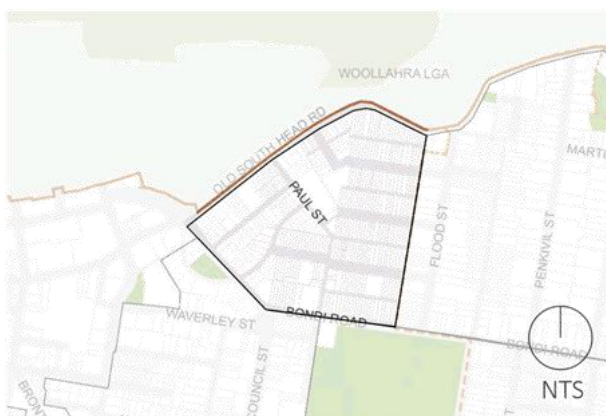


Figure 12-1 - Local character area #12 map



Figure 12-4 - Street corridor, Kenilworth Street



Figure 12-2 - Semi-detached dwellings, Woodstock Street



Figure 12-5 - Old South Head Road frontage, looking west

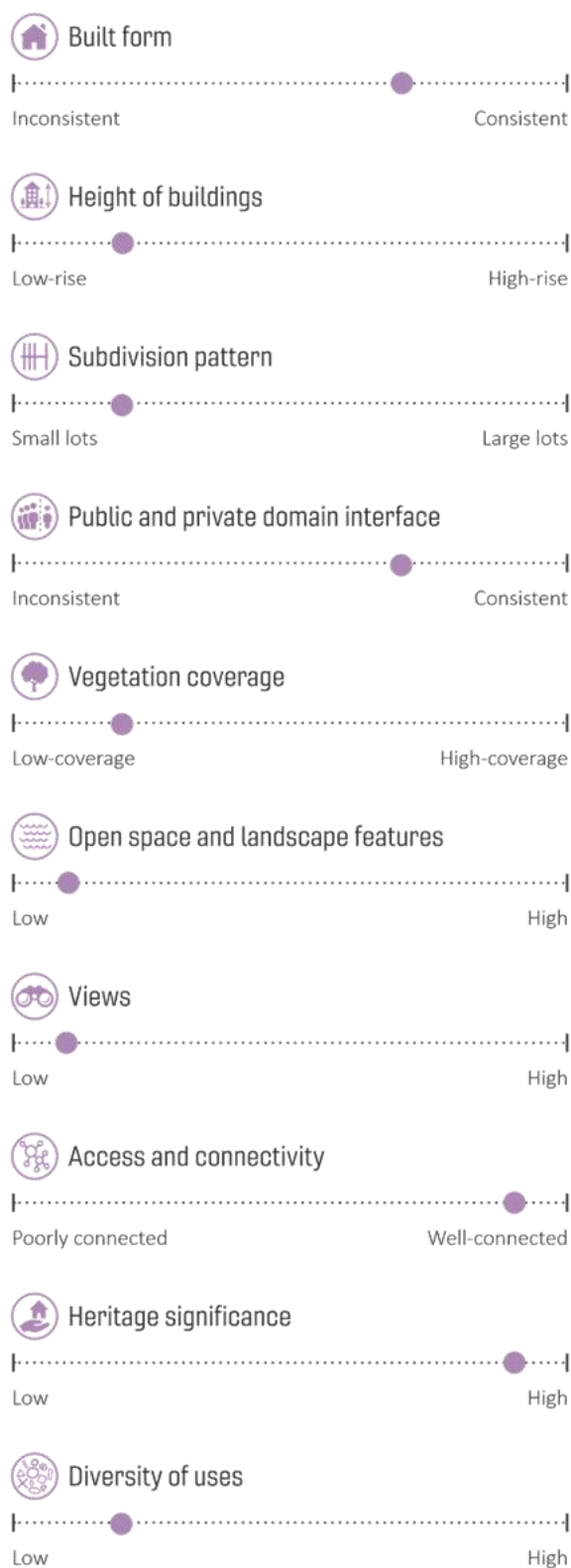


Figure 12-3 - Terrace dwellings, Woodstock Street



Figure 12-6 - Waverley Crescent, looking west

Existing character attributes



Desired future character

Vision

The desired future character is an historic and consistent residential neighbourhood within easy walking distance of local shops and services, and open space.

Objectives

The objectives for development on land identified within the character area are as follows:

- To preserve the heritage items and the integrity of the Urban Conservation Area,
- To maintain the predominant low-rise character of the area,
- To maintain the historically distinctive pattern of subdivision and associated terrace, semi-detached and detached dwelling typologies,
- To discourage demolition and promote sympathetic additions that retain the scale and massing of front elevations and the original roof form as viewed from the primary street frontage,
- To ensure new development respects the historic patterns, original built form, architectural styles, materials and details of the area,
- To promote the high-quality design of new contemporary dwellings to the north-west of the area, fronting OSHR and Bondi Road, and recognising this area as a key gateway to the Waverley LGA,
- To ensure secondary dwellings and ancillary development enhance rear laneways and maintain a high standard of residential amenity for surrounding dwellings,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To limit new driveway crossovers and car parking within the front setback, by retaining vehicle access from rear lanes,
- To retain and increase tree canopy in the public and private domains.

108 Existing character description

History

The area has evolved from the subdivision and development of early land holdings along OSHR. The primary land holding being Barnett Levey’s 60 acres granted in 1826. Levey was the third grantee of land in the Waverley municipality. Levey’s Waverley Crescent Subdivision was advertised on 30 January 1828 in The Australian, comprising 68 allotments. One of Waverley’s earliest villas, ‘Ben Eden’ constructed in 1863, remains. The subdivision of the Levey Estate to the west of Paul Street, and John B Jones’ and Edward Flood’s Estates to the east, established the current street pattern and narrow allotments. Improvements in transport and the rapid expansion of suburban Sydney between 1880-1915 saw the consolidation of Bondi Junction and further development of the established Victorian streetscapes for housing. The area remains predominantly residential, with supporting clusters of religious and educational institutions, including the Central Synagogue, and commercial and retail premises along Bondi Road and within the Flood Street local centre on OSHR.

Configuration and connectivity

Blocks are arranged in an irregular grid pattern across the area’s relatively flat topography. To the east of Paul Street, parallel east-west streets and rear laneways establish consistent blocks from Bondi Road. The cranked alignment of Paul Street changes the block configuration to the west, with streets running parallel to OSHR. Waverley Crescent (see Figure 12-6) and Bon Accord Avenue are curvilinear in nature. North-south street connectivity is limited to Flood Lane and Paul Street. Internal streets are bound by primary arterial routes OSHR (see Figure 12-5) and Bondi Road, which provide the primary access to and from the area. The area benefits from its close proximity to Bondi Junction and Bronte Road, where heavy rail and bus services provide access to the coast, Sydney CBD and surrounding areas. The area also benefits from the commercial, retail and social services offered at Bondi Junction.



Figure 12-7 - Built form, uses and heritage area #12

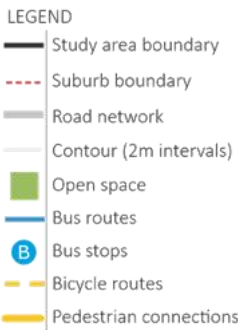
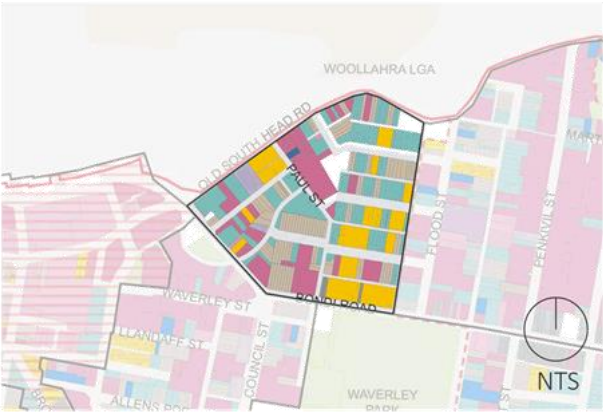


Figure 12-8 - Road network and movement area #12

Built form

The area is predominantly characterised by 1-2 storey Late Victorian and Federation-style terraces, detached and semi-detached dwellings, reflective of the area being the first attempt at a consolidated residential setting within Waverley and a rare example of Georgian town planning in Sydney’s east. Features of these styles include masonry, moulded render, decorative tile and iron detailing. Exemplar, heritage-listed dwellings are located on Ben Eden Street and Woodstock Street (see Figure 12-3). Later Inter-War style semi-detached dwellings as located on Gowrie Avenue (see Figure 12-10). These dwellings are characterised by low gabled roofs, brick construction, and arched verandah openings. Inter-War residential flat buildings (RFBs) of 3-4 storeys front OSHR. Consistent bulk, scale and style creates coherent streetscapes. The area forms the heritage-listed Woodstock Conservation Area. Intrusive construction of Mid-century and late 20th century-style RFBs of 9+ storeys are inconsistent with the prevailing character of the area, for example fronting Bondi Road, Gowrie Avenue and Woodstock and Kenilworth streets. Contemporary infill emulates the bulk, scale and detailing of the earlier styles. The population density of the area is approximately 100 persons/ha.



- LEGEND
- Study area boundary
 - Cadastre boundary
 - Detached dwelling
 - Semi-detached dwelling
 - Dual-occupancy
 - Terrace
 - Residential flat building
 - Manor home
 - Multi-dwelling housing
 - Shop-top housing
 - Commercial
 - Other

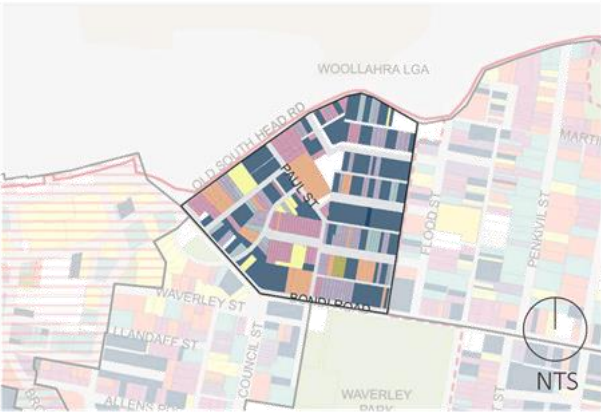
Figure 12-9 - Dwelling typologies area #12



Figure 12-10 - Semi-detached dwellings, Gowrie Street



Figure 12-11 - RFB, Paul Street



- LEGEND
- Study area boundary
 - Cadastre boundary
 - Colonial
 - Early Victorian
 - Late Victorian
 - Federation
 - Inter-war
 - Mid-Century Modern
 - Late 20th Century
 - 21st Century

Figure 12-12 - Architectural styles area #12

110 Existing character description cont.

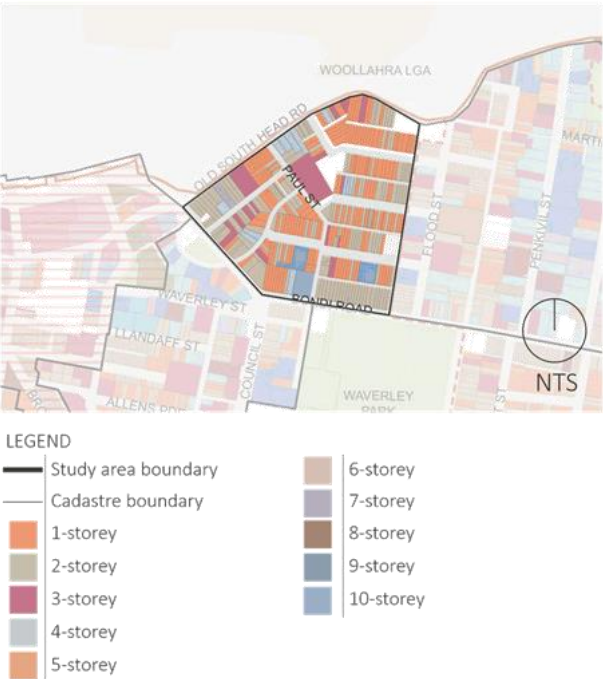


Figure 12-13 - Building height in storeys area #12



Figure 12-14 - Terrace dwelling, Kenilworth Street



Figure 12-15 - Garage frontages, Gowrie Avenue

Public and private domain interface

The interface between the public and private domain varies across the area. East of Paul Street, street corridors are typically wide, contributing to a sense of openness. To the west of Paul Street, street corridors narrow, creating a sense of enclosure. Predominantly narrow street verge and footpath widths combined with shallow front setbacks result in a short physical and visual transition between the public and private domains, for example Bon Accord Avenue compared to Waverley Crescent (see Figure 12-17 and Figure 12-18). Terraces, detached, and semi-detached dwellings are typically closely set, with low front fences and limited landscaping. Terraces typically have upper-level balconies which overlook the public domain. Inter-War RFBs also typically comprise nil-narrow setbacks and limited landscaping, with a solid brick façade (facades vary), small window openings and entry presenting to the street, for example Paul Street. Mid-late 20th century RFBs comprise greater setbacks and are typically raised above street level. Upper balconies overlook the public domain (see Figure 12-19). The majority of dwellings within the area are serviced by off-street parking due to the prevalence of rear lanes, with later additions of hard-stand, carport or garage parking. Lanes are dominated by garage doors, high fences and walls and landscape screening. Rear parking results in a consistent streetscape not interrupted by driveway crossovers. Whilst not common, later additions of car-parking (carports or garages) encroach on the front setback.



Figure 12-16 - RFB, Bondi Road



Figure 12-17 - Waverley Crescent, looking west



Figure 12-18 - Terrace dwellings, Bon Accord Avenue



Figure 12-19 - RFB, Woodstock Street

Recent development

Recent development comprises alterations and additions to existing dwellings, including second-storey additions, carports / garages, attic conversions; construction of secondary dwellings, such as loft / studio over rear garages; limited construction of new infill detached, semi-detached dwellings and RFBs, particularly on OSHR; and boarding house uses. Indicating a trend toward additional floor space and maximising site coverage whilst respecting the existing character of the Woodstock Conservation Area.

Natural environment

The area is devoid of open space, however benefits from close proximity to Fingleton Reserve, Waverley Park and Cooper Park (Woollahra). 'Green links' aid connectivity. Vegetation across the area is predominantly low, particularly on the area's boundaries along OSHR and Bondi Road. This is due to the varying capacity of the street verge to accommodate planting. Densely built form also inhibits significant planting within the private domain. These factors contribute to a medium-high heat vulnerability.

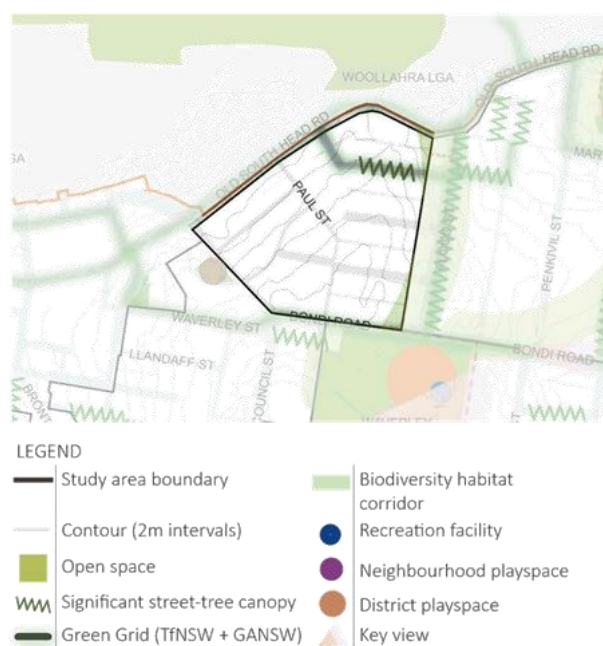


Figure 12-20 - Open space and vegetation area #12



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

13



CHARACTER AREA – 13

“The area is defined by long north-south blocks comprising predominantly medium-high rise residential flat buildings of various styles, situated on large, irregular lots, amongst a mature tree canopy; limited street presence with front setbacks of car-parking, paved areas and limited landscaping; district views to and from the area”.

The study area is generally bound by Old South Head Road (OSHR) and Francis Street to the north, Wellington Street to the west, Bondi Road to the south and Flood Lane to the east.



Figure 13-1 - Local character area #13 map



Figure 13-4 - RFBs, Penkivil Street



Figure 13-2 - Street corridor, Penkivil Street



Figure 13-5 - RFBs, Ocean Street North

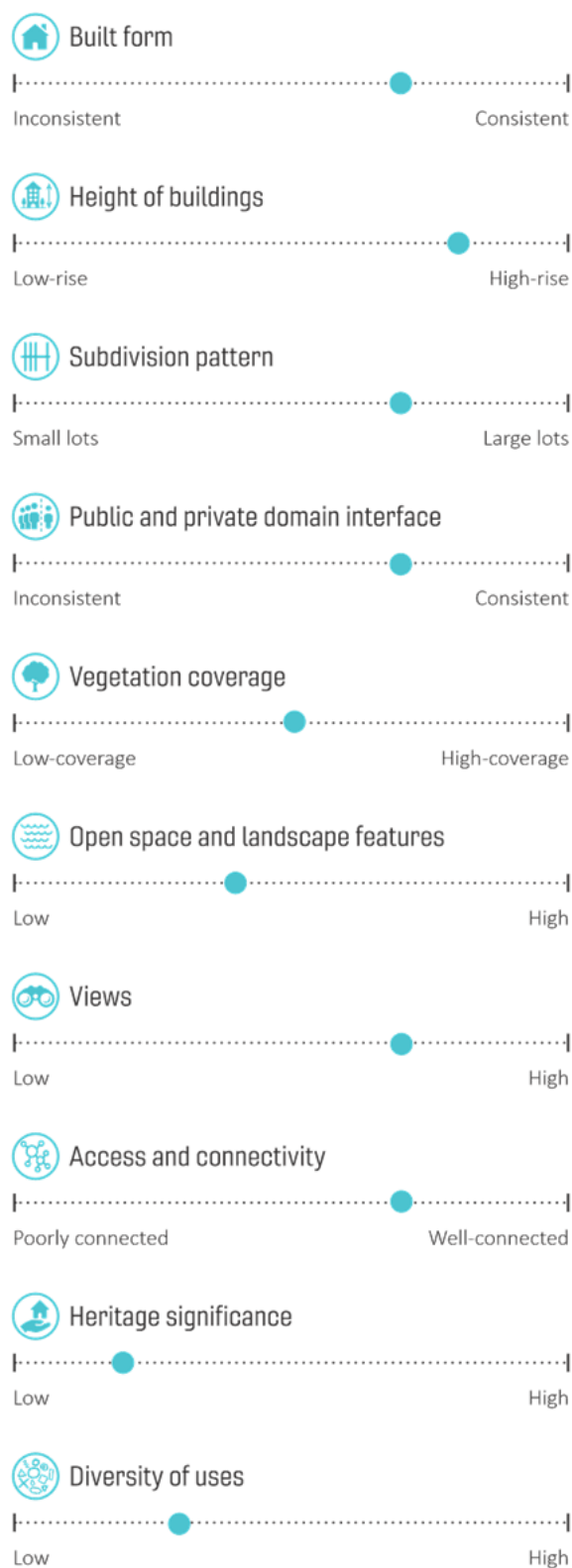


Figure 13-3 - RFBs, Penkivil Street



Figure 13-6 - Terrace dwellings, Watkins Street

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by high-density residential area within walking distance to local shops and services, and open space and the coastline. Development is well integrated into the streetscape through design excellence and landscape design including mature tree canopy.

Objectives

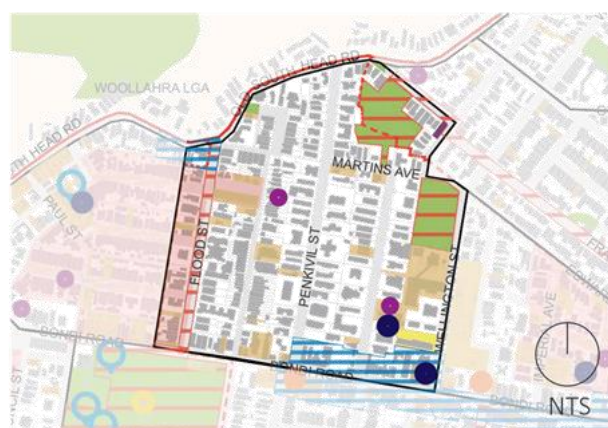
The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area, particularly along Bondi Road,
- To celebrate the heritage and maintain the fine-grain nature of the historic shopfronts,
- To effectively manage the retail / commercial and residential interface,
- To preserve the heritage items and the integrity of the Urban Conservation Area,
- To discourage demolition of earlier Late-Victorian, Federation and Inter-War-style dwellings, and promote sympathetic additions that retain the scale and massing of front elevations and the original roof form as viewed from the primary street frontage,
- To encourage high-quality contemporary medium-high rise RFB development within the area. Consider site amalgamation where appropriate,
- To maintain and enhance the significant landscape features of the area, including the large street tree canopy,
- To maintain the significant landscape buffer at the rear of properties abutting Thomas Hogan Reserve and Dickson Park,
- To promote the reduction of hard surfaces, particularly within the front setback, to increase landscaping,
- To promote through-block connections, where appropriate,
- To minimise the impact on existing views and vistas from the private domain and maintain residential amenity in terms of overlooking and noise.
- To reduce pedestrian, cyclist and vehicle conflicts, and to improve pedestrian and cyclist amenity on Bondi Road.

Existing character description

History

Development of the area has evolved from the progressive subdivision of a number of early land holdings and Victorian estates between OSHR and Bondi Road, including 'Anglesea Estate', from the mid-1800s. The linear north-south pattern of the holdings and initial streets, including Flood, Anglesea and Penkivil streets, established along their boundaries are reflected in the configuration of the area today. Up until the early 1890s the area comprised scattered free-standing houses and assorted buildings. One of the late-Victorian villas 'Scarba', constructed in 1884, remains at 30 Wellington Street. From the early 1900s, improvements in tram transport along Bondi Road and the consolidation of Bondi Junction saw further subdivision and housing construction through the area. Today, the area comprises predominantly residential uses with supporting commercial and retail uses along Bondi Road local centre, and educational and religious establishments.



LEGEND

- | | |
|-----------------------------|-----------------------------|
| — Study area boundary | Urban Conservation Area |
| — Cadastre boundary | Landscape Conservation Area |
| - - - Suburb boundary | General heritage item |
| ■ Building footprint | |
| ■ Open space | |
| ■ Village Centre | |
| ■ Educational establishment | |
| ● Religious institution | |
| ● Childcare centre | |
| ● Medical centre | |
| ● Community facility | |

Figure 13-7 - Built form, uses and heritage area #13

Configuration and connectivity

Long north-south blocks extend across the area's relatively flat topography. Lot sizes vary. A well-defined street network comprises primary through streets (Flood, Penkivil, Wellington streets) and secondary streets (Anglesea and Ocean streets), extending north-south, with limited east-west street connections, which results in reduced permeability. Penkivil Lane maintains pedestrian connectivity between Orr Street and Martins Avenue. Streets are predominantly 2-way with on-street parking. OSHR and Bondi Road provide the primary access to and from the area. Multiple bus services along OSHR and Bondi Road connect the area to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD. The area also benefits from its proximity to the commercial, retail and social services offered at Bondi Junction.

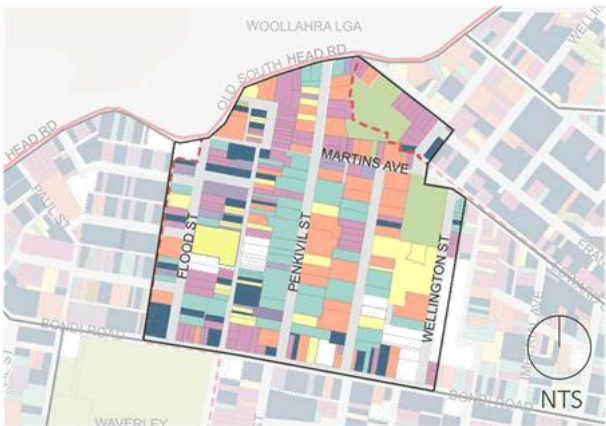
Built form

The area is characterised predominantly by residential flat buildings (RFBs), of various styles, from Inter-War, Mid-century Modern, to late 20th century and 21st century styles. RFBs are typically situated on large, irregular lots and are of 3-9 storeys in height. Dominance of the RFB typology results in the area being the highest density residential precinct within the LGA. The population density of the area is approximately 162 persons/ha. Clusters of 3-4 storey Inter-War style RFBs are evident on Francis, Simpson, Ocean and Penkivil streets (see Figure 13-3 and Figure 13-4). Inter-War RFBs are typically characterised by a solid brick façade (facades vary), small window openings and entry presenting to the street (see Figure 13-16). Mid-century Modern to 21st century style RFBs, of 4+ storeys define Wellington, Ocean and Penkivil streets. RFBs of these styles comprise red, blond or rendered brick, upper-level balconies and ground or underground parking. A number of these properties have been built around or to the rear of earlier Victorian and Federation-style dwellings. To the west of the area, on Anglesea, New and Watkins streets, the typology changes to 1-2 storey terraces, detached and semi-detached dwellings of earlier Victorian, Federation and Inter-War style dwellings (see Figure 13-6 and Figure 13-15). Watkins Street forms the heritage-listed Watkins Street Conservation Area. The subdivision pattern changes to narrow-fronted deep allotments. Elements of these styles include brick construction, tiled roofs, timber verandah posts and decorative iron or timber balustrades, fences etc. Modifications to these earlier dwellings have occurred over time. The bulk, scale, materiality and articulation of the area varies as a result.



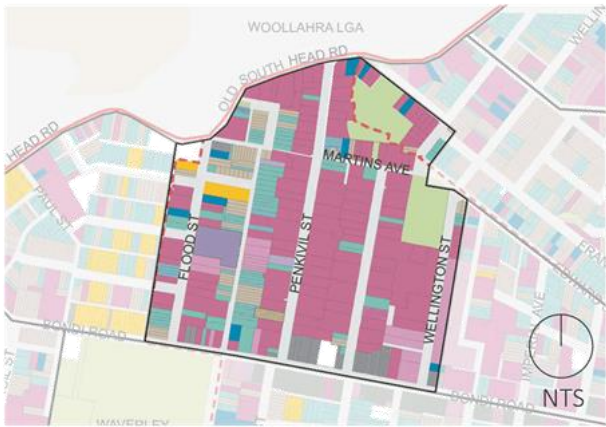
- LEGEND
- Study area boundary
 - Suburb boundary
 - Road network
 - Contour (2m intervals)
 - Open space
 - Bus routes
 - Bus stops
 - Bicycle routes
 - Pedestrian connections

Figure 13-8 - Road network and movement area #13



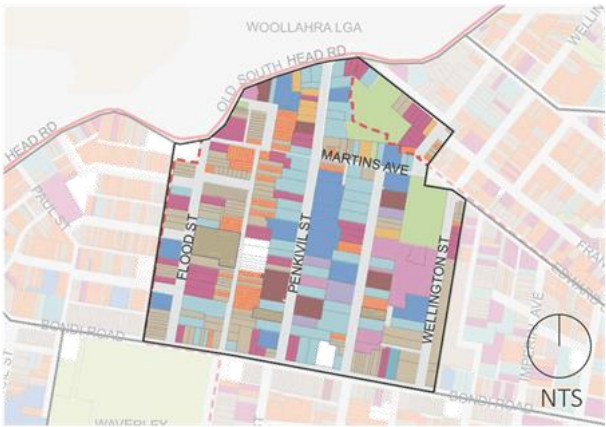
- LEGEND
- Study area boundary
 - Cadastral boundary
 - Colonial
 - Early Victorian
 - Late Victorian
 - Federation
 - Inter-war
 - Mid-Century Modern
 - Late 20th Century
 - 21st Century

Figure 13-10 - Architectural styles area #13



- LEGEND
- Study area boundary
 - Cadastral boundary
 - Detached dwelling
 - Semi-detached dwelling
 - Dual-occupancy
 - Terrace
 - Residential flat building
 - Manor home
 - Multi-dwelling housing
 - Shop-top housing
 - Commercial
 - Other

Figure 13-9 - Dwelling typologies area #13 Building height in storeys area #13



- LEGEND
- Study area boundary
 - Cadastral boundary
 - 1-storey
 - 2-storey
 - 3-storey
 - 4-storey
 - 5-storey
 - 6-storey
 - 7-storey
 - 8-storey
 - 9-storey
 - 10-storey

Figure 13-11 - Building height in storeys area #13

118 Existing character description cont.

Public and private domain interface

The interface between the public and private domain varies across the area as a result of different street and dwelling typologies. Wider versus narrower street corridors, verges and footpaths vary the physical distance and visual transition between the domains, for example Anglesea Street compared to Penkivil Street. The varying capacity of the verge to accommodate trees also influences the visual transition, and contributes to a sense of openness or enclosure. Terraces, detached, and semi-detached dwellings are typically closely set, with narrow front setbacks. Front landscaping and fence materiality and height varies, influencing the visibility of the dwelling from the public domain, for example New Street (see Figure 13-12). Encroachment into the front setback for carparking structures, can further obscure the dwelling and detract from the streetscape. Inter-War RFBs also typically comprise narrow setbacks and limited landscaping. The irregular lot layout also results in the sides of buildings presenting to the street. Mid-late 20th century RFBs comprise greater setbacks and are typically raised above street level, with ground-level parking structures, for example Penkivil Street (see Figure 13-13). Upper balconies overlook the public domain. Through the Bondi Road centre, fine-grain shopfronts abut the public domain, creating a highly visible, active frontage. Laneways provide rear access and off-street parking, typically with high fence lines and enclosed garages at the boundary.



Figure 13-12 - Dwellings fronting New Street



Figure 13-13 - RFBs, Penkivil Street

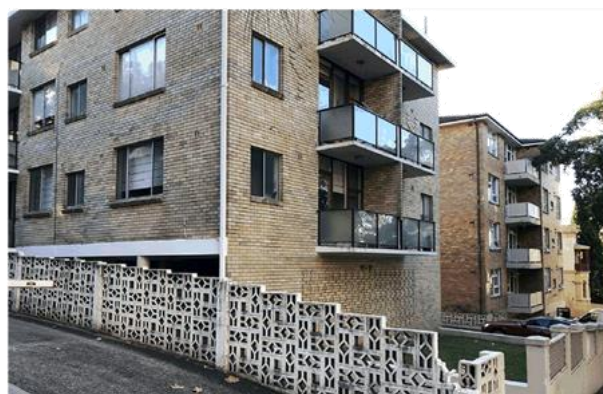


Figure 13-14 - RFBs, Ocean Street North

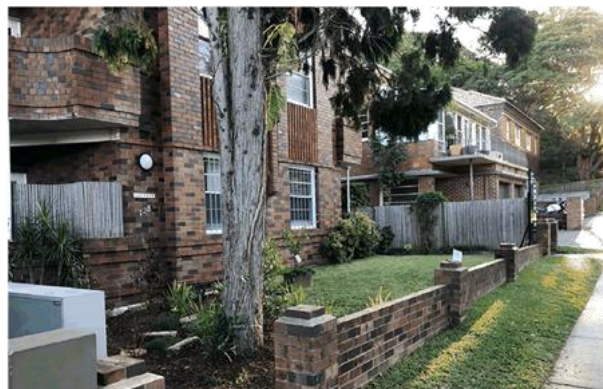


Figure 13-16 - Inter-War RFB, Francis Street



Figure 13-15 - Detached dwelling, Watkins Street

Natural environment

The area is located within close proximity (<450m) of significant public open space, including Thomas Hogan Reserve (see Figure 13-18) and Dickson Park within the north-east of the area, Waverley Park, Cooper Park (Woollahra) as well as the coastal open space network. 'Green links' aid connectivity. A biodiversity corridor traverses the area between the parks. The area is located at a topographical high point, which provides district views to and from the area. The topography slopes steeply at Martins Avenue into Thomas Hogan Reserve and Dickson Park. Vegetation coverage varies across the area. Substantial street trees define Flood, Watkins, New, Penkivil and Ocean streets (see Figure 13-19), increasing coverage and contributing to the area's landscape character. These factors contribute to a low-medium heat vulnerability.

Recent development

Recent development includes the construction of new RFBs, through amalgamation of sites, demolition of existing dwellings or construction of new buildings to rear of existing dwellings; alterations and additions to existing dwellings, including second-storey and garage addition. This reflects development capacity under the planning controls, a desire to intensify valuable land, more floor space and car-parking in the area.

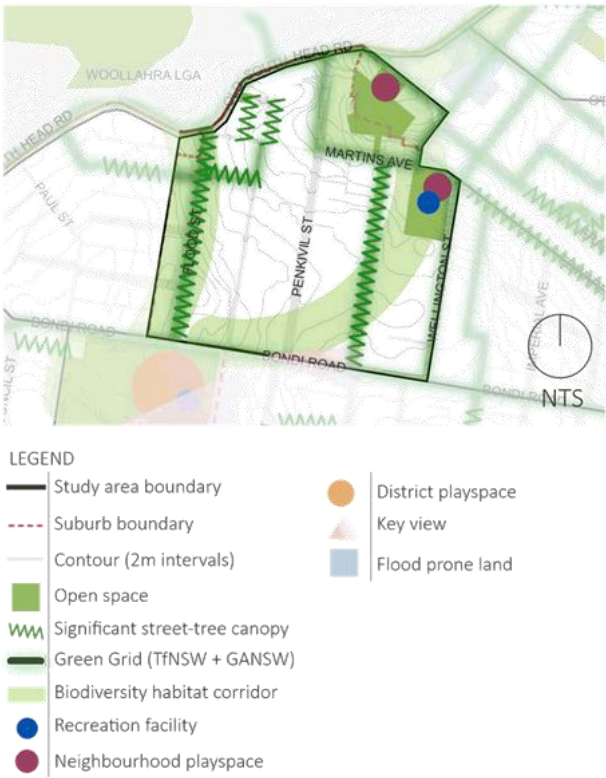


Figure 13-17 - Open space and vegetation area #13

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



Figure 13-18 - Thomas Hogan Reserve



Figure 13-19 - Tree coverage, Ocean Street North



CHARACTER AREA

14



CHARACTER AREA – 14

“The area is characterised by irregular blocks arrayed east-west; predominantly low-medium rise detached and semi-detached dwellings and residential flat buildings, of various styles and materiality; fine-grain, active shopfronts within the Bondi Road local centre; wide street corridors with irregularly spaced trees”.

The study area is generally bound by Edward Street to the north, Bondi Road to the south and Wellington Street to the east.

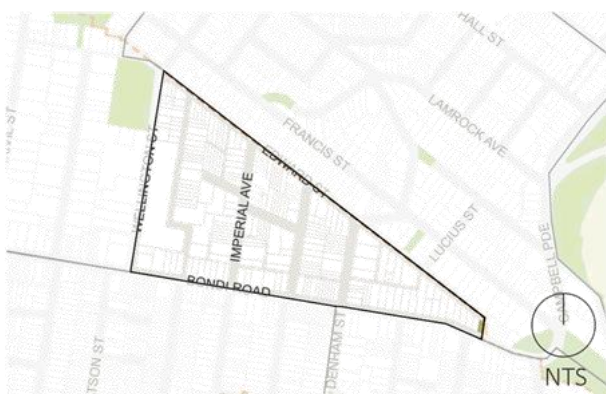


Figure 14-1 - Local character area #14 map



Figure 14-4 - Detached dwelling, Castlefield Street



Figure 14-2 - Imperial Avenue, looking south



Figure 14-5 - Semi-detached dwellings, Imperial Avenue

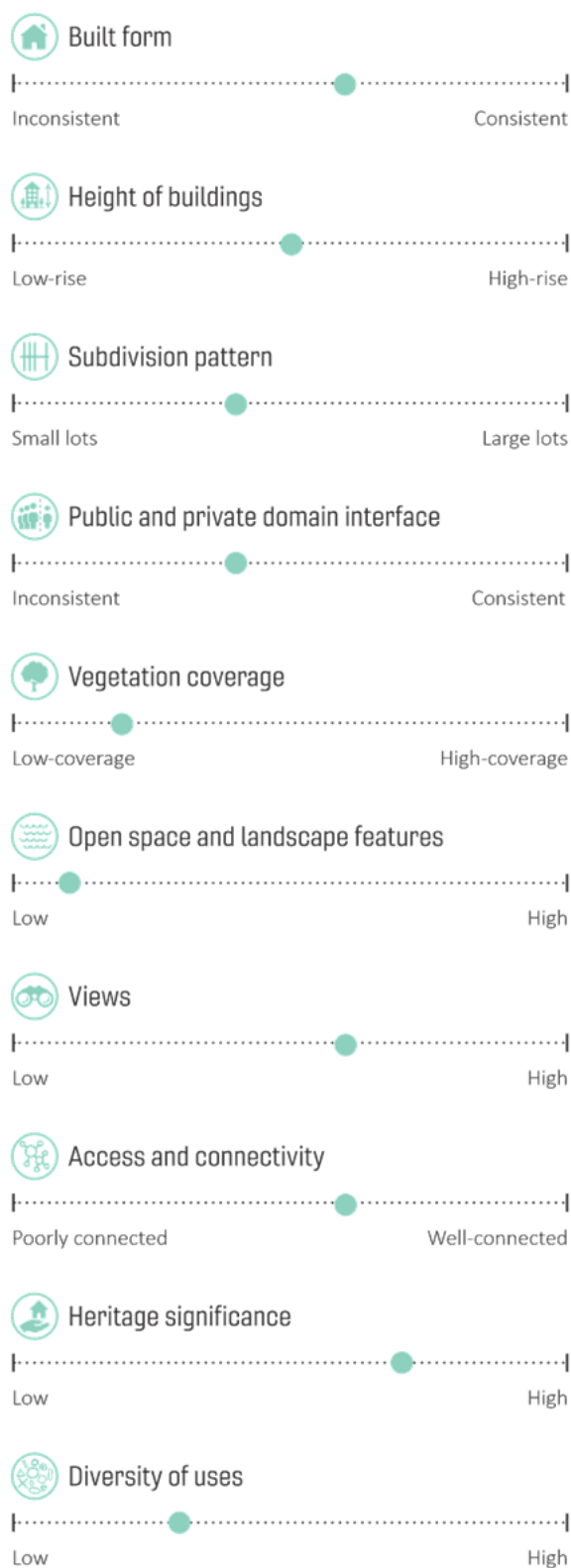


Figure 14-3 - RFB, cnr Imperial Avenue and Miller Street



Figure 14-6 - RFBs, Edward Street

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by high-density residential area within walking distance to local shops and services, and open space and the coastline. Development is well integrated into the streetscape through design excellence and landscape design including mature tree canopy.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area, particularly along Bondi Road,
- To celebrate the heritage and maintain the fine-grain nature of the historic shopfronts,
- To effectively manage the retail / commercial and residential interface,
- To preserve the heritage items and the integrity of the Urban Conservation Area,
- To reinforce the consistent and unified Federation and Inter-war-style detached and semi-detached dwelling elevations and frontages to internal streets, including pitched roofscape and front setbacks,
- To promote sympathetic alterations and additions that respect the form, architectural style, height, materials and details of existing dwellings, particularly semi-detached dwellings,
- To encourage high-quality contemporary low-medium rise RFB development at the southern and western edges of the area. Consider site amalgamation where appropriate,
- To promote the reduction of hard surfaces and an increase of landscaping in the front, rear and side of properties,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To minimise the impact on existing views and vistas from the private domain and maintain residential amenity in terms of overlooking and noise,
- To reduce pedestrian, cyclist and vehicle conflicts, and to improve pedestrian and cyclist amenity on Bondi Road.

124 Existing character description

History

Development of the area has evolved from the subdivision of the Castlefield Estate which was located east of Bondi Road. The Castlefield residence, built in the 1870s, was noted for its magnificent garden and views over Bondi Bay. The Estate was subdivided and sold in 1907. The subdivision and a second sale in 1909, established the prevailing block pattern and streets, including Edward, Denham and Castlefield streets and Imperial Avenue. Development progressively extended along Bondi Road and into the area, driven by access to public transport, the Sydney tram service reaching Bondi Beach in 1894 and the rising popularity of seaside excursions and bathing. Bondi Road became a major transport route, with businesses servicing the growing local population and tourist trade. Bondi Public School, in Wellington Street, opened in 1883. Enrolment growth continued until the 1930s. Today, the area comprises predominantly residential uses with supporting commercial and retail uses within the Bondi Road local centre, and educational and religious establishments.

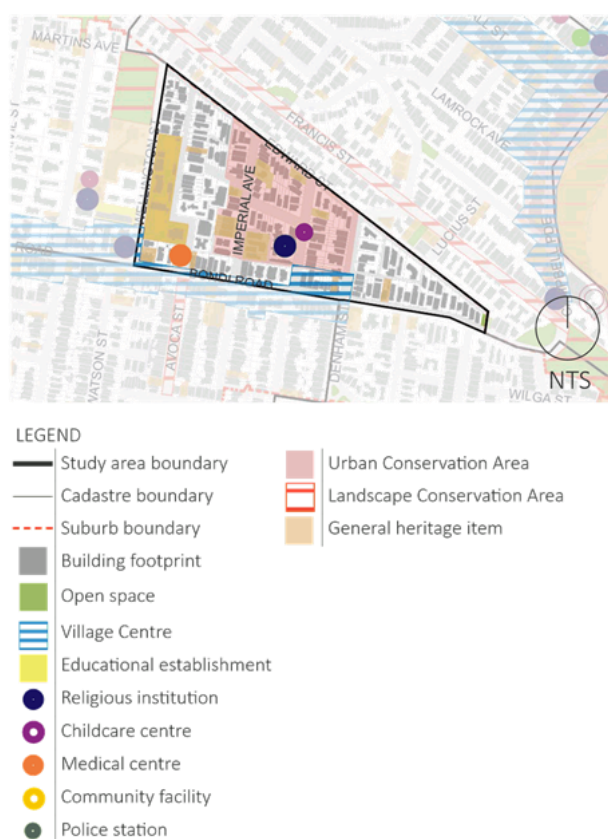


Figure 14-7 - Built form, uses and heritage area #14

Configuration and connectivity

Irregular blocks are arrayed east-west across the prevailing slope, tapering to the east. A well-defined street network comprises east-west bounding streets (Edward Street and Bondi Road), north-south through-streets (Wellington, Edward, Denham streets and Imperial Avenue) and inner streets and rear laneways. The network results in a high level of permeability. Wellington Street and Bondi Road provide the primary access to and from the area. Multiple bus services along Bondi Road connect the area to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD.

Built form

The area is characterised by predominantly 1-2 storey detached and semi-detached dwellings and 3-4 storey residential flat buildings (RFBs), of various styles. The earliest remaining dwellings date from 1907 to 1915 and are of the Federation-style. Clusters of detached and semi-detached Federation bungalows are located on Imperial Avenue (see Figure 14-5), Castlefield Street and Denham Street. Federation-style shopfronts front Bondi Road. Inter-War style detached and semi-detached bungalows and RFBs constructed between 1915 to 1940 are also evident throughout the area, for example on Moore Street, Edward Street (see Figure 14-15) and Bondi Road. Bungalows of these styles are characterised by brick construction, low gabled terracotta tiled roofs, with thick masonry or timber verandah posts and decorated timber gable trim. Inter-War RFBs are typically characterised by a solid brick façade (facades vary), small window openings and entry presenting to the street. These styles reflect the significant land and housing booms between 1881-1939. Heritage listing of much of the area as the Imperial Avenue Conservation Area has helped retain the character of these early streetscapes, however alterations including second-storey and parking additions, as well as later infill development, have impacted on their integrity. Later mid-century development, including Mid-century Modern style RFBs eroded the earlier setting, capitalising on views to the ocean and availability of land around earlier dwellings or through property amalgamation. RFBs of this style are typically red or blonde brick (see Figure 14-3 and Figure 14-13). Infill development of late 20th century and 21st style development has continued throughout the area, particularly along the Bondi Road. The area has a population density of approximately 115 persons/ha. The dominant tenure type is private rental.

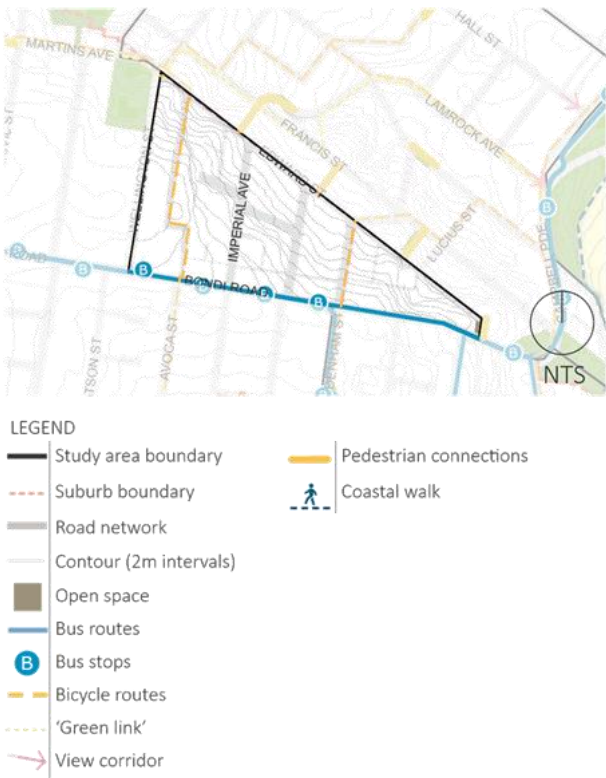


Figure 14-8 - Road network and movement area #14

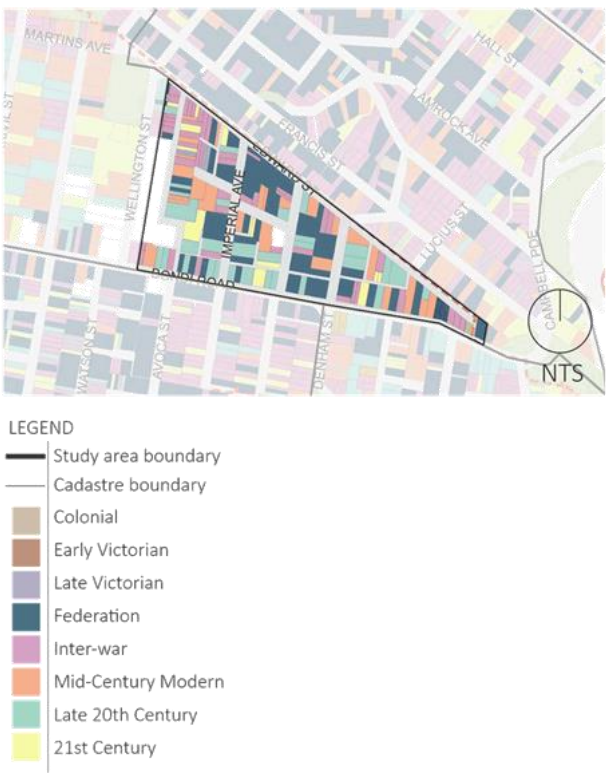


Figure 14-10 - Architectural styles area #14

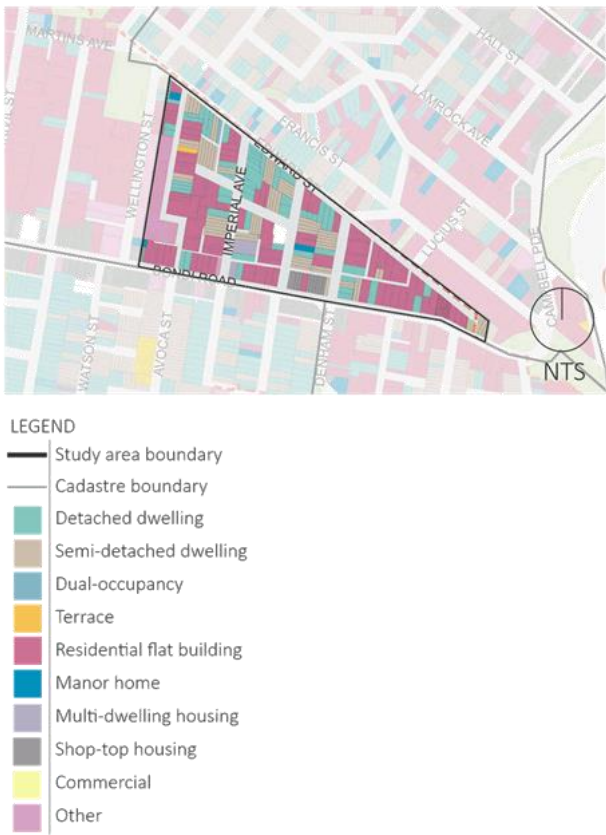


Figure 14-9 - Dwelling typologies area #14

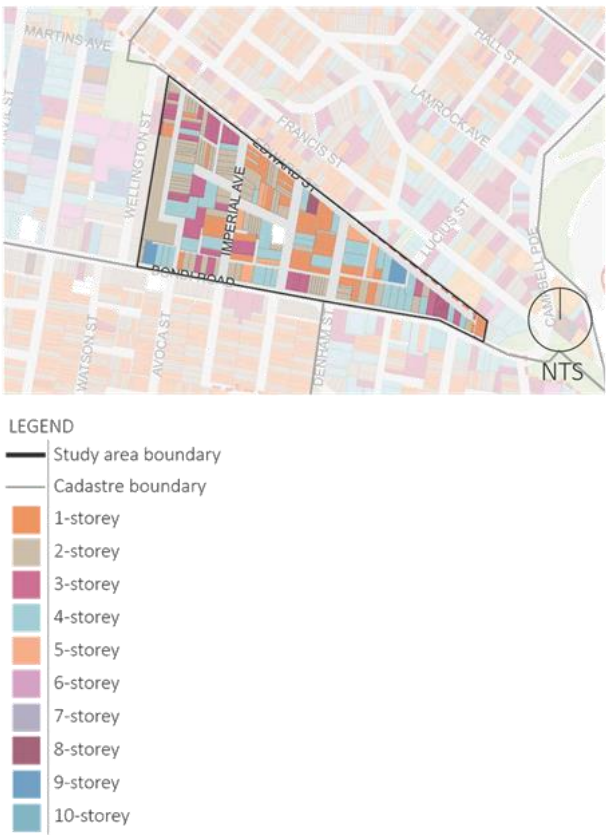


Figure 14-11 - Building height in storeys area #14

Existing character description cont.

Public and private domain interface

The interface between the public and private domain varies across the area as a result of different street and dwelling typologies. Wide street corridors contribute to a sense of openness. Wider versus narrower street verges and footpaths vary the physical and visual transition between the domains, for example Imperial Avenue compared with Moore Street (see Figure 14-16). The varying capacity of the verge to accommodate trees also influences the visual transition, altering the level of visibility between the street and dwelling. On-street planting can increase the sense of enclosure, for example Edward Street. Setbacks vary across the area, as does the alignment of dwellings to the street, for example on Imperial Avenue. Front landscaping, fence materiality and height also vary, however the cohesive use of sandstone retaining walls is a key feature. The later addition of car-parking within the front setback, either hard-stand, carport or garage further obscures the dwelling. Mid-late 20th century RFBs are raised above street level incorporating ground-floor carparking (see Figure 14-12). Upper balconies overlook the public domain. Through the Bondi Road centre, fine-grain shopfronts abut the public domain, creating a highly visible, active frontage. Laneways provide rear access and off-street parking, typically with high fence lines and enclosed garages at the boundary (see Figure 14-14).



Figure 14-12 - RFB, Cutler Street



Figure 14-13 - RFB, Denham Street



Figure 14-14 - Garage frontages, Cutler Street



Figure 14-15 - Detached dwellings, Edward Street



Figure 14-16 - Moore Street, looking north

Natural environment

The area is largely devoid of open space, however benefits from its close proximity to Dickson Park, Thomas Hogan Reserve and Waverley Park to the west and Bondi Beach and the coastal open space network to the east, which provide active and passive recreation opportunities. 'Green links' aid connectivity. The area's topography, which slopes to the north / north-east provides views out to the coastline from the public and private domains. Vegetation coverage is predominantly low-medium across the area. This is attributable to the area's coastal location with sandy soils and high sun exposure. Coverage is particularly low along Bondi Road and rear lanes, however, improves along inner streets and Edward Street. Extensive on-site planting, particularly in rear yards, provides background tree lines to many dwellings and improves coverage. These factors contribute to a medium-high heat vulnerability.

Recent development

Recent development includes alterations and additions to existing detached and semi-detached dwellings, including second-storey additions; alterations and additions to older RFBs, including balcony additions or materiality changes; amalgamation and construction of new RFBs, particularly along Bondi Road. Suggesting a desire to intensify valuable land close to public transport, for more floor space and car-parking in the area whilst maintaining the existing character of the Imperial Avenue Conservation Area.



Figure 14-18 - Open space and vegetation area #14



Figure 14-17 - Tree canopy, Castlefield Street



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

15



CHARACTER AREA – 15

“The area is defined by its coastal setting, with the headland parks and beach forming an iconic corridor of open space, a strong local centre along Campbell Parade and Hall Street, cohesive streetscapes of predominantly low-medium rise dwellings of the Federation and Inter-War styles, varying vegetation coverage”.

The study area is bound by O’Brien and Roscoe streets to the north, Campbell Parade and Notts Avenue to the east, Edward Street to the south and Simpson Street and Old South Head Road (OSHR) to the west.

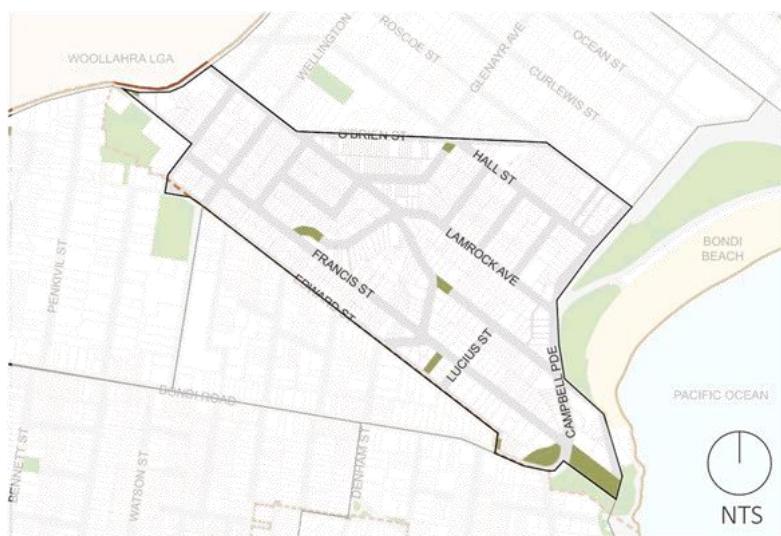


Figure 15-1 - Local character area #15 map



Figure 15-4 - RFB, Edward Street



Figure 15-2 - RFBs, Francis Street



Figure 15-5 - Semi-detached dwellings, Francis Street

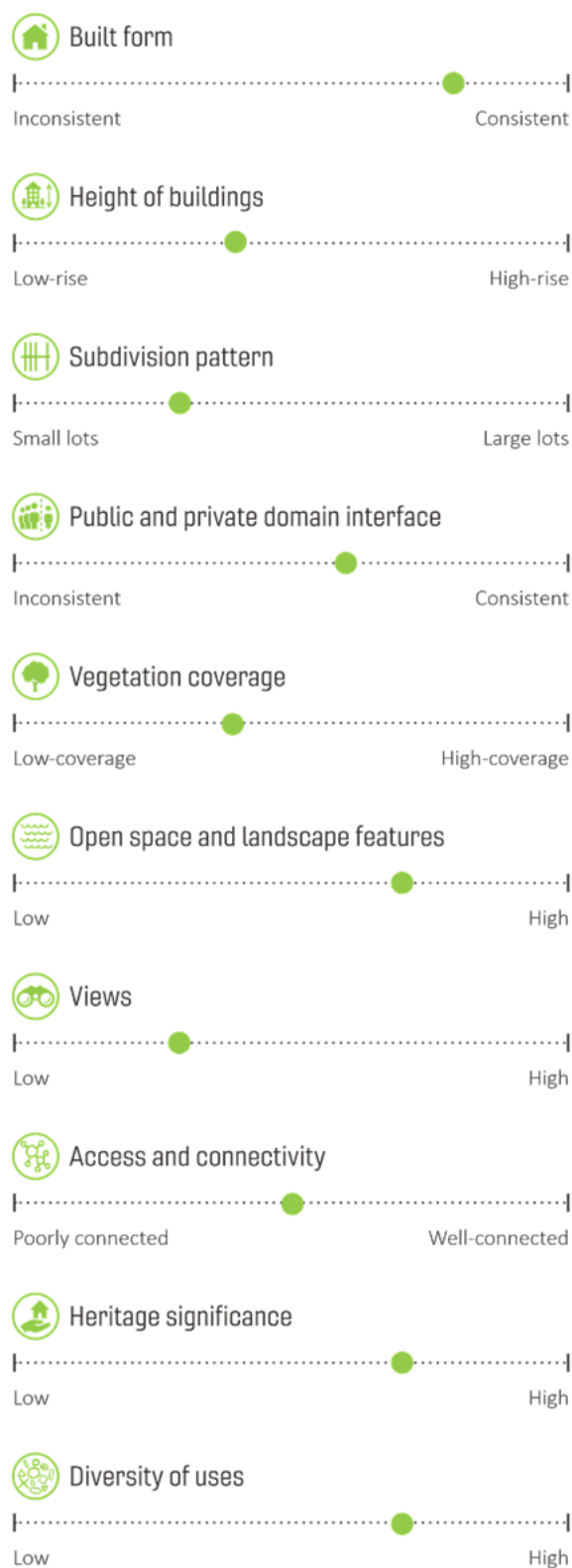


Figure 15-3 - RFBs, Denham Street



Figure 15-6 - Tree canopy, Ormond Street

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by the iconic coastline and Bondi Beach Centre. Surrounding the centre is a historic residential area with cohesive and attractive streetscapes.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area,
- To effectively manage the retail / commercial and residential interface,
- To reinforce the consistent and unified Federation and Inter-war-style detached and semi-detached dwellings and RFBs, including consistent elevations, pitched roofscapes and front setbacks,
- To promote sympathetic alterations and additions that respect the form, architectural style, height, materials and details of existing buildings,
- To ensure alterations and additions to the rear of dwellings retain the scale and massing of front elevations and to retain the original roof form,
- To ensure additions to the rear of dwellings maintain a high-level of amenity for surrounding properties,
- To ensure new development responds to the historic patterns, original built form, architectural styles, materials and details of the area,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To maintain and enhance the significant street tree canopy,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape,
- To discourage new driveway crossovers,
- To reduce pedestrian, cyclist and vehicle conflicts.

132 Existing character description

History

The area has a rich pre-and post-European settlement history. The Waverley area was occupied by the Bidjigal and Gadigal people, with Bondi Beach the location of an open campsite(s), midden(s) and burial place. Development of the area has evolved from the first land grant in the Waverley municipality, being 200 acres between OSHR and Bondi Beach granted to William Roberts in 1810. The land was used for grazing, quarries, brickworks etc. In 1851 the land was sold, undeveloped, to E.S (Monitor) Hall in trust for his daughter, Georgiana, who was married to Francis O’Brien. O’Brien attempted to subdivide part of the property in 1852 but was unsuccessful. In 1854-1856 negotiations saw a 21-acre reserve established at the southern end of the Beach. A further 25 acres was resumed in 1882 for public use.

The Bondi Beach commercial area evolved from a setting of small tea houses catering to day excursions to a flourishing tourist destination by the early 1990s, driven by the construction of the Bondi Baths (1886), rising popularity of sea bathing and the extension of the Sydney tram service to Bondi Beach by 1894. Progressive sale of the O’Brien estate provided sites for rapid Federation-era speculative subdivision on the southern ridge and slope of the basin. The O’Brien family remained in residence at “The Homestead” near Lucius Street until 1911, when it was sold and subdivided for housing during the Inter-War period. By 1939 Campbell Parade was a completed setting of hotels and shopfronts. Today, the area comprises predominantly residential uses supported by a strong retail and commercial centre , tourist and visitor accommodation and iconic coastal open space.

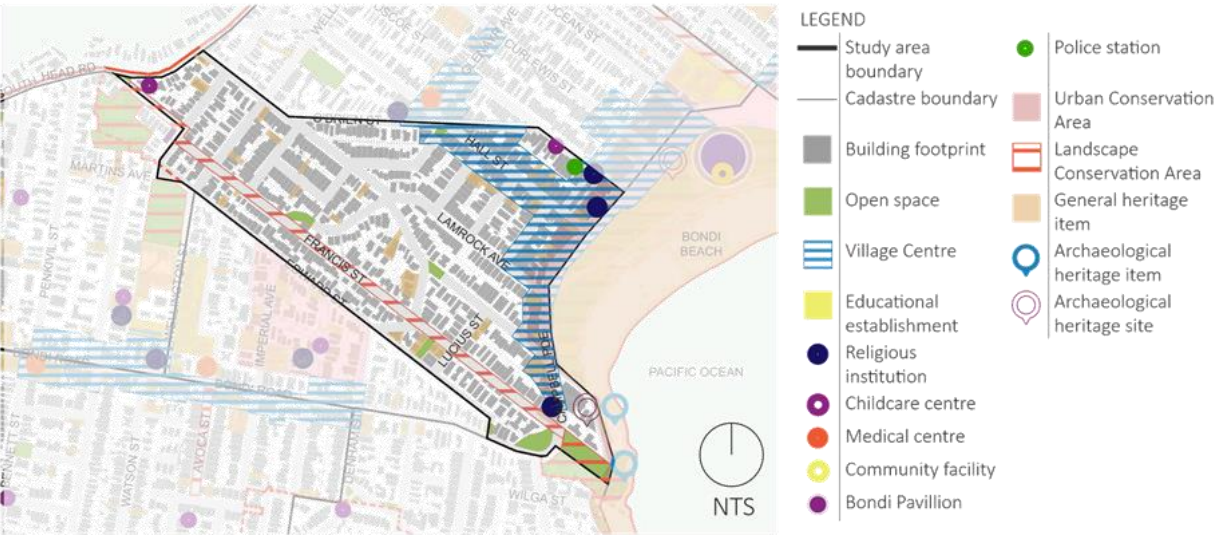


Figure 15-7 - Built form, uses and heritage area #15

Configuration and connectivity

The prevailing block and street pattern of the area reflects the 1852 subdivision plan instigated by O’Brien and early streets that appeared from 1878. Long north-west / south-east streets, with short cross streets, follow the steeply sloping topography from Edward Street into the basin and out to the coastline. The street alignment alters around Francis Street, Forest Knoll Avenue and Sir Thomas Mitchell Road, due to a localised gully. Topographical limitations result in no-through streets, which combined with multiple 1-way-only streets reduce access and circulation within the area. Pedestrian connectivity is

maintained along the coastal walk, ‘Green links’ and through-block connections, for example Edward Street to Frances Street, Cox Avenue to O’Brien Street and Ormond Street to Sir Thomas Mitchell Road. Lamrock Avenue includes a dedicated on-street bicycle lane. Campbell Parade, Bondi Road and OSHR provide the primary access to and from the area. Multiple bus services along Campbell Parade, O’Brien Street and OSHR connect the area to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD.

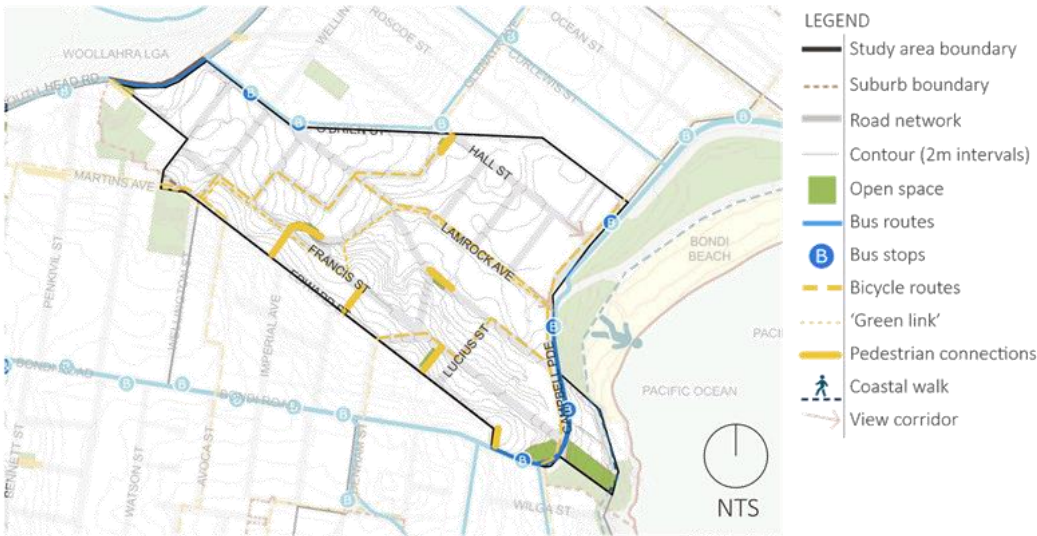


Figure 15-8 - Road network and movement area #15

Built form

The area is characterised by streetscapes of predominantly 1-2 storey semi-detached and detached dwellings and 3-5 storey residential flat buildings (RFBs), of the Federation and Inter-War styles. The Federation period (1890-1919) saw housing construction begin on the south-west slopes of the basin, fuelled by the partial sale of the O'Brien Estate in 1910. Generous freestanding dwellings on the upper slopes, for example Edward and Francis streets, and more close-set semi-detached dwellings to the lower slopes, for example Barracluff, Rickard (see Figure 15-9) and Lamrock avenues, reflect the relative amenity of the subdivisions. These streetscapes comprise cohesive styles and materials, namely brick construction, low gabled terracotta tiled roofs, with thick masonry or timber verandah posts and decorated timber gable trim. The subsequent Inter-War period (1919-1939) saw the emergence of the RFB, employed within new subdivisions of the O'Brien Estate around Lucius Street and on opportunity sites on the southern slopes, for example Edward Street and Sir Thomas Mitchell Road. Campbell Parade and Hall Street reflect an Inter-War commercial streetscape. Inter-War RFBs are typically characterised by a solid rendered or unrendered brick façade (facades vary in style), terracotta tiled roof, small window openings and entry presenting to the street. Alterations including second-storey and street-front carparking additions, as well as later infill development, has impacted the integrity of the early streetscapes. Later insertions of Mid-century Modern, 20th century and 21st century dwellings are evident throughout the area, for example Notts Avenue, OSHR and within the Bondi Beach local centre. The population density is approximately 107 persons/ha.



Figure 15-9 - Semi-detached dwellings, Rickard Avenue



Figure 15-10 - Inter-War RFB, Francis Street



Figure 15-11 - RFB, Rickard Avenue

134 Existing character description cont.

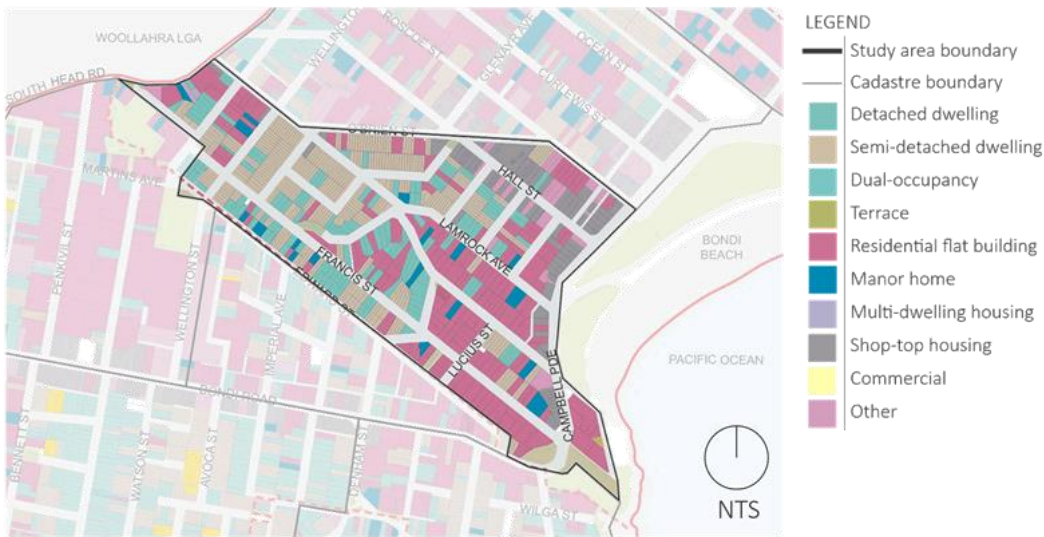


Figure 15-12 - Dwelling typologies area #15

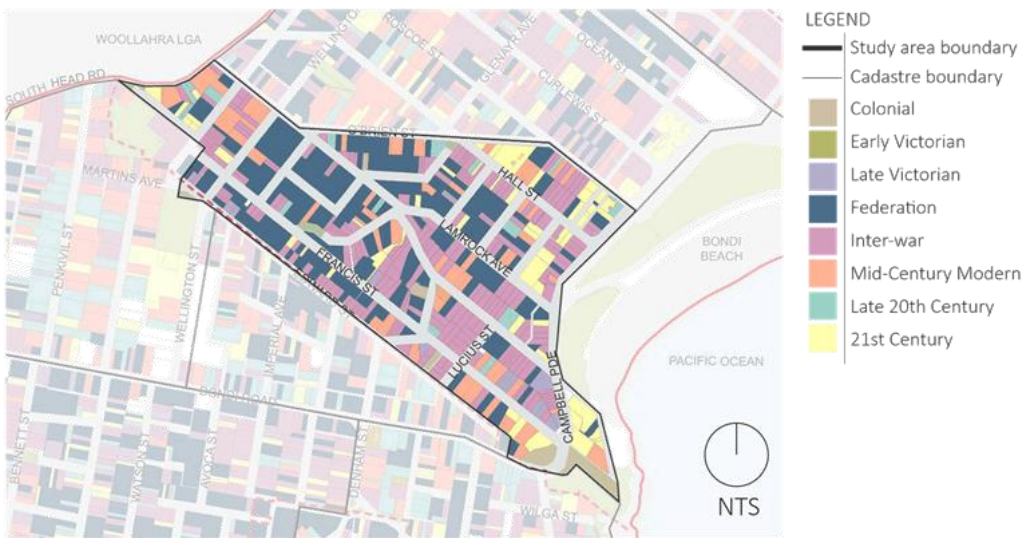


Figure 15-13 - Architectural styles area #15

Public and private domain interface

The interface between the public and private domain varies across the area in response to dwelling typology and topography changes. Detached and semi-detached dwellings typically have consistent front setbacks of shallow depth. Fence lines vary from low solid or picket fence to higher solid fence, for example Rickard Street (see Figure 15-9). Front landscaping is typically limited. This results in physical and visual proximity between the public and private domains. Front setbacks for RFBs vary; at times the dwelling abuts the boundary, for example Lamrock Avenue, or is setback further, providing for additional front landscaping, for example Francis Street (see

Figure 15-2). Buildings remain closely set. Mid-late 20th century RFBs are raised above street level with ground-level parking and upper balconies overlooking the public domain (see Figure 15-11). On-street planting also creates a varied visual transition between the domains. Sloping topography creates an upper and lower side of some streets. Dwellings on upper sites are raised above the public domain, with built retaining walls, high fences and / or garage doors of varying materials and finishes at street-level, for example Francis Street (east) and Notts Avenue. Dwellings on lower sites are often partially or completely obscured from the street, for example Edward Street (west). The later addition of street

level hard-stand parking, carports and garages where the topography or front setbacks allow can further obscure the dwelling. However, due to these factors many dwellings are not serviced by off-street parking. Topographical changes and clustering of dwellings in irregular blocks, for example Ormond Street to Forest Knoll Avenue, can result in overlooking. Along Campbell Parade and Hall Street, fine-grain shopfronts and dwellings abut the public domain, creating a highly visible, active frontage.

Natural environment

The area is characterised by its coastal setting and long history of seaside leisure and recreation. The heritage-listed, Bondi Beach, Biddigal Reserve and Hunter Park contribute to an iconic corridor of public open space of significant aesthetic, landscape and recreational value. The areas comprise Aboriginal archaeological sites and European built form including the current Bondi Baths (1931) and Bondi Park design (1923). Vegetation coverage is predominantly low-medium across the area. Coverage reduces to very low over the beach, Notts Avenue and through the local centre, however, improves to medium-high in the gully around Forest Knoll Avenue and surrounding streets including Ormond Street (see Figure 15-6) and Consett, Jacques and Cox Avenues. On-street avenue planting contributes to a sense of enclosure. The corridor supports and biodiversity corridor. The beach, parks and Bondi-Coogee coastal walk offer passive and active recreation opportunities and support public gathering and events.



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?

Recent development

Recent development comprises alterations and additions to existing detached and semi-detached dwellings, including second-storey additions; alterations and additions to older RFBs, including additional apartments, balcony additions or materiality changes; construction of new 2-3 storey detached and semi-detached dwellings (including dual occupancy) and RFBs, particularly along OSHR and within the local centres. It is also noted that Bondi has the second highest proportion of dwellings by suburb listed on Airbnb within Australia, indicating a trend of use of private dwellings for short-term rental accommodation.



Figure 15-14 - Open space and vegetation area #15



CHARACTER AREA

16



CHARACTER AREA – 16

“The area is characterised by its basin topography; streetscapes of predominantly low-medium rise dwellings of Federation and Inter-War styles; strong local centres which create a highly active public domain; limited vegetation coverage; its coastal setting, with the beach forming an iconic corridor of open space”.

The study area is bound by Blair Street and Warners Avenue to the north, Campbell Parade to the east, Roscoe and O’Brien streets to the south and Old South Head Road to the west.



Figure 16-1 - Local character area #16 map



Figure 16-4 - RFBs, Beach Road (Source: Google Images)



Figure 16-2 - Curlewis Street centre



Figure 16-5 - RFBs, O'Brien Street (Source: Google Images)



Figure 16-3 - Semi-detached dwellings and RFB, Beach Road (Source: Google Images)



Figure 16-6 - Semi-detached dwellings, Roscoe Street (Source: Google Images)

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by the iconic coastline and Bondi Beach Centre. Surrounding the centre is a historic residential area with cohesive and attractive streetscapes.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area,
- To promote the retention of urban services in the area and manage the interface between urban services and residential development,
- To effectively manage the retail / commercial and residential interface,
- To maintain the predominant 2-4 storey height character of the area,
- To preserve the diversity of dwelling typologies, architectural periods and heights in the area by retaining buildings from the Federation and Inter-war periods,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality, particularly semi-detached dwellings,
- To ensure high-quality design of contemporary development with well-articulated forms, rooflines and materiality,
- To encourage high-quality, contemporary, low rise development at the western edges of the area, fronting OSHR. Consider site amalgamation where appropriate,
- To ensure front fences and landscaping relate to the period and architectural style of the dwelling, and maintain visual connection between the dwelling and street,
- To promote the reduction of hard surfaces and an increase of landscaping in the front, rear and side of properties,
- To retain and increase street tree plantings on all streets to enhance the streetscape character,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Discourage carparking structures in front setback unless the predominant,
- To reduce new vehicle crossings on key roads.

140 Existing character description

History

The area has a rich pre-and post-European settlement history. The Waverley area was occupied by the Bidjigal and Gadigal people, with Bondi Beach the location of an open campsite(s), midden(s) and burial place. The area initially noted as ‘barren lands’ (1791 survey), characterised by mobile sand dunes interspersed with hinterland lagoons surrounded by windswept vegetation on higher ground, remained largely undeveloped until the 20th century. Development of the area evolved from the first land grant in the Waverley municipality, being 200 acres between OSHR and Bondi Beach, granted to William Roberts in 1810. In 1851 the land was sold, undeveloped, to E.S (Monitor) Hall in trust for his daughter, Georgiana, who was married to Francis O’Brien. O’Brien attempted to subdivide part of the property in 1852 but was unsuccessful. Reclamation of beach front land and progressive sale of the O’Brien Estate saw the southern end of the basin grow by the early 1900s. However, the sand formations in the middle and north side of the basin restricted development of the area. Remediation and removal of sand occurred, with the allocation of State funding, between 1900 and 1920, providing land for the Inter-War housing boom. Development was supported by the extension of the Bondi tramline to North Bondi in 1911 and the Bellevue Hill to Bondi Beach line in 1914. The Bondi Beach Public School opened in 1926. By 1939 Campbell Parade was a completed setting of hotels and shopfronts. Today, the area comprises predominantly residential uses supported by a strong retail and commercial centre, tourist and visitor accommodation, educational establishments and adjacent coastal open space.



Figure 16-8 - Semi-detached dwellings, Curlewis Street (Source: Google Images)



Figure 16-9 - RFBs, Beach Road (Source: Google Images)



Figure 16-10 - RFB, Curlewis Street (Source: Google Images)

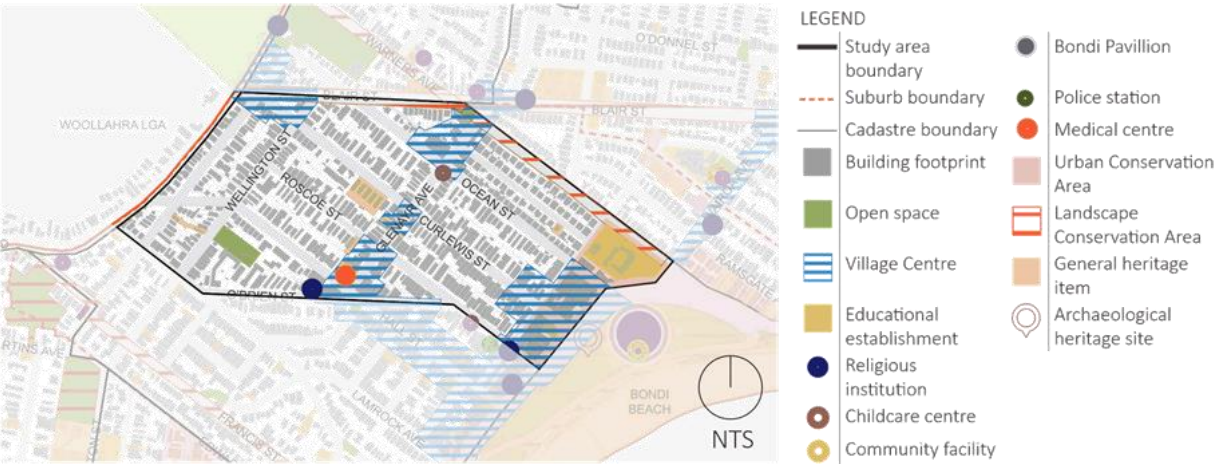


Figure 16-7 - Built form and uses area #16

Configuration, connectivity and built form

The prevailing block and street pattern of the area reflects the 1852 subdivision plan instigated by O'Brien. Street planning, including the wide avenue of Blair Street and the cross route of Warners Avenue, reflect the Garden Suburb philosophy. Long north-west / south-east streets, and limited north-east / south-west cross streets, are arranged in an irregular grid pattern across the area's relatively flat topography. Multiple 1-way-only streets reduce internal vehicle access and circulation. Campbell Parade, OSHR, O'Brien and Curlewis streets provide the primary access to and from the area. Pedestrian connectivity remains high, assisted by the defined street network, flat topography and continuous coastal open space. Dedicated on-street bicycle lanes are located on Glenayr Avenue, Curlewis Street and Blair Street. Multiple buses service the area providing connections to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD.

The area is characterised by cohesive groups of 1-2 storey detached and semi-detached dwellings and 3-4 storey residential flat buildings (RFBs), of predominantly Federation and Inter-War styles, interspersed with later interventions of Mid-century Modern, late 20th century and 21st century style dwellings. The local centres along Glenayr Avenue, Curlewis Street and Campbell Parade are characterised by shop-top housing of various styles. Rapid development of the area during the late-Federation and Inter-War periods, following the sand dune remediation, resulted in consistency of streetscapes of housing with a predominant aesthetic of face-brick exteriors with terra cotta tile roofs. Inter-War style RFBs make an import contribution to the distinctive character of the LGA. Clusters of Federation and Inter-War styles are evident along Curlewis Street, Wellington Street, Beach Road, Glasgow Avenue and Warner Avenue. Alterations including second-storey and street-front carparking additions, as well as later infill development of up to 8-storeys.

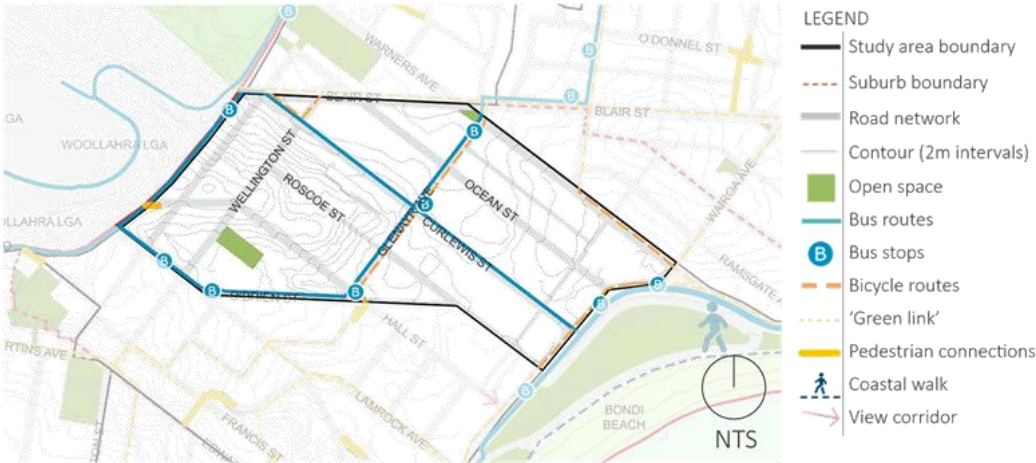


Figure 16-11 - Road network and movement area #16



Figure 16-12 - Dwelling typologies area #16

142 Existing character description cont.



Figure 16-13 - Architectural styles area #16



Figure 16-14 - Building height in storeys area #16

Later insertions of Mid-century Modern to 21st century dwellings are evident along O'Brien, Curlew (see Figure 16-10), Roscoe and Glenayr (north) streets. The population density is approximately 129 persons/ha. The dominant tenure is private rental.

Public and private domain interface

The interface between the public and private domains varies significantly across the area, as a result of differing dwelling typologies, front setback depths, front landscaping, fence treatment and parking structures. Federation and Inter-War style dwellings typically have consistent front setbacks of shallow depth. This results in physical and visual proximity between the public and private domains. The visibility of the dwelling varies due to fence treatment and front landscaping, for example Warners Avenue, Curlew Street and Simpson Street. The addition of street-level hard-stand parking, carports and garages within the front setback of detached and

semi-detached dwellings can further obscure the dwelling. Excavation to accommodate parking has also occurred in early semi-detached dwellings fronting Curlew Street (west). Mid-late 20th century RFBs typically comprise greater front setbacks and are raised above street level with ground-level parking and upper balconies overlooking the public domain, for example Roscoe (see Figure 16-15) and O'Brien streets. Inter-War style RFBs can also comprise street-level garages, for example Gould Street, creating a solid inactive wall. Along Glenayr Street, Curlew Street and Campbell Parade, commercial and retail uses abut the public domain, creating a highly visible, active frontage. Vegetation coverage is predominantly low-medium across the area, as a result of closely set development which limits the opportunity for significant private open space in the front or rear yards as well as the varying capacity of the street verge to accommodate planting. On-street planting also varies the visual transition between the domains.

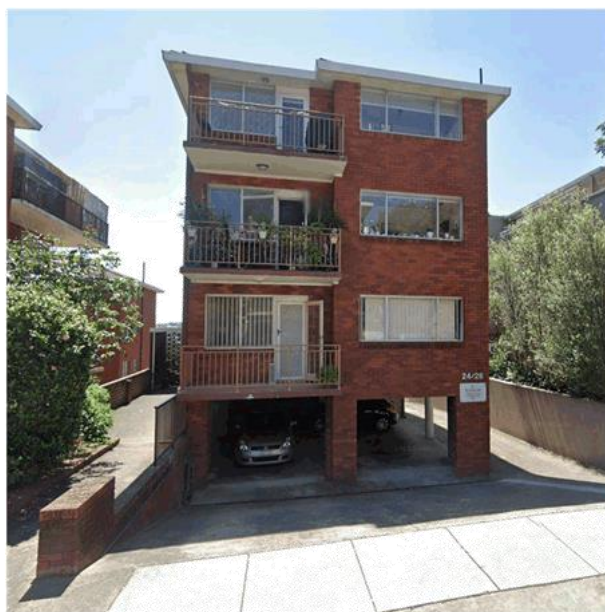


Figure 16-15 - RFB, Roscoe Street (Source: Google Images)

Natural environment

The area is recognised for its long history of seaside bathing and leisure. In 1903, Bondi Beach was provided with its first daytime bathing facilities and in 1911 the first bathing shed opened. The North Bondi Surf Life Saving Club (SLSC) and the Bondi SLSC club opened in 1906 and 1907. The heritage-listed, Bondi Beach and Park, contribute to an iconic corridor of public open space of significant aesthetic, landscape and recreational value. The areas comprise Aboriginal archaeological sites and European built form including the Bondi Pavilion (1929) and Bondi Park design (1923). The beach, parks and Bondi-Cooee coastal walk offer passive and active recreation opportunities and support public gathering and events.



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?

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Recent development

Recent development history includes alterations and additions to existing detached and semi-detached dwellings, including second-storey additions; alterations and additions to older RFBs, including additional apartments, balcony additions or materiality changes; construction of new 2-3 storey detached and semi-detached dwellings (including dual occupancy) and RFBs, particularly along OSHR and within the local centres. It is also noted that Bondi has the second highest proportion of dwellings (by suburb) listed on Airbnb within Australia, indicating a trend towards the use of private dwellings for short-term rental accommodation.

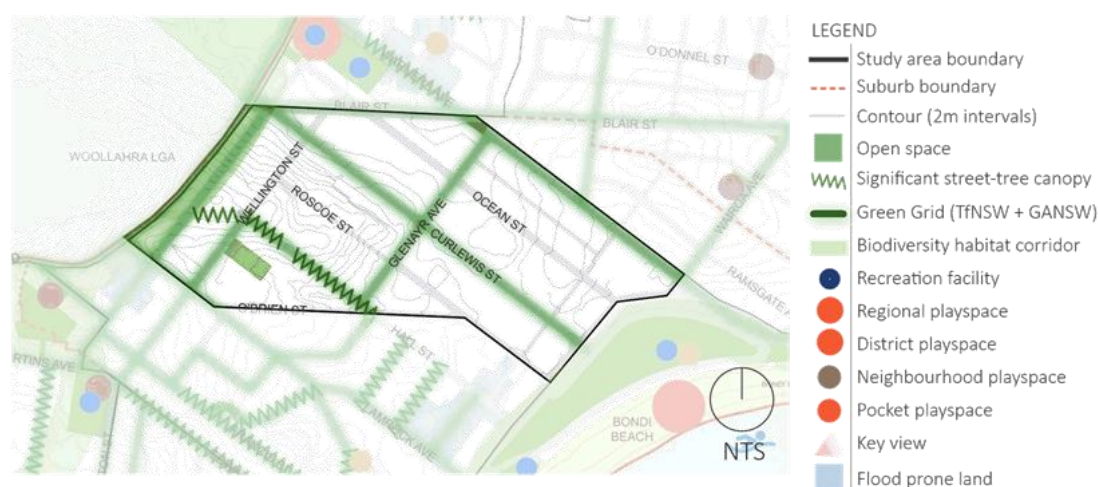


Figure 16-16 - Open space and vegetation area #16



CHARACTER AREA

17



CHARACTER AREA - 17

"The area is defined by its prominent headland location with exposed rock platforms and steep sandstone cliffs; panoramic views of the ocean; uniform subdivisions aligned along parallel streets, comprising densely set, largely boxy proportioned low-medium rise detached and residential flat buildings of various styles and colours".

The study area is generally bound by the Bondi Golf and Diggers Club to the north, the Pacific Ocean to east and south, Bondi Beach to the west, and Campbell Parade to the north-west.



Figure 17-1 - Local character area #17 map



Figure 17-4 - Sam Fiszman Park, looking west



Figure 17-2 - RFBs, Ramsgate Avenue



Figure 17-5 - Ramsgate Avenue streetscape

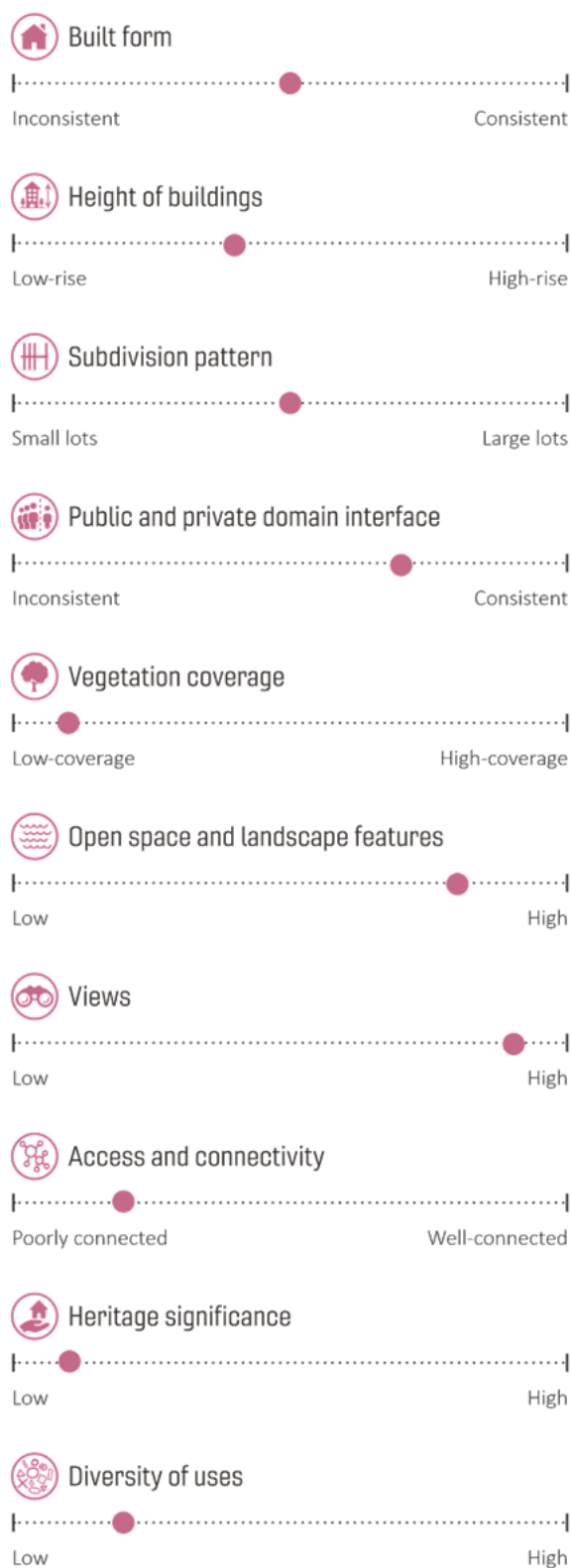


Figure 17-3 - RFBs, Hastings Parade



Figure 17-6 - Ramsgate Avenue centre and beach frontage

Existing character attributes



Desired future character

Vision

The desired future character for the area is defined by its prominent headland location forming a key part of the iconic view of Bondi Beach. Development is characterised by design excellence and vegetation that is appropriate to the natural setting. Key views from the public domain and retained and celebrated.

Objectives

The objectives for development on land identified within the character area are as follows:

- To maintain the headland character of Ben Buckler through the landscaping of front setbacks and street verges,
- To retain and protect the visibility of sandstone cliffs and outcrops by integrating the landscaping of the adjoining public and private areas,
- To retain the current lot sizes as an import feature of this area. Lot amalgamations are discouraged,
- To reinforce the prevailing street pattern of rectilinear, boxy proportioned building forms, architectural elements and range of materials and finishes,
- To ensure side setbacks maintain glimpses of the beach or ocean,
- To ensure high-quality design of new contemporary development with well-articulated forms, rooflines and materiality. Flat roofs are supported for view sharing,
- To promote sympathetic alterations and additions that respect the form, architectural style, height, materials and details of existing dwellings,
- To maintain views and vistas from the public domain,
- To minimise the impact on existing views and vistas from the private domain and maintain residential amenity in terms of overlooking and noise,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Integrate carparking within new dwellings.

148 Existing character description

History

Development of the area has evolved from the second land grant within the Waverley municipality. In 1809, 30 acres, extending from the coastline to Wairoa Avenue was granted to John Hurd. However, the grant was not confirmed and legal confusion over the claim for ownership resulted in the land remaining undeveloped. In 1841, a lease was finally re-granted to Parry Long, who retained the land as an investment. Subdivision of the early grant for estate development, including the Queenscliff Estate, occurred around the 1880s. The Federation period saw the subdivision of the estates and housing construction begin, albeit slowly. The popularity of Bondi Beach in the early 1900s and transport improvements, including the extension of the tramline to the North Bondi terminus in 1929, spurred development of the area during the late-Federation and Inter-War periods. The area remains predominantly residential with supporting retail and commercial businesses within the North Bondi neighbourhood centre and Ramsgate Avenue (east).



- LEGEND
- Study area boundary
 - Suburb boundary
 - Cadastral boundary
 - Building footprint
 - Open space
 - Village Centre
 - Urban Conservation Area
 - Landscape Conservation Area
 - General heritage item
 - Archaeological heritage item
 - Archaeological heritage site

Figure 17-7 - Built form, uses and heritage area #17

Configuration, connectivity and built form

The character of the area is defined by its prominent headland location. Exposed rock platforms and steep sandstone cliffs rise from the ocean, forming a rocky plateau which slopes gently to the west. The area's elevated location offers panoramic views of the coastline, ocean and surrounding areas. Topped with densely set built form, the area provides a distinctive backdrop to the iconic Bondi Beach.

Uniform subdivisions are aligned along parallel streets extending from Campbell Parade and terminating at the coast. These street corridors are important view axes, revealing and framing vistas of the coastline and beach. Lots are predominantly orientated east-west. The configuration of internal north-south streets varies, providing for 1 and / or 2-way traffic along loop and no-through streets, which reduces access and circulation. East-west access is limited to Bay Street, which provides for 1-way traffic only. Pedestrian connectivity is maintained along the street network and through-block links between Brighton Boulevard and Ramsgate Avenue. Campbell Parade provides the primary access to and from the area and supports an on-street cycleway and bus route. Bus services connect the area to the northern suburbs of the LGA, Bondi Road, Bondi Junction and Sydney CBD.



- LEGEND
- Study area boundary
 - Suburb boundary
 - Road network
 - Contour (2m intervals)
 - Open space
 - Bus routes
 - Bus stops
 - Bicycle routes
 - Pedestrian connections
 - Coastal walk
 - View corridor

Figure 17-8 - Road network and movement area #17

The area is characterised by a variety of dwelling typologies and styles. The dominant typologies are detached dwellings and residential flat buildings (RFBs) of Inter-War, Mid-century Modern and 21st century styles. These typologies are interspersed with semi-detached dwellings (including dual occupancy) and shop-top housing, of the same predominant styles. Dwellings are predominantly 2-4 storeys in height. Inter-War and Mid-century Modern style RFBs responded to the rising popularity of the area for housing from the 1920s. Many replaced earlier Federation cottages, others occupied vacant sites on the periphery of the area overlooking the beach. These styles are characterised by largely boxy proportioned buildings of brick and painted masonry punctuated by glazed openings and hipped tile roofs (see Figure 17-2). Façade articulation varies, for example Art Deco brickwork. The dominance of 21st century-style dwellings reflects the ongoing redevelopment of the area. Contemporary dwellings retain the largely boxy form, with painted masonry, floor-to-ceiling glass windows, linear elements, incorporating natural materials e.g. timber battens, and a combination of pitched, flat or architectural roofscapes (see Figure 17-13). Varied styles are unified by the orientation of balconies, decks and windows towards the coastline. The area has a population density of approximately 66 persons/ha (density increases to the east). Private rental is the dominant tenure type.



Figure 17-9 - Dwelling typologies area #17



Figure 17-10 - RFBs, Ramsgate Avenue



Figure 17-11 - RFBs, Brighton Boulevard



Figure 17-12 - RFBs, Hastings Parade



Figure 17-13 - Detached dwellings, Hastings Parade

150 Existing character description cont.



Figure 17-14 - Architectural styles area #17

Public and private domain interface

The public and private domain interface varies across the area. Wider (Brighton Boulevard) versus narrower (Hastings Parade and Ramsgate Avenue (see Figure 17-5)) street carriageways, verges and footpaths influence the physical distance and visual transition between the domains. The varying width of the street verge and the prevalence of wide driveway crossovers results in sparse street trees. On-street parking and parking on driveway crossovers creates visual clutter in the streetscape. Cross-falls to the west result in a high and low side of the street. Dwellings on the high sides are typically highly visible, raised above the public domain, with built retaining walls, high fences and / or garage doors of varying materials and finishes, fronting the street, for example Hastings Parade (see Figure 17-13). Front landscaping also varies. Upper balconies typically overlook the public domain and are oriented to provide expansive views to the ocean. Dwellings on the low sides often have their ground level partially or completely obscured from the street as a result of the sloping topography, vegetation, fencing and / or garages at the boundary, for example Brighton Boulevard. Front setbacks are inconsistent across the area, however encroachment into the front setback by parking structures including car ports and enclosed garages is common. Dwellings are closely set, with

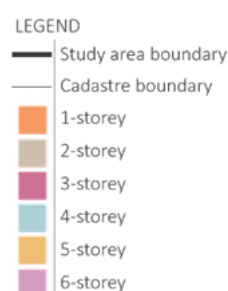


Figure 17-15 - Building heights in storeys area #17

glimpses out to the ocean and sky available through narrow side setbacks (see Figure 17-2 and Figure 17-10). This contributes to a sense of enclosure. Viewed from Bondi Beach, the area presents as a dense wall of layered built form.

Natural environment

The area benefits from access to substantial public open space including Ray O'Keefe Reserve, Sam Fisman Park (see Figure 17-19), Biddigal Reserve and Bondi Beach, which form part of the continuous coastal open space network and offer passive and active recreational opportunities. The area is recognised for its long history of seaside bathing and leisure. In 1903, Bondi Beach was provided with its first daytime bathing facilities and in 1911 the first bathing shed opened. The North Bondi Surf Life Saving Club (SLSC) and the Bondi SLSC club opened in 1906 and 1907. The headland has significant aesthetic, landscape and archaeological value. These areas support a biodiversity corridor. Vegetation coverage across the area is low, characteristic of the elevated and exposed coastal location and the need to maintain views. The elevated location provides for prevailing coastal winds from the east, which combined with the open character results in low-medium heat vulnerability.



Figure 17-16 - Open space and vegetation #17



Figure 17-17 - Ramsgate Avenue to Brighton Boulevard link



Figure 17-18 - Sam Fisman Park, looking west to Bondi Beach

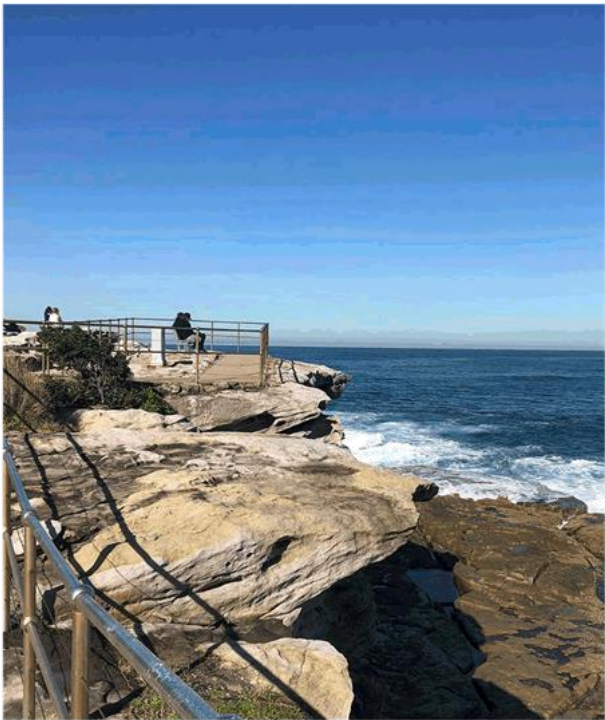



Figure 17-19 - Coastal lookout, Sam Fisman Park

Recent development

Recent development history includes alterations and additions to existing dwellings, including older RFBs such as materiality changes and balcony additions; construction of 2-3 storey detached or semi-detached dwellings of contemporary forms, with integrated garages. Indicating an anecdotal desire to maintain existing dwellings and a demand for more floor space and carparking.



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

18



CHARACTER AREA 18 - NORTH BONDI

"The area is characterised by blocks set in two irregular grids comprising predominantly low-rise detached, semi-detached dwellings and residential flat buildings, tiled roofscapes, varying front setback depths, front landscaping, fences and parking structures, and low vegetation coverage".

The study area is generally bound by Murrivier Road to the north, the Pacific Ocean and Campbell Parade to the east, Campbell Parade and Warners Avenue to the south and Plowman Street to the west.



Figure 18-1 - Local character area #18 map



Figure 18-4 - Inter-War RFB, O'Donnell Street



Figure 18-2 - Semi-detached dwellings, O'Donnell Street



Figure 18-5 - RFB, Blair Street

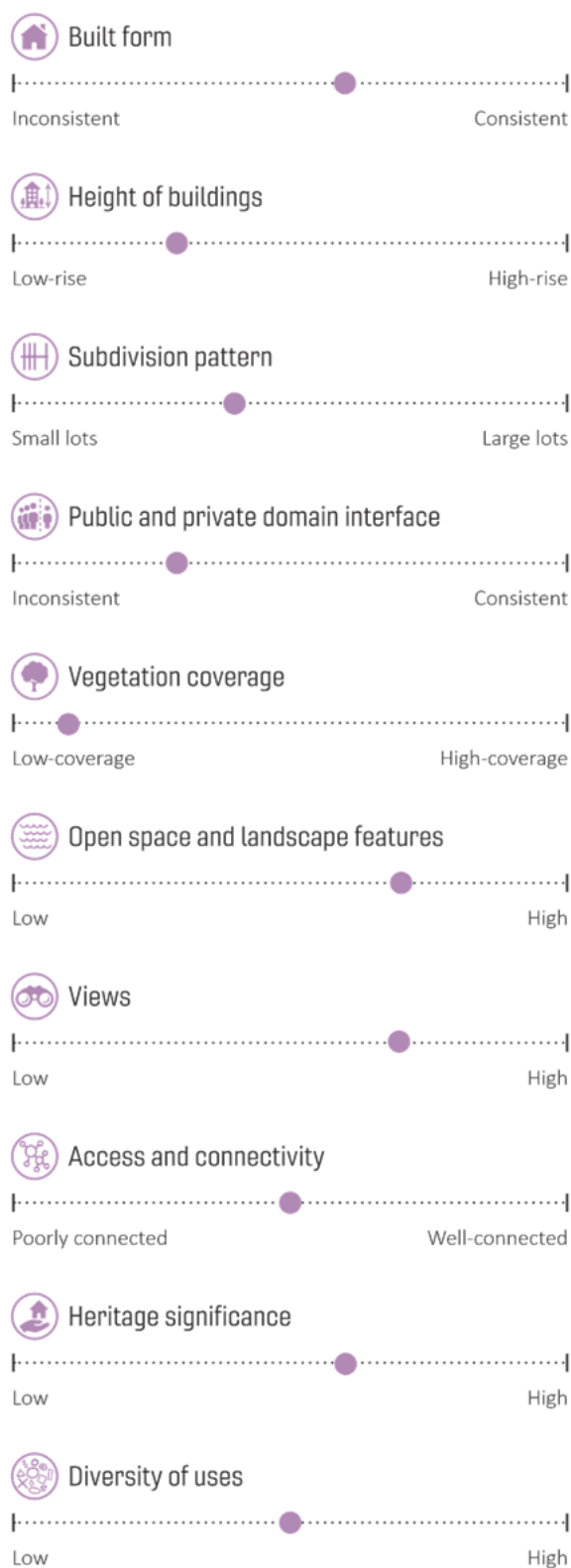


Figure 18-3 - Semi-detached dwellings, Mitchell Street



Figure 18-6 - Inter-War RFB, Ramsgate Avenue

Existing character attributes



Desired future character

Vision

The desired future character for the area is defined by low-rise residential development within easy walking distance of neighbourhood shops and services, the iconic coastline and Bondi Beach.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area,
- To promote the diversity of dwelling typologies in the area, including semi-detached (dual occupancy),
- To ensure Inter-War-style dwellings are retained and conserved,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality,
- To maintain the appearance of semi-detached development as one of a pair, demonstrating consistent scale, style and materiality,
- To ensure new development respects the historic patterns, original built form, architectural styles, materials and details of the area,
- To reinforce the existing pitched roofscape and promote consistency in roofing materials,
- To retain and extend street tree plantings on all streets to enhance the streetscape character,
- To promote the reduction of hard surfaces and an increase of landscaping in the front, rear and side of properties,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Integrate carparking within new dwellings,
- To discourage new driveway crossovers,
- To ensure the safety of pedestrians along the coastal cliffscape.

156 Existing character description

History

The area encompasses lands originally within and adjacent to the first two land grants within the Waverley municipality, being 200-acres between OSHR and Bondi Beach, granted to William Roberts in 1810 and 30-acres extending from the coastline to Wairoa Avenue granted to John Hurd in 1809. The area, characterised by mobile sand dunes interspersed with hinterland lagoons surrounded by windswept vegetation on higher ground, was used for agricultural purposes and remained largely undeveloped until the 20th century. Remediation and removal of sand occurred, with the allocation of State funding, between 1900 and 1920, providing land for the subsequent Inter-War housing boom. The 1920s saw rapid expansion of housing across the area driven by population growth, a post-war economic boom, speculative building of residential flat buildings (RFBs) in response to housing shortages, the new Company Title land title system (providing separate ownership of apartments) and the extensive tram transport network (the Bondi tramline was extended to North Bondi in 1926). The speed of subdivision and construction saw consistent streetscapes emerge. Today, the area comprises predominantly residential uses with supporting community-focused commercial and retail centres at Murrivier Road (east), Glenayr Avenue (Seven Ways), Wairoa Road and Campbell Parade, clusters of educational institutions and coastal open space.



Figure 18-8 - Inter-War RFB with addition, Ramsgate Avenue



Figure 18-9 - Inter-War RFB, Ramsgate Avenue



Figure 18-7 - Built form and uses area #18

Configuration and connectivity

The prevailing block and street pattern comprise two irregular grids separated by Blair Street. To the south of Blair Street, long streets extend north-west / south-east, with limited north-east / south-west cross streets. To the north of Blair Street block sizes reduce, with parallel north-south streets and east-west cross streets. The wide avenue of Blair Street and the cross route of Warners Avenue and Wairoa Avenue reflect the Garden Suburb philosophy. The street grid increases permeability of the area, however, multiple 1-way-only streets reduce internal vehicle access and circulation, particularly in the south-west corner. Campbell Parade, Military Road and Blair Street provide to primary connection to and from the area. Pedestrian connectivity remains high, however access to the coastline is limited. Dedicated on-street bicycle lanes are located on Campbell Parade, Wairoa Avenue and Blair Street. Multiple buses service the area providing connections to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD.

Built form

The area is characterised by a variety of dwelling typologies, including detached dwellings clustered to the east of Wairoa Avenue, semi-detached dwellings clustered to the north of Blair Street and manor homes and RFBs clustered in the north-east and south-west of the area. Dwellings are predominantly 1-4 storeys in height. The predominant architectural style is the Inter-War style, reflective of the area's rapid development during the 1920s. Inter-War sub-styles include California Bungalows, Old English 'Tudor' and Mediterranean/ Mission Style detached

and semi-detached dwellings, Art-Deco, Inter-War Georgian and Mediterranean (see Figure 18-9) style RFBs. Streetscapes of Inter-War dwellings are evident along Campbell Parade (forming part of the Bondi Beach Conservation Area), Ramsgate Avenue (see Figure 18-6 and Figure 18-8), Brighton Boulevard west (forming the Brighton Boulevard Conservation Area), Knowles Avenue, Middleton Avenue and O'Donnell Street (see Figure 18-4) and Oakley Road. Dominance of this style creates an aesthetic of face-brick exteriors with terra cotta tile roofs. Dwellings have been modified overtime. Modifications include, second-storey and garage additions. Inter-War styles are interspersed with earlier Federation-style dwellings and later Mid-century Modern, late 20th century and 21st century style dwellings. Prominent clusters of Mid-century and late 20th century RFBs are located along Military Road and the block bound by Blair Street (see Figure 18-5), Military Road and Wallis Parade (a former quarry). The area has a population density of approximately 103 persons/ha.

157



Figure 18-11 - Semi-detached dwellings, Glenayr Avenue



Figure 18-10 - Road network and movement #18

158 Existing character description cont.

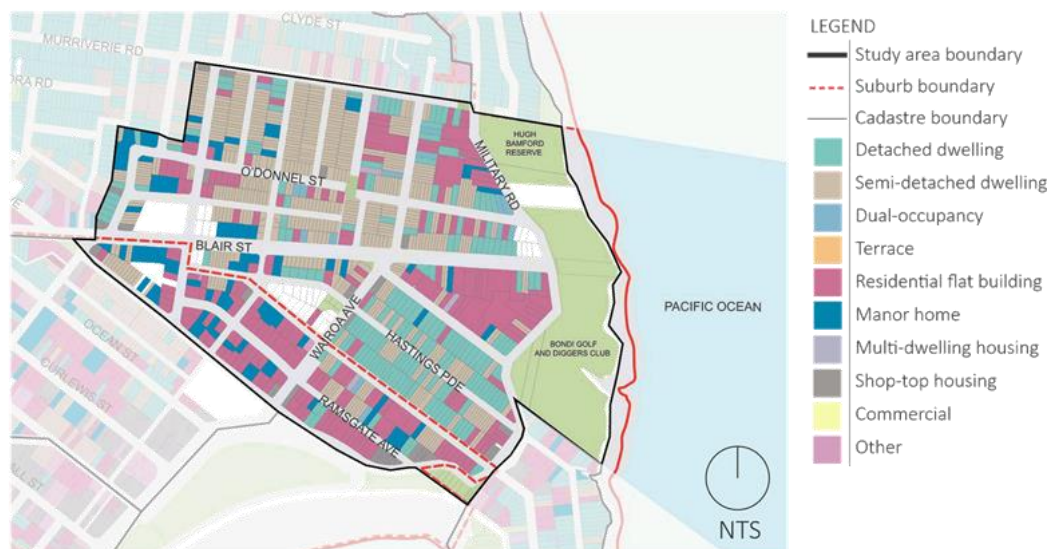


Figure 18-12 - Dwelling typologies area #18



Figure 18-13 - Architectural styles area #18

Public and private domain interface

Varying street corridor and verge widths change the physical and visual transition between the public and private domains. Limited on-street planting also influences this transition, increasing visibility of the private domain. The interface between the domains varies across the area, as a result of differing dwelling typologies, front setback depths, front landscaping, fence treatment and parking structures. Detached and semi-detached dwellings typically comprise a consistent front setback of narrow-average depth. The visibility of the dwelling varies, however, due to differing fence treatment (low, high, solid, picket or

railing, brick, stone, vegetation) and front landscaping, for example Mitchell and Gould streets. Hard-stand parking, carports or enclosed garages at the boundary further obscure the private domain and detract from the generally consistent building alignment, for example Middleton Avenue and Hastings Parade (east). Inter-War manor homes and RFBs typically comprise narrow-front setbacks and limited landscaping, with a solid brick façade (facades types vary), small window openings and entry presenting to the street. Closely set RFBs can result in a continuous wall of built form, for example Ramsgate Avenue. Mid-late 20th century RFBs typically comprise greater front

setbacks and are raised above street level with upper balconies overlooking the public domain, for example Wallis Parade. Hard-stand parking areas, enclosed garages and limited planting characterise street level. Through local centres varying uses (community, retail, commercial, cafes) abut the public domain, creating a highly visible, active frontage.

Natural environment

Open space is limited within the area comprising pocket parks O'Donnell Street Reserve and Wairoa Reserve. On the area's eastern boundary Hugh Bamford Reserve and the Bondi Golf and Diggers Club (Williams Park) contribute to the coastal open space network, which is of significant aesthetic, landscape and recreational value. The heritage-listed area's comprise Aboriginal and European archaeological sites, remnant vegetation and a striking natural coastal cliffscape, for example Meriverie Pass. The coastal open space network, including Bondi Beach supports passive and active recreation opportunities and support public gathering and events. The area is recognised for its long history of seaside bathing and leisure. Vegetation coverage is low across the area due to its sandy soil composition and prevailing coastal winds. The street verge typically comprises sporadic low planting. Private planting is typically restricted to the rear yard, however, and is limited due to closely set development. These factors result in a high heat vulnerability across the area.

Recent development

Recent development comprises alterations and additions to detached and semi-detached dwellings, including second-storey and attic additions, materiality



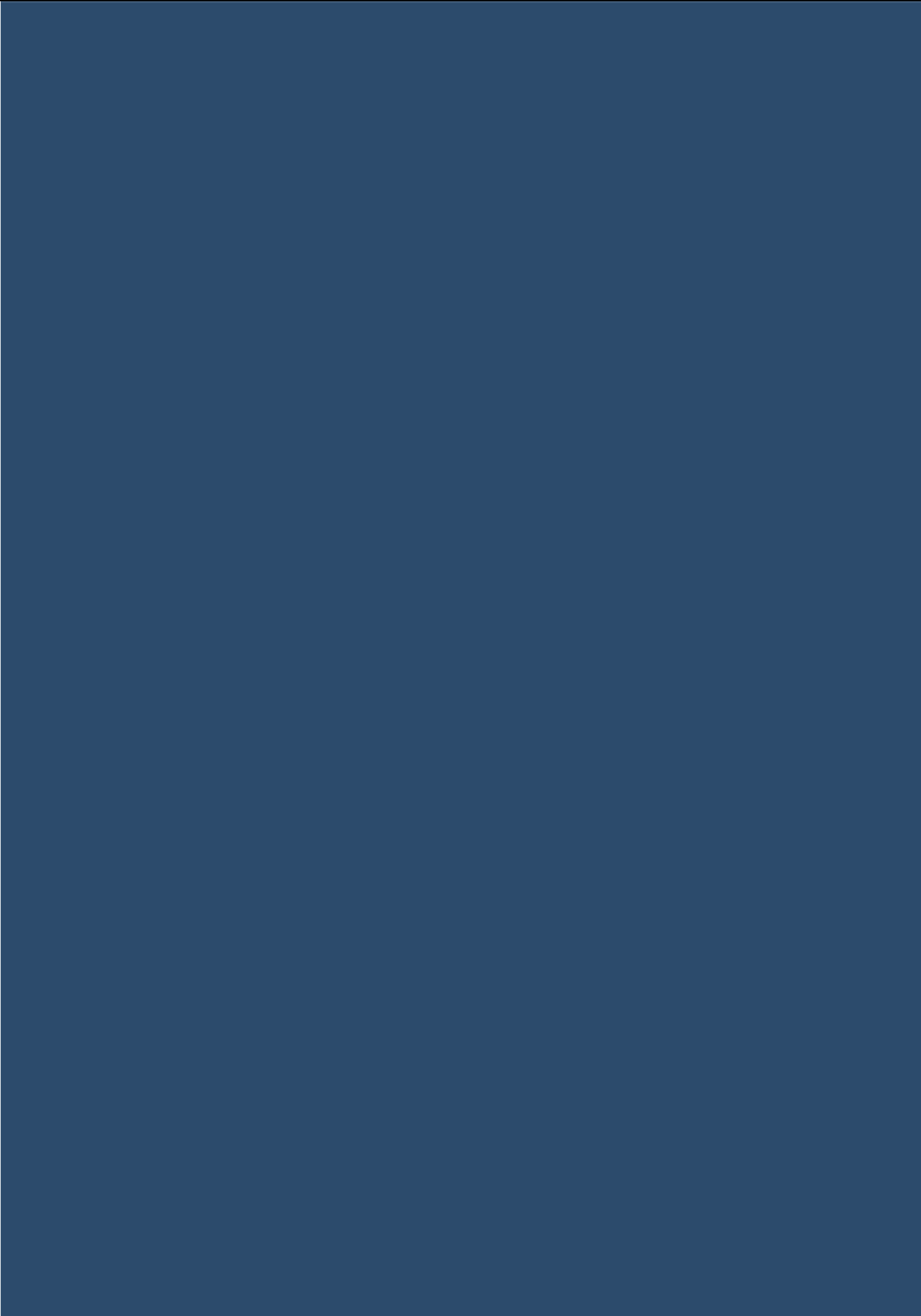
Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?

changes, hard-stand parking space, car ports, garages (with or without associated drive-way crossover); alterations and additions to older RFBs; construction of 2-3 storey detached or semi-detached dwellings (including dual occupancy); and limited amalgamation, demolition of existing dwellings and construction of RFBs. Suggesting a desire to maintain existing dwellings and a demand for more floor space and carparking. It is also noted that North Bondi has the third highest proportion of dwellings by suburb listed on Airbnb within Australia, indicating a trend of use of private dwellings for short-term rental accommodation.



Figure 18-14 - Open space, vegetation and heritage area #18



CHARACTER AREA

19



CHARACTER AREA – 19

“The area is defined by predominantly low-rise detached and semi-detached dwellings of varying styles, however with a common pitched roof form, wide street corridors with sporadic low plantings, inconsistent front setbacks with varying fence treatments, landscaping and carparking structures”.

The study area is generally bound by William Street, Hardy Street and MacLeay Street to the north, Military Road to the east, Murrivier Road to Blair Street to the south and Old South Head Road (OSHR) to the west.

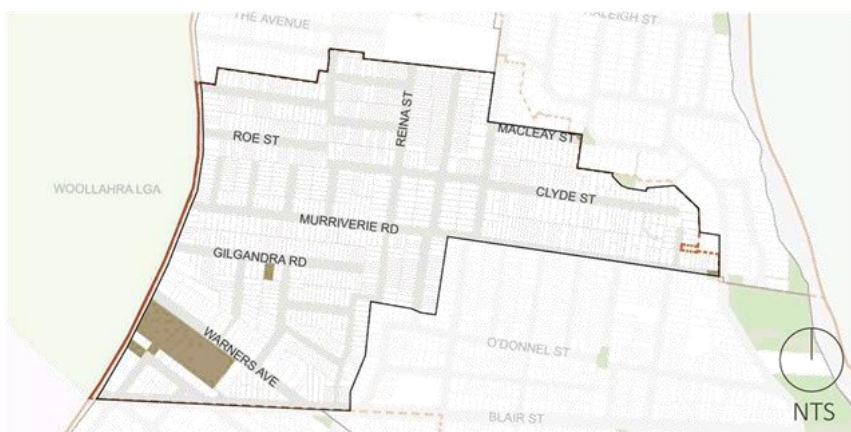


Figure 19-1 - Local character area #19 map



Figure 19-4 - Stewart Street dwellings



Figure 19-2 - Roofscape, viewed from Gilgandra Road, looking east



Figure 19-5 - Owen Street dwellings

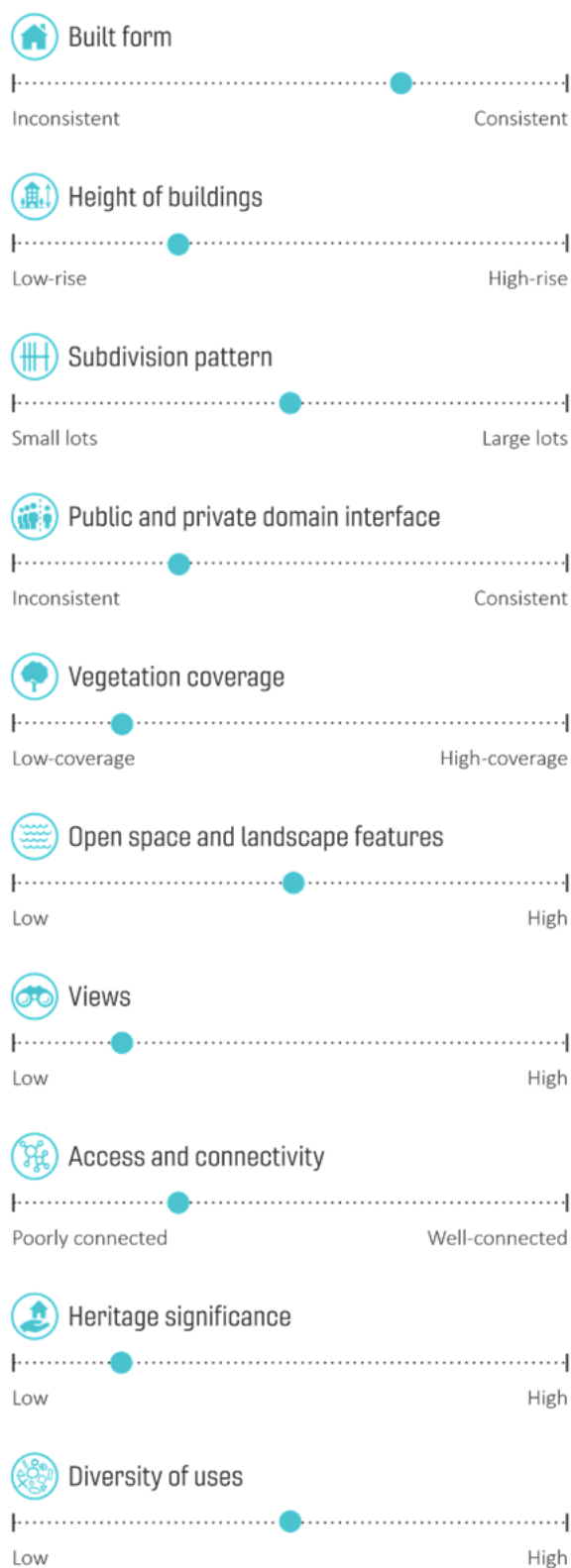


Figure 19-3 - Streetscape, Nancy Street



Figure 19-6 - Roe Street dwellings

Existing character attributes



Desired future character

Vision

The desired future character for the area is defined by wide leafy residential streets, with an interesting and varied built form.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area,
- To promote the retention of urban services in the area and manage the interface between urban services and residential development,
- To maintain the predominant low-rise detached and semi-detached dwelling typologies in the area,
- To maintain the consistent setbacks / building alignments in the area,
- To encourage the conservation of Federation and Inter-War-style dwellings,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality,
- To maintain the appearance of semi-detached development as one of a pair, demonstrating consistent scale, style and materiality,
- To ensure high-quality design of contemporary development with well-articulated forms, rooflines and materiality,
- To encourage high-quality, contemporary, low rise development at the western edges of the area, fronting OSHR. Consider site amalgamation where appropriate,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Integrate carparking within new dwellings, and discourage carparking structures in front setback unless the predominant interface of adjoining dwellings,
- To discourage new driveway crossovers,
- To promote the reduction of hard surfaces and an increase of public and private landscaping,
- To retain and protect the visibility of sandstone cliffs and edges by integrating the landscaping of adjoining public and private spaces, for example MacLeay Street.

164 Existing character description

History

Development of the area was slow due to the area’s remoteness, the attractiveness of land to the west of OSHR in Woollahra and the proximity to the sand dunes and hinterland lagoons that characterised the Bondi Basin. Tanneries and dairies were established on OSHR just north of present day Murriverie Road as early as the 1820s. The land remained agricultural (dairies, poultry) up until the late 1890s. East-west cross-streets from OSHR, for example Murriverie Road, began to appear from the early 1890s. Housing development began during the late Federation period (1890-1915) initially along OSHR and then progressively east, during the Inter-War period (1919-1939) following the remediation and removal of the sand dunes (1900-1920). The area remains predominantly residential with supporting commercial and retail uses within the Murriverie Road (west), Curlewis Street, Glenayr Avenue (Seven Ways) local centres , religious institutions and childcare centres.

Configuration and connectivity

Irregular blocks typically extend east-west following the early cross streets from OSHR. Blair Street and OSHR provide the primary connections to and from the area. Multiple no-through streets limit inter-block connectivity and circulation throughout the area. Pedestrian connectivity is maintained with through-block links, for example between OSHR, Brassie and Niblick streets, Stewart Street to Roe Street and Clyde Street to Waratah Street. Dedicated on-street bicycle lanes are located on Blair Street and OSHR (western side). Public transport is limited to bus services along OSHR, Blair Street and Hardy Street, which provide connections to the northern and southern suburbs of the LGA, Bondi Junction and Sydney CBD.

Built form

The area is characterised by predominantly 1-2 storey detached and semi-detached dwellings of the Inter-War style, interspersed with earlier Federation-style



Figure 19-7 - Built form, uses and heritage area #19

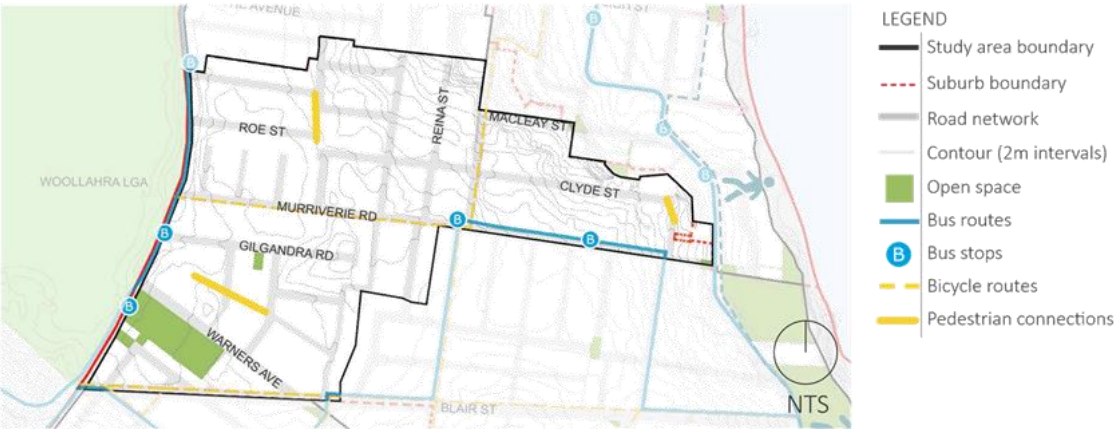


Figure 19-8 - Road network and movement area #19

and later Mid-century Modern, late 20th century and 21st century-style detached, semi-detached (including dual occupancy) dwellings and 3-4 storey residential flat buildings (RFBs). Clusters of Federation and Inter-War style bungalows are evident along Roe (see Figure 19-6), Narelle, Stewart, Reina (see Figure 19-11) and Nancy streets and Gilgandra Road. These styles are characterised by brick construction, pitched terracotta tiled roofs, with thick masonry or timber verandah posts and decorated timber gable trim. Early dwellings have been significantly modified overtime. Modifications include, second-storey and garage additions. Inter-War style RFBs (Art Deco, Georgian Revival, Spanish Mission sub-styles), are evident along Warners Avenue (see Figure 19-16) and Blair Street, are typically characterised by a solid brick façade (facades types vary), small window openings and entry presenting to the street. The concentration of Mid-century, late 20th century and 21st century styles increases to the east of the area, fronting MacLeay Street, Clyde Street and Murrivier Road. The area has a population density of approximately 81 persons/ha.



165

Figure 19-11 - Semi-detached dwellings, Reina Street

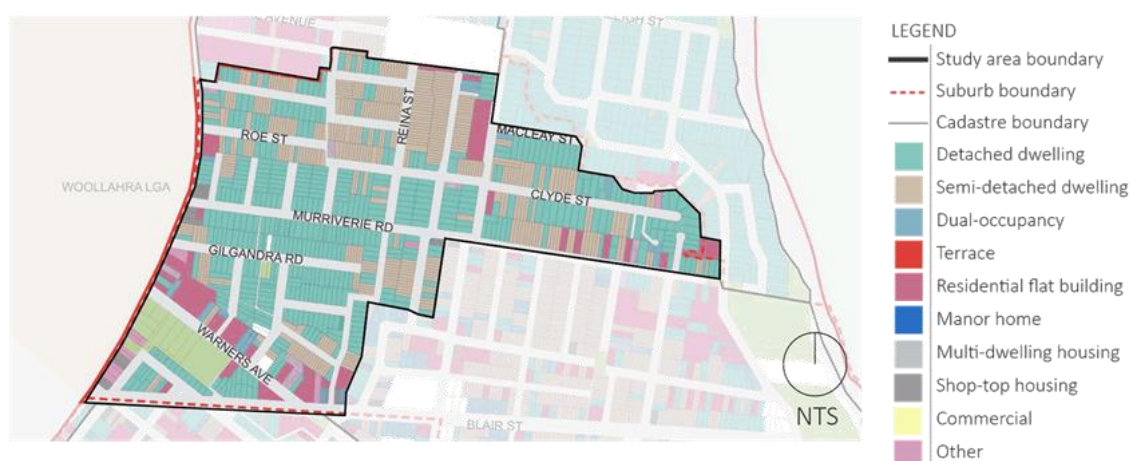


Figure 19-9 - Dwelling typologies area #19

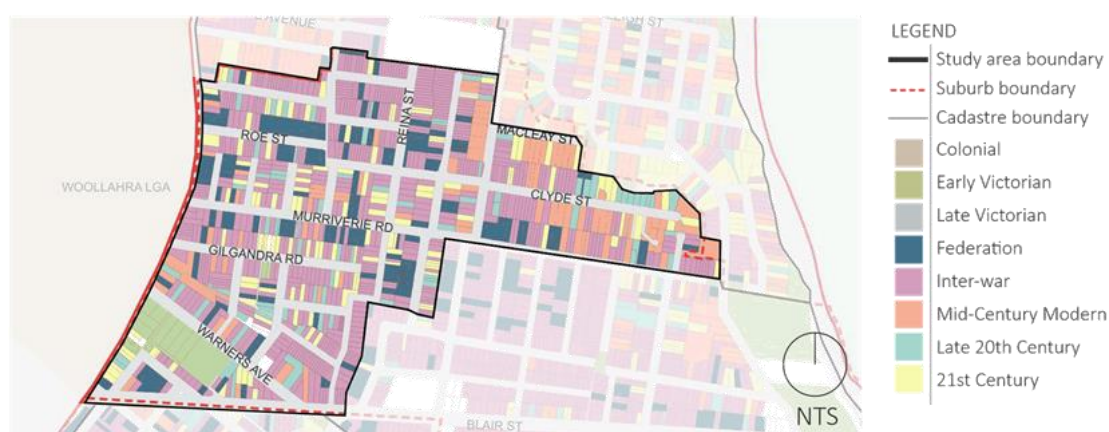


Figure 19-10 - Architectural styles area #19

Existing character description cont.

Public and private domain interface

Wide street verges provide a gradual physical and visual transition between the public and private domains. On-street planting also influences this visual transition. The interface between the domains is characterised by varying built form, materiality and landscaping. Detached and semi-detached dwellings typically comprise a consistent front setback of average depth. The visibility of the dwelling varies, however, due to differing fence treatment (low, high, solid, picket or railing, brick, stone, vegetation) and front landscaping, for example Clyde, Waratah and Nancy streets (see Figure 19-12). Street-level hard-stand parking, carports or enclosed garages at the boundary further obscure the private domain and detract from the generally consistent building alignment, for example Reina and Roe streets. Continuous garages can create a solid street wall, for example Justus Street. On sloping sites, dwellings on the high side are raised above street level, with dwellings on the lower side partially obscured from street level, for example Narelle Street and Murrivier Road (east). Inter-War RFBs also typically comprise narrow setbacks and limited landscaping, with a solid brick façade (facades vary), small window openings and entry presenting to the street. Mid-late 20th century RFBs are raised above street level incorporating ground-floor carparking, for example OSHR. Upper balconies overlook the public domain. Through the local centres, retail and commercial uses abut the public domain.



Figure 19-12 - Property frontages, Nancy Street



Figure 19-13 - Detached dwellings, Patterson Street



Figure 19-14 - Semi-detached dwellings, Owen Street



Figure 19-15 - Garage frontages, Gilgandra Road



Figure 19-16 - Inter-War RFB, Warners Avenue

Natural environment

Open space is limited within the area comprising Barracluff Park (see Figure 19-18) and pocket parks at Beach Road and Gilgandra Road. However, the area benefits from access to surrounding coastal and harbour public open space. Together with the Bondi Bowling Club and Tennis Centre these spaces offer passive and active recreation opportunities. Steep topographical changes in the north-east of the area, exposes the escarpment wall at MacLeay and Clyde Street. Distant views across the basin to the coastline are available from Hardy and Waratah streets. Vegetation coverage is predominantly low-medium across the area. The street verge typically comprises sporadic low plantings, except for the avenue planting along Warners Avenue and Blair Street (Landscape Conservation Areas). Private planting is typically restricted to the rear yard. These conditions result in a low-medium heat vulnerability across the area.

Recent development

Recent development history includes alterations and additions to detached and semi-detached dwellings, including second-storey and attic additions, materiality changes, hard-stand parking space, car ports, garages (with or without associated drive-way crossover); and construction of 2-storey detached or semi-detached dwellings (including dual occupancy). Suggesting a desire to maintain existing dwellings and a demand for more floor space and carparking.



Figure 19-17 - Open space and vegetation area #19



Figure 19-18 - Barracluff Park, looking south-east

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA 20



CHARACTER AREA - 20

"The area is characterised by a patchwork of dwelling typologies and styles, of simple geometry and pitched roofs to contemporary forms with wider modulation and flat roofs; uniform front setbacks with varying landscaping, fence materiality and height and carparking structures; district views from topographical high points".

The study area is generally bound by Lancaster Road to the north, Gilbert and Hardy streets to the east, William Street to the south and Old South Head Road (OHSR) to the west.

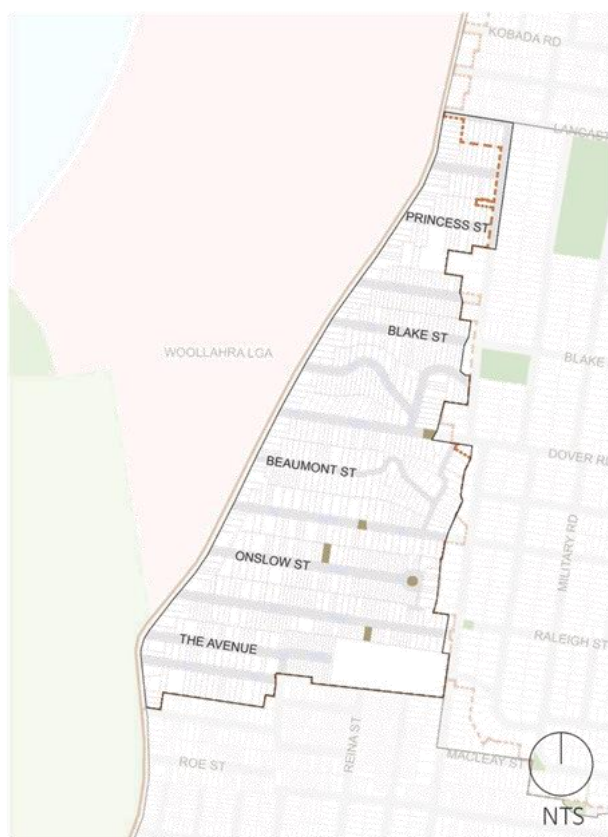


Figure 20-1 - Local character area #20 map



Figure 20-3 - Detached dwelling, Beaumont Street



Figure 20-4 - RFB, Liverpool Street

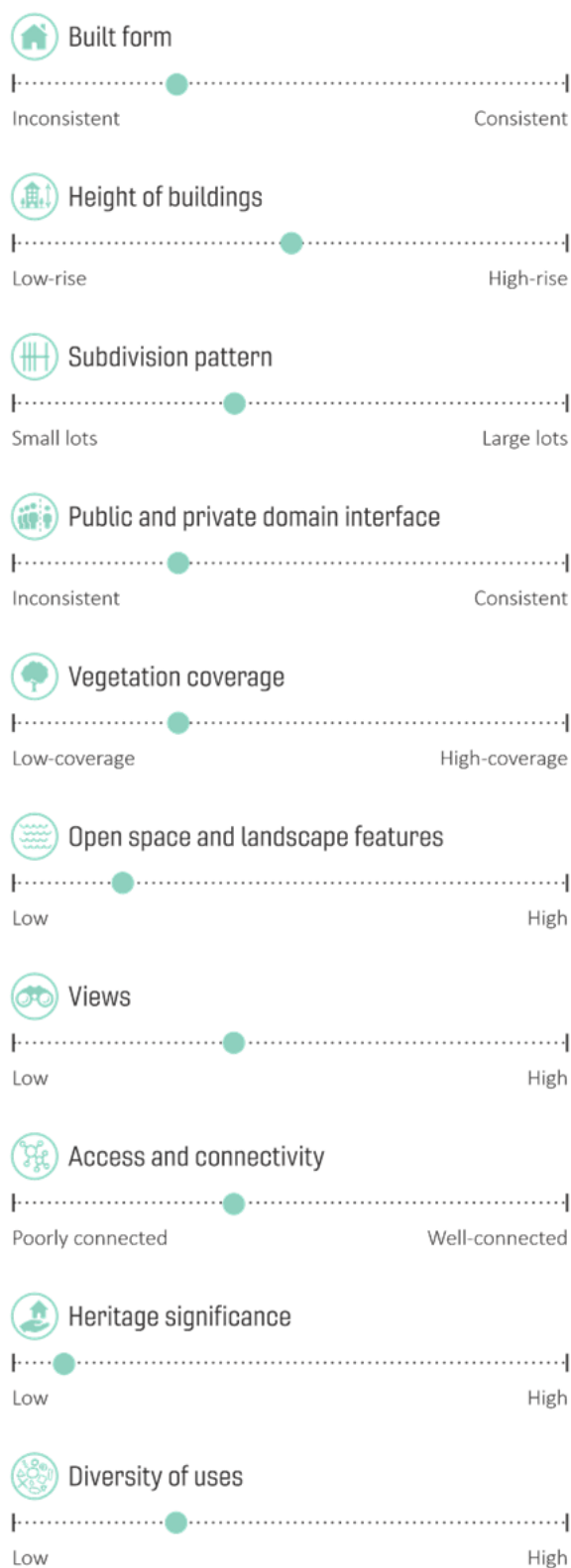


Figure 20-2 - Detached dwellings, Gilbert Street



Figure 20-5 - Liverpool Street example of east-west street corridor

Existing character attributes



Desired future character

Vision

The desired future character for the area is defined by leafy residential streets along an undulating topography, within walking distance of local shops and services available at Rose Bay South centre.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise Rose Bay South as an important centre for providing goods and services to the local community,
- To retain a fine-grain shopfront pattern to development within the Rose Bay centre,
- To promote and coordinate consistent development along the eastern and western side of the Rose Bay centre,
- To recognise and retain the diversity of uses along OSHR,
- To minimise new vehicle crossings on Old South Head Road,
- To promote sympathetic alterations and additions that respect the form, architectural style, height, materials and details of existing dwellings, particularly semi-detached dwellings,
- To ensure high-quality design of contemporary detached and semi-detached dwellings (including dual occupancy), with appropriate façade and material articulation. Flat roofs may be supported for view sharing,
- To encourage high-quality, contemporary, low rise RFB development at the western edges of the area, fronting OSHR and replacement of existing RFBs. Consider site amalgamation where appropriate,
- To retain a consistent building alignment,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape,
- To promote the reduction of hard surfaces and an increase of public and private landscaping,
- To retain and protect the visibility of sandstone cliffs and edges by integrating the landscaping of adjoining public and private spaces, for example Hardy Street and rear of properties on Onslow Place,
- To maintain and minimise the impact on views and vistas from the public and private domain.

172 Existing character description

History

Early land grants were slow to develop due to the area’s remoteness and the attractiveness of land to the west of OSHR in Woollahra. By the 1880s east-west cross streets began to appear off OSHR, which spurred housing development during the late Federation period, initially fronting OSHR, and then progressing into the escarpment, and onto former quarry sites, such as Onslow Street, during the Inter-War period. The area remains predominantly residential with supporting commercial and retail uses within the South Rose Bay village centre and educational establishments, including Rose Bay Secondary College.

Configuration and connectivity

Irregular blocks extend east-west following the early cross streets from OSHR across the prevailing east-west slope. Subdivision has resulted in predominantly north-south facing lots of varying sizes, which contrasts the configuration of Dover Heights to the east. Steep topographical changes south of Blake Street result in terraced sites, curvilinear and no-through streets, for example Dover Road, Liverpool Street and Onslow Street. OSHR, Hardy and Gilbert streets provide the primary north-south connection to, from and through the area. Internal north-south connections are minimal. Pedestrian access and connectivity are maintained with through-block links, for example Chaley Street to Onslow Street and Onslow Street to Liverpool Street. Public transport is limited to bus services along OSHR, Victory Street, Dover Road and Hardy Street.



Figure 20-6 - Built form, uses and heritage area #20

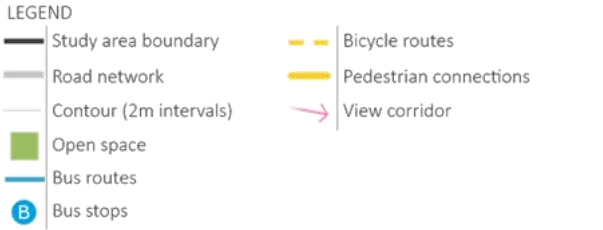
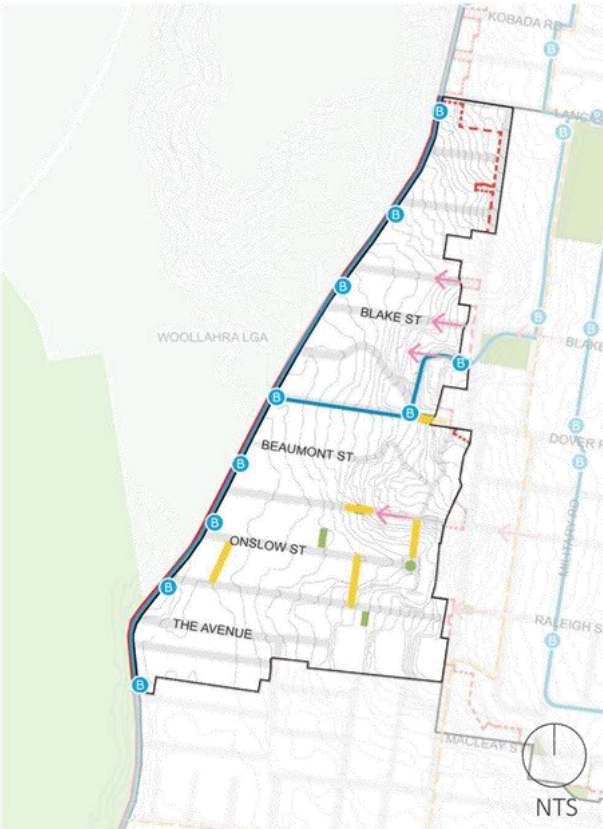
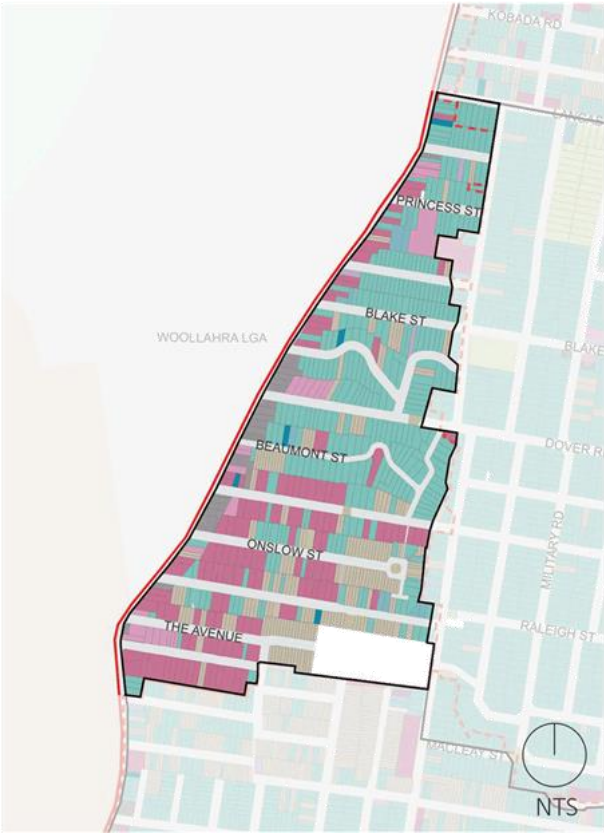


Figure 20-7 - Road network and movement area #20

Built form

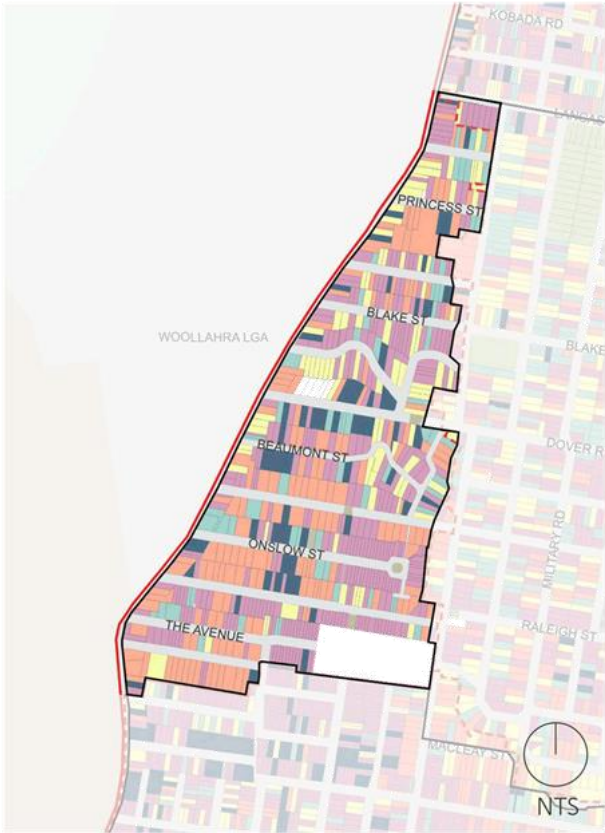
The area is characterised by a patchwork of dwelling typologies and styles. The area to the north of Liverpool Street is characterised by 1-3 storey detached dwellings located on cross streets, and 1-4 storey residential flat buildings (RFBs) and shop-top housing fronting OSHR and through the South Rose Bay village centre. Styles include Federation and Inter-War style detached and semi-detached bungalows, Mid-century style RFBs and newer late 20th century (see Figure 20-12) and 21st century detached dwellings, which reflects recent and ongoing redevelopment of the area. The area to the south of Liverpool Street is characterised by a high concentration of 1-2 storey Inter-War style detached

and semi-detached dwellings and 3-4 storey Mid-century to late 20th century RFBs (see Figure 20-4). RFB development resulted in the erosion of previously cohesive Inter-War streetscapes. Earlier architectural styles (pre-Mid-century) are defined by simple geometry, rendered or un-rendered red or light-coloured brick, terracotta pitched roofs, and varying façade detailing. Despite noticeable styles, early dwellings have been significantly modified overtime. Modifications include, second-storey and garage additions. Later contemporary architectural styles are defined by flat-roofed, contemporary forms with wider modulation, wide integrated garages, large balconies and areas of transparent glass for doors, windows and balcony balustrades (see Figure 20-2).



- LEGEND
- | | |
|---------------------------|------------------|
| Study area boundary | Shop-top housing |
| Cadastre boundary | Commercial |
| Detached dwelling | Other |
| Semi-detached dwelling | |
| Dual-occupancy | |
| Terrace | |
| Residential flat building | |
| Manor home | |
| Multi-dwelling housing | |

Figure 20-8 - Dwelling typologies area #20



- LEGEND
- | |
|---------------------|
| Study area boundary |
| Cadastre boundary |
| Colonial |
| Early Victorian |
| Late Victorian |
| Federation |
| Inter-war |
| Mid-Century Modern |
| Late 20th Century |
| 21st Century |

Figure 20-9 - Architectural styles area #20

174 Existing character description cont.

Public and private domain interface

The public and private domain interface varies across the area, due to different street corridor widths, topographical changes and dwelling typologies and styles. Wider versus narrower street carriageways, verges and footpaths influence the physical distance and visual transition between the domains. The varying capacity of the street verge to accommodate trees also influences this transition, creating a sense of openness or enclosure within the street corridor, for example Victory Street versus Onslow Street. The verge is punctuated by driveway crossovers due to the prevalence of on-site parking within the area. Dwellings tend to have a uniform front setback, however, varying front landscaping, fence materiality and height influence the visibility of the dwelling from the public domain, for example Strickland and Beaumont streets (see Figure 20-11). Encroachment into the front setback by parking structures including car ports and enclosed garages is common and further obscures the dwelling from the public domain, for example Northcote Street. This can also result in a continuous wall of garages. On sloping sites dwellings are raised above the street level, with built retaining walls, high fences and garage doors typically fronting the street, with the dwelling setback further within the lot, for example Victory Street. Front landscaping varies. RFBs are typically raised above street level, with ground-level carparking and upper balconies overlooking the public domain (see Figure 20-10).



Figure 20-10 - RFB, Chaleyer Street



Figure 20-11 - Street corridor, Beaumont Street



Figure 20-12 - Semi-detached dwellings, Onslow Street



Figure 20-13 - Detached dwellings, Victory Street



Figure 20-14 - RFB, Chaleyer Street

Natural environment

Topographical high points in the north and east of the area provide district views from the public and private domain to the harbour and Sydney CBD, for example Hardy Street and Liverpool Street. Open space is limited within the area comprising pocket parks at Dover Road and Onslow Street, however the area benefits from access to surrounding coastal and harbour public open space. Walkability to these areas is influenced by topographical changes. Stone retaining walls and exposed sandstone bedrock are prominent landscape features. Vegetation coverage across the area varies. Within the private domain coverage is typically low-medium, this is largely due to high site coverage and prevalence of pools. Within the public domain, coverage remains low in the north of the area, likely due to varying verge widths and to need to maintain views. Coverage increases within the south of the area. These conditions result in a low-medium heat vulnerability.

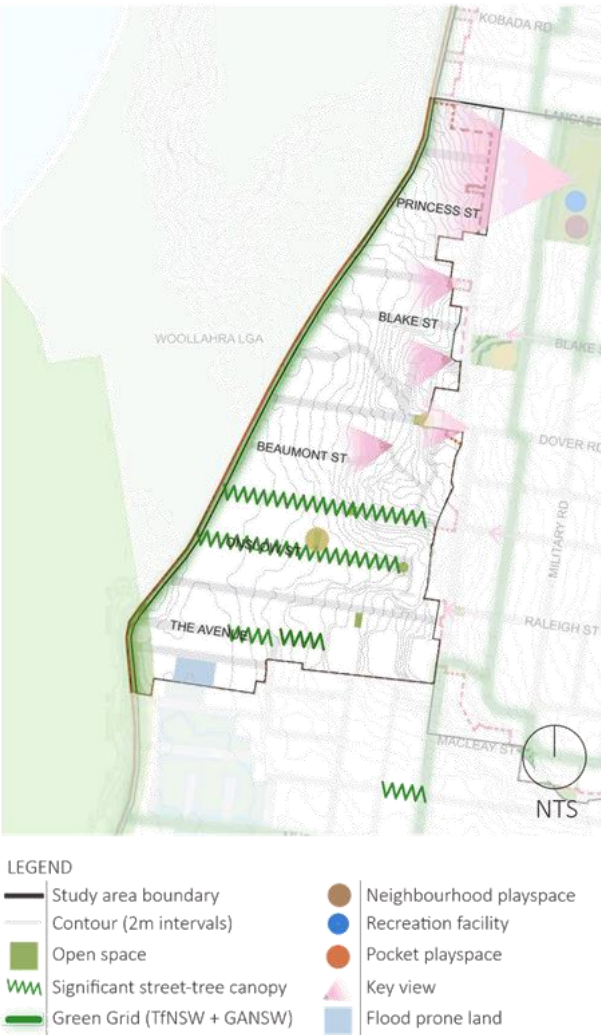


Figure 20-15 - Open space and vegetation area #20



Figure 20-16 - Bangalla Road, looking west to Sydney CBD

Recent development

Recent development history comprises alterations and additions of existing dwellings, including second-storey, car ports and materiality changes; construction of 2-storey dwellings of contemporary forms with integrated garages; limited amalgamation and construction of RFBs and town houses, particularly along OSHR; and limited strata consolidation. Indicating an anecdotal demand for newer dwellings, more floor space and more on-site carparking in the area.

Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

21



CHARACTER AREA - 21

"The area is characterised by its elevated coastal location offering panoramic views; wide streets arranged in a grid pattern, comprising 1-3 storey detached and semi-detached dwellings of varying architectural styles; street-level garages and low landscaping; and significant public open space".

The study area is bound by Lancaster Road to the north, Gilbert and Hardy Streets to the west, MacLeay Street, Loombah Road and Military Road to the south, and the Pacific Ocean to the east.



Figure 21-1 - Local character area #21 map



Figure 21-3 - Semi-detached dwellings, Hunter Street



Figure 21-4 - Lyons Street, looking east

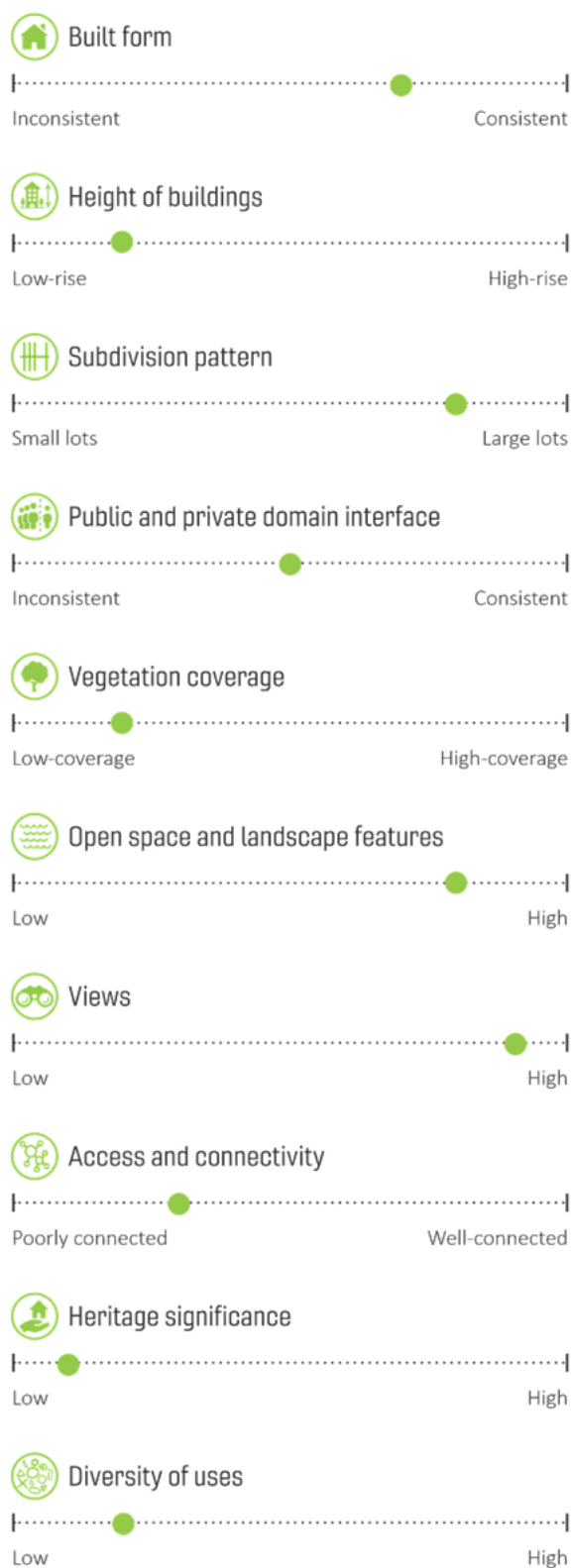


Figure 21-2 - Raleigh Reserve, looking south from Liverpool Street



Figure 21-5 - Detached dwellings, Portland Street

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by wide street corridors with regularly placed, low street trees, framing views to the ocean; consistent building line with front setbacks comprising low landscaping, to soften the visual transition between the public and private domains; well-designed low-rise dwellings of contemporary forms; integrated garages that do not dominate the streetscape or dwelling; and shared view opportunities.

Objectives

The objectives for development on land identified within the character area are as follows:

- To maintain the predominant 2-3 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography and avoid excessive excavation,
- To ensure that subdivision or amalgamation respects the predominant development pattern of the area,
- To ensure high-quality design of contemporary detached and semi-detached dwellings (including dual occupancy), with appropriate façade and material articulation. Flat roofs are supported for view sharing,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality,
- To maintain a consistent front setback (including upper floors) to preserve the wide street corridors, reinforce views to coastline and allow for landscaping to soften the transition from public to private domains,
- To ensure that car parking structures do not dominate or adversely impact upon the streetscape,
- To maintain and minimise the impact on views and vistas from the public and private domain,
- To minimise development density along the cliff-front to reduce risk from coastal processes and climate change,
- To ensure the safety of pedestrians along the coastal cliff,
- To promote the reduction of hard surfaces and an increase of public and private landscaping.

Existing character description

History

Population and development of the area was minimal until the 1880s when cross routes from Old South Head Road began to develop. The Federation period saw subdivision and housing construction begin, albeit slowly. Transport improvements resulted in extensive development through the Inter-War and Post-War periods. More recently extensive amalgamation and replacement of older properties has occurred, particularly on the escarpment. The area is predominantly residential with supporting commercial uses, educational and religious institutions, including Kesser Torah College. The Blake Street local centre comprises a mix of local and community-focused retail and hospitality uses.

Configuration and connectivity

The area is situated on a coastal plateau, sloping gently to the west and south, becoming steeper on the western edge at Hardy Street. The area's elevated position provides panoramic views from the public and private domain to the Pacific Ocean, coastal cliffs, Sydney CBD and surrounding area.

Wide streets are typically arranged in a rectangular grid pattern, with the long axis in the north-south direction. Block size decreases south of Dover Road, providing greater north-south connectivity. Streets cater for predominantly local traffic.

The area has a mostly uniform subdivision pattern consisting of large (>500m²) rectangular lots, of predominantly east-west orientation, except for the lots adjoining east-west street corridors which terminate at the coastline. These corridors are important view axis', framing ocean views and providing a sense of direction and orientation. South of Raleigh Street the street layout and subdivision pattern are more irregular, reflecting the local topography.



Figure 21-6 - Views west from Caffyn Park

Built form

The area is characterised by predominantly 1-3 storey detached and semi-detached dwellings of varying architectural styles, from Inter-War to Mid-Century to 21st Century styles (see Figure 21-12). Older, Inter-War to Mid-Century style dwellings are typically of simple geometry with terracotta pitched roofs, masonry and fine modulation such as semi-circular bay windows, round fillet walls, influenced by the Art Deco period. Exemplar buildings include heritage listed 14 Aboukir Street and 2 Lord Howe Street. Newer dwellings are typically 3-storey flat-roofed cascading buildings with wider modulation, wide integrated garages, large balconies and areas of transparent glass for doors, windows and balcony balustrades. Some newer dwellings comprise complex and curvilinear forms introducing a new aesthetic. The population density is approx. 40 persons/ha. The dominant tenure type is home ownership, with low private rental.

Public and private domain interface

Dwellings tend to have a uniform front setback, enhancing the wider street corridor and open landscape character. Low planting and front fences aid surveillance and visual transition from street to dwelling.



Figure 21-7 - Detached dwelling frontages, Portland Street



Figure 21-8 - Hunter Street, looking east

On steep sites landscaping is often used to form a soft transition from the street to elevated dwellings (see Figure 21-5).

The prevalence of wide garages, often built to the front boundary or within the front setback, limits front landscaping, obscures the building façade and detracts from the public-private transition. The dominance of garages is attributable to <30 lots having no private parking, reflective of car ownership rates now and when the area developed and lack of public transport connectivity (bus only).

Natural environment

The area benefits from significant public open space including the Dover Heights Coastal Reserves, Hugh Bramford Reserve, Dudley Page Reserve and Caffyn Park which, forming part of the continuous coastal open space network, offer passive and active recreation opportunities. The Coastal Reserves hold Indigenous and European archaeological significance. The area is generally void of any remnant native vegetation except for some low coastal shrubs and grasses along the coastal reserves, which supports a biodiversity habitat corridor. Caffyn Park features mature tree planting. The natural sandstone bedrock is exposed throughout the area and along the coastal escarpment. Landscaping in the public and private domains is generally low, characteristic of the elevated and exposed coastal location and to need to maintain views. The elevated coastal location provides for prevailing coastal winds from the east, which combined with the open character results in low heat vulnerability.



Figure 21-9 - Dudley Page Reserve, looking west



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?

Recent development

Recent development history shows a trend towards, property amalgamation; alterations and additions to existing or construction of new detached or semi-detached dwellings of 2-3 storeys contemporary forms, with large integrated garages, balconies and minimal landscaping. Indicating a demand for more floor space and more on-site parking in the area.

182 Existing character description cont.



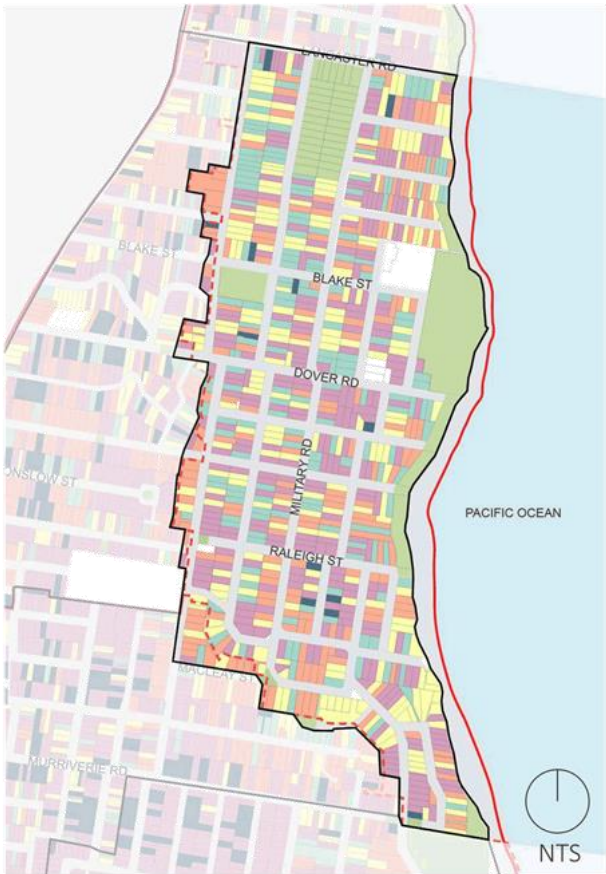
- LEGEND
- Study area boundary
 - Cadastral boundary
 - Building footprint
 - Open space
 - Blake Street Village Centre
 - Educational establishment
 - Religious institution
 - Landscape Conservation Area
 - General heritage item
 - Archaeological heritage item
 - Archaeological heritage site

Figure 21-10 - Built form, key uses and heritage area #21



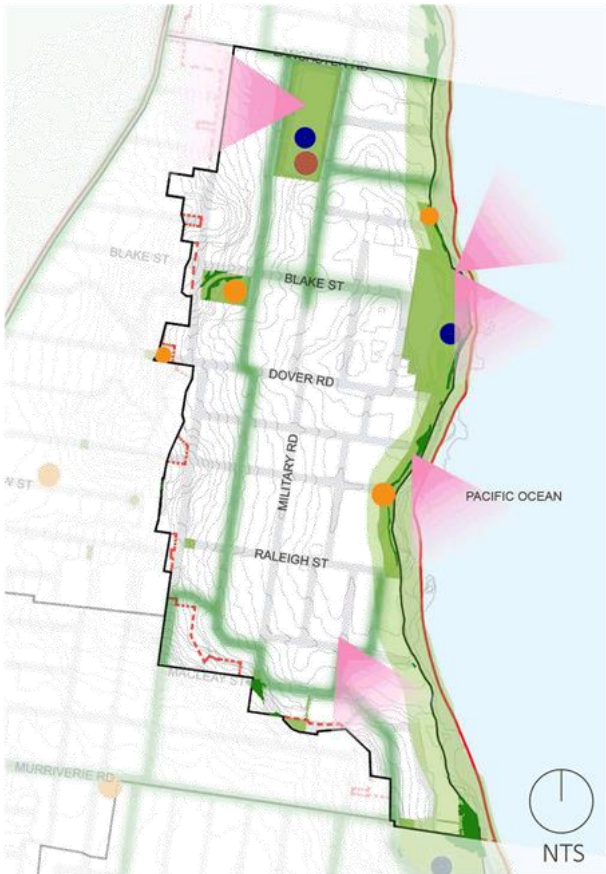
- LEGEND
- Study area boundary
 - Road network
 - Contour (2m intervals)
 - Open space
 - Bus routes
 - Bus stops
 - Bicycle routes
 - Coastal walk
 - View corridor

Figure 21-11 - Road network and movement area #21



- LEGEND
- Study area boundary
 - Cadastral boundary
 - Colonial
 - Early Victorian
 - Late Victorian
 - Federation
 - Inter-war
 - Mid-Century Modern
 - Late 20th Century
 - 21st Century

Figure 21-12 - Architectural styles area #21



- LEGEND
- Study area boundary
 - Suburb boundary
 - Contour (2m intervals)
 - Open space
 - Biodiversity habitat corridor
 - Remnant vegetation
 - Green Grid (TfNSW + GANSW)
 - Neighbourhood playspace
 - Pocket playspace
 - Recreation facility
 - Key view

Figure 21-13 - Open space and vegetation area #21



CHARACTER AREA

22



CHARACTER AREA - 22

"The area is defined by its elevated coastal location with panoramic views; irregular block pattern comprising low-rise predominantly detached and semi-detached dwellings interspersed with residential flat buildings; uniform front setbacks, enhancing wide street corridors; street-level garages and low landscaping; coastal open space".

The study area is generally bound by Military Road and Kimberley Street to the north, the Pacific Ocean to the east, Lancaster Road to the south and Old South Head Road (OSHR) to the west.



Figure 22-1 - Local character area #22 map



Figure 22-4 - Residential flat buildings, Military Road



Figure 22-2 - Oceanview Avenue, looking east



Figure 22-5 - Military Road street frontage, looking south-west

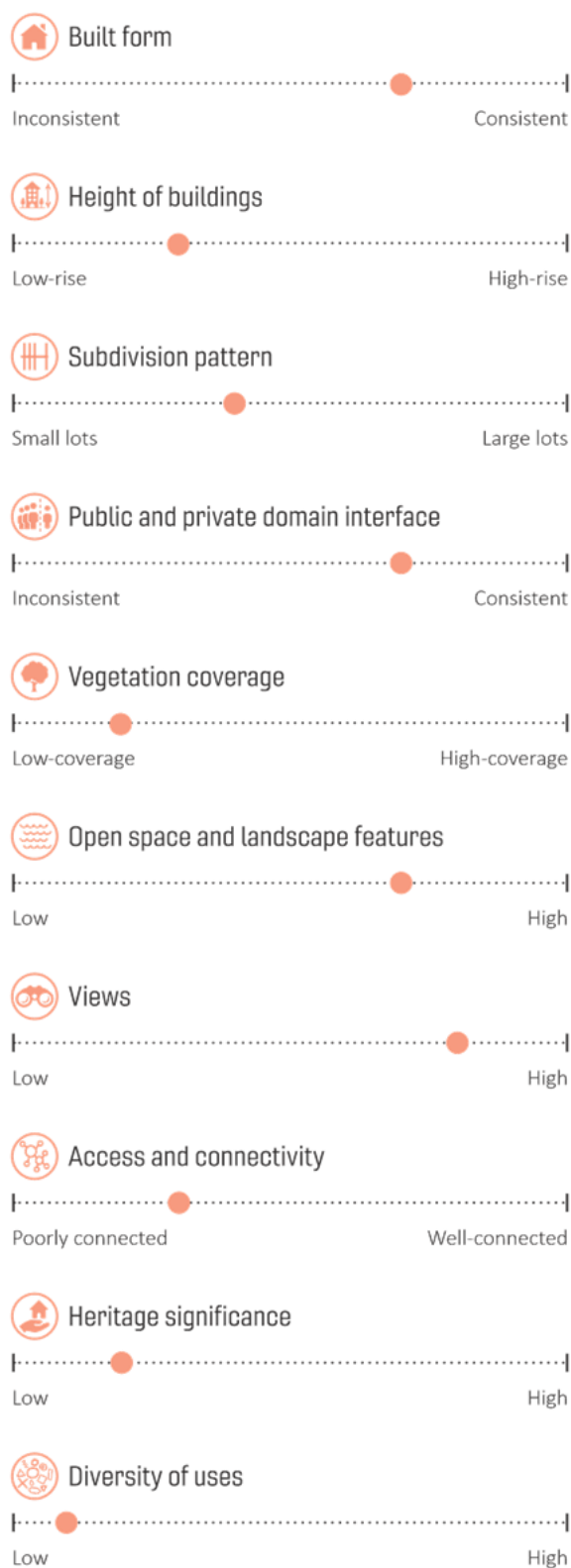


Figure 22-3 - Ethel Street, looking south



Figure 22-6 - Eastern Avenue Reserve, looking south

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by wide street corridors with panoramic ocean and Sydney CBD skyline views and is within walking access to local shops and services along Old South Head Road.

Objectives

The objectives for development on land identified within the character area are as follows:

- To recognise and maintain the diversity of uses in the area, particularly along OSHR,
- To maintain the predominant 2-4 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography and avoid excessive excavation,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality,
- To ensure high-quality design of contemporary detached and semi-detached dwellings (including dual occupancy), with appropriate façade and material articulation. Flat roofs are supported for view sharing,
- To encourage high-quality, contemporary, low rise RFB development at the western edges of the area, fronting OSHR. Consider site amalgamation where appropriate,
- To maintain a consistent front setback (including upper floors) to preserve the wide street corridors, reinforce views to coastline and allow for landscaping to soften the transition from public to private domains,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape,
- To maintain views and vistas from the public domain,
- To minimise the impact on existing views and vistas from the private domain,
- To minimise development density along the cliff-front to reduce risk from coastal processes and climate change,
- To ensure the safety of pedestrians along the coastal cliff,
- To promote the reduction of hard surfaces and an increase of public and private landscaping.

188 Existing character description

History

The area’s isolated location resulted in limited population and development until the 1900s, during the Federation period. Transport improvements, including introduction of bus services to supplement tram services along OSHR and the rise of private vehicle ownership, spurred development of the area for housing during the Inter-War and Post-War periods. The area remains predominantly residential with supporting commercial and retail uses within the North Rose Bay village centre and coastal open space.

Configuration, connectivity and built form

Subdivision of the area has resulted in an irregular block pattern. Wide east-west streets typically extend from OSHR, following the area’s prevailing east-west slope traversing the natural north-south ridgeline along Military Road, and terminating at the coastline. These corridors are important view axes’, framing ocean views and providing a sense of direction and orientation. OSHR and Military Road provide the primary north-south connection to, from and through the area. Pedestrian access and connectivity are increased with through-block links, for example Wilfield Avenue to Oceanview Avenue and Bulga Road to Military Road as well as the coastal cliff walk. Public transport is limited to bus services along OSHR and Military Road.

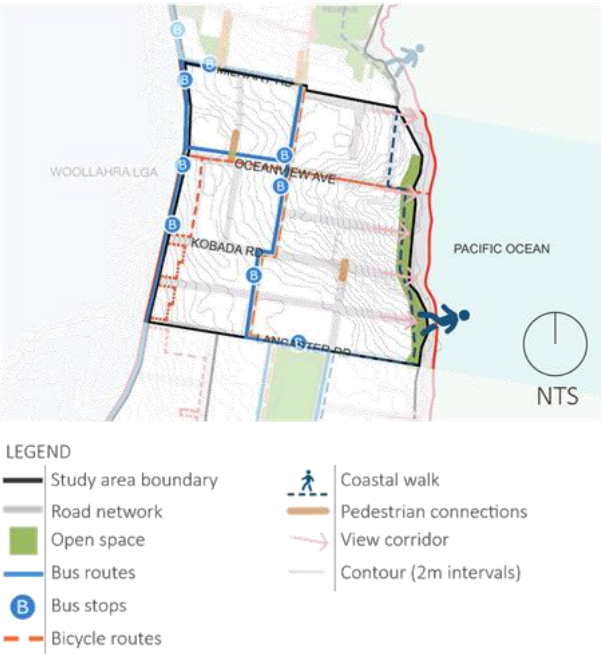


Figure 22-7 - Road network and movement area #22



Figure 22-8 - Eastern Avenue, looking east



Figure 22-9 - Kobada Road, looking west

The area is characterised by predominantly detached and semi-detached (including dual occupancy) dwellings, interspersed with residential flat buildings (RFBs). The dominant architectural styles are Inter-War and Mid-century styles reflective of the primary periods of development. These styles are defined by simple geometry, rendered or un-rendered red or light-coloured brick, terracotta pitched roofs or flat roofs on RFBs, large windows and generally have integrated street-level garages. Early detached and semi-detached dwellings have been significantly modified overtime. Modifications include, second-storey, balcony and garage additions. Redevelopment of older dwellings is occurring throughout the area. Newer, late 20th century and 21st century-style detached, and semi-detached dwellings are defined by flat-roofed, contemporary forms with wider modulation, wide integrated garages, large balconies and areas of transparent glass for doors, windows and balcony balustrades. Some newer dwellings comprise complex and curvilinear forms. Dwellings are typically 1-3 storeys in height. RFBs are typically 3-5 storeys. The bulk and scale of dwellings throughout the area varies, due to the different forms, façade modulation / articulation and roof types. Character is created through the lack of uniformity in the built form.



Figure 22-10 - Dwelling typologies area #22

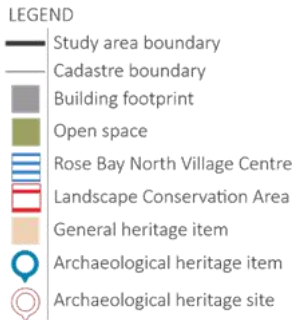


Figure 22-12 - Built form, uses and heritage area #22



Figure 22-11 - Dwelling typologies area #22



Figure 22-13 - Semi-detached dwellings, George Street



Figure 22-14 - Detached dwellings, Elvina Street

Existing character description cont.

Public and private domain interface

Dwellings tend to have a uniform front setback, enhancing the wider street corridor and open landscape character. Front landscaping, fence materiality and height varies throughout the area influencing the visibility of the dwelling from the public domain. On sloping sites dwellings on the high side can be raised above street level, with built retaining walls, high fences or garage doors fronting the street, for example OSHR and Oceanview Avenue. Dwellings on the low side, for example those fronting the coast on Ray Street, can be partially or completely obscured from street level as a result of the sloping topography, vegetation and carports and garages at the boundary. RFBs are typically raised above street level, with ground-level carparking and upper balconies overlooking the public domain. Overall, the streetscape is characterised by wide verges with limited street trees, punctuated by driveway cross-overs and car-parking structures typically located at the boundary.



Figure 22-15 - Military Road, looking east



Figure 22-16 - Oceanview Avenue street frontage



Figure 22-17 - Obscured dwellings, Ray Street



Figure 22-18 - Garage frontages, Oceanview Street

Natural environment

The area benefits from access to public open space including Eastern Avenue Reserve and Lancaster Road Reserve, which form part of the continuous coastal open space network (including the southern beaches) and offer passive and active recreation opportunities. The coastal reserves and cliffscapes are of significance landscape and archaeological value. The area comprises remnant native vegetation and supports a biodiversity habitat corridor. The natural sandstone bedrock is exposed along the coastal escarpment. The area's elevated position offers panoramic views from the public and private domain to the Pacific Ocean, coastline and surrounding areas. Landscaping in the public and private domains is generally low, characteristic of the elevated and exposed coastal location and to need to maintain views. These factors result in low-medium heat vulnerability across the area.



Figure 22-19 - Open space and vegetation area #22



Figure 22-20 - Detached dwelling, Elvina Street



Figure 22-21 - Semi-detached dwellings, Oceanview Avenue

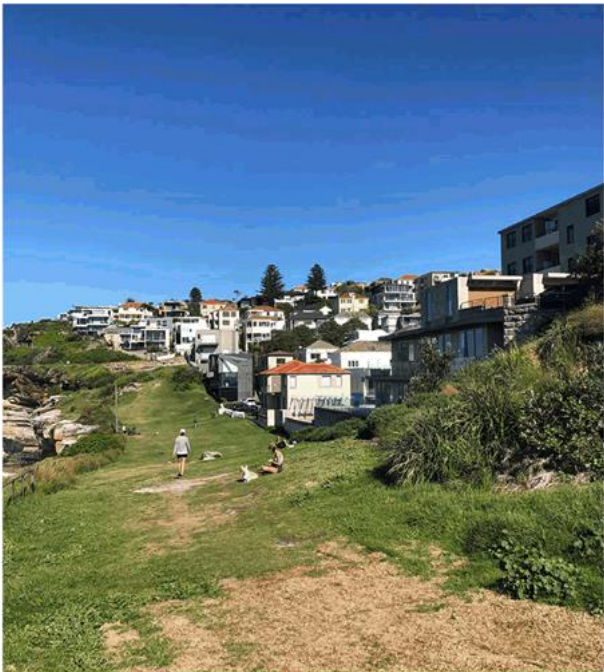



Figure 22-22 - Eastern Avenue Reserve, looking south

Recent development

Recent development history includes alterations and additions to existing dwellings, including second-storey additions and garage structures; property amalgamation; demolition of existing dwellings and construction of two-three storey dwellings of contemporary forms, with large integrated garages, balconies and minimal landscaping. Indicating an anecdotal demand for more floor space and more on-site parking in the area.



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



CHARACTER AREA

23



CHARACTER AREA - 23

"The area is characterised by natural landscape setting, of coastal reserves and cliffscapes; panoramic views; two defined areas of low-rise to medium-rise development comprising predominantly detached, semi-detached and residential flat buildings of various styles; uniform setbacks; street-level carparking; limited vegetation coverage".

The study area is generally bound by Christison Park to the north, the Pacific Ocean to the east, Kimberley Street and Military Road to the south and Old South Head Road (OSHR) to the west.



Figure 23-1 - Local character area #23 map



Figure 23-2 - Residential flat buildings, Diamond Bay Road



Figure 23-3 - Southern coastline from Chris Bang Crescent

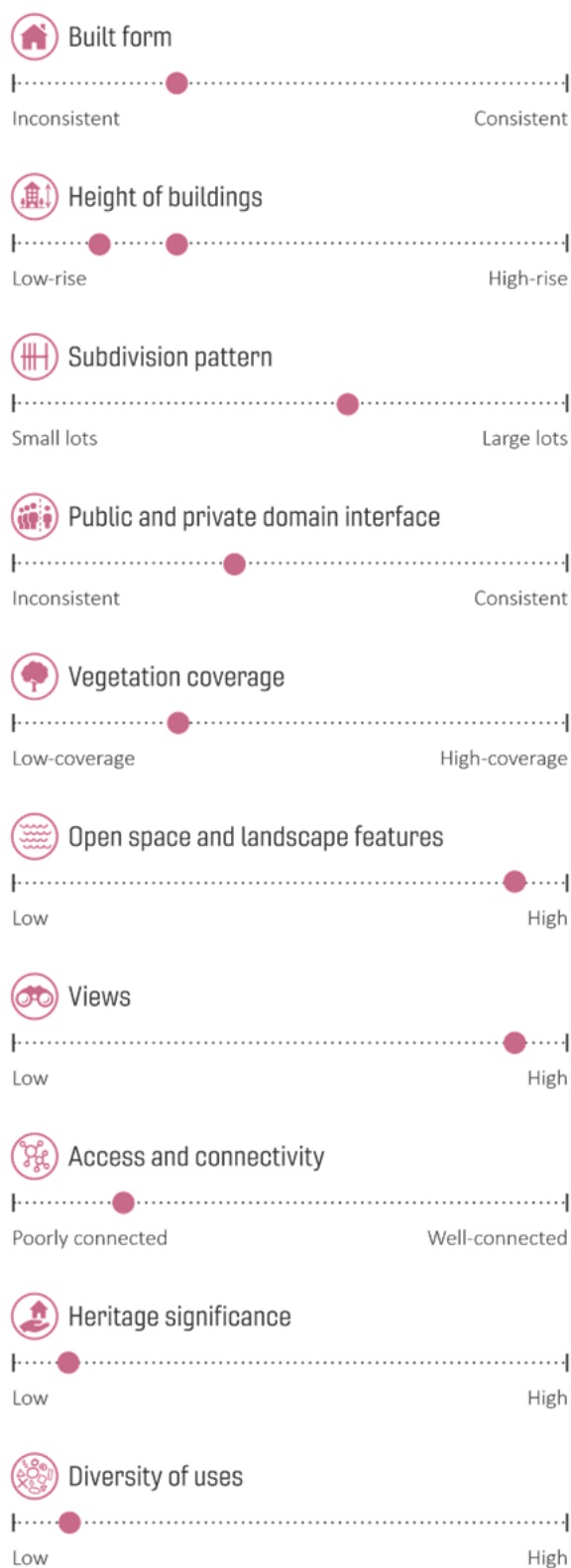


Figure 23-4 - Semi-detached dwellings, Marne Street



Figure 23-5 - Residential flat building, Diamond Bay Road

Existing character attributes



Desired future character

Vision

The desired future character of the area is defined by the coastal cliffscape and panoramic views to the ocean, a diversity of residential development with limited vegetation coverage.

Objectives

The objectives for development on land identified within the character area are as follows:

- To maintain the predominant 1-3 storey height character of the area, noting that buildings on sloping sites should adjust the relative level and height to follow the natural topography and avoid excessive excavation,
- To ensure alterations and additions read as a cohesive part of the existing dwelling and extension of historic form and materiality,
- To ensure high-quality design of new contemporary detached, semi-detached dwellings and RFB, with appropriate façade and material articulation. Flat roofs are supported for view sharing,
- To prevent the intensification of development along the coastal fringe,
- To ensure that carparking structures do not dominate or adversely impact upon the streetscape. Integrate carparking within new dwellings,
- To maintain views and vistas from the public domain,
- To minimise the impact on existing views and vistas from the private domain,
- To retain and enhance public and private recreation areas,
- To retain and protect the visibility of sandstone cliffs and edges by integrating the landscaping of the adjoining public and private areas,
- To minimise development density along the cliff-front to reduce risk from coastal processes and climate change,
- To ensure the safety of pedestrians along the coastal cliff, To promote the reduction of hard surfaces and an increase of public and private landscaping.

196 Existing character description

History

Population and development of the area was minimal until the 1900s, with only isolated residences on early land grants constructed during the Federation period. Transport improvements, including introduction of bus services to supplement tram services along OSHR and the rise of private vehicle ownership, spurred development of the area during the Inter-War and Post-War periods. The area remains predominantly residential with limited commercial uses within the Vaucluse village centre and substantial public and private open space, including the heritage-listed South Head Cemetery.

Configuration and connectivity

The area is defined by its natural landscape setting. Situated on a coastal plateau, the topography slopes from the natural ridgeline along OSHR, south and south-east towards Diamond Bay Reserve and the coastal cliffs. The area’s elevated position offers panoramic views from the public and private domain to the Pacific Ocean, coastline and surrounding areas. Constrained by the natural topography and areas of open space, progressive subdivision of the

area has resulted in an irregular block and street network, creating two confined areas of development. These areas comprise circuit and no-through streets accessed by OSHR, which provides the primary connection to and from the area. Wide street corridors frame views and provide a sense of direction and orientation. Diamond Bay Reserve restricts north-south vehicle movements between the two areas. Pedestrian access and connectivity are maintained by multiple through-block links, for example Mons Street to MacDonald Street, Chris Bang Crescent to Craig Avenue and Isabel Avenue to Military Road as well as the coastal cliff walk. Public transport is limited to bus services along OSHR and Military Road.

Built form

The area is characterised by predominantly detached, semi-detached and residential flat buildings (RFBs) dwelling typologies, of Inter-War, Mid-century Modern to 21st century styles. Clusters of early Federation and Inter-War style detached, and semi-detached bungalows are evident throughout the area, for example Jenson Street. These early dwellings have been significantly modified overtime. Modifications include, second-storey and garage additions.

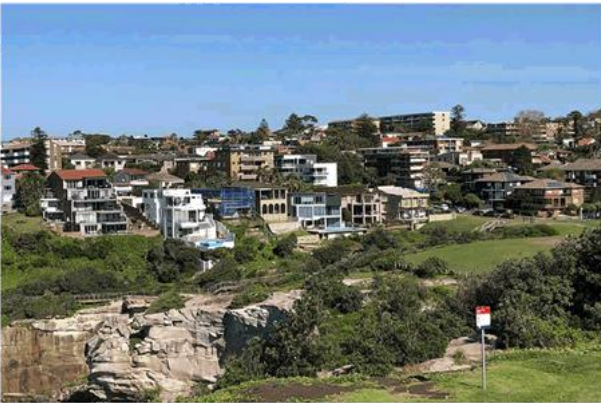


Figure 23-6 - Diamond Bay Reserve from Chris Bang Crescent



Figure 23-7 - Young Street, looking south



- LEGEND
- Study area boundary
 - Road network
 - Contour (2m intervals)
 - Open space
 - Bus routes
 - Bus stops
 - Bicycle routes
 - Coastal walk
 - Pedestrian connections
 - View corridor

Figure 23-8 - Road network and movement area #23

The dominance of Mid-century Modern-style dwellings across the area reflects the primary period of development, post-WWII. Features of this style typically include, rendered or un-rendered red or light-coloured brick, flat or single-angled rooflines, floor-to-ceiling windows, integrated ground level parking and upper balconies for RFBs. The prominent apartment block on Kimberley Street, designed by Modernist architect Harry Seidler is a key example of this style. The area to the north, bound by Young Street, Chris Bang Crescent to Clarke Street, has a prevailing low-to-medium density character with detached and semi-detached Mid-century-style dwellings, and a significant cluster of Mid-century-style RFBs on Clarke and Tower streets. The area to the west, bound by Diamond Bay Road and Isabel Avenue, has a prevailing medium density character, comprising predominantly Mid-century Modern to 21st century-style RFBs and multi-dwelling housing. This area has also seen more recent multi-dwelling and RFB development, of late 20th and 21st century styles. Less than 10% of dwellings in this area are detached and are largely located on Craig Avenue. Redevelopment of peripheral lots, for example along Chris Bang Crescent, results in late 20th century and 21st style dwellings. Dwelling heights reflect the diversity of typologies and styles. Detached and semi-detached dwellings are typically 1-2 storeys in height, increasing to 3-storeys where the topography provides for a ground-level garage, for example Chris Bang Crescent. RFBs are typically 3-4 storeys. RFBs of 7+ storeys are also evident within the area, however, are inconsistent with the prevailing height character.



Figure 23-9 - Mid-Century Modern RFBs, Isabel Avenue



Figure 23-10 - Architectural styles area #23



Figure 23-11 - Dwelling typologies area #23

198 Existing character description cont.



LEGEND

- Study area boundary
- Cadastre boundary
- 1-storey
- 2-storey
- 3-storey
- 4-storey
- 5-storey
- 6-storey
- 7-storey
- 10-storey

Figure 23-12 - Building heights in storeys area #23



LEGEND

- Study area boundary
- Cadastre boundary
- Building footprint
- Open space
- Vaucluse Village Centre
- Landscape Conservation Area
- General heritage item
- Archaeological heritage item
- Archaeological heritage site

Figure 23-13 - Built form, uses and heritage area #23

Public and private domain interface

Dwellings tend to have a uniform front setback, enhancing the wider street corridor and open landscape character. Front landscaping, fence materiality and height varies, influencing the visibility of the dwelling from the public domain, for example Clarke Street. Later additions of a carport or enclosed garage at the boundary encroach on the front setback and can further obscure the dwelling, for example Marne Street and Jensen Avenue. RFBs are typically raised above street level, with ground-level carparking and upper balconies overlooking the public domain. As a result, the streetscape is characterised by wide verges with limited street trees, punctuated by driveway cross overs and adjacent carparking structures.



Figure 23-14 - Clarke Street, looking east



Figure 23-15 - MacDonald Street, looking east



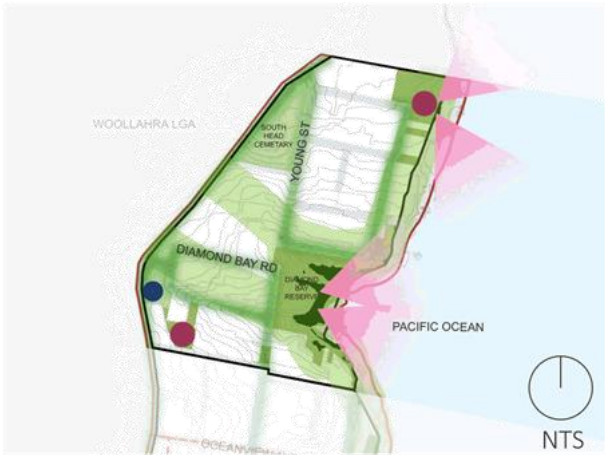
Figure 23-16 - Marne Street, looking south-west

Natural environment

The area benefits from access to significant public open space including Diamond Bay Reserve and Clarke Reserve, which form part of the continuous coastal open space network and offer passive and active recreation opportunities. The coastal reserves and cliffscapes are of significant landscape and archaeological value. Diamond Bay Reserve comprises remnant native vegetation and Aboriginal carvings. The area supports a biodiversity habitat corridor. Kimberley Reserve includes a community hall available for hire. Landscaping in the public and private domains is generally low, characteristic of the elevated and exposed coastal location and the need to maintain views. These factors result in low-medium heat vulnerability across the area.



Figure 23-17 - Clarke Reserve and playground



- LEGEND
- Study area boundary
 - Contour (2m intervals)
 - Open space
 - Biodiversity habitat corridor
 - Remnant vegetation
 - Green Grid (TfNSW + GANSW)
 - Neighbourhood playspace
 - Recreation facility
 - Key view


Figure 23-18 - Open space and vegetation area #23



Figure 23-19 - Diamond Bay Reserve and coastal walk

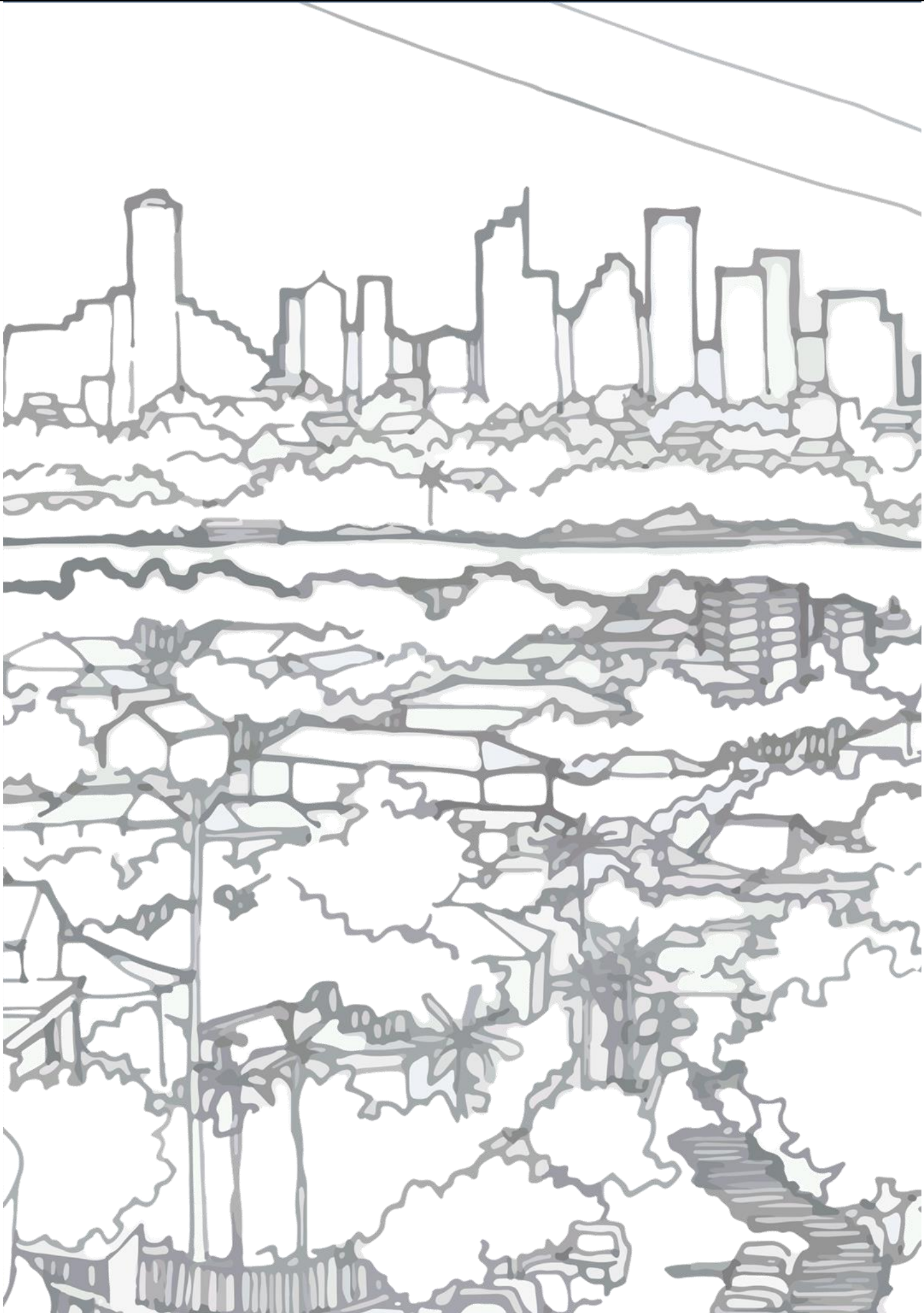
Recent development

Recent development history comprises alterations and additions to existing dwellings, including second-storey additions and garage structures; demolition of existing dwellings and construction of two-three storey dwellings of contemporary forms, including dual occupancy; consolidation and demolition of existing dwellings and construction of RFBs (particularly to the west of Diamond Bay); and some limited secondary dwellings (studios). Indicating an anecdotal demand for intensification of valuable land, more floor space and more on-site parking in the area.



Have Your Say!

1. Is the boundary of the area accurate?
2. Do you think this is an accurate description of the areas existing character?
3. What characteristics of the area do you value most?
4. Are the desired future character objectives listed above helpful in maintaining and enhancing the areas valued characteristics?



GLOSSARY

Table 3 - Common terms and abbreviations

Legislation, plans or policies	
CSP	Community Strategic Plan
DCP	Development Control Plan
District Plan	Eastern City District Plan, Greater Sydney Commission, 2018
EP&A Act	<i>Environmental Planning and Assessment Act 1979</i>
LCO	Local Character Overlay
LEP	Local Environmental Plan
LHS	Local Housing Strategy
LRMDHC	Low-Rise Medium-Density Housing Code
LSPS	Local Strategic Planning Statement
Region Plan	Greater Sydney Region Plan, A Metropolis of Three Cities, GSC, 2018
SEPP	State Environmental Planning Policy
VCS	Draft Waverley Village Centres Strategy 2020
WAMP	Waverley Architectural and Urban Typologies Mapping Project
Organisations	
DPIE	Department of Planning Industry and Environment
GSC	Greater Sydney Commission
Other	
HCA	Heritage Conservation Area
LCA	Landscape Conservation Area
LGA	Local Government Area
RFB	Residential flat building

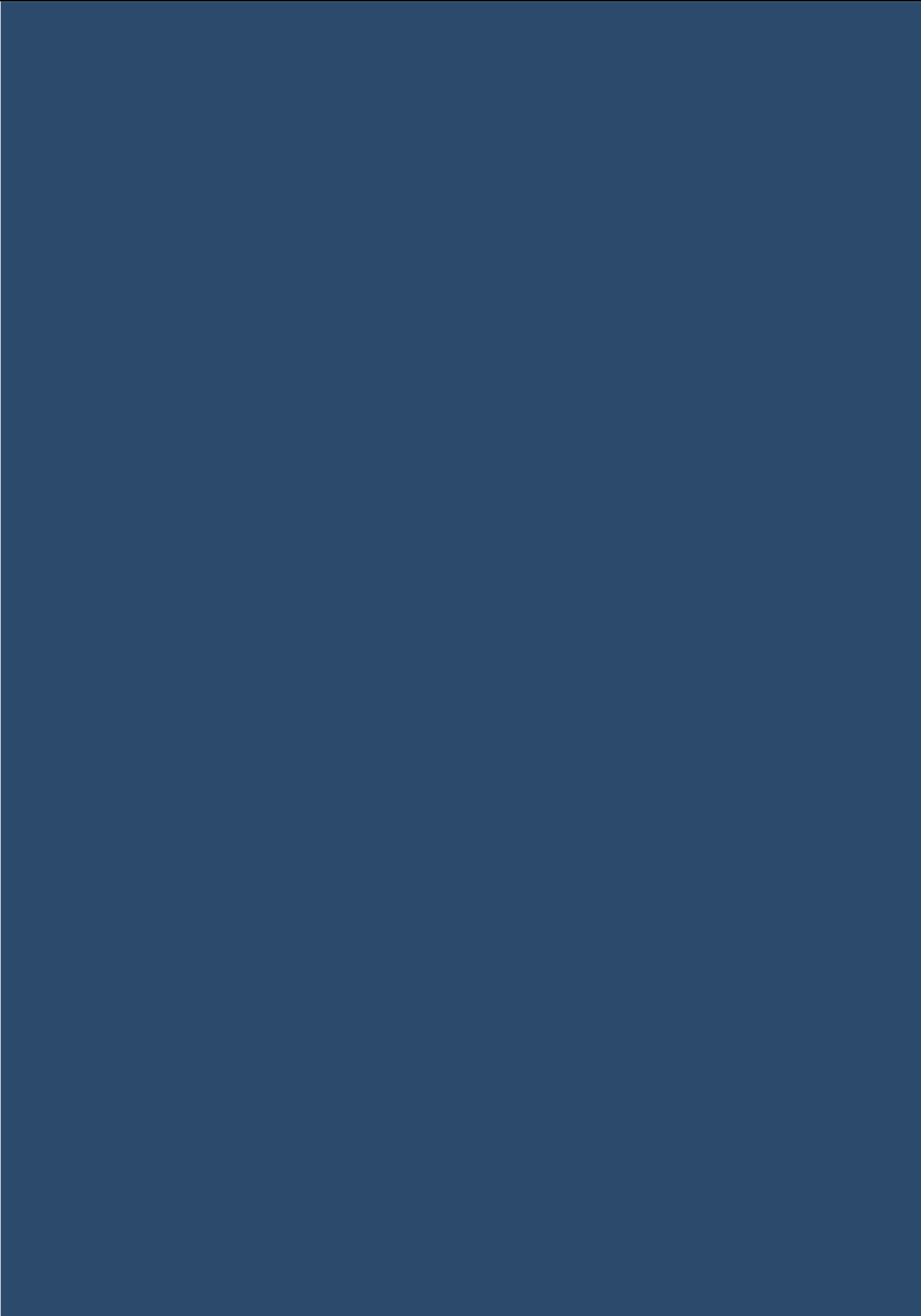
Table 4 - Glossary of terms

Term	Definition
Complying development	<p>Complying development is a development that meets specific standards in the State Environmental Planning Policy (Exempt and Complying Development Codes) 2008 (Codes SEPP) or other SEPPs such as the State Environmental Planning Policy (Educational Establishments and Child Care Facilities) 2017 (Education SEPP).</p> <p>Some categories of works that are complying development:</p> <ul style="list-style-type: none"> • construction of a new dwelling or secondary dwelling • alterations and additions to an existing dwelling • demolition of a building • internal alterations (fit-outs) of commercial / retail premises. <p>Applications can be determined by a council or private certifier without the need for a full development application. This provides a faster approval process, whilst ensuring stringent planning and environmental requirements are met through a set of prescribed approval conditions.</p>
Detached dwelling	A building containing one dwelling, on a single block of land that is not attached to any other dwelling.
Development control plan (DCP)	A plan that provides detailed planning and design guidelines for specific types or developments or areas (e.g. heritage items and heritage conservation areas) to support the planning controls in a LEP.
District strategic plan	A 20-year plan prepared by the State Government (Greater Sydney Commission) that address the community's needs for housing, jobs, infrastructure and a healthy environment for the districts within metropolitan Sydney.
Dual occupancy	<p>Dual occupancy (attached) means two dwellings on one lot of land that are attached to each other either side by side or above and below, by a common wall / floor.</p> <p>Dual occupancy (detached) means two detached dwellings on one lot of land.</p>
Exempt development	Some development is prescribed as exempt development under the Codes SEPP and / or the WLEP. Provided the development is in accordance with any provisions set out in the Codes SEPP or WLEP, an applicant does not need approval to undertake works.
Local environmental plan (LEP)	An environmental planning instrument (EPI) developed by a local planning authority, generally a council. An LEP sets the statutory planning framework for a Local Government Area.
Manor home	Means a group of four dwellings provided within a single building predominantly of two-storey height (with some limited three-storey elements).
Multi-dwelling housing	Means three or more dwellings (whether attached or detached) on one lot of land each with access at ground level, but does not include a residential flat building.
Regional strategic plan	A 20-year plan prepared by the State Government (Greater Sydney Commission) that address the community's needs for housing, jobs, infrastructure and a healthy environment for metropolitan Sydney.

Term	Definition
Residential flat building	A building that contains three or more dwellings and is two or more storeys.
Secondary dwelling	Means a self-contained dwelling that: a) is established in conjunction with another dwelling (the principal dwelling), and b) is on the same lot of land as the principal dwelling, and c) is located within, or is attached to, or is separate from, the principal dwelling.
Semi-detached dwelling	A dwelling attached to one other dwelling by a common wall. Typically a vertical common wall connecting two dwellings that are side by side. This definition covers a Dual occupancy (attached) after it has been subdivided so that each dwelling is on its own torrens titled lot.
Shop-top housing	Means one or more dwellings located above ground floor retail, commercial or business premises.
State Environmental Planning Policy (SEPP)	An environmental planning instrument (EPI) developed by the DPIE, that relates to planning matters that are state significant or are applicable across the state.
Terrace	Means a building containing 3 or more dwellings, where: a) each dwelling is attached to another dwelling by a common wall, and b) located side by side, and c) each dwelling is on its own lot of land.

Table 5 - Architectural style and relevant time period

Architectural style	Time period
Colonial	Initial European Settlement - 1788-1820s
Early Victorian	1830s-1860s
Late Victorian	1870s-1880s
Federation	1890-1915
Inter-War	Inter-War years - 1919-1939
Mid-Century Modern	Post-War years - 1945-1970
Late 20th Century	1970-2000
21st Century	2000-now





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Bondi Junction Urban Design Review Update 2020

ACKNOWLEDGEMENT

We acknowledge the Bidjigal and Gadigal people who
traditionally occupied the Sydney coast.

We also acknowledge Aboriginal Elders both past, present
and emerging.

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Executive Summary



A number of strategies have been completed for Bondi Junction in the past decade or so, outlining a series of recommendations and principles for future development in Bondi Junction that informed the current policy settings. Since around 2012 there has been significant redevelopment in Bondi Junction and there is continual pressure on redevelopment on this centre.

Given this context of change, the purpose of the *Bondi Junction Urban Design Review Update 2020* (the Review) is to evaluate the current principles and policy framework applying to development in Bondi Junction to ensure that they are valid and up to date.

The Review examines three studies – the original Bondi Junction Urban Design Review (BJUDR), the West Oxford Street Design Charette and the Bondi Junction Town Square Review – looking at their recommendations and principles and asking the following key questions:

- How these have been implemented?
- If they weren't implemented, are they still valid?

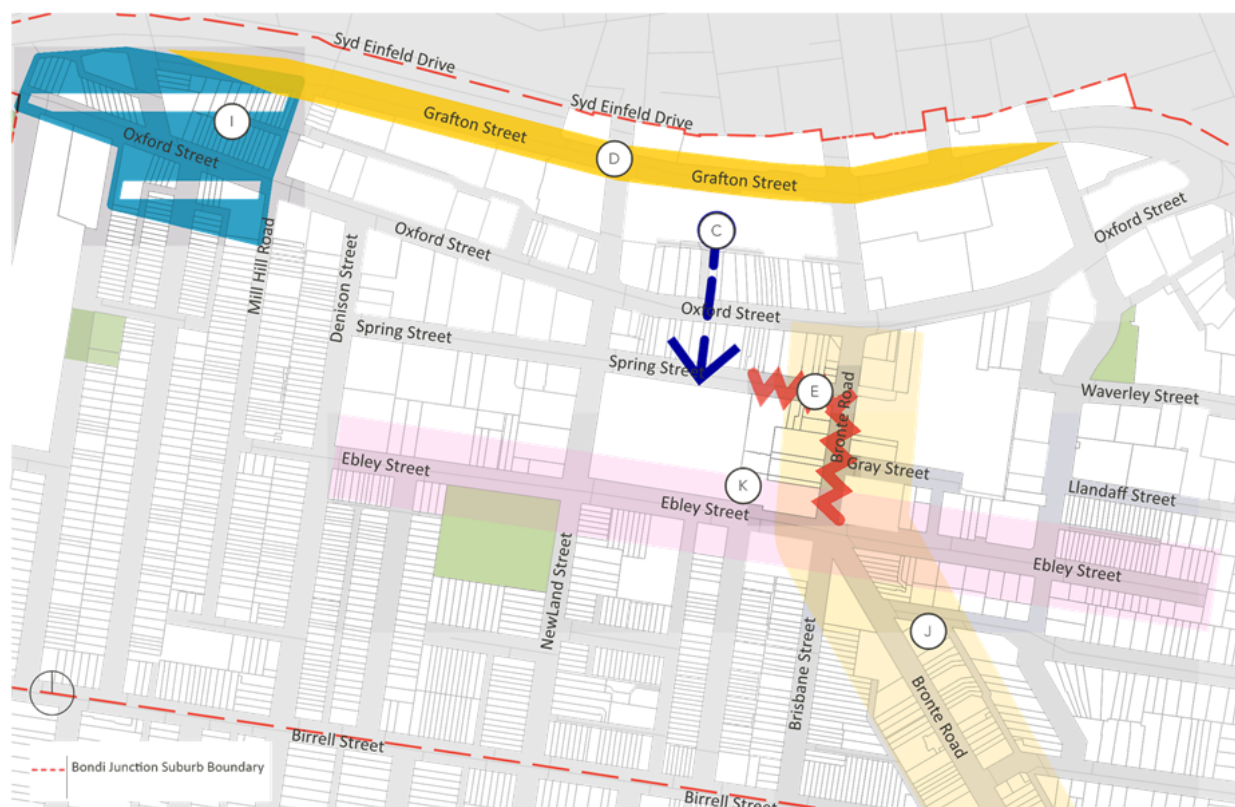
- What has happened since their implementation?
- What have the community said in response to new development?
- Do the original recommendations still stand in the context of change?

Recommendations from these studies have been considered as: public domain amenity, development potential and policy and design recommendations. Most of the recommendations from these studies have been actioned and no further work is required for these.

Where recommendations have not been fulfilled, the Review makes recommends further work or in some instances no further work. One of the most significant recommendations from the original BJUDR was for a transition in building height towards the edges of Bondi Junction.

This principle is enshrined in one of the aims of the WLEP 2012. This Review recommends that this principle and associated planning controls be upheld.

Summary of Recommendations



Public Domain Amenity Recommendations

Recommendation A	Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the BJ Complete Streets Project.
Recommendation B	Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the Complete Streets project and Waverley's People, Movement and Places Strategy.
Recommendation C	Continue to investigate using Rowe Street to link Oxford Street and Spring Street to the Bondi Junction rail interchange.
Recommendation D	Improve public domain of Grafton Street towards and underneath Syd Enfield Drive in connection with the relevant recommendations of the Open Space and Recreation Strategy & Bondi Junction Night Activation Strategy.
Recommendation E	Investigate further mechanisms to reduce pedestrian and vehicle conflicts in Spring Street and Bronte Road.

6

Development Potential Recommendations

Recommendation F	Continue to work with the DPIE to update planning controls so that commercial floorspace is retained and grows in Bondi Junction Strategic Centre.
Recommendation G	Investigate LEP height and DCP storey control inconsistencies further with assistance from the Development Assessment Team with an aim to resolving this conflict within either the comprehensive LEP review or DCP review.

Planning Policy and Design Recommendations

Recommendation H	Continue to apply the Waverley VPA policy for developments and planning proposals that are seeking uplift above their current development standards.
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Edge Area Controls - West Oxford Street

Recommendation I	Maintain the current LEP controls for the West Oxford Street area and do not allow for increased development standards in order to respect the existing low scale built form and amenity of the area.
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Edge Area Controls - Bronte Road Corridor

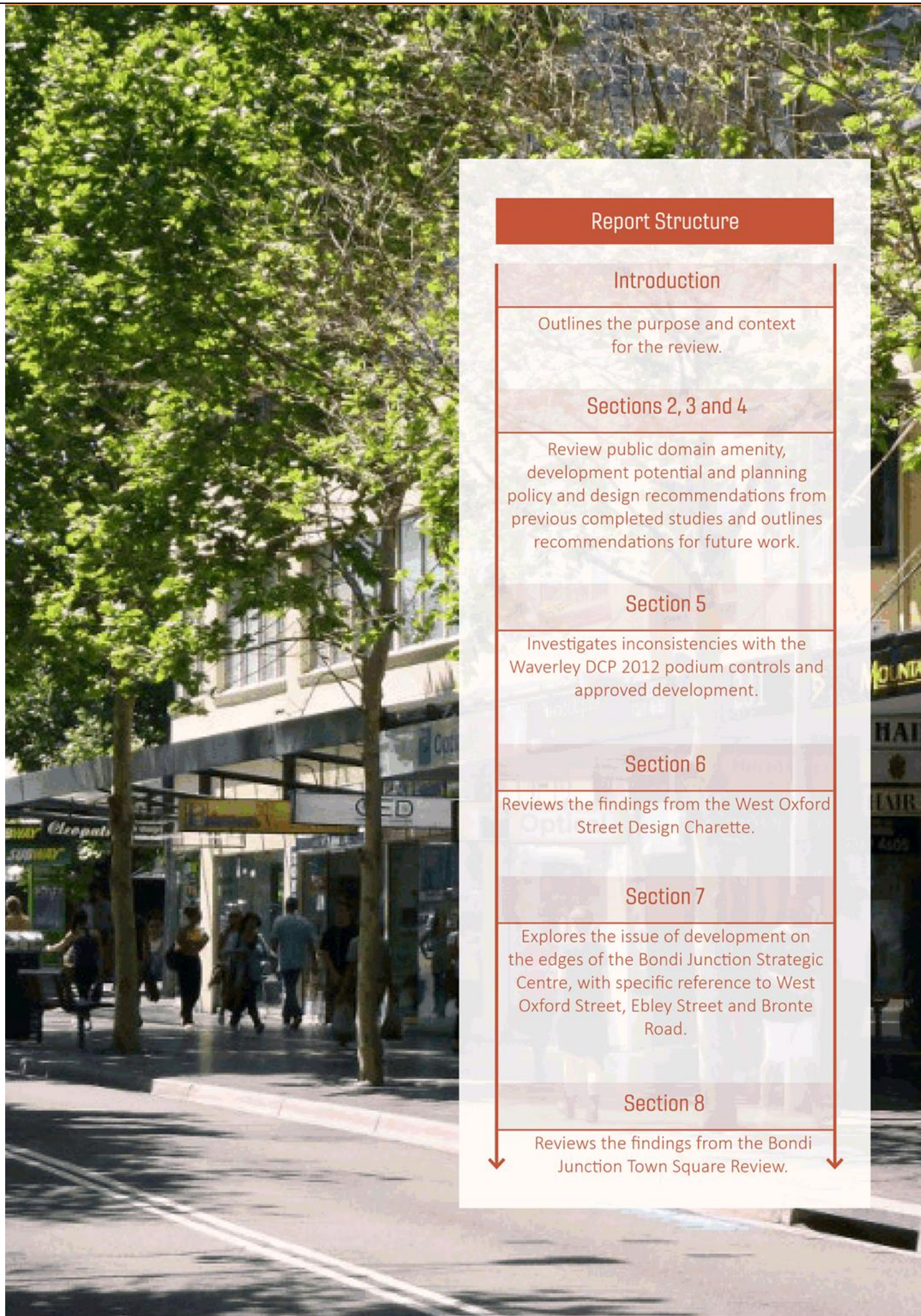
Recommendation J	Maintain the existing LEP controls along the Bronte Road corridor.
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Edge Area Controls - Ebley Street

Recommendation K	Maintain the LEP controls on Ebley Street to ensure that development in these edge areas remains transitional to reduce the impact on surrounding areas.
-------------------------	--

Bondi Junction Town Square Peer Review 2007

Recommendation L	Ensure that the FSRs for sites are compatible with the height of buildings in any changes to the LEP.
-------------------------	---



Report Structure

Introduction

Outlines the purpose and context for the review.

Sections 2, 3 and 4

Review public domain amenity, development potential and planning policy and design recommendations from previous completed studies and outlines recommendations for future work.

Section 5

Investigates inconsistencies with the Waverley DCP 2012 podium controls and approved development.

Section 6

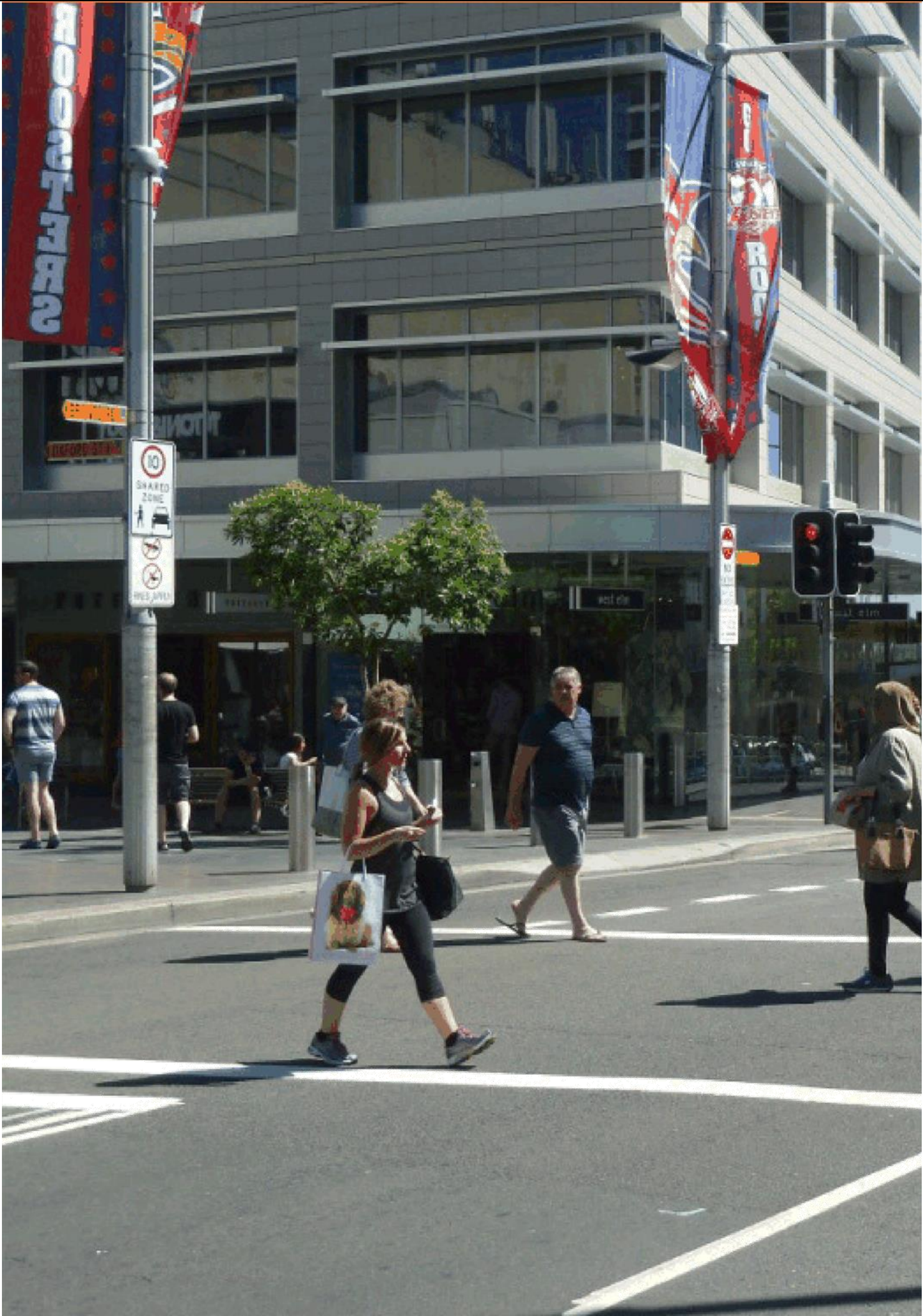
Reviews the findings from the West Oxford Street Design Charette.

Section 7

Explores the issue of development on the edges of the Bondi Junction Strategic Centre, with specific reference to West Oxford Street, Ebley Street and Bronte Road.

Section 8

Reviews the findings from the Bondi Junction Town Square Review.



1.0 Introduction

The Bondi Junction Urban Design Review (the BJUDR) was completed in 2013 (CityPlan) with a view to identifying planning issues, accommodating development pressures and providing solutions to them within the Bondi Junction Strategic Centre. The BJUDR made recommendations primarily on built-form controls, the public domain and other planning policy and design. The recommendations and findings of the BJUDR were implemented in the Planning Proposal for the new Standard Instrument LEP of 2012, the DCP as well as applied to Planning Proposals and Development Applications for land within the Bondi Junction Strategic Centre.

Since that time there has been considerable development activity and continued development pressure in Bondi Junction (Figure 1.1 & 1.2). The purpose of this update is to analyse how planning within the Bondi Junction Strategic Centre has been implemented, including other studies completed for Bondi Junction such as the West Oxford Street Design Charette and the Bondi Junction Town Square Review. In particular, this analysis will examine Development Applications (DAs) and Planning Proposals (PPs) in the centre from 2013 to understand how the vision, aims and recommendations – in the form of LEP and DCP controls – of the BJUDR have stood the test of time. In undertaking this analysis this update makes recommendations for planning within the centre, with particular focus on whether the original aims and implemented controls are still relevant and suitable, and hence should continue to be applied, or if new controls should be considered.



10 Figure 1.2 Bondi Junction Development Status Table

	Development Site	Status	FSR exceed- ance (%)	VPA contribu- tion amount
1	95-97 Grafton Street & 422 Oxford Street, (Meriton Tower)	Under Assessment	Complied	N/A
2	55 Grafton Street, Demolition & construction of 20 storey mixed use tower	Under Assessment	Complied	N/A
4	5-11 Hollywood Avenue, Demolition & construction of a new 11 storey hotel	Under Assessment	Complied	N/A
5	21 Waverley Crescent, Demolition & construction of a 9 storey RFB	Under Assessment	Complied	N/A
3	10 Denison Street, Demolition & construction of mixed use tower	Refused	N/A	N/A
6	7-11 Bondi Road, Demolition & construction of a 9 storey RFB (inc. affordable housing)	Approved	Complied	N/A
7	34-38 Bondi Road, Demolition & construction of a 3 storey RFB	Approved	18%	N/A
8	Demolition of the building fronting Council Street (8-10 Council Street) and partial demolition of the building fronting Paul Street (27 Paul Street), and construction of a two and three storey affordable housing development with a total of 18 units and basement car parking	Approved	Complied	N/A
9	16A + 16B Llandaff Street, Demolition & construction of a 9 storey RFB	Approved	16%	\$1,035,538
10	Bondi Junction RSL, 1-9 Gray Street, Demolition & construction of a 10 storey mixed use building	Approved	Complied	N/A
11	362-374 Oxford Street, Demolition & construction of a 14 storey mixed use building	Approved	15%	\$2,026,313
12	59-75 Grafton Street, Demolition & construction of a mixed use tower	Approved	Complied	N/A
13	45-57 Oxford Street, Demolition & construction of mixed use building & alts & adds to existing heritage buildings	Approved	15%	Affordable housing provided
14	292 Oxford Street, Demolition & construction of 12 storey mixed use development	Under Construction	14%	\$1,687,239.08
15	59-69 Oxford Street, Mixed use development including existing pub on ground floor	Under Construction	19%	\$4,713,956
16	87-99 Oxford Street, and 16-22 Spring Street, Demolition & construction of 14 storey mixed use building	Under Construction	15%	\$5,164,236.85
17	552-568 Oxford Street, Demolition & construction of a mixed use building	Under Construction	1.5%	N/A
18	20 + 22 Llandaff Street, Demolition & construction of RFB building	Under Construction	15%	\$633,077
19	50 Waverley Street, Alts & Adds to existing RFB	Under Construction	15%	\$130,033

	Development Site	Status	FSR exceed- ance (%)	VPA contribu- tion amount
20	91-95 Old South Head Road, Demolition & construction of RFB	Completed	125%	N/A
21	33 Waverley Street, Demolition & construction of RFB	Completed	2%	\$200,000.33
22	253-255 Oxford Street, Demolition & construction of a 20 storey mixed use building (inc. affordable housing)	Completed	15%	Affordable housing provided
23	570-588 Oxford Street, Demolition & construction of a 22 storey RFB with ground floor retail	Completed	1.8%	\$338,440
24	241 Oxford Street, Demolition & construction of mixed use building	Completed	15%	\$3,000,000
25	6 Bronte Road, Demolition & construction of 8 storey commercial building	Completed	Complied	N/A
26	73-89 Ebley Street, Demolition & construction of a 4 storey mixed use development	Completed	5%	\$316,200
27	139 Oxford Street, Demolition & construction of a 9 storey commercial building with hotel uses	Completed	Complied	N/A
28	109 Oxford Street, Demolition & construction of a new 12 storey mixed use building	Completed	15%	\$2,844,438
29	350-354 Oxford Street, Demolition & construction of a mixed use building	Completed	5%	\$985,711.55
30	310 Oxford Street, Demolition & construction of a 10 storey mixed development	Completed	2%	\$3,000,000
31	304 Oxford Street, Demolition & construction of a 12 storey mixed use development	Completed	15%	\$1,451,812
32	95-97 Grafton Street, Construction of new serviced apartments over 4 levels	Completed	26%	Pre-VPA Policy

12 2.0 Public Domain Amenity Recommendations

2.1 Develop a Public Domain masterplan to identify key Public Domain projects.

2.1.1 Findings/Analysis

One of the key recommendations of the BJUDR relating to public domain was for a masterplan to be developed identifying key public domain projects and spaces in Bondi Junction. This recommendation has been actioned through the creation of the Complete Streets project. The Complete Streets project directly responds to various recommendations made in the BJUDR relating to public domain principles. The Complete Streets project addresses this recommendation as it considers the various needs of all users of public space by having provisions to ensure the streets are pedestrian-friendly, provide good access to public transport, accommodate cyclists, are safe and benefit the local economy.

2.1.2 Recommendation

Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the Complete Streets project.

2.2 Develop a movement strategy that integrates pedestrian and vehicular movements and has a clear focus on the quality of the public domain.

2.2.1 Findings/Analysis

This recommendation has been actioned through the creation of the Complete Streets project and Waverley's *People, Movement and Places Strategy*. Waverley's *People, Movement and Places Strategy* was developed to address the issues associated with pedestrian and vehicular movements within Waverley more broadly. This was created to address current transport issues and to best meet the changing demands of the future. The *People, Movement and Places Strategy* also outlines provisions for improving the public domain through upgrading cycleways, footpaths, roads and shared pedestrian and vehicle spaces. The Complete Streets project also considers the needs of pedestrians and vehicles equally in direct response to this recommendation of the BJUDR.

2.2.2 Recommendation

Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the Complete Streets project and Waverley's *People, Movement and Places Strategy*.

2.3 Improve the connection from the rail interchange to Oxford Street and further on to Spring Street

2.3.1 Findings/Analysis

Council has been actively pursuing a means of improving the connection from the rail interchange to Oxford Street and Spring Street. Council has identified Rowe Street as a possibility to create a direct connection to the railway station as the street has been closed and made available for redevelopment. Rowe Street was official closed as a road in 2016 to facilitate the development of this link. A feasibility study was completed in 2019 and was reported to Council who adopted the recommendation to sell the Rowe St property.

2.3.2 Recommendation

To prepare for the sale of the Rowe St incorporating a satisfactory legal framework to ensure that the public benefits including design outcomes and the connection to the Transport Interchange are safeguarded.

2.4 Create through site links

2.4.1 Findings/Analysis

The BJUDR outlined that through-site links should be negotiated as a public benefit from new development, particularly where they provide a demonstrable time saving through long, impassable blocks. Planning Proposals in recent years have often included tokenistic through-site links that do little in the way of reducing walk times and hence are not considered to provide public benefit and as such should not be considered as a redeeming feature of a development.

2.4.2 Recommendation

Continue to support and request provision of through-site links in DAs and PPs if they provide a demonstrable public benefit.

2.5.0 Improve public domain of Grafton Street and towards and underneath Syd Enfield Drive

2.5.1 Findings/Analysis

One of the actions of the Complete Streets project is to consider adaptive reuse of space under Syd Enfield Drive with a long-term goal to create more street level activity. It also recommends improving lighting and access of carparks under Syd Enfield Drive with a vision to properly activating this space past the point of being carparking. This is also being investigated as part of Council's Open Space and Recreation Strategy. This strategy will provide recommendations as to how the space under Syd Enfield Drive can be used for public urban recreation space. This strategy is intended to be finished in 2020.

Therefore, this recommendation should remain as a recommendation and should also reference the Open Space and Recreation Strategy.

2.5.2 Recommendation

This recommendation is being investigated as part of the Open Space and Recreation Strategy.

2.6.0 Create share zones in parts of Spring Street and Bronte Road

2.6.1 Findings/Analysis

The BJUDR found that there were no shared zones in Spring Street or Bronte Road. The four-way pedestrian crossing point at the corner of Oxford Street and Bronte Road and the crossing at Oxford Street and Ebley Street were considered appropriate crossing points for pedestrian foot traffic within this area. The BJUDR recommended further investigations to determine whether it would be appropriate to have this section closed to general traffic and only allow for buses, taxis and deliveries.

2.6.2 Recommendation

Investigate further mechanisms to reduce pedestrian and vehicle conflicts in Spring Street and Bronte Road.

2.7.0 Create solar access planes for important public open spaces within the Bondi Junction Centre

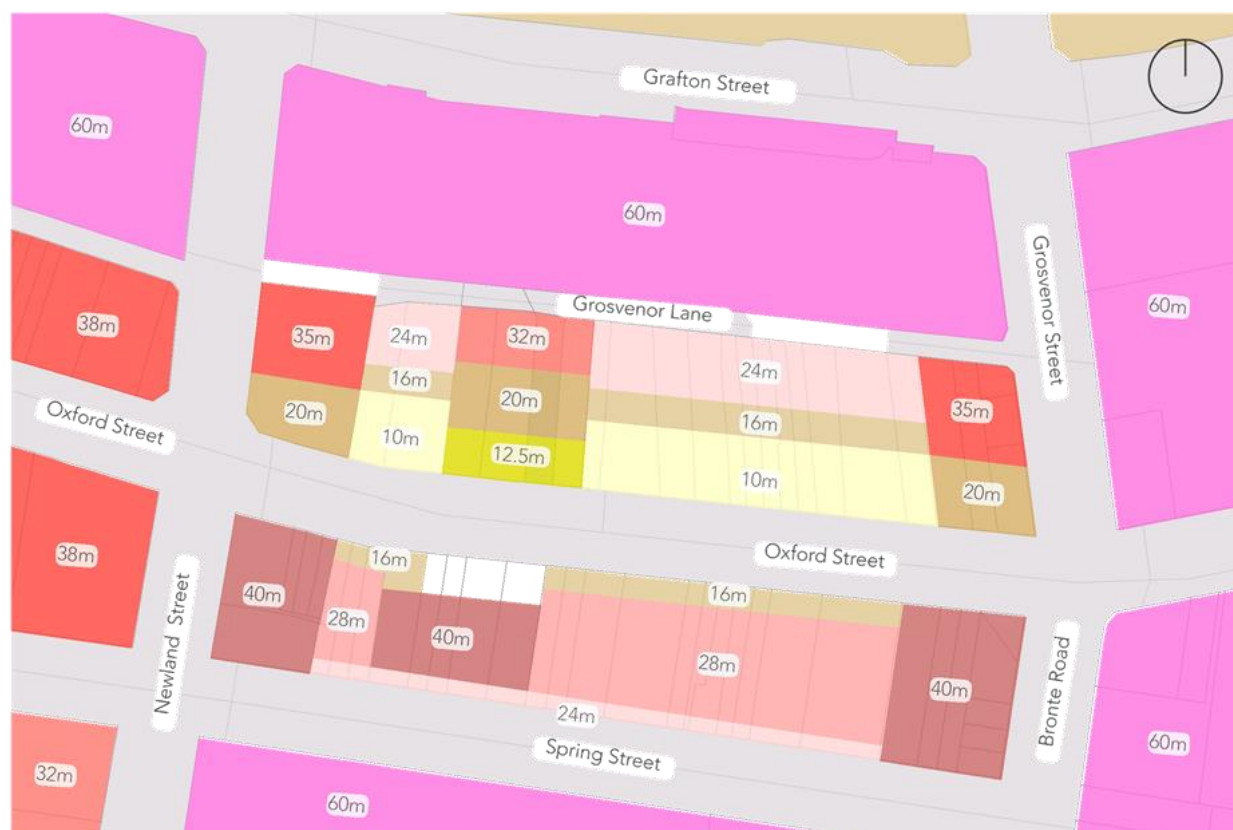


Figure 3 Stepped maximum height of building controls surrounding Oxford Street Mall

2.7.1 Findings/Analysis

As a result of this recommendation an Additional Local Provision was added into the Waverley LEP 2012. Clause 6.7 *Solar access to public spaces in Bondi Junction* in the Waverley LEP was added into the LEP to ensure buildings were designed to maximise sunlight access to important public places in Bondi Junction. The clause outlines the provision that development consent must not be granted to development that results in any part of a building causing an additional shadow impact at 12 noon on 21 June (winter solstice) at the following places: *Clemenston Park, Waverley Street Mall, Eora Park, Norman Lee Place & Oxford Street Mall*.

This clause also aligns with recommendations from the Bondi Junction Town Square Peer Review Submission

that suggested stepped height controls based on the angle of the winter sun, so that Oxford Street Mall could continue to receive sunlight regardless of new development. The stepped heights can be seen in Figure 3.

2.7.2 Recommendation

This recommendation has been actioned. No further action required with this recommendation.

2.8.0 Prevent midwinter lunchtime overshadowing of Clemenston Park by amending the WLEP 2012 height and FSR along the northern side of Ebley Street

2.8.1 Findings/Analysis

The original findings of the BJUDR outlined that the height of buildings on the Ebley Street sites should be reduced from 32m to 28m. This height limit was decided to allow for sunlight to reach Clemenston at 12 noon on 21st June (Winter Solstice).

Clause 6.7 *Solar access to public spaces in Bondi Junction* was applied incorrectly and as it is currently written does not allow any increase on those sites above the height of the existing building. This was not the intention of the original recommendation of the Urban Design Review; the intention was to allow redevelopment of those sites up to a proposed reduced maximum height limit of 28m.

2.8.2 Recommendation

This clause should be reviewed to ensure an appropriate balance between daylight access to the park and the ability of the sites on Ebley Street to achieve their LEP height restrictions.



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3.0 Development Potential Recommendations

3.1.0

Improve flexibility in land use by amending the WLEP 2012 to change the zone of the commercial core that lies to the West of Newland Street from B3 Commercial Core to B4 Mixed Use



Figure 4 Areas rezoned from B3 Commercial Core to B4 Mixed Use as part of the WLEP 2012

3.1.1 Findings/Analysis

This recommendation was actioned as part of the Waverley Local Environment Plan 2012, the areas in Figure 4 were rezoned from B3 Commercial Core to B4 Mixed Use.

The intention of this was to allow for more residential space within Bondi Junction so that there could be an equal distribution of commercial and residential floorspace with the intention that residents could live and work in Bondi Junction. Council originally sought controls to ensure that new development had a

50/50 mix of commercial and residential. This was not supported by the DPIE. Instead of creating an equal distribution of commercial and residential uses, the rezonings led to an outcome which saw residential uses dominate commercial uses as residential development was much more profitable to developers. As a result, BJ has experienced a loss of 9,000m² in commercial floorspace since 2014 with a further loss of 19,000m² projected in the pipeline.

The *Bondi Junction Strategic Centre: Protecting and Promoting Commercial Floorspace PP* was lodged with the DPIE in mid-2019 in order to stem the loss of commercial floorspace and ensuring that quasi-residential uses such as serviced apartments could not be used in the commercial floorspace areas. This can be achieved through retaining all the remaining B3 Commercial Core zoned land in Bondi Junction and prohibiting quasi-residential uses such as serviced apartments in this zone (which the PP is attempting to do).

3.1.2 Recommendation

This recommendation has been implemented, with the unanticipated perverse outcome of not delivering a truly mixed-use outcome. Continue to work with the DPIE to update planning controls so that commercial floorspace is retained and grows in Bondi Junction Strategic Centre.

3.2.0 Consider tall office buildings on the block between Oxford Street, Bronte Road and Gray Street (facilitating the opportunity for A-grade office space in Bondi Junction).

3.2.1 Findings/Analysis

It was decided to not implement this recommendation as part of the comprehensive LEP 2012 as there was no mechanism in place to capture any contribution from the windfall gain that these office towers would be getting as a result of this recommendation. It was considered that, as the community could not share in this uplift, and that increasing the development controls would only serve to benefit individual landowners through increased land values, that this recommendation would not be actioned.

3.2.2 Recommendation

No further action required.

3.3.0 Use potential for additional storeys within LEP height limit at selected locations (at present the number of storeys is limited by the DCP which at some locations does not correspond with the full LEP height potential).

3.3.1 Findings/Analysis

There has been an issue arising from development applications within Bondi Junction that has highlighted an inconsistency between the LEP maximum height limit and the DCP floor to ceiling height controls. This has meant that many developments have seemingly not been able to achieve a maximum amount of storeys due to floor to ceiling heights not matching with LEP height controls. This has meant many development applications have used Clause 4.6 Variation applications to gain heights exceeding the LEP height limit.

3.3.2 Recommendation

Investigate this further with assistance from the Development Assessment team with an aim to resolving this conflict within either the Comprehensive LEP review or DCP review.

3.4.0 Acknowledge that certain sites may be the subject of acceptable Development Applications which exceed development controls, if they do not create unacceptable impacts and they meet obligation to improve infrastructure and facilities.

3.4.1 Findings/Analysis

This recommendation was actioned through the creation of Council's Voluntary Planning Agreement (VPA) policy in 2014 (discussed further below). The VPA policy has been used to positive effect in numerous DAs where it could be demonstrated that exceedances of the development controls would not have adverse impacts on the surrounds and could provide a positive public benefit for the surrounding area.

3.4.2 Recommendation

Continue to negotiate VPAs for sites where it is deemed appropriate to exceed development controls.



4.0 Planning Policy and Design Recommendations

4.1.0 Develop a framework for VPA and (now) S7.11 contributions particularly for sites that might be the subject of Development Applications significantly over the existing controls.

4.1.1 Findings/Analysis

Waverley's VPA policy was created in 2014 in order to secure a public benefit from developments and PPs that were receiving a value uplift. The policy provides developers the opportunity to voluntarily provide public benefits to the community in the form of monetary contributions, land dedications and public domain and infrastructure upgrades.

The Planning Proposal at 194-214 Oxford Street and 2 Nelson Street, Bondi Junction (194 Oxford St PP) is an example of how the VPA policy could be implemented to achieve positive outcomes for the community. Council had attempted on various occasions to refuse the Planning Proposal stating issues with the controls being out of context with surrounds, however the NSW Department of Planning, Industry and Environment used its delegation to approve the development. The VPA policy was used to great effect, as although the Planning Proposal was approved, Council was able to gain a \$7.45million contribution from the developers that would be used to help improve community facilities.

Since its creation in 2014 the VPA policy has been used to secure \$16.75million for the purposes of community benefits, approximately \$2.3million of which has been secured for the purposes of affordable housing.

4.1.2 Recommendations

This recommendation has been actioned and as such the resulting VPA policy should continue to be used to secure public benefits. Continue to use the Waverley VPA policy for developments and planning proposals that are seeking uplift above their current development standards.

4.2.0 Require development applications to consider impact on surrounding sites. This may include three dimensional testing of overshadowing.

4.2.1 Findings/Analysis

All DAs and PPs are required to assess the impact on surrounding sites, this is one of the key factors in merit assessment. Shadow diagrams form a pivotal part of assessing the impact of a development on surrounding lots and as such are a requirement for applications for dwelling houses, dual occupancies, multi-residential development, mixed use development and commercial development over one storey high.

4.2.2 Recommendations

This recommendation has been actioned. No further recommendation required.

4.3.0 Require development applications to illustrate how neighbouring sites can be renewed or redeveloped adjacent to the proposal.

4.3.1 Findings/Analysis

This has been a key factor in assessing both DAs and PPs. This assessment is undertaken to ensure that the development opportunities of surrounding sites are not sterilised due to restraints enforced on them by the proposal being assessed. This assessment ensures that development occurring on one site does not adversely impact on another site or jeopardise another site's development potential.

4.3.2 Recommendations

This recommendation has been actioned. No further recommendation required.

4.4.0 Introduce environmental design into the DCP including passive solar design and natural ventilation.

4.4.1 Findings/Analysis

Environmental design has been added into the DCP in Chapter B2 Ecologically Sustainable Development. This chapter includes provisions pertaining to passive design, water conservation, renewable energy, natural ventilation and general sustainable building practices.

4.4.2 Recommendations

This recommendation has been actioned. No further recommendation required.

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4.5.0 Permit commercial uses on sites on the south side of Ebley Street between Hollywood Avenue and Denison Street where they face potential overshadowing from development to the north.

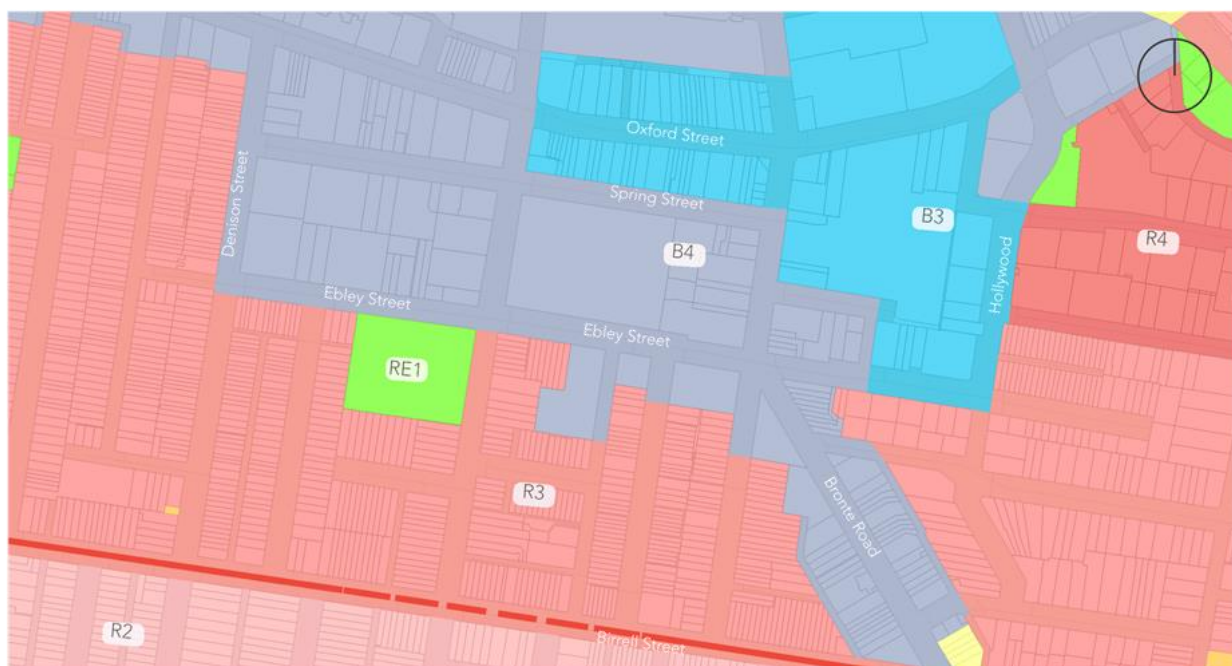


Figure 4 Sites originally identified for additional permitted uses

4.5.1 Findings/Analysis

The sites outlined in Figure 4 have remained without additional permitted commercial uses. The uses within this area are largely residential and the inclusion of any commercial uses within this area would likely require redevelopment, which would be difficult in this area due to their inclusion within a Heritage Conservation Area and the fine grain, fragmented lot pattern.

4.5.2 Recommendations

No further action required.

4.6.0 Conduct a block by block analysis to assess building massing options for redevelopment sites. This may determine appropriate setbacks in the context of existing building massing, block depth, lot size and access consideration.

4.6.1 Findings/Analysis

This exercise was completed and informed the comprehensive LEP 2012.

4.6.2 Recommendations

No further action required.

4.7.0 Develop a strategic plan for the renewal of the area between Ebley Street and Birrell Street to consider the possible expansion of Bondi Junction southward.



Figure 5 Fine grain lot size between Ebley Street & Birrell Street

4.7.1 Findings/Analysis

The expansion southward of Bondi Junction Strategic Centre did not occur as the area between Ebley Street and Birrell Street remains zoned as R3 Medium Density Residential. This area likely remains zoned as such due to the fine grain nature of the lot sizes in this area and the existence of Heritage Conservation Areas (Figure 5). For mixed use development to occur here various adjoining lots would have to be bought and consolidated to create viable floorspace for commercial use.

4.7.2 Recommendations

Do not pursue the possibility of expanding the Bondi Junction Strategic Centre southwards to include the area between Ebley Street and Birrell Street, given the existing HCAs and fragmented lot pattern.

5.0 Waverley DCP Podium Controls Review

5.1.0 Findings/Analysis

The current Waverley DCP podium controls apply to development in the Bondi Junction commercial centre. The controls list that areas without heritage on the street must build a podium to the street alignment up to six storeys, areas with heritage on the street must build a podium to the street alignment up to two/three storeys. There has been an inconsistency of compliance and non-compliance with these controls with developments in Bondi Junction as seen in the examples below in Table 3.

Table 3 Examples of podium heights and setbacks for tower developments in Bondi Junction

Address	Podium Height	Compliance	Setback
292-302 Oxford Street	5 storey podium	Yes	No
306 Oxford Street	3 storey podium	Yes	Yes
310 Oxford Street	3 storey podium	Yes	Yes
59-75 Grafton Street	6 storey podium	Yes	Yes
241 Oxford Street	2 storey podium	No	No
253-255 Oxford Street	2 storey podium	No	Yes
570-588 Oxford Street	7 storey podium	Yes	No

The issue that is arising from many developments within Bondi Junction is that the storeys above the top podium level are not setting back the required 6m, so the top podium level and the continuation of the tower above are almost non-distinguishable. Many of the sites developed have smaller setbacks meaning the tower is not pronounced, which has set a precedent that many other tower developments are following; potentially resulting in greater overshadowing and impacts on amenity. The inconsistencies in compliance with these controls brings the viability of the DCP controls into question, which is important when considering how to apply the controls to the remaining developable sites.

There are currently four sites in Bondi Junction that have yet to redevelop in recent years and are likely to redevelop in the future. They are listed below:

- 47-53 Spring Street
- 376-382 Oxford Street
- 6-8 Denison Street
- 10 Denison Street (DA lodged in December 2019)

The developments that have occurred on streets marked with heritage (306 and 310 Oxford Street) have built to compliance with the controls and therefore helped to create and maintain a consistent tower built form in this area. Therefore, it is important that the 376-382 Oxford Street site also complies with the development controls to maintain the consistent built form of the towers in this area. Being also identified in the heritage streets in the DCP, 6-8 and 10 Denison Street also need to comply with the 2/3 storey podium and setback controls to ensure the consistency of tower forms in this area.

The towers on Spring Street have not complied with the podium height or setback requirements, as has been a somewhat common theme in the non-heritage listed streets. Therefore, it could be considered unreasonable to expect development occurring on 47-53 Spring Street to follow the controls of the DCP as the development would look out of place with the rest of the towers in this area. In this instance any tower development should be designed to be consistent in scale and form with the towers surrounding the site.

5.1.1 Recommendations

The podium height and setback controls listed for 376-382 Oxford Street, 6-8 Denison Street and 10 Denison Street should be maintained and enforced. The podium height and setback controls for any development on 47-53 Spring Street should be assessed on their merit for the consistency with the built form of surrounding towers.

6.0 West Oxford Street Design Charette

The West Oxford Street Design Charette was developed in 2015 with the intention of generating ideas for the long-term redevelopment of the area into a precinct that provides a fitting threshold to Bondi Junction, with better connections to adjacent areas, and a better place for the community to live, work, shop and enjoy. The West Oxford Street precinct area is identified in Figure 6.



Figure 6 West Oxford Street Precinct Area outlined in red

Arising out of the charette were recommendations pertaining to built-form, public domain and road re-configurations for the short, medium and long-term.

Much of the public domain works recommendations consisted of upgrades to the footbridge adjoining West Oxford Street and Woollahra, upgrades to footpaths and cycleways in the immediate vicinity, formulating incentives for developers to offset provisions for cultural assets and street tree planting.

The built-form recommendations outlined in the charette included increases to maximum heights and floorspace ratios, development of the bus depot, creation of through-site links, key sites being identified, and design excellence provisions being required for these key sites. The recommendations for changing the development controls included:

- A continuous 6 storey edge along Oxford St with small footprint development up to 9 stories along the Syd Einfeld frontage to a maximum depth of 30m
- 4 storeys on the southern side of Oxford St east of St James Rd to the south side of Mill Hill Road
- Development up to 4 stories over the bus depot, with bus access beneath, for a maximum depth of 20m along Oxford Street frontage (subject to discussions with TfNSW and Sydney Buses)
- Varied FSR's to modulate built form along Oxford St

A recent PP at 194-214 Oxford Street, showcased a resounding community sentiment to maintain the current development controls listed in the LEP and that developments exceeding the controls were not supported. Notwithstanding this sentiment, the NSW Government approved a new maximum height limit of 36m, allowing a tower form at the low rise edge of Bondi Junction, surrounded by 15m and 9.5m height limited and isolated from the cluster of towers to the east. Given the overwhelming community feedback on the 194 Oxford Street PP to maintain the existing height and FSR controls, it is unlikely that the community would support any development above the bus depot or any other changes to height in the West Oxford Street area. To reflect community sentiment, it is envisioned that the current LEP controls in West Oxford Street will remain unchanged.

The sites outlined in the West Oxford Street Design Charette also act as transitional sites from the large-scale towers of the commercial centre to the much smaller scale residential properties in the surrounds. Therefore, it is important to keep these areas as transitional so that the scale of building heights gradually steps down to the residential areas so that the amenity of these areas is not adversely impacted. An important point to note is that the Design Charette was inconsistent with one of the aims of the LEP to provide a transition from taller commercial centres to residential surrounds. The next section will explore this transitional principle in more detail, focussing on controls and recent development in edge areas.

7.0 Treatment of controls in edge areas

As alluded to above, Clause 1.2(d) of the Waverley LEP outlines the control “to provide an appropriate transition in building scale around the edge of the commercial centres to protect the amenity of surrounding residential areas”. This control reflects the principles outlined in the BJUDR (Figure below), which argues that there should be a transition in heights on the edges of Bondi Junction. In particular, the control is most pertinent for West Oxford Street, Bronte Road and Ebley Street. Each of these areas are facing development pressure in the form of DAs and PPs. In this context, there areas are analysed to determine whether the transition principle has been upheld and is still appropriate for the future.

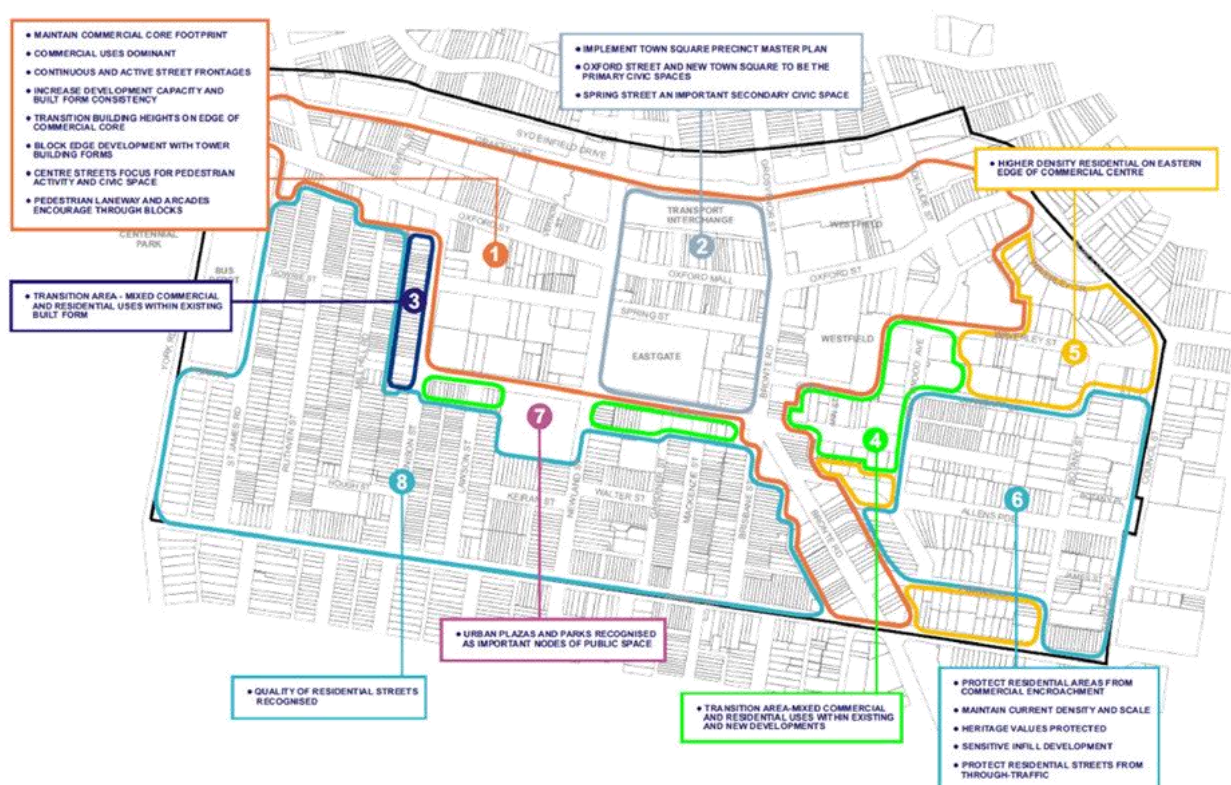


Figure 7 Area specific recommendations of the original Bondi Junction Urban Design Review

7.1.0 West Oxford Street

7.1.1 Findings/Analysis

The current LEP height controls for the West Oxford Street area show a transition from 38m to 12.5m to 9.5m in order to minimise impacts of the commercial centre on the surrounding residential area, as seen in Figure 8.

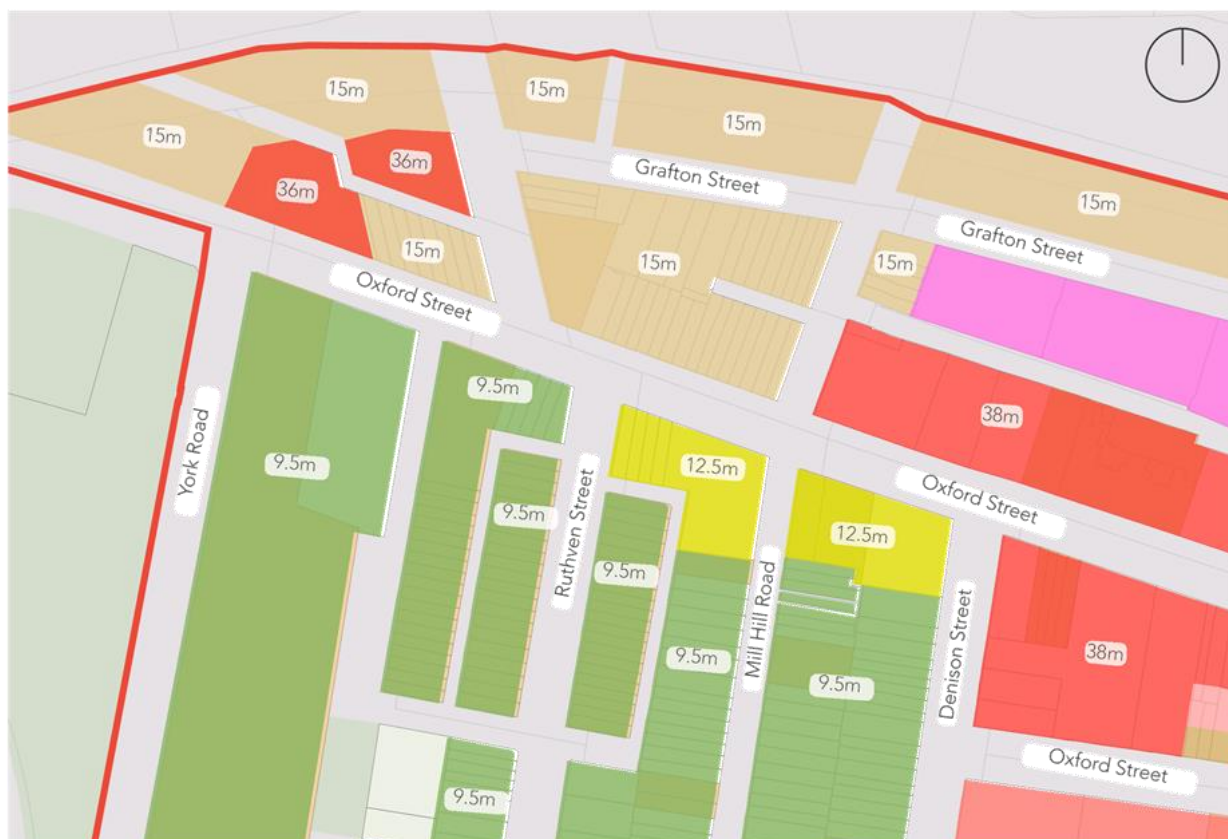


Figure 8 LEP height controls West Oxford Street Area

Many of the West Oxford Street buildings are two storeys in height and maintain a consistent streetscape and built form. West Oxford Street is one of the few remaining areas in Bondi Junction that has maintained this low-scale form. Much of the transitional nature of the West Oxford Street area is retained in the low-density residential character of the residential streets and two-three storey shop top housing buildings on Oxford Street as seen in Figure 9.



Figure 9 Consistent two storey shop top housing along Oxford Street

There is consistent height of buildings west of Denison Street with many retaining the 9.5m height in the form of terrace housing as seen in Figure 9. This built form is maintained with each street heading West and even exhibits greater levels of one storey buildings. Given that this area comprises rows of terraces, is covered by a Heritage Conservation Area and with numerous listed items, this area will remain largely unchanged. This underscores the need for transitional development so that towers do not adversely impact this residential area, especially its heritage status with issues such as overshadowing.



Figure 10 Terrace housing on Denison Street, Bondi Junction

As discussed in the previous section, West Oxford Street has been the subject of a controversial Planning Proposal that could set a poor precedent for development within the area. The 194 Oxford Street Planning Proposal increased the permissible height on the sites from 15m to 36m and increased the FSR from 1.5:1 to 3.5:1. Both

this height and bulk are exceedingly out of scale with the surrounding areas and betray the both the transitional objectives and low scale residential nature of the area. These increased controls allow for development that would significantly tower above the surrounding buildings and would resemble developments that occur in the heart of the commercial centre, not the edge residential areas. Any development to such a large size on the 194 Oxford Street sites would be considered an outlier development. The community echoed this sentiment during the exhibition of the PP as there was over 400 submissions opposing the development and only 10 submissions supporting it. As such, this PP and subsequent development should not be used as precedence to inform an increase in controls in the West Oxford Street area as it would be out of scale with the surrounding area and would be met with great community backlash. The recently adopted LSPS for Waverley contains a key action, based on overwhelming community feedback, to “enhance public domain throughout Bondi Junction, and curate a ‘village feel’ along West Oxford Street and Bronte Road”. Figure A below shows the renderings of the 194 Oxford Street PP building height that would be allowed under the new controls. It can clearly be seen that the building is significantly out of scale with the surrounding West Oxford Street area and substantially impacts on the amenity of the area as an anomaly on the two-storey view along West Oxford Street.

The transitional 12.5m heights from the centre’s 38m height is also currently being maintained as the Continental tyres site remains as a single storey building and the neighbouring hardware store stands only at two storeys. Both developments immediately adjacent to the 38m height limit of the centre maintain a modest height and as a result have minimal impact on the low-scale heritage residential area to the south. The compound of units at 39-43 Oxford Street on the corner of Mill Hill Rd and Oxford Street, is the only development in this 12.5m height transitional zone that is above two storeys and yet it remains within the limit at a total height of 11.7m.

The current LEP height limit of 12.5m that is set for the northern most lots between Denison St and Ruthven St is intended to be used as a transitional height to minimise development impacts on the surrounding residential dwellings. Given that the current buildings in this area are consistent with the controls, it is considered that these heights are fulfilling this objective. The sites within the 9.5m area are complying with these height limits as well as most dwellings in this area maintain a consistent two storey height with some even standing at single storey. Given the prevailing character of one and two storey residential dwellings in the West Oxford Street area, and the weight of community feedback received for this area to maintain the existing character, it is important that the LEP controls only allow for development that will respect the existing character and residential amenity of this area.



Figure A Renderings of 194 Oxford Street PP building height

7.1.2 Recommendations

Remain consistent with the LSPS direction of maintaining the character and neighbourhood feel of West Oxford Street by maintaining the current LEP controls and do not allow for increased development standards in order to respect the existing low-scale built-form and amenity of area.

7.2.0 Bronte Road

7.2.1 Findings/Analysis

Similar to the West Oxford Street area, the Bronte Road corridor acts as a transitional zone between the heights of the Bondi Junction commercial centre and the surrounding residential area. Similarly, the Bronte Road corridor is flanked by conservation areas immediately to the east and west of the corridor and the corridor itself contains clusters of heritage listed items. However, unlike the West Oxford Street area, there is less consistency in built form and heights as the centre is characterised by a variety of different building heights and typologies.

The building typologies along Bronte Road range from two storey shop top housing, single storey light industrial uses, four storey mixed use shop-top housing, terraces and residential flat buildings. At either end of the Bronte Road corridor are developments that are considered outliers as they both tower over surrounding development, standing at 8 storeys on the northern end and 6 storeys on the southern end respectively. Like the 194 Oxford Street development, these outlier developments are in contravention to the transition aim of the LEP and are not considered a precedence for the rest of the Bronte Road corridor to follow. The Bronte Road corridor built form can be viewed in Figure 11 below.

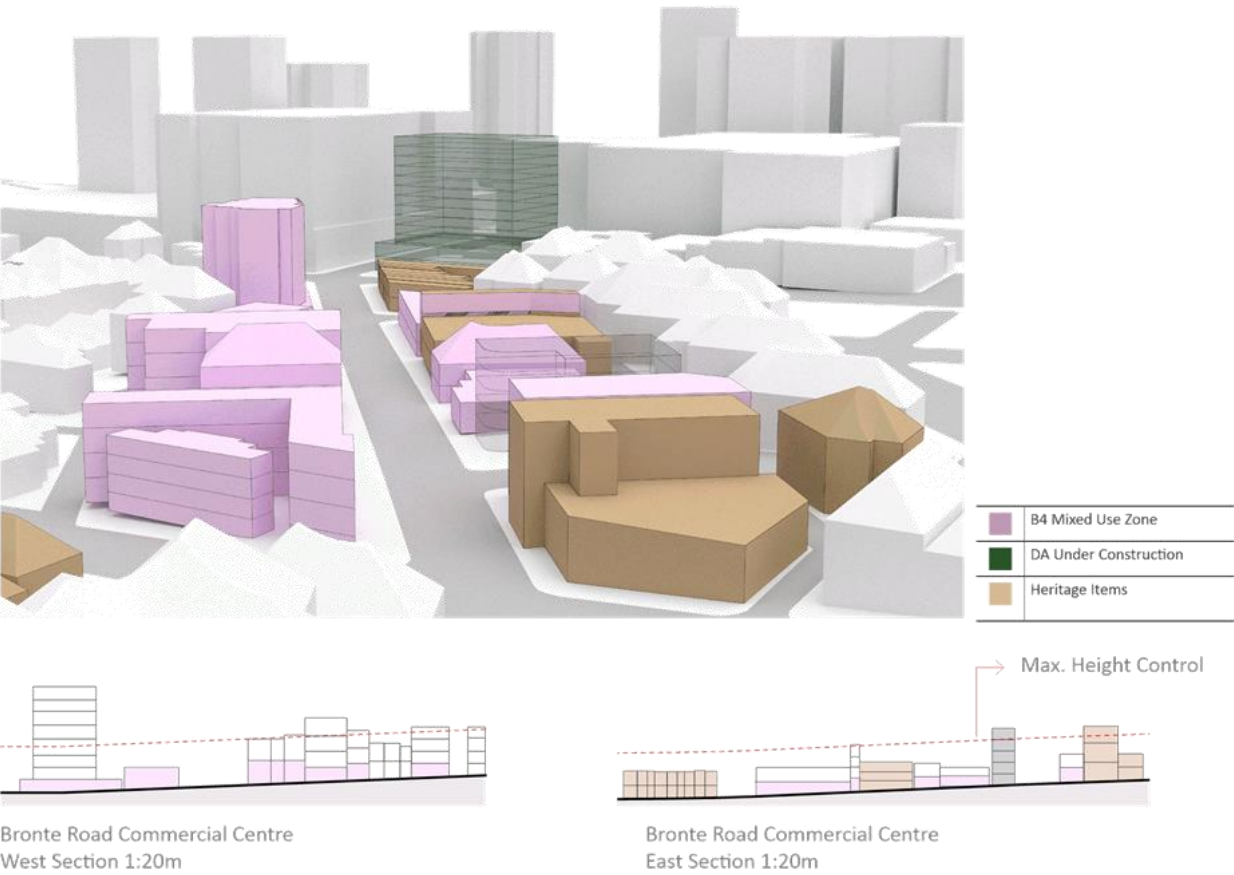


Figure 11 Bronte Road corridor built form

Much of the residential area to the west of Bronte Road consists of single storey housing and two storey terrace housing on fine grain lots; most of this area is also listed as a Heritage Conservation Area. It is therefore considered very important to minimise the impacts of development along Bronte Road on this area to not diminish the setting of the Heritage Conservation Area. Council’s current height and heritage controls are shown on Figure 12 below.



Figure 12 Bronte Road corridor LEP height and heritage mapping

To the east of the Bronte Road corridor is much larger lot and building sizes ranging from two-three storeys in height. Many of the developments on these lots consist of three storey residential flat buildings and two storey single dwelling houses. As seen above in Figure 12 this area is also listed as a heritage conservation area and so therefore any potential impacts from development on Bronte Road would have to be considered in this area. The larger lot sizes and taller heights of the buildings in this area does mean that development on the eastern side of Bronte Road would have less impact on the neighbouring HCA. In order to protect the significance of the two heritage conservation areas on either side of the Bronte Road corridor it is important that any new development be in line with the existing controls listed along this centre.

7.2.2 Recommendations

Maintain the existing LEP controls along the Bronte Road corridor.

7.3.0 Ebley Street

7.3.1 Findings/Analysis

Ebley Street acts as an edge area that transitions from the towers of the commercial centre to the residential areas to the south. In order to determine if the development controls applying to this fringe area are still appropriate, recent PPs and DAs along Ebley Street will be analysed to see how they did or didn't comply with the LEP development standards. The 96-122 Ebley Street Planning Proposal will be analysed, as will the Bondi Junction RSL development application and the Grand Hotel redevelopment. The locations of these are shown below in Figure 13.



Figure 13 Ebley Street recent Planning Proposals and Development Applications

The most recent Planning Proposal on Ebley Street was located at 96-122 Ebley Street, Bondi Junction. This PP initially proposed a change from 32m in height to 35m in height but the increased height was not supported citing issues of impacts on the amenity of the surrounding residential area. The finalised version that Council has supported has maintained the existing LEP height controls and has received an increase in FSR from 4:1 to 6:1 to incentivise redevelopment of the sites. As this site is on the fringe of the Bondi Junction commercial centre it is more difficult to attract commercial tenants as it is further away from amenities and services such as access to the Bondi Junction rail interchange. Given this difficulty to find a willing developer, the FSR was increased from 4:1 to 6:1 with a residential FSR of 1:1, this was done to help incentivise redevelopment of the site as residential development is a much easier and secure development for developers to undertake. This instance has shown

that the existing controls in the area are suitable and that any change to FSR has simply been done to incentivise redevelopment which will help the sites to reach their commercial floor space potential. This PP suggests that the LEP heights are still appropriate and perhaps a minor increase in FSR could be considered, where any additional impacts are acceptable.

The Bondi Junction RSL redevelopment was approved in November 2019 for a 10 storey mixed use building containing a registered club, retail and 80 residential units. The development was approved with a FSR far below the maximum FSR, the maximum being a ratio of 6:1 and the development being approved for 4.52:1. Due to discussions with Council and a likely refusal, the height was reduced from 13 storeys to 10 storeys. This 10 storey height represented only a 3m height exceedance at its highest point which was a result of lift/overrun heights and as such was considered acceptable by the Land and Environment Court. Given that this was such a significant development over various sites, it is considered that this minor exceedance in height does not suggest that the controls were necessarily inappropriate for the area, especially as the FSR was considerably under the maximum ratio allowed on the site.

The Grand Hotel redevelopment finished construction in late 2019. The original DA proposed a height of up to 22m in a 15m height limit and a FSR of 2.34:1 in a max FSR limit of 2:1. Council refused these controls on the grounds of negative impacts on the surrounding area with specific issues of bulk and scale being referenced. The DA was later approved with a FSR exceedance of only 5% which was 2.05:1 and a height totalling 17m lift/overrun which was only 2m higher than the height limit. Given that both are only relatively small exceedances from the LEP development standards it is reasonable to suggest that the controls on this site are applicable, especially given that they were used to bring the original proposed height and FSR down to something more reasonable.

From analysing the recent developments and Planning Proposal it is reasonable to suggest that the current LEP height and FSR controls are still applicable and have in fact been used to achieve a more acceptable development with minimal impacts on the surrounding area. Whilst there was a need for an increase in FSR in the Ebley Street PP there was ample FSR left unused as part of the Bondi Junction RSL development and the Grand Hotel development was only minimally above the FSR. Similarly, whilst the Grand Hotel had a 13% exceedance in height and the RSL 9%, this exceedance was a result of lift overruns and the bulk of the building is relatively consistent with the development standards. The Planning Proposal maintaining the height also shows that this control was suitable.

7.3.2 Recommendations

Maintain the LEP controls on Ebley Street to ensure that development in these edge areas remains transitional to reduce the impact on surrounding areas.

8.0 Bondi Junction Town Square Review 2007 Recommendations

8.1.0 Land Use; Land use proposed for the commercial centre - B3 Commercial Core.

8.1.1 Findings/Analysis

This recommendation was supported and Oxford Street Mall was zoned B3 Commercial Core.

8.1.2 Recommendations

No further action required.

8.2.0 Floor Space Ratio; Rowe Street Development Sites 5:1, Bronka Arcade Development Sites FSR 7:1.



Figure 14 LEP Floor Space Ratio Map - Oxford Street Mall

8.2.1 Findings/Analysis

This recommendation was not implemented in the Waverley LEP but FSRs of similar sizes were added. Figure 14 shows the current LEP FSRs of Oxford Street Mall, these are not too far removed from the recommended FSRs.

8.2.2 Recommendations

Ensure that the FSRs for sites are compatible with the height of buildings in any changes to the LEP.

8.2.0 Proposed Building Heights

Rowe Street Sites: A range of heights stepping from 10m on Oxford Street Mall, a mid height of 20m rising up to a maximum of 24m along Grosvenor Lane (see Figure 15 below).

Bronka Arcade Sites (see Figure 15 below):

- 20m maximum height on the proposed public Square
- 15m over Spring Street
- 28m to the West of the proposed Square
- 24m to the East of the proposed Square up to a maximum of 45m on the corner of Bronte Road



Figure 15 2004 Bondi Junction Town Square Masterplan Proposed building heights, from The Bondi Junction Town Square Peer Review Subission, 2008.

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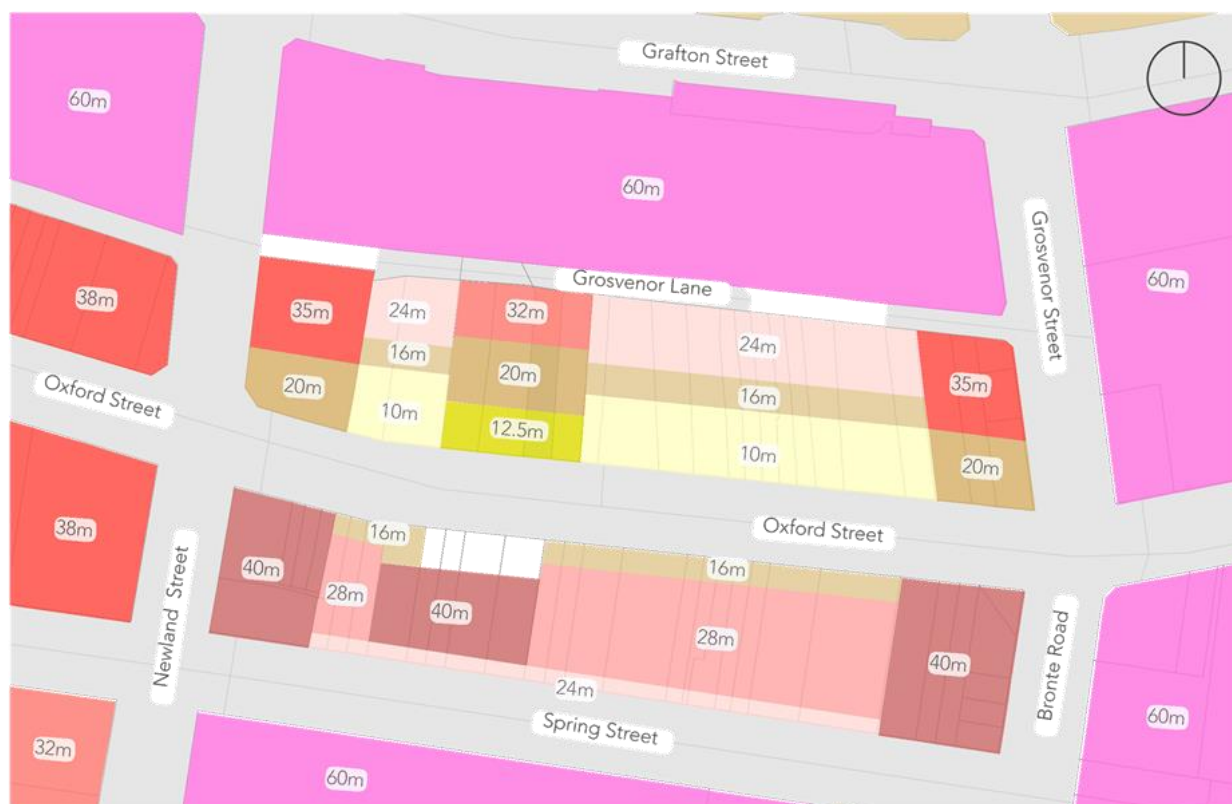


Figure 16 Current LEP heights of Oxford Street Mall

8.3.1 Findings/Analysis

The recommendations shown in Figure 16 were not carried through to the Waverley LEP, however the stepped building heights concept was carried through. Figure 16 shows the current building heights of Oxford Street Mall.

Whilst there are different heights to what was proposed the stepped height concept remains. The stepped height is derived from the winter sun angle and ensuring that the mall receives adequate sunlight year-round.

Little development has occurred around Oxford Street mall, since the WLEP 2012 compared to other parts of Bondi Junction. This likely reflects the price differential for residential vs commercial development and the highly fragmented nature of lot ownership. The challenge that these lot sizes represent is that individual redevelopment of each lot could create sub-optimal built form outcomes as each lot would have a single, narrow development. Ways to incentives new development in and around the mall should be explored to ensure that Bondi Junction maintains its employment status as a Strategic Centre. There could

be development incentives for lot amalgamation, creating a preferable built form outcome. Similarly, an residential incentive may be another way of encouraging commercial development, like the Ebley Street PP. Such changes would have to be considered carefully as introducing residential uses into the B3 zone could compromise the commercial integrity of that zone.

8.3.2 Recommendations

Investigate ways to facilitate office floor space development in this area.





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Bondi Junction Urban Design Review Update 2020

ACKNOWLEDGEMENT

We acknowledge the Bidjigal and Gadigal people who
traditionally occupied the Sydney coast.

We also acknowledge Aboriginal Elders both past, present
and emerging.

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Executive Summary



A number of strategies have been completed for Bondi Junction in the past decade or so, outlining a series of recommendations and principles for future development in Bondi Junction that informed the current policy settings. Since around 2012 there has been significant redevelopment in Bondi Junction and there is continual pressure on redevelopment on this centre.

Given this context of change, the purpose of the *Bondi Junction Urban Design Review Update 2020* (the Review) is to evaluate the current principles and policy framework applying to development in Bondi Junction to ensure that they are valid and up to date.

The Review examines three studies – the original Bondi Junction Urban Design Review (BJUDR), the West Oxford Street Design Charette and the Bondi Junction Town Square Review – looking at their recommendations and principles and asking the following key questions:

- How these have been implemented?
- If they weren't implemented, are they still valid?

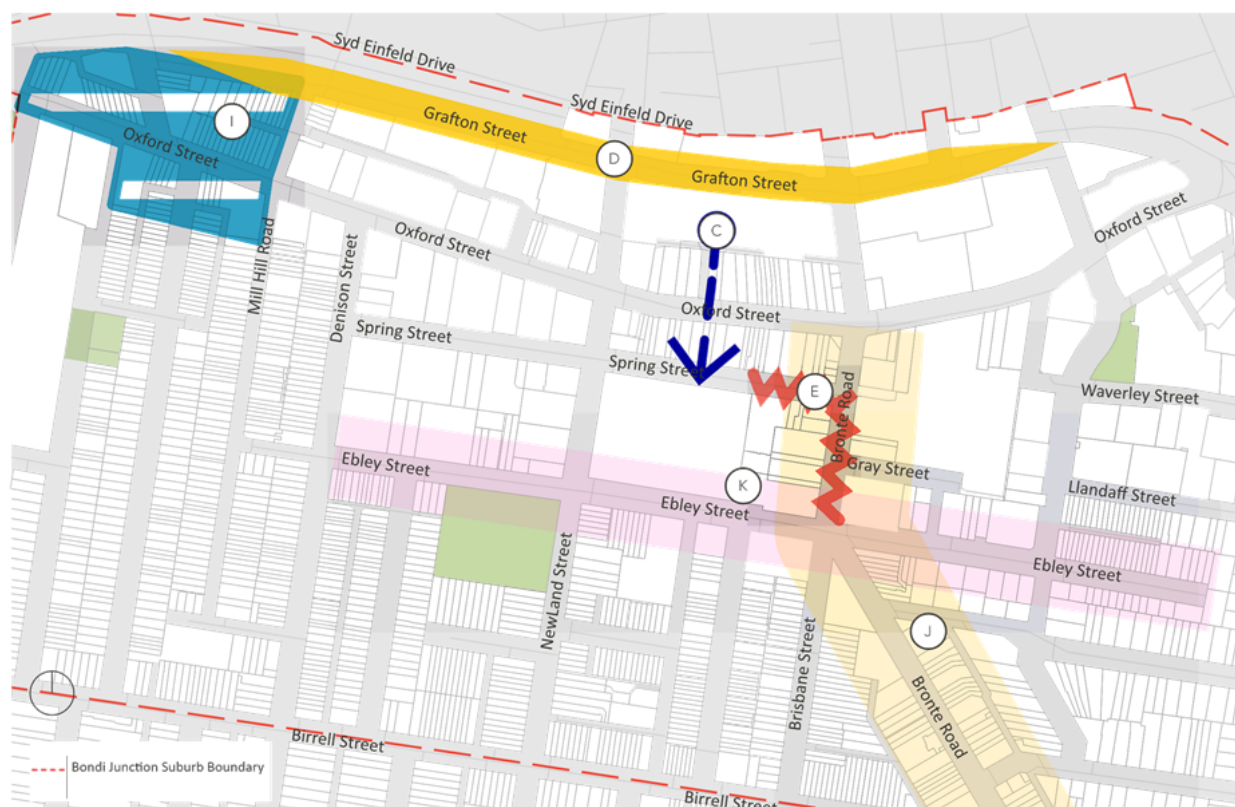
- What has happened since their implementation?
- What have the community said in response to new development?
- Do the original recommendations still stand in the context of change?

Recommendations from these studies have been considered as: public domain amenity, development potential and policy and design recommendations. Most of the recommendations from these studies have been actioned and no further work is required for these.

Where recommendations have not been fulfilled, the Review makes recommends further work or in some instances no further work. One of the most significant recommendations from the original BJUDR was for a transition in building height towards the edges of Bondi Junction.

This principle is enshrined in one of the aims of the WLEP 2012. This Review recommends that this principle and associated planning controls be upheld.

Summary of Recommendations



Public Domain Amenity Recommendations

Recommendation A	Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the BJ Complete Streets Project.
Recommendation B	Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the Complete Streets project and Waverley's People, Movement and Places Strategy.
Recommendation C	Continue to investigate using Rowe Street to link Oxford Street and Spring Street to the Bondi Junction rail interchange.
Recommendation D	Improve public domain of Grafton Street towards and underneath Syd Enfield Drive in connection with the relevant recommendations of the Open Space and Recreation Strategy & Bondi Junction Night Activation Strategy.
Recommendation E	Investigate further mechanisms to reduce pedestrian and vehicle conflicts in Spring Street and Bronte Road.

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Development Potential Recommendations

Recommendation F	Continue to work with the DPIE to update planning controls so that commercial floorspace is retained and grows in Bondi Junction Strategic Centre.
Recommendation G	Investigate LEP height and DCP storey control inconsistencies further with assistance from the Development Assessment Team with an aim to resolving this conflict within either the comprehensive LEP review or DCP review.

Planning Policy and Design Recommendations

Recommendation H	Continue to apply the Waverley VPA policy for developments and planning proposals that are seeking uplift above their current development standards.
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Edge Area Controls – West Oxford Street

Recommendation I	Maintain the current LEP controls for the West Oxford Street area and do not allow for increased development standards in order to respect the existing low scale built form and amenity of the area.
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Edge Area Controls – Bronte Road Corridor

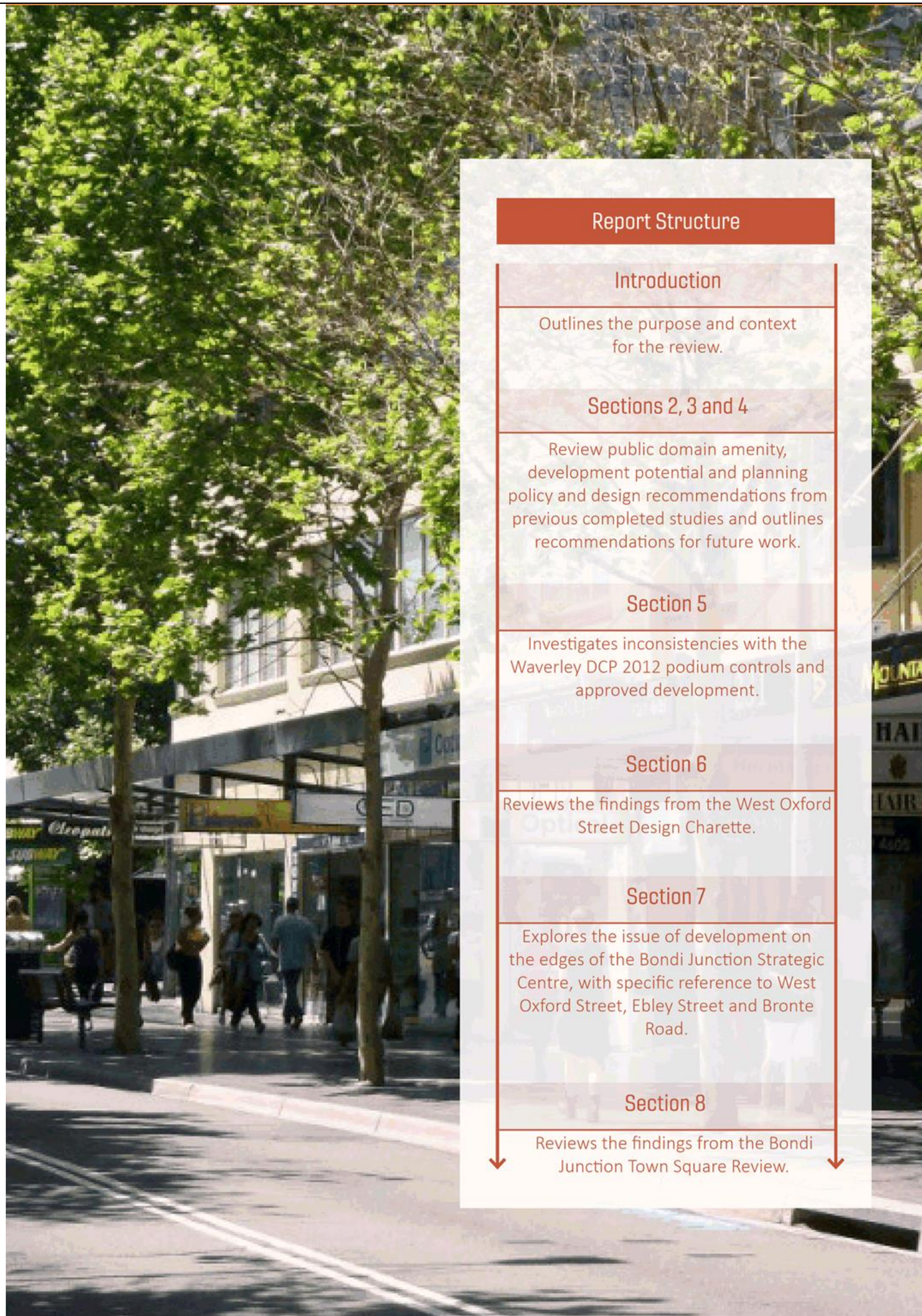
Recommendation J	Maintain the existing LEP controls along the Bronte Road corridor.
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Edge Area Controls – Ebley Street

Recommendation K	Maintain the LEP controls on Ebley Street to ensure that development in these edge areas remains transitional to reduce the impact on surrounding areas.
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Bondi Junction Town Square Peer Review 2007

Recommendation L	Ensure that the FSRs for sites are compatible with the height of buildings in any changes to the LEP.
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Report Structure

Introduction

Outlines the purpose and context for the review.

Sections 2, 3 and 4

Review public domain amenity, development potential and planning policy and design recommendations from previous completed studies and outlines recommendations for future work.

Section 5

Investigates inconsistencies with the Waverley DCP 2012 podium controls and approved development.

Section 6

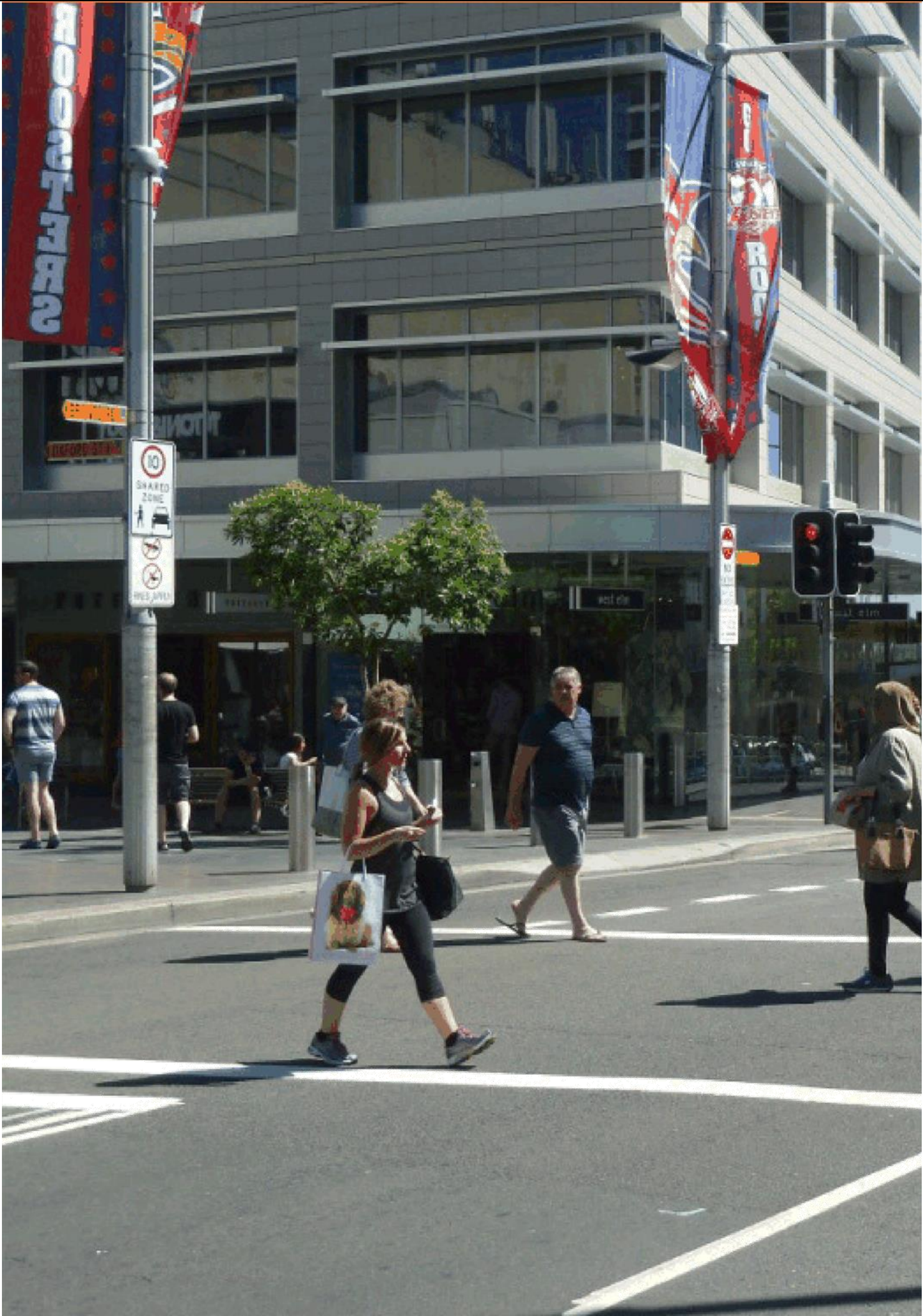
Reviews the findings from the West Oxford Street Design Charette.

Section 7

Explores the issue of development on the edges of the Bondi Junction Strategic Centre, with specific reference to West Oxford Street, Ebley Street and Bronte Road.

Section 8

Reviews the findings from the Bondi Junction Town Square Review.



1.0 Introduction

The Bondi Junction Urban Design Review (the BJUDR) was completed in 2013 (CityPlan) with a view to identifying planning issues, accommodating development pressures and providing solutions to them within the Bondi Junction Strategic Centre. The BJUDR made recommendations primarily on built-form controls, the public domain and other planning policy and design. The recommendations and findings of the BJUDR were implemented in the Planning Proposal for the new Standard Instrument LEP of 2012, the DCP as well as applied to Planning Proposals and Development Applications for land within the Bondi Junction Strategic Centre.

Since that time there has been considerable development activity and continued development pressure in Bondi Junction (Figure 1.1 & 1.2). The purpose of this update is to analyse how planning within the Bondi Junction Strategic Centre has been implemented, including other studies completed for Bondi Junction such as the West Oxford Street Design Charette and the Bondi Junction Town Square Review. In particular, this analysis will examine Development Applications (DAs) and Planning Proposals (PPs) in the centre from 2013 to understand how the vision, aims and recommendations – in the form of LEP and DCP controls – of the BJUDR have stood the test of time. In undertaking this analysis this update makes recommendations for planning within the centre, with particular focus on whether the original aims and implemented controls are still relevant and suitable, and hence should continue to be applied, or if new controls should be considered.



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	Address	Status
1	95-97 Grafton Street & 422 Oxford Street, (Meriton Tower)	Under Assessment
2	55 Grafton Street, Demolition & construction of 20 storey mixed use tower	Under Assessment
3	10 Denison Street, Demolition & construction of mixed use tower	Under Assessment
4	5-11 Hollywood Avenue, Demolition & construction of a new 11 storey hotel	Under Assessment
5	21 Waverley Crescent, Demolition & construction of a 9 storey RFB	Under Assessment
6	7-11 Bondi Road, Demolition & construction of a 9 storey RFB (inc. affordable housing)	Approved
7	34-38 Bondi Road, Demolition & construction of a 3 storey RFB	Approved
8	27 Paul Street and 8-10 Council Street, Alts & Adds to existing heritage building, demolition & construction of 3 storey RFB (inc. affordable housing)	Approved
9	16A + 16B Llandaff Street, Demolition & construction of a 9 storey RFB	Approved
10	Bondi Junction RSL, 1-9 Gray Street, Demolition & construction of a 10 storey mixed use building	Approved
11	362-374 Oxford Street, Demolition & construction of a 14 storey mixed use building	Approved
12	59-75 Grafton Street, Demolition & construction of a mixed use tower	Approved
13	45-57 Oxford Street, Demolition & construction of mixed use building & alts & adds to existing heritage buildings	Approved
14	292 Oxford Street, Demolition & construction of 12 storey mixed use development	Under Construction
15	59-69 Oxford Street, Mixed use development including existing pub on ground floor	Under Construction
16	87-99 Oxford Street, and 16-22 Spring Street, Demolition & construction of 14 storey mixed use building	Under Construction
17	552-568 Oxford Street, Demolition & construction of a mixed use building	Under Construction
18	20 + 22 Llandaff Street, Demolition & construction of RFB building	Under Construction

19	50 Waverley Street, Alts & Adds to existing RFB	Completed
20	91-95 Old South Head Road, Demolition & construction of RFB	Completed
21	33 Waverley Street, Demolition & construction of RFB	Completed
22	253-255 Oxford Street, Demolition & construction of a 20 storey mixed use building (inc. affordable housing)	Completed
23	570-588 Oxford Street, Demolition & construction of a 22 storey RFB with ground floor retail	Completed
24	241 Oxford Street, Demolition & construction of mixed use building	Completed
25	6 Bronte Road, Demolition & construction of 8 storey commercial building	Completed
26	73-89 Ebley Street, Demolition & construction of a 4 storey mixed use development	Completed
27	139 Oxford Street, Demolition & construction of a 9 storey commercial building with hotel uses	Completed
28	109 Oxford Street, Demolition & construction of a new 12 storey mixed use building	Completed
29	350-354 Oxford Street, Demolition & construction of a mixed use building	Completed
30	310 Oxford Street, Demolition & construction of a 10 storey mixed development	Completed
31	3014 Oxford Street, Demolition & construction of a 12 storey mixed use development	Completed
32	95-97 Grafton Street, Construction of new serviced apartments over 4 levels	Completed

Figure 1.2 Bondi Junction Development Status Table

2.0 Public Domain Amenity Recommendations

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2.1 Develop a Public Domain masterplan to identify key Public Domain projects.

2.1.1 Findings/Analysis

One of the key recommendations of the BJUDR relating to public domain was for a masterplan to be developed identifying key public domain projects and spaces in Bondi Junction. This recommendation has been actioned through the creation of the Complete Streets project. The Complete Streets project directly responds to various recommendations made in the BJUDR relating to public domain principles. The Complete Streets project addresses this recommendation as it considers the various needs of all users of public space by having provisions to ensure the streets are pedestrian-friendly, provide good access to public transport, accommodate cyclists, are safe and benefit the local economy.

2.1.2 Recommendation

Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the Complete Streets project.

2.2 Develop a movement strategy that integrates pedestrian and vehicular movements and has a clear focus on the quality of the public domain.

2.2.1 Findings/Analysis

This recommendation has been actioned through the creation of the Complete Streets project and Waverley's *People, Movement and Places Strategy*. Waverley's *People, Movement and Places Strategy* was developed to address the issues associated with pedestrian and vehicular movements within Waverley more broadly. This was created to address current transport issues and to best meet the changing demands of the future. The *People, Movement and Places Strategy* also outlines provisions for improving the public domain through upgrading cycleways, footpaths, roads and shared pedestrian and vehicle spaces. The Complete Streets project also considers the needs of pedestrians and vehicles equally in direct response to this recommendation of the BJUDR.

2.2.2 Recommendation

Continue to ensure any future developments or upgrades to public domain areas are done in accordance with the Complete Streets project and Waverley's *People, Movement and Places Strategy*.

2.3 Improve the connection from the rail interchange to Oxford Street and further on to Spring Street

2.3.1 Findings/Analysis

Council has been actively pursuing a means of improving the connection from the rail interchange to Oxford Street and Spring Street. Council has identified Rowe Street as a possibility to create a direct connection to the railway station as the street has been closed and made available for redevelopment. Rowe Street was official closed as a road in 2016 to facilitate the development of this link. A feasibility study was completed in 2019 and was reported to Council who adopted the recommendation to sell the Rowe St property.

2.3.2 Recommendation

To prepare for the sale of the Rowe St incorporating a satisfactory legal framework to ensure that the public benefits including design outcomes and the connection to the Transport Interchange are safeguarded.

2.4 Create through site links

2.4.1 Findings/Analysis

The BJUDR outlined that through-site links should be negotiated as a public benefit from new development, particularly where they provide a demonstrable time saving through long, impassable blocks. Planning Proposals in recent years have often included tokenistic through-site links that do little in the way of reducing walk times and hence are not considered to provide public benefit and as such should not be considered as a redeeming feature of a development.

2.4.2 Recommendation

Continue to support and request provision of through-site links in DAs and PPs if they provide a demonstrable public benefit.

2.5.0 Improve public domain of Grafton Street and towards and underneath Syd Enfield Drive

2.5.1 Findings/Analysis

One of the actions of the Complete Streets project is to consider adaptive reuse of space under Syd Enfield Drive with a long-term goal to create more street level activity. It also recommends improving lighting and access of carparks under Syd Enfield Drive with a vision to properly activating this space past the point of being carparking. This is also being investigated as part of Council's Open Space and Recreation Strategy. This strategy will provide recommendations as to how the space under Syd Enfield Drive can be used for public urban recreation space. This strategy is intended to be finished in 2020.

Therefore, this recommendation should remain as a recommendation and should also reference the Open Space and Recreation Strategy.

2.5.2 Recommendation

This recommendation is being investigated as part of the Open Space and Recreation Strategy.

2.6.0 Create share zones in parts of Spring Street and Bronte Road

2.6.1 Findings/Analysis

The BJUDR found that there were no shared zones in Spring Street or Bronte Road. The four-way pedestrian crossing point at the corner of Oxford Street and Bronte Road and the crossing at Oxford Street and Ebley Street were considered appropriate crossing points for pedestrian foot traffic within this area. The BJUDR recommended further investigations to determine whether it would be appropriate to have this section closed to general traffic and only allow for buses, taxis and deliveries.

2.6.2 Recommendation

Investigate further mechanisms to reduce pedestrian and vehicle conflicts in Spring Street and Bronte Road.

2.7.0 Create solar access planes for important public open spaces within the Bondi Junction Centre

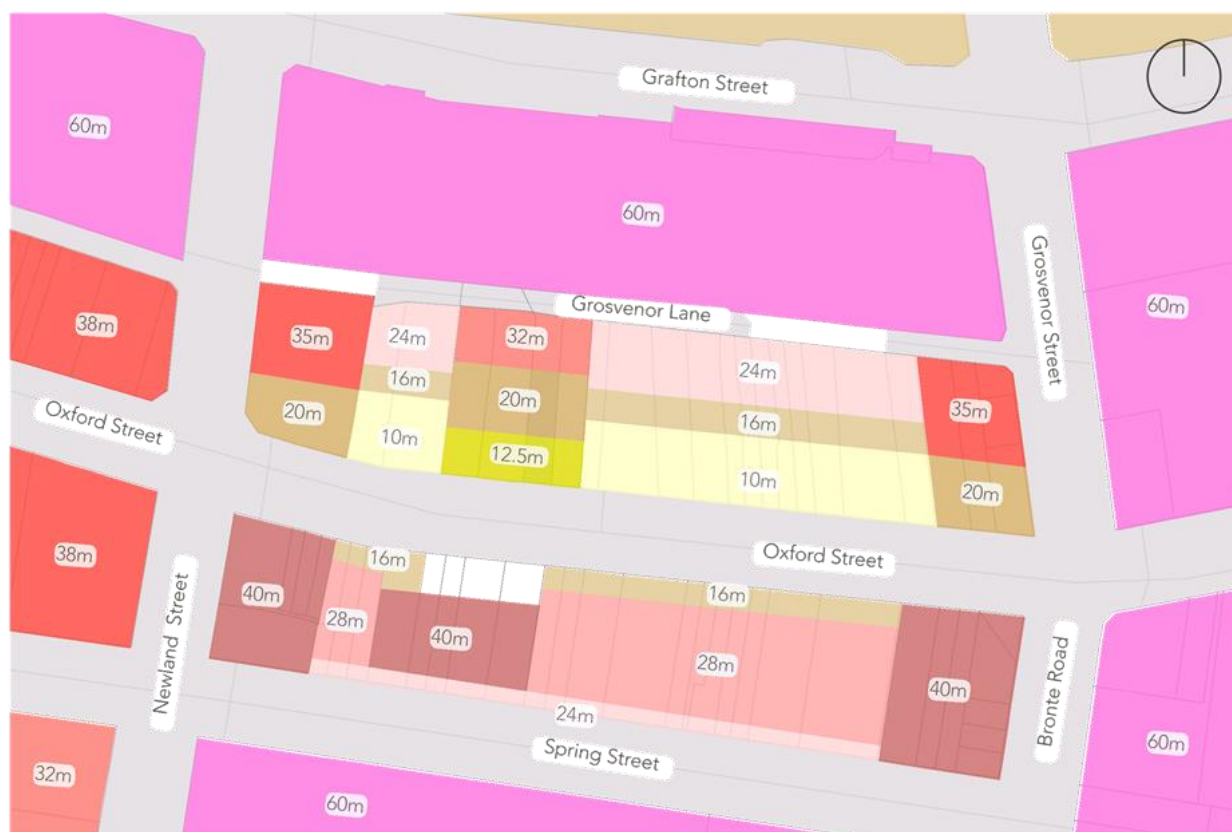


Figure 3 Stepped maximum height of building controls surrounding Oxford Street Mall

2.7.1 Findings/Analysis

As a result of this recommendation an Additional Local Provision was added into the Waverley LEP 2012. Clause 6.7 *Solar access to public spaces in Bondi Junction* in the Waverley LEP was added into the LEP to ensure buildings were designed to maximise sunlight access to important public places in Bondi Junction. The clause outlines the provision that development consent must not be granted to development that results in any part of a building causing an additional shadow impact at 12 noon on 21 June (winter solstice) at the following places: *Clemenston Park, Waverley Street Mall, Eora Park, Norman Lee Place & Oxford Street Mall*.

This clause also aligns with recommendations from the Bondi Junction Town Square Peer Review Submission

that suggested stepped height controls based on the angle of the winter sun, so that Oxford Street Mall could continue to receive sunlight regardless of new development. The stepped heights can be seen in Figure 3.

2.7.2 Recommendation

This recommendation has been actioned. No further action required with this recommendation.

2.8.0 Prevent midwinter lunchtime overshadowing of Clemenston Park by amending the WLEP 2012 height and FSR along the northern side of Ebley Street

2.8.1 Findings/Analysis

Whilst the height and FSR along the northern side of Ebley Street was not changed, Clemenston Park is protected from overshadowing due to the provisions of Clause 6.7 *Solar access to public spaces in Bondi Junction*. As discussed in the previous recommendation, Clause 6.7 provides protection to Clemenston Park as it outlines that any development that causes additional overshadowing impact to Clemenston Park must not be granted development consent.

2.8.2 Recommendation

Whilst this specific recommendation has not been actioned, the intent of the outcome has been actioned. No further action is required for this recommendation.



3.0 Development Potential Recommendations

3.1.0 Improve flexibility in land use by amending the WLEP 2012 to change the zone of the commercial core that lies to the West of Newland Street from B3 Commercial Core to B4 Mixed Use

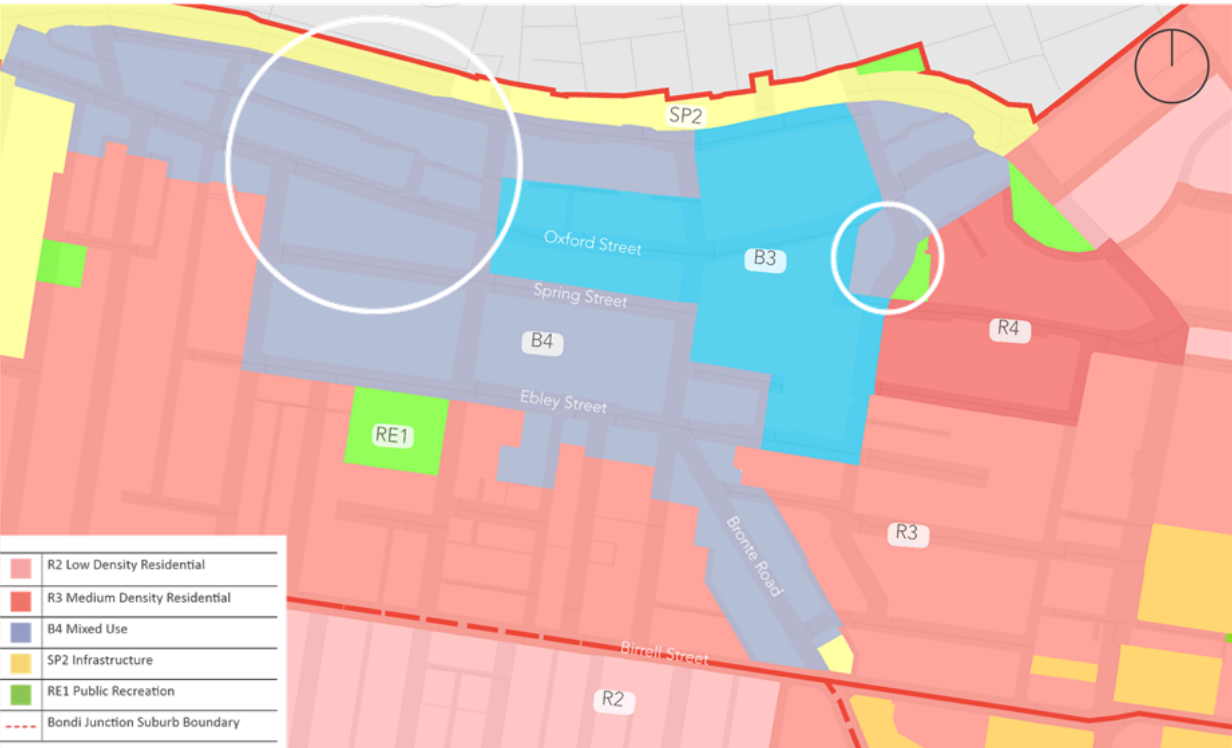


Figure 4 Areas rezoned from B3 Commercial Core to B4 Mixed Use as part of the WLEP 2012

3.1.1 Findings/Analysis

This recommendation was actioned as part of the Waverley Local Environment Plan 2012, the areas in Figure 4 were rezoned from B3 Commercial Core to B4 Mixed Use.

The intention of this was to allow for more residential space within Bondi Junction so that there could be an equal distribution of commercial and residential floorspace with the intention that residents could live and work in Bondi Junction. Council originally sought controls to ensure that new development had a

50/50 mix of commercial and residential. This was not supported by the DPIE. Instead of creating an equal distribution of commercial and residential uses, the rezonings led to an outcome which saw residential uses dominate commercial uses as residential development was much more profitable to developers. As a result, BJ has experienced a loss of 9,000m² in commercial floorspace since 2014 with a further loss of 19,000m² projected in the pipeline.

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The *Bondi Junction Strategic Centre: Protecting and Promoting Commercial Floorspace PP* was lodged with the DPIE in mid-2019 in order to stem the loss of commercial floorspace and ensuring that quasi-residential uses such as serviced apartments could not be used in the commercial floorspace areas. This can be achieved through retaining all the remaining B3 Commercial Core zoned land in Bondi Junction and prohibiting quasi-residential uses such as serviced apartments in this zone (which the PP is attempting to do).

3.1.2 Recommendation

This recommendation has been implemented, with the unanticipated perverse outcome of not delivering a truly mixed-use outcome. Continue to work with the DPIE to update planning controls so that commercial floorspace is retained and grows in Bondi Junction Strategic Centre.

3.2.0 Consider tall office buildings on the block between Oxford Street, Bronte Road and Gray Street (facilitating the opportunity for A-grade office space in Bondi Junction).

3.2.1 Findings/Analysis

It was decided to not implement this recommendation as part of the comprehensive LEP 2012 as there was no mechanism in place to capture any contribution from the windfall gain that these office towers would be getting as a result of this recommendation. It was considered that, as the community could not share in this uplift, and that increasing the development controls would only serve to benefit individual landowners through increased land values, that this recommendation would not be actioned.

3.2.2 Recommendation

No further action required.

3.3.0 Use potential for additional storeys within LEP height limit at selected locations (at present the number of storeys is limited by the DCP which at some locations does not correspond with the full LEP height potential).

3.3.1 Findings/Analysis

There has been an issue arising from development applications within Bondi Junction that has highlighted an inconsistency between the LEP maximum height limit and the DCP floor to ceiling height controls. This has meant that many developments have seemingly not been able to achieve a maximum amount of storeys due to floor to ceiling heights not matching with LEP height controls. This has meant many development applications have used Clause 4.6 Variation applications to gain heights exceeding the LEP height limit.

3.3.2 Recommendation

Investigate this further with assistance from the Development Assessment team with an aim to resolving this conflict within either the Comprehensive LEP review or DCP review.

3.4.0 Acknowledge that certain sites may be the subject of acceptable Development Applications which exceed development controls, if they do not create unacceptable impacts and they meet obligation to improve infrastructure and facilities.

3.4.1 Findings/Analysis

This recommendation was actioned through the creation of Council's Voluntary Planning Agreement (VPA) policy in 2014 (discussed further below). The VPA policy has been used to positive effect in numerous DAs where it could be demonstrated that exceedances of the development controls would not have adverse impacts on the surrounds and could provide a positive public benefit for the surrounding area.

3.4.2 Recommendation

Continue to negotiate VPAs for sites where it is deemed appropriate to exceed development controls.



4.0 Planning Policy and Design Recommendations

4.1.0 Develop a framework for VPA and (now) S7.11 contributions particularly for sites that might be the subject of Development Applications significantly over the existing controls.

4.1.1 Findings/Analysis

Waverley's VPA policy was created in 2014 in order to secure a public benefit from developments and PPs that were receiving a value uplift. The policy provides developers the opportunity to voluntarily provide public benefits to the community in the form of monetary contributions, land dedications and public domain and infrastructure upgrades.

The Planning Proposal at 194-214 Oxford Street and 2 Nelson Street, Bondi Junction (194 Oxford St PP) is an example of how the VPA policy could be implemented to achieve positive outcomes for the community. Council had attempted on various occasions to refuse the Planning Proposal stating issues with the controls being out of context with surrounds, however the NSW Department of Planning, Industry and Environment used its delegation to approve the development. The VPA policy was used to great effect, as although the Planning Proposal was approved, Council was able to gain a \$7.45million contribution from the developers that would be used to help improve community facilities. Since its creation in 2014 the VPA policy has been used to secure \$16.75million for the purposes of community benefits.

4.1.2 Recommendations

This recommendation has been actioned and as such the resulting VPA policy should continue to be used to secure public benefits. Continue to use the Waverley VPA policy for developments and planning proposals that are seeking uplift above their current development standards.

4.2.0 Require development applications to consider impact on surrounding sites. This may include three dimensional testing of overshadowing.

4.2.1 Findings/Analysis

All DAs and PPs are required to assess the impact on surrounding sites, this is one of the key factors in merit assessment. Shadow diagrams form a pivotal part of assessing the impact of a development on surrounding lots and as such are a requirement for applications for dwelling houses, dual occupancies, multi-residential development, mixed use development and commercial development over one storey high.

4.2.2 Recommendations

This recommendation has been actioned. No further recommendation required.

4.3.0 Require development applications to illustrate how neighbouring sites can be renewed or redeveloped adjacent to the proposal.

4.3.1 Findings/Analysis

This has been a key factor in assessing both DAs and PPs. This assessment is undertaken to ensure that the development opportunities of surrounding sites are not sterilised due to restraints enforced on them by the proposal being assessed. This assessment ensures that development occurring on one site does not adversely impact on another site or jeopardise another site's development potential.

4.3.2 Recommendations

This recommendation has been actioned. No further recommendation required.

4.4.0 Introduce environmental design into the DCP including passive solar design and natural ventilation.

4.4.1 Findings/Analysis

Environmental design has been added into the DCP in Chapter B2 Ecologically Sustainable Development. This chapter includes provisions pertaining to passive design, water conservation, renewable energy, natural ventilation and general sustainable building practices.

4.4.2 Recommendations

This recommendation has been actioned. No further recommendation required.

4.5.0 Permit commercial uses on sites on the south side of Ebley Street between Hollywood Avenue and Denison Street where they face potential overshadowing from development to the north.

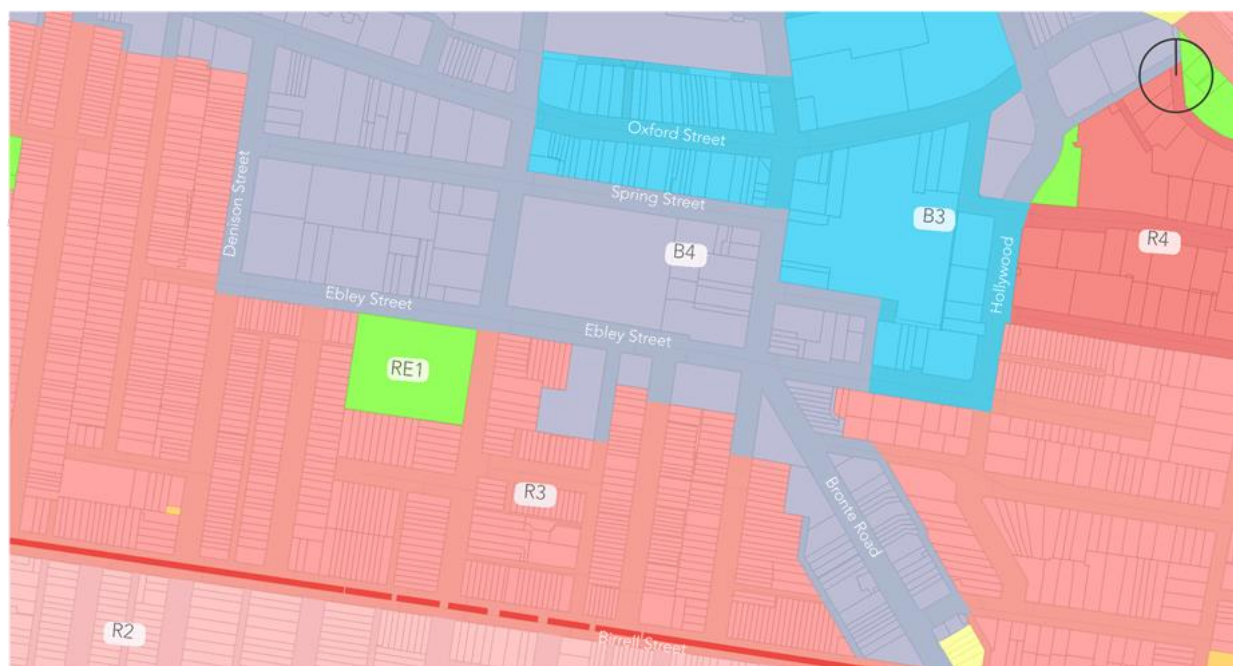


Figure 4 Sites originally identified for additional permitted uses

4.5.1 Findings/Analysis

The sites outlined in Figure 4 have remained without additional permitted commercial uses. The uses within this area are largely residential and the inclusion of any commercial uses within this area would likely require redevelopment, which would be difficult in this area due to their inclusion within a Heritage Conservation Area and the fine grain, fragmented lot pattern.

4.5.2 Recommendations

No further action required.

4.6.0 Conduct a block by block analysis to assess building massing options for redevelopment sites. This may determine appropriate setbacks in the context of existing building massing, block depth, lot size and access consideration.

4.6.1 Findings/Analysis

This exercise was completed and informed the comprehensive LEP 2012.

4.6.2 Recommendations

No further action required.

4.7.0 Develop a strategic plan for the renewal of the area between Ebley Street and Birrell Street to consider the possible expansion of Bondi Junction southward.



Figure 5 Fine grain lot size between Ebley Street & Birrell Street

4.7.1 Findings/Analysis

The expansion southward of Bondi Junction Strategic Centre did not occur as the area between Ebley Street and Birrell Street remains zoned as R3 Medium Density Residential. This area likely remains zoned as such due to the fine grain nature of the lot sizes in this area and the existence of Heritage Conservation Areas (Figure 5). For mixed use development to occur here various adjoining lots would have to be bought and consolidated to create viable floorspace for commercial use.

4.7.2 Recommendations

Do not pursue the possibility of expanding the Bondi Junction Strategic Centre southwards to include the area between Ebley Street and Birrell Street, given the existing HCAs and fragmented lot pattern.

5.0 Waverley DCP Podium Controls Review

5.1.0 Findings/Analysis

The current Waverley DCP podium controls apply to development in the Bondi Junction commercial centre. The controls list that areas without heritage on the street must build a podium to the street alignment up to six storeys, areas with heritage on the street must build a podium to the street alignment up to two/three storeys. There has been an inconsistency of compliance and non-compliance with these controls with developments in Bondi Junction as seen in the examples below in Table 3.

Table 3 Examples of podium heights and setbacks for tower developments in Bondi Junction

Address	Podium Height	Compliance	Setback
292-302 Oxford Street	5 storey podium	Yes	No
306 Oxford Street	3 storey podium	Yes	Yes
310 Oxford Street	3 storey podium	Yes	Yes
59-75 Grafton Street	6 storey podium	Yes	Yes
241 Oxford Street	2 storey podium	No	No
253-255 Oxford Street	2 storey podium	No	Yes
570-588 Oxford Street	7 storey podium	Yes	No

The issue that is arising from many developments within Bondi Junction is that the storeys above the top podium level are not setting back the required 6m, so the top podium level and the continuation of the tower above are almost non-distinguishable. Many of the sites developed have smaller setbacks meaning the tower is not pronounced, which has set a precedent that many other tower developments are following; potentially resulting in greater overshadowing and impacts on amenity. The inconsistencies in compliance with these controls brings the viability of the DCP controls into question, which is important when considering how to apply the controls to the remaining developable sites.

There are currently four remaining sites in the Bondi Junction commercial centre that have redevelopment potential, they are listed below:

- 47-53 Spring Street
- 376-382 Oxford Street
- 6-8 Denison Street
- 10 Denison Street (DA lodged in December 2019)

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The developments that have occurred on streets marked with heritage (306 and 310 Oxford Street) have built to compliance with the controls and therefore helped to create and maintain a consistent tower built form in this area. Therefore, it is important that the 376-382 Oxford Street site also complies with the development controls to maintain the consistent built form of the towers in this area. Being also identified in the heritage streets in the DCP, 6-8 and 10 Denison Street also need to comply with the 2/3 storey podium and setback controls to ensure the consistency of tower forms in this area.

The towers on Spring Street have not complied with the podium height or setback requirements, as has been a somewhat common theme in the non-heritage listed streets. Therefore, it could be considered unreasonable to expect development occurring on 47-53 Spring Street to follow the controls of the DCP as the development would look out of place with the rest of the towers in this area. In this instance any tower development should be designed to be consistent in scale and form with the towers surrounding the site.

5.1.1 Recommendations

The podium height and setback controls listed for 376-382 Oxford Street, 6-8 Denison Street and 10 Denison Street should be maintained and enforced. The podium height and setback controls for any development on 47-53 Spring Street should be assessed on their merit for the consistency with the built form of surrounding towers.

6.0 West Oxford Street Design Charette

The West Oxford Street Design Charette was developed in 2015 with the intention of generating ideas for the long-term redevelopment of the area into a precinct that provides a fitting threshold to Bondi Junction, with better connections to adjacent areas, and a better place for the community to live, work, shop and enjoy. The West Oxford Street precinct area is identified in Figure 6.



Figure 6 West Oxford Street Precinct Area outlined in red

Arising out of the charette were recommendations pertaining to built-form, public domain and road re-configurations for the short, medium and long-term.

Much of the public domain works recommendations consisted of upgrades to the footbridge adjoining West Oxford Street and Woollahra, upgrades to footpaths and cycleways in the immediate vicinity, formulating incentives for developers to offset provisions for cultural assets and street tree planting.

The built-form recommendations outlined in the charette included increases to maximum heights and floorspace ratios, development of the bus depot, creation of through-site links, key sites being identified, and design excellence provisions being required for these key sites. The recommendations for changing the development controls included:

- A continuous 6 storey edge along Oxford St with small footprint development up to 9 stories along the Syd Einfeld frontage to a maximum depth of 30m
- 4 storeys on the southern side of Oxford St east of St James Rd to the south side of Mill Hill Road
- Development up to 4 stories over the bus depot, with bus access beneath, for a maximum depth of 20m along Oxford Street frontage (subject to discussions with TfNSW and Sydney Buses)
- Varied FSR's to modulate built form along Oxford St

A recent PP at 194-214 Oxford Street, showcased a resounding community sentiment to maintain the current development controls listed in the LEP and that developments exceeding the controls were not supported. Notwithstanding this sentiment, the NSW Government approved a new maximum height limit of 36m, allowing a tower form at the low rise edge of Bondi Junction, surrounded by 15m and 9.5m height limited and isolated from the cluster of towers to the east. Given the overwhelming community feedback on the 194 Oxford Street PP to maintain the existing height and FSR controls, it is unlikely that the community would support any development above the bus depot or any other changes to height in the West Oxford Street area. To reflect community sentiment, it is envisioned that the current LEP controls in West Oxford Street will remain unchanged.

The sites outlined in the West Oxford Street Design Charette also act as transitional sites from the large-scale towers of the commercial centre to the much smaller scale residential properties in the surrounds. Therefore, it is important to keep these areas as transitional so that the scale of building heights gradually steps down to the residential areas so that the amenity of these areas is not adversely impacted. An important point to note is that the Design Charette was inconsistent with one of the aims of the LEP to provide a transition from taller commercial centres to residential surrounds. The next section will explore this transitional principle in more detail, focussing on controls and recent development in edge areas.

7.0 Treatment of controls in edge areas

As alluded to above, Clause 1.2(d) of the Waverley LEP outlines the control “to provide an appropriate transition in building scale around the edge of the commercial centres to protect the amenity of surrounding residential areas”. This control reflects the principles outlined in the BJUDR (Figure below), which argues that there should be a transition in heights on the edges of Bondi Junction. In particular, the control is most pertinent for West Oxford Street, Bronte Road and Ebley Street. Each of these areas are facing development pressure in the form of DAs and PPs. In this context, there areas are analysed to determine whether the transition principle has been upheld and is still appropriate for the future.

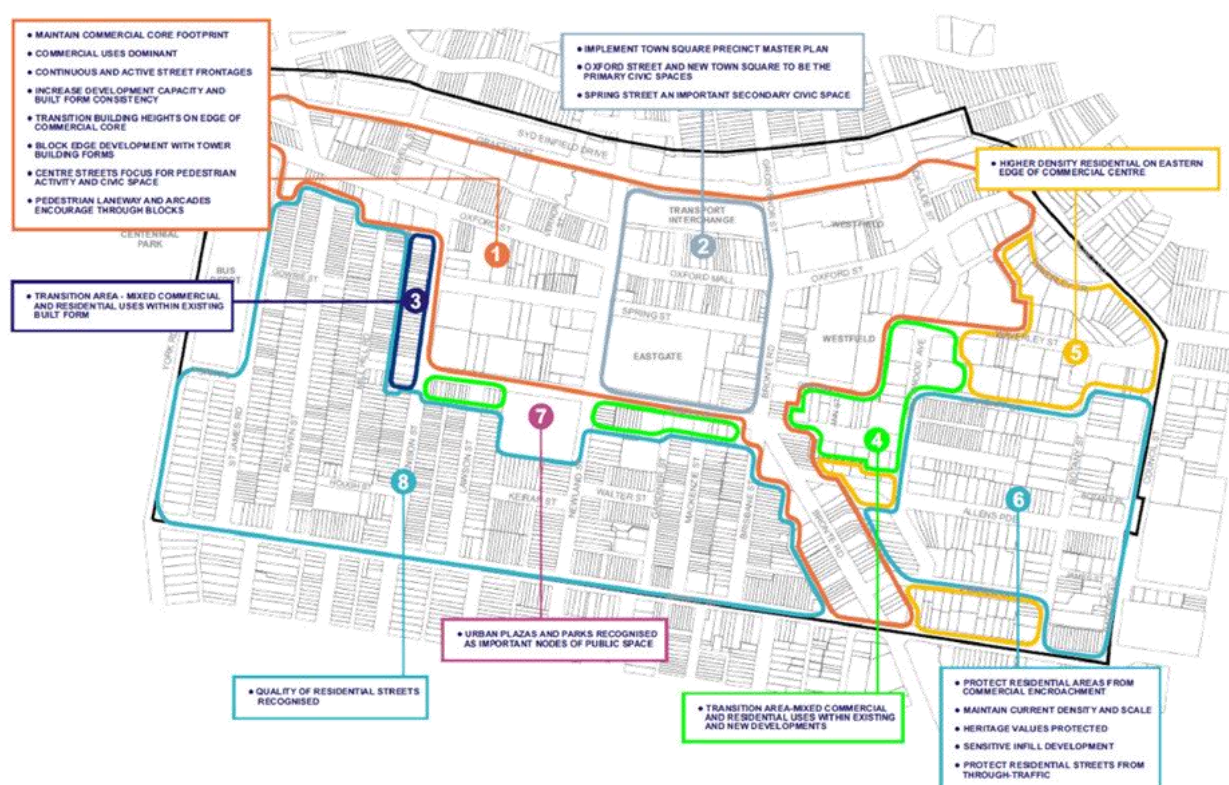


Figure 7 Area specific recommendations of the original Bondi Junction Urban Design Review

7.1.0 West Oxford Street

7.1.1 Findings/Analysis

The current LEP height controls for the West Oxford Street area show a transition from 38m to 12.5m to 9.5m in order to minimise impacts of the commercial centre on the surrounding residential area, as seen in Figure 8.

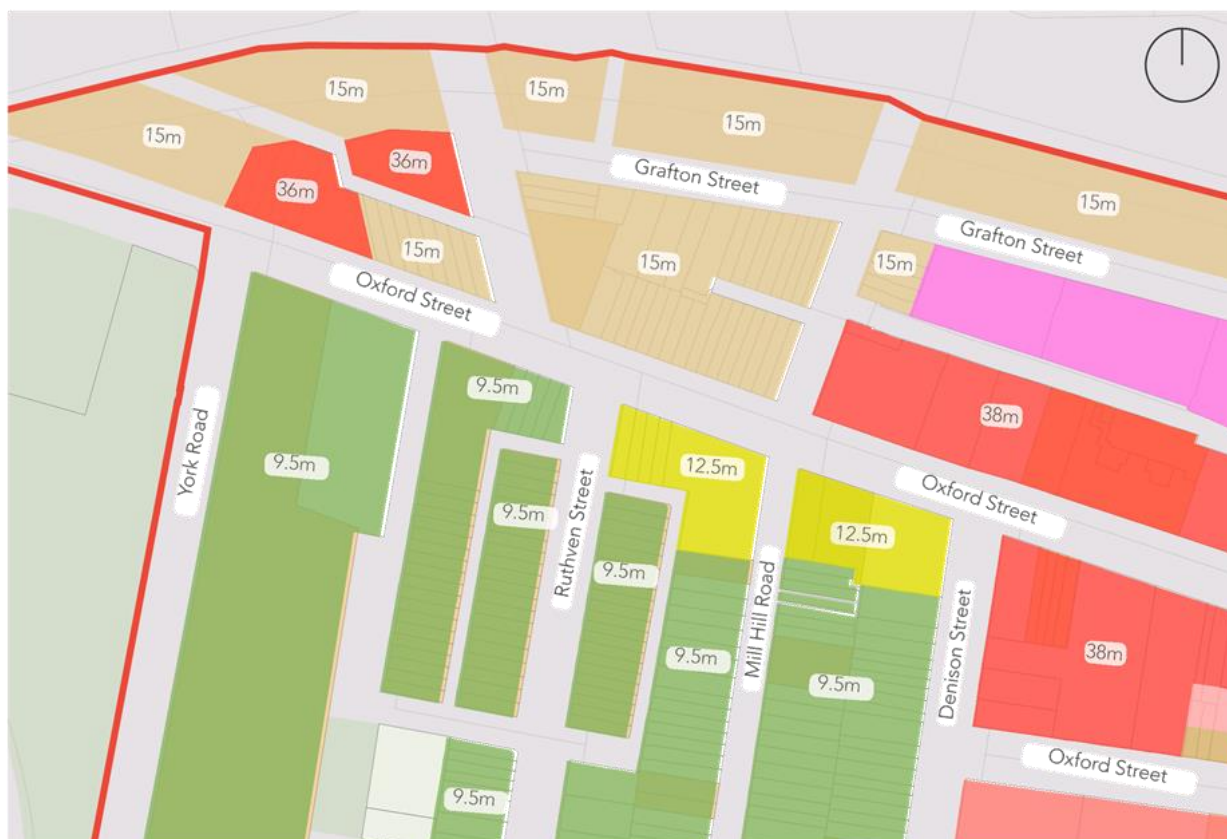


Figure 8 LEP height controls West Oxford Street Area

Many of the West Oxford Street buildings are two storeys in height and maintain a consistent streetscape and built form. West Oxford Street is one of the few remaining areas in Bondi Junction that has maintained this low-scale form. Much of the transitional nature of the West Oxford Street area is retained in the low-density residential character of the residential streets and two-three storey shop top housing buildings on Oxford Street as seen in Figure 9.



Figure 9 Consistent two storey shop top housing along Oxford Street

There is consistent height of buildings west of Denison Street with many retaining the 9.5m height in the form of terrace housing as seen in Figure 9. This built form is maintained with each street heading West and even exhibits greater levels of one storey buildings. Given that this area comprises rows of terraces, is covered by a Heritage Conservation Area and with numerous listed items, this area will remain largely unchanged. This underscores the need for transitional development so that towers do not adversely impact this residential area, especially its heritage status with issues such as overshadowing.



Figure 10 Terrace housing on Denison Street, Bondi Junction

As discussed in the previous section, West Oxford Street has been the subject of a controversial Planning Proposal that could set a poor precedent for development within the area. The 194 Oxford Street Planning Proposal increased the permissible height on the sites from 15m to 36m and increased the FSR from 1.5:1 to 3.5:1. Both

this height and bulk are exceedingly out of scale with the surrounding areas and betray the both the transitional objectives and low scale residential nature of the area. These increased controls allow for development that would significantly tower above the surrounding buildings and would resemble developments that occur in the heart of the commercial centre, not the edge residential areas. Any development to such a large size on the 194 Oxford Street sites would be considered an outlier development. The community echoed this sentiment during the exhibition of the PP as there was over 400 submissions opposing the development and only 10 submissions supporting it. As such, this PP and subsequent development should not be used as precedence to inform an increase in controls in the West Oxford Street area as it would be out of scale with the surrounding area and would be met with great community backlash. The recently adopted LSPS for Waverley contains a key action, based on overwhelming community feedback, to “enhance public domain throughout Bondi Junction, and curate a ‘village feel’ along West Oxford Street and Bronte Road”. Figure X below shows the renderings of the 194 Oxford Street PP building height that would be allowed under the new controls. It can clearly be seen that the building is significantly out of scale with the surrounding West Oxford Street area and substantially impacts on the amenity of the area as an anomaly on the two-storey view along West Oxford Street.

The transitional 12.5m heights from the centre’s 38m height is also currently being maintained as the Continental tyres site remains as a single storey building and the neighbouring hardware store stands only at two storeys. Both developments immediately adjacent to the 38m height limit of the centre maintain a modest height and as a result have minimal impact on the low-scale heritage residential area to the south. The compound of units at 39-43 Oxford Street on the corner of Mill Hill Rd and Oxford Street, is the only development in this 12.5m height transitional zone that is above two storeys and yet it remains within the limit at a total height of 11.7m.

The current LEP height limit of 12.5m that is set for the northern most lots between Denison St and Ruthven St is intended to be used as a transitional height to minimise development impacts on the surrounding residential dwellings. Given that the current buildings in this area are consistent with the controls, it is considered that these heights are fulfilling this objective. The sites within the 9.5m area are complying with these height limits as well as most dwellings in this area maintain a consistent two storey height with some even standing at single storey. Given the prevailing character of one and two storey residential dwellings in the West Oxford Street area, and the weight of community feedback received for this area to maintain the existing character, it is important that the LEP controls only allow for development that will respect the existing character and residential amenity of this area.

7.1.2 Recommendations

Maintain the current LEP controls for the West Oxford Street area and do not allow for increased development standards in order to respect the existing low-scale built-form and amenity of the area.

7.2.0 Bronte Road

7.2.1 Findings/Analysis

Similar to the West Oxford Street area, the Bronte Road corridor acts as a transitional zone between the heights of the Bondi Junction commercial centre and the surrounding residential area. Similarly, the Bronte Road corridor is flanked by conservation areas immediately to the east and west of the corridor and the corridor itself contains clusters of heritage listed items. However, unlike the West Oxford Street area, there is less consistency in built form and heights as the centre is characterised by a variety of different building heights and typologies.

The building typologies along Bronte Road range from two storey shop top housing, single storey light industrial uses, four storey mixed use shop-top housing, terraces and residential flat buildings. At either end of the Bronte Road corridor are developments that are considered outliers as they both tower over surrounding development, standing at 8 storeys on the northern end and 6 storeys on the southern end respectively. Like the 194 Oxford Street development, these outlier developments are in contravention to the transition aim of the LEP and are not considered a precedence for the rest of the Bronte Road corridor to follow. The Bronte Road corridor built form can be viewed in Figure 11 below.

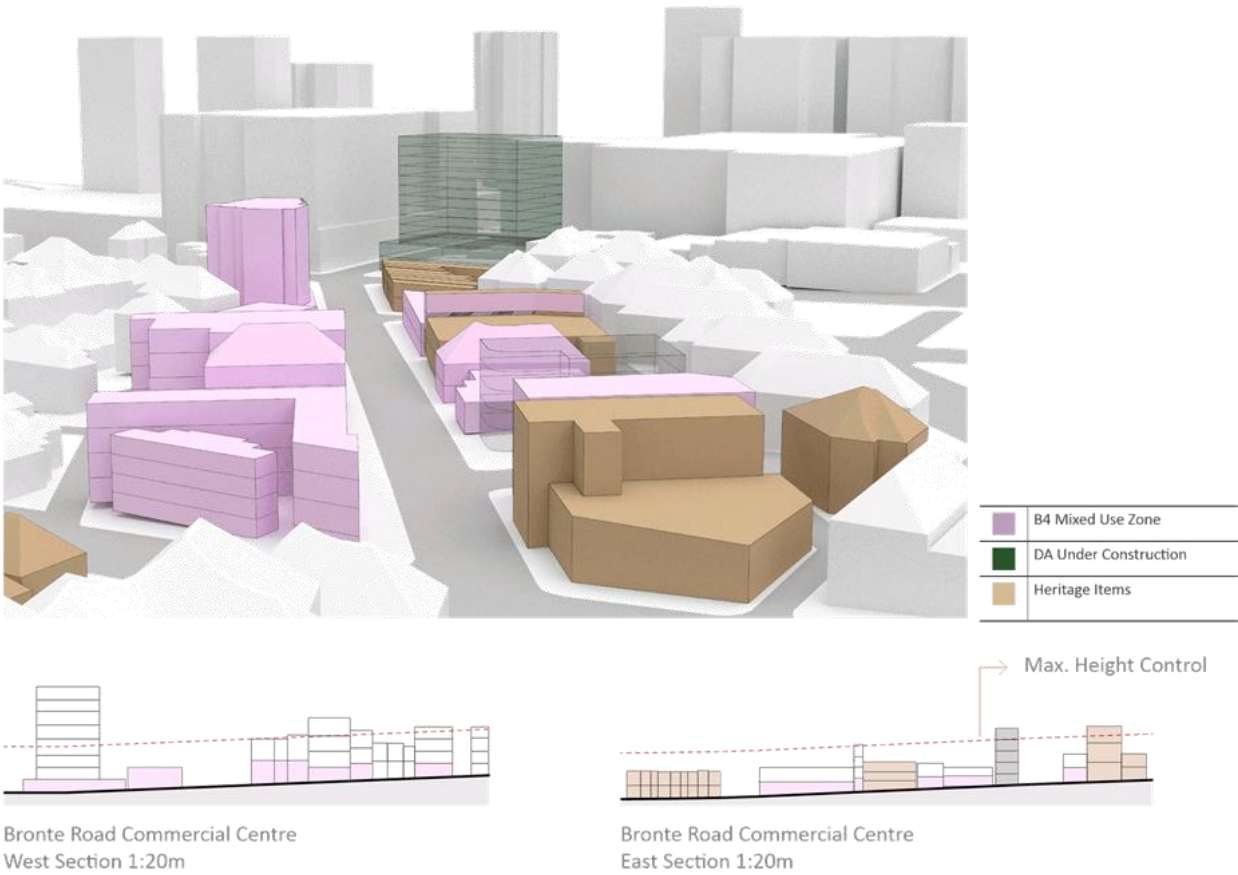


Figure 11 Bronte Road corridor built form

Much of the residential area to the west of Bronte Road consists of single storey housing and two storey terrace housing on fine grain lots; most of this area is also listed as a Heritage Conservation Area. It is therefore considered very important to minimise the impacts of development along Bronte Road on this area to not diminish the setting of the Heritage Conservation Area. Council’s current height and heritage controls are shown on Figure 12 below.



Figure 12 Bronte Road corridor LEP height and heritage mapping

To the east of the Bronte Road corridor is much larger lot and building sizes ranging from two-three storeys in height. Many of the developments on these lots consist of three storey residential flat buildings and two storey single dwelling houses. As seen above in Figure 12 this area is also listed as a heritage conservation area and so therefore any potential impacts from development on Bronte Road would have to be considered in this area. The larger lot sizes and taller heights of the buildings in this area does mean that development on the eastern side of Bronte Road would have less impact on the neighbouring HCA. In order to protect the significance of the two heritage conservation areas on either side of the Bronte Road corridor it is important that any new development be in line with the existing controls listed along this centre.

7.2.2 Recommendations

Maintain the existing LEP controls along the Bronte Road corridor.

7.3.0 Ebley Street

7.3.1 Findings/Analysis

Ebley Street acts as an edge area that transitions from the towers of the commercial centre to the residential areas to the south. In order to determine if the development controls applying to this fringe area are still appropriate, recent PPs and DAs along Ebley Street will be analysed to see how they did or didn't comply with the LEP development standards. The 96-122 Ebley Street Planning Proposal will be analysed, as will the Bondi Junction RSL development application and the Grand Hotel redevelopment. The locations of these are shown below in Figure 13.



Figure 13 Ebley Street recent Planning Proposals and Development Applications

The most recent Planning Proposal on Ebley Street was located at 96-122 Ebley Street, Bondi Junction. This PP initially proposed a change from 32m in height to 35m in height but the increased height was not supported citing issues of impacts on the amenity of the surrounding residential area. The finalised version that Council has supported has maintained the existing LEP height controls and has received an increase in FSR from 4:1 to 6:1 to incentivise redevelopment of the sites. As this site is on the fringe of the Bondi Junction commercial centre it is more difficult to attract commercial tenants as it is further away from amenities and services such as access to the Bondi Junction rail interchange. Given this difficulty to find a willing developer, the FSR was increased from 4:1 to 6:1 with a residential FSR of 1:1, this was done to help incentivise redevelopment of the site as residential development is a much easier and secure development for developers to undertake. This instance has shown

that the existing controls in the area are suitable and that any change to FSR has simply been done to incentivise redevelopment which will help the sites to reach their commercial floor space potential. This PP suggests that the LEP heights are still appropriate and perhaps a minor increase in FSR could be considered, where any additional impacts are acceptable.

The Bondi Junction RSL redevelopment was approved in November 2019 for a 10 storey mixed use building containing a registered club, retail and 80 residential units. The development was approved with a FSR far below the maximum FSR, the maximum being a ratio of 6:1 and the development being approved for 4.52:1. Due to discussions with Council and a likely refusal, the height was reduced from 13 storeys to 10 storeys. This 10 storey height represented only a 3m height exceedance at its highest point which was a result of lift/overrun heights and as such was considered acceptable by the Land and Environment Court. Given that this was such a significant development over various sites, it is considered that this minor exceedance in height does not suggest that the controls were necessarily inappropriate for the area, especially as the FSR was considerably under the maximum ratio allowed on the site.

The Grand Hotel redevelopment finished construction in late 2019. The original DA proposed a height of up to 22m in a 15m height limit and a FSR of 2.34:1 in a max FSR limit of 2:1. Council refused these controls on the grounds of negative impacts on the surrounding area with specific issues of bulk and scale being referenced. The DA was later approved with a FSR exceedance of only 5% which was 2.05:1 and a height totalling 17m lift/overrun which was only 2m higher than the height limit. Given that both are only relatively small exceedances from the LEP development standards it is reasonable to suggest that the controls on this site are applicable, especially given that they were used to bring the original proposed height and FSR down to something more reasonable.

From analysing the recent developments and Planning Proposal it is reasonable to suggest that the current LEP height and FSR controls are still applicable and have in fact been used to achieve a more acceptable development with minimal impacts on the surrounding area. Whilst there was a need for an increase in FSR in the Ebley Street PP there was ample FSR left unused as part of the Bondi Junction RSL development and the Grand Hotel development was only minimally above the FSR. Similarly, whilst the Grand Hotel had a 13% exceedance in height and the RSL 9%, this exceedance was a result of lift overruns and the bulk of the building is relatively consistent with the development standards. The Planning Proposal maintaining the height also shows that this control was suitable.

7.3.2 Recommendations

Maintain the LEP controls on Ebley Street to ensure that development in these edge areas remains transitional to reduce the impact on surrounding areas.

8.0 Bondi Junction Town Square Review 2007 Recommendations

8.1.0 Land Use; Land use proposed for the commercial centre – B3 Commercial Core.

8.1.1 Findings/Analysis

This recommendation was supported and Oxford Street Mall was zoned B3 Commercial Core.

8.1.2 Recommendations

No further action required.

8.2.0 Floor Space Ratio; Rowe Street Development Sites 5:1, Bronka Arcade Development Sites FSR 7:1.



Figure 14 LEP Floor Space Ratio Map - Oxford Street Mall

8.2.1 Findings/Analysis

This recommendation was not implemented in the Waverley LEP but FSRs of similar sizes were added. Figure 14 shows the current LEP FSRs of Oxford Street Mall, these are not too far removed from the recommended FSRs.

8.2.2 Recommendations

Ensure that the FSRs for sites are compatible with the height of buildings in any changes to the LEP.

8.2.0 Proposed Building Heights

Rowe Street Sites: A range of heights stepping from 10m on Oxford Street Mall, a mid height of 20m rising up to a maximum of 24m along Grosvenor Lane (see Figure 15 below).

Bronka Arcade Sites (see Figure 15 below):

- 20m maximum height on the proposed public Square
- 15m over Spring Street
- 28m to the West of the proposed Square
- 24m to the East of the proposed Square up to a maximum of 45m on the corner of Bronte Road



Figure 15 2004 Bondi Junction Town Square Masterplan Proposed building heights, from The Bondi Junction Town Square Peer Review Subission, 2008.

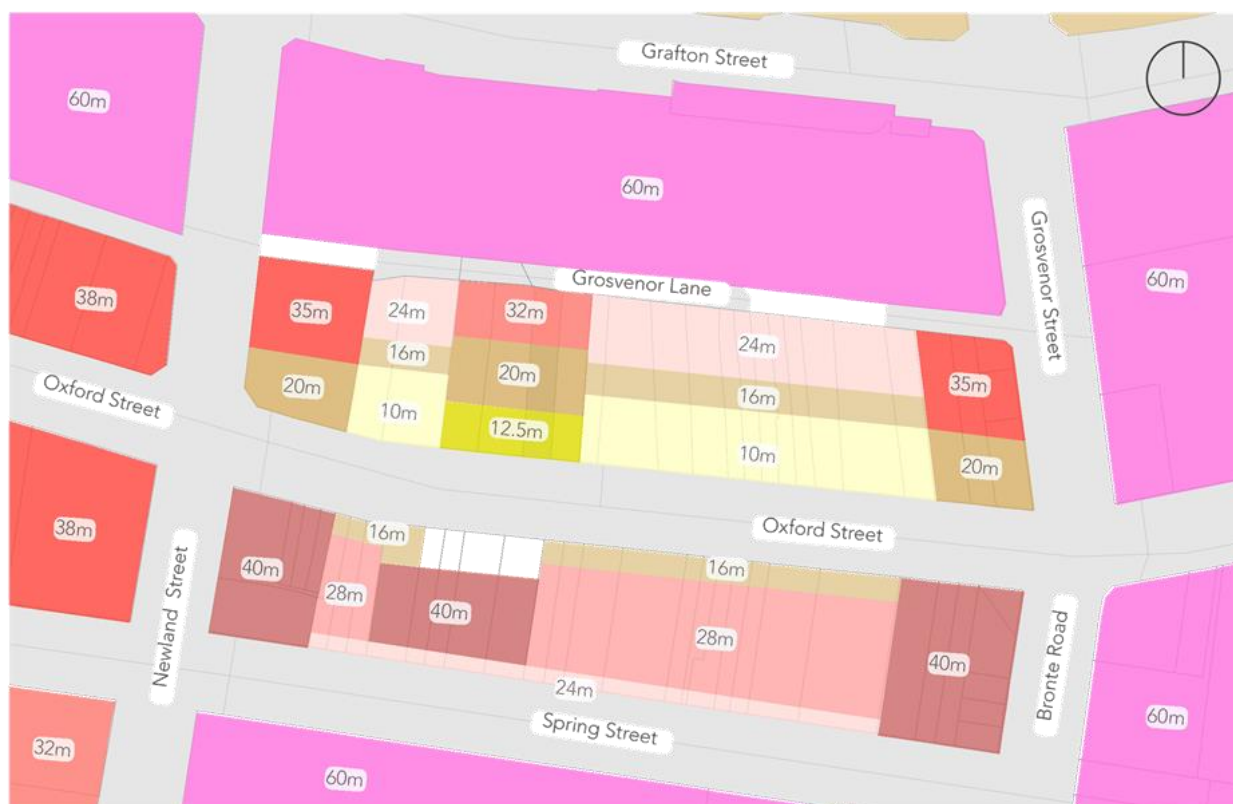


Figure 16 Current LEP heights of Oxford Street Mall

8.3.1 Findings/Analysis

The recommendations shown in Figure 16 were not carried through to the Waverley LEP, however the stepped building heights concept was carried through. Figure 16 shows the current building heights of Oxford Street Mall.

Whilst there are different heights to what was proposed the stepped height concept remains. The stepped height is derived from the winter sun angle and ensuring that the mall receives adequate sunlight year-round.

Little development has occurred around Oxford Street mall, since the WLEP 2012 compared to other parts of Bondi Junction. This likely reflects the price differential for residential vs commercial development and the highly fragmented nature of lot ownership. The challenge that these lot sizes represent is that individual redevelopment of each lot could create sub-optimal built form outcomes as each lot would have a single, narrow development. Ways to incentives new development in and around the mall should be explored to ensure that Bondi Junction maintains its employment status as a Strategic Centre. There could

be development incentives for lot amalgamation, creating a preferable built form outcome. Similarly, an residential incentive may be another way of encouraging commercial development, like the Ebley Street PP. Such changes would have to be considered carefully as introducing residential uses into the B3 zone could compromise the commercial integrity of that zone.

8.3.2 Recommendations

Investigate the viability of the stepped building heights in Oxford Street Mall and make recommendations pertaining to changing or leaving the controls as is as part of the Comprehensive LEP review.



