



## **WAVERLEY TRAFFIC COMMITTEE MEETING**

A meeting of the WAVERLEY TRAFFIC COMMITTEE will be held at Waverley Council Chambers,  
Cnr Paul Street and Bondi Road, Bondi Junction at:

**10.00AM, THURSDAY 23 MARCH 2017**

Emily Scott  
**Acting Director, Waverley Renewal**

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## AGENDA

### Apologies

### Declarations of Interests

### Adoption of previous Minutes by Council - 23 February 2017 ..... 8

The recommendations contained in Part 1 - Matters Proposing That Council Exercise Its Delegated Functions - of the Minutes of the Waverley Traffic Committee meeting held on 23 February 2017 are scheduled to be adopted by Council at its meeting on 21 March 2017.

### PART 1 – MATTERS PROPOSING THAT COUNCIL EXERCISE ITS DELEGATED FUNCTIONS

**NOTE:** *The matters listed under this part of the Agenda propose that Council either does or does not exercise the traffic related functions delegated to it by the RMS. The recommendations made by the Committee under this part of the Agenda will be submitted to Council for adoption.*

#### TC/C      STATE ELECTORATE OF COOGEE

#### TC/C.01/17.03      6-8 Bronte Road, Bondi Junction - Work Zone/ Construction Vehicle and Pedestrian Plan of Management (DA-110/2016) .....18

#### COUNCIL OFFICER'S PROPOSAL:

That Council

1. Installs a 15m long, fenced off, 'No Parking 7am-5pm Mon-Fri 8am-3pm Sat Council Authorised Vehicles Excepted' and 'No Stopping At Other Times' zone outside 6-8 Bronte Road incorporating 9m at the site and an additional 6m across the Tea Gardens Hotel frontage immediately to the north for a period of 13 weeks to allow the standing of trucks as approved in the Construction Vehicle and Pedestrian Plan of Management.
2. Delegates Authority to the Executive Manager, Creating Waverley to extend the duration of the Work Zone as necessary.
3. Approves the Construction Vehicle and Pedestrian Plan of Management (CVPPM), prepared by Traffix, dated 10 February 2017 ( Ref: 16.546r01v03) subject to the following:
  - a) The application for a Construction Certificate shall be taken as accepting these conditions of approval.
  - b) Occupants of neighbouring properties shall be advised of the demolition, excavation and

construction activities.

- c) Pedestrian access to neighbouring properties shall be maintained at all times.
- d) No building materials shall be placed, dumped or left on any Council road or footpath area at any time.
- e) The footpaths remaining in a safe condition for use by pedestrians at all times.
- f) All vehicles, plant and equipment shall to be operated in accordance with the [NSW] *Road Rules 2008*.
- g) Truck access to the site shall be via Syd Enfield Drive, then west on Oxford Street, then north on Adelaide Street, then west on Grafton Street and then north on Bronte Road.
- h) Truck egress from the site shall be via south on Bronte Road, then west on Birrell Street, then north on York Road and then Syd Enfield Drive.
- i) Truck servicing the site shall be limited in size to 8.8m overall length. No approval is given for the use of articulated vehicles (semi-trailers and truck/trailer combinations).
- j) Trucks, on arriving at the site, shall enter the fenced off loading /unloading area at the front of the site immediately. Trucks not able to enter the fenced off area immediately shall not wait on Bronte Road but drive on and wait remote from the site and enter Bronte Road only when the area is clear for access. The applicant shall where necessary co-ordinate truck movements with other nearby building activities.
- k) There shall be no blockage to through traffic on Bronte Road, other than for short periods of time when manoeuvring vehicles into and out of the approved fenced off loading/unloading zone.
- l) All traffic and pedestrian control shall be in accordance with the RMS's *Traffic Control at Work Sites* manual.
- m) No barricades or delineators – including bollards, witches hats, etc – shall be placed without the prior approval of Council.
- n) Separate approval is required from the Waverley Traffic Committee, Waverley Council, NSW Police Force and Roads and Maritime Service of NSW before any roads are closed to through traffic.
- o) Separate approval is required from the Waverley Traffic Committee and Waverley Council for:
  - (i) The use of any concrete pump.
  - (ii) Installation of a tower or other on site crane.
  - (iii) Crane or hoist over a footpath.
  - (iv) The erection of a hoarding.
- p) The builder being advised that during busy periods in the Bondi Junction business area such as at Christmas, Easter and mid-year sales when traffic volumes increase substantially, access to the site by trucks and other building related vehicles may be denied.

- q) Detailed drawings of the works to be undertaken on Council's road and footpath area to stand trucks in the fenced off area shall be submitted to Council for the approval of the Executive Manager, Creating Waverley prior to works commencing at the site. The detailed drawings shall include but be not limited to:
- (i) The area of footpath paving to be removed.
  - (ii) The location, height and type of proposed fencing.
  - (iii) The location of the B class hoarding.
  - (iv) Any temporary materials to be installed on the road /footpath area to cater for the expected wheel loadings.
  - (v) All street furniture and garden beds to be temporarily removed.
  - (vi) Adjustments required to the existing Truck/Taxi zone to the north.
  - (vii) Adjustments required to existing traffic signs.
  - (viii) Details of all temporary street lighting to maintain lighting to the footpath area under the hoarding.
  - (ix) Measures to be put in place to have pedestrians kept clear of the fenced off area of footpath when crossing Bronte Road.

**TC/C.02/17.03            Bayview Street, Bronte - Works Zone (A03/2514-04).....70**

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 20m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone on the bend on the southern side of Bayview Street opposite No.29 subject to there being no blockage to through traffic on Bayview Street other than for short periods of time when manoeuvring vehicles into and out of the development site.
2. Delegate's authority to the Executive Manager, Creating Waverley, to extend the duration, length and location of the zone as necessary.

**TC/C.03/17.03 Multiple Streets - Installation of 'No Stopping' Zones (A02/0637-02) .....74****COUNCIL OFFICER'S PROPOSAL:**

That Council installs 'No Stopping' zones as follows:

1. Ashton Street at York Road, Queens Park:
  - (a). 10m on the eastern side of Ashton Street, north of York Road,
  - (b). 10m on the western side of Ashton Street, north of York Road,
  - (c). 10m on the northern side of York Road, west of Ashton Street,
  - (d). 10m on the northern side of York Road, east of Ashton Street.
2. Alt Street at Birrell Street, Queens Park:
  - (a). 10m on the eastern side of Alt Street, south of Birrell Street.

**TC/C.04/17.03 Birrell Street, Queens Park - Works Zone (DA-621/2014) .....78****COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 9m long, "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone on the Birrell Street frontage of 125 Bronte Road starting 10m from a "No Stopping" zone at Blenheim Street subject to there being no blockage to through traffic on Birrell Street other than for short periods of time when manoeuvring vehicles into and out of the Works Zone.
2. Delegate's authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone as necessary.

**TC/V STATE ELECTORATE OF VAUCLUSE****TC/V.01/17.03 Ocean Street, Bondi - Works Zone (A03/2514-04) .....140****COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs an 18m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone across the frontage of 21-23 Ocean Street North, subject to there being no blockage to through traffic on Ocean Street other than for short periods of time when manoeuvring vehicles into and out of the 'Construction Zone'.
2. Delegate's authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone, as necessary.

**TC/V.02/17.03            Ocean Street North, Bondi - No Parking Zone (A02/0637-02).....143****COUNCIL OFFICER'S PROPOSAL:**

That Council installs "No Parking" signs to align with the driveway lines at the northern driveway to 27-31 Ocean Street North, Bondi.

**TC/V.03/17.03            Anzac Day - Ramsgate Avenue and Campbell Parade, North Bondi – Temporary Road Closure (A02/0216) .....147****COUNCIL OFFICER'S PROPOSAL:**

That Council approves the Transport Management Plan for the temporary closure of Ramsgate Avenue and Campbell Parade , North Bondi on 25 April 2017 between 4am and 8am as per the submitted Transport Control Plans (TCP's), subject to the following:

1. The applicant undertakes the following:
  - a) Submits the Transport Management Plan to the Roads and Maritime Services for the approval of the Traffic Management Centre.
  - b) Provides Public Liability Insurance for the event.
  - c) Obtains NSW Police Force approval and assessment of the event classification.
  - d) Notifies the State Transit Authority, NSW Ambulance Service and NSW Fire & Rescue (Bondi, Woollahra and Randwick Fire Stations) not less than seven (7) days prior to the event.
  - e) Notifies local residents and businesses at least seven (7) days prior to the event.
  - f) Uses only RMS-accredited Traffic Controllers to regulate traffic.
  - g) Covers all costs associated with traffic control.
  - h) Submits a copy of the approved R.O.L to the Executive Manager, Creating Waverley prior to the event taking place.
2. The Executive Manager, Creating Waverley or his representatives being delegated authority to audit the TMP and cancel the approval if required.

**TC/CV      ELECTORATES OF COOGEE AND VAUCLUSE****TC/CV.01/17.03      Multiple Streets - 'P DISABILITY ONLY' Zones (A02/0273-02) .....210****COUNCIL OFFICER'S PROPOSAL:**

That Council:

1.      Installs 5.5m of 'P DISABILITY ONLY' parking outside No. 3 Birrell Street, Queens Park immediately to the west of the "No Stopping" zone at Alt Lane.
2.      Extends the length of the existing 'P DISABILITY ONLY' parking by 6.5 m to occupy the full frontage of the unit block at No. 7 Brae Street, Bronte.
3.      Installs 6.5m of 'P DISABILITY ONLY' parking outside No. 13 Barracluff Avenue, Bondi Beach.
4.      Installs 5.5m of 'P DISABILITY ONLY' parking outside No. 49 Glenayr Avenue, Bondi.
5.      Installs 6.5m of 'P DISABILITY ONLY' parking outside No. 25 Park Parade, Bondi.
6.      Removes the existing 'P Disability Only' zone outside No. 26 Knowles Avenue, North Bondi.

**PART 2 – TRAFFIC ENGINEERING ADVICE**

***NOTE: The matters listed under this part of the Agenda seek the advice of the WTC only and do not propose that Council exercise its delegated functions at this point in time (though they may or may not require it in the future).***

**TC/TEAC    STATE ELECTORATE OF COOGEE**

Nil

**TC/TEAV    STATE ELECTORATE OF VAUCLUSE**

Nil

**TC/TEACV    ELECTORATES OF COOGEE AND VAUCLUSE**

Nil

**MINUTES OF THE WAVERLEY TRAFFIC  
COMMITTEE MEETING HELD AT WAVERLEY  
COUNCIL CHAMBERS, CNR PAUL STREET AND  
BONDI ROAD, BONDI JUNCTION ON  
THURSDAY, 23 FEBRUARY 2017**



**Voting Members Present:**

Cr T Kay	Waverley Council (Chair)
Cr B Mouroukas	Waverley Council (Deputy Chair) for items TC/V.01/17.02 to TC/V.05/17.02 and item TC/CV.01/17.02
Mr J Morrison	Representing Gabrielle Upton, MP, Member for Vacluse and Bruce Notley-Smith, MP, Member for Coogee
Mr M Tita	Roads and Maritime Services – Traffic Management (South)

**Also Present:**

Mr E Graham	Sydney Buses (Eastern Region)
Mr D Joannides	Waverley Council – Executive Manager, Creating Waverley
Mr G Garnsey	Waverley Council – Manager, Transport and Development
Mr S Samadian	Professional Engineer
Ms S Cassidy	Waverley Council – Project Manager, Bondi Junction Infrastructure Implementation
Mr. A Collins	Waverley Council – Senior Project Manager

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*At the commencement of proceedings at 10.06am, those present were as listed above.*

**Apologies**

Apologies were received and accepted from Sgt L Barrett of NSW Police.

**Declarations of Pecuniary and Non-Pecuniary Interests**

The Chair called for declarations of interest and none were received.

**Adoption of previous Minutes by Council - 24 November 2016**

The recommendations contained in Part 1 - Matters Proposing That Council Exercise Its Delegated Functions - of the Minutes of the Waverley Traffic Committee meeting held on 24 November 2016 are scheduled to be adopted by Council at its meeting on 16 December 2016.



**PART 1 – MATTERS PROPOSING THAT COUNCIL EXERCISE ITS DELEGATED FUNCTIONS**

**NOTE:** *The matters listed under this part of the Agenda propose that Council either does or does not exercise the traffic related functions delegated to it by the RMS. The recommendations made by the Committee under this part of the Agenda will be submitted to Council for adoption.*

**TC/C        STATE ELECTORATE OF COOGEE****TC/C.01/17.02        Alice Lane, Waverley - Parking restrictions    (A02/0637-02)****COUNCIL OFFICER'S PROPOSAL:**

That Council Installs:

1.     A 25 m long 'No Parking' zone on the western side of Alice Lane, Waverley immediately south of the statutory 'No Stopping' zone as shown on *Figure 2* of the report.
2.     A 10m long 'No Stopping' zone on the western side of Alice Lane, south of Macpherson Street, Waverley.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be amended to read as follows:

"That the matter be deferred for further investigation in regard to concerns raised by the resident at the meeting."

*Voting members present for this item: Representative of the Member for Coogee, RMS representative and Waverley Council representative (Chair).*

*V Grey (resident) addressed the meeting.*

**TC/V        STATE ELECTORATE OF VAUCLUSE****TC/V.01/17.02        Beach Road, Bondi Beach - Construction Zone    (A03/2514-04)**

*Cr Kay relinquished the Chair for this item and Cr Mouroukas assumed the Chair.*

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1.     Installs an 11m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone across the frontage of No.88 Beach Road, subject to the following conditions:
  - (i)    There being no blockage to through traffic on Beach Road other than for short periods of time when manoeuvring vehicles into and out of the 'Construction Zone'.

2. Delegates authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone, as necessary.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Vaucluse, RMS representative and Waverley Council representative (Chair).*

**TC/V.02/17.02                      Curlewis Street, Bondi - Construction Zone (A03/2514-04)**

*Cr Kay relinquished the Chair for this item and Cr Mouroukas assumed the Chair.*

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 12m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone in Simpson Street adjacent to No.9 Curlewis, Bondi for a period of 13 weeks subject to the following condition:
  - (i) There being no blockage to through traffic on Simpson Street other than for short periods of time when manoeuvring vehicles into and out of the 'Construction Zone'.
2. Delegates authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone, as necessary.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Vaucluse, RMS representative and Waverley Council representative (Chair).*

**TC/V.03/17.02                      Brighton Boulevard, North Bondi - Installation of motorbike only zone (A02/0637-02)**

*Cr Kay relinquished the Chair for this item and Cr Mouroukas assumed the Chair.*

**COUNCIL OFFICER'S PROPOSAL:**

That Council installs 3m of 'P Motor Bikes Only' between the driveways to 83A and 85 Brighton Boulevard, North Bondi.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Vaucluse, RMS representative and Waverley Council representative (Chair).*

**TC/V.04/17.02 Roscoe Street, Bondi Beach - Construction Zone (A03/2514-04)**

*Cr Kay relinquished the Chair for this item and Cr Mouroukas assumed the Chair.*

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 7m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone across the frontage of No.53 Roscoe Street, subject to the following conditions:
  - (i) There being no blockage to through traffic on Roscoe Street other than for short periods of time when manoeuvring vehicles into and out of the 'Construction Zone'.
2. Delegates authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone, as necessary.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Vaucluse, RMS representative and Waverley Council representative (Chair).*

**TC/V.05/17.02 O'Brien Street, Bondi Beach - Construction Zone (A03/2514-04)**

*Cr Kay relinquished the Chair for this item and Cr Mouroukas assumed the Chair.*

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 14m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone across the frontage of No.91-93 O'Brien Street, subject to the following conditions:
  - (i) There being no blockage to through traffic on O'Brien Street other than for short periods of time when manoeuvring vehicles into and out of the 'Construction Zone'.
2. Delegates authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone, as necessary.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Vacluse, RMS representative and Waverley Council representative (Chair).*

**TC/V.06/17.02 Ben Eden Street, Bondi Junction - No Parking (A02/0637-02)**

**COUNCIL OFFICER'S PROPOSAL:**

That:

1. Council installs "No Parking" signs across the driveway to No.11 Ben Eden Street, Bondi Junction.
2. Council relocates the existing "No Parking" arrow right sign/ 2P- 8am-6pm Mon-Sat Permit Holders Excepted Area 26" arrow left sign located on the Ben Eden Street frontage of 20 Waverley Street a distance of 5m to the south-west to provide an additional on street parking space across a redundant driveway.
3. Council's officers contact the owner of 20 Waverley Crescent to discuss the removal of the redundant driveway on the property's Ben Eden Street frontage.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Vacluse, RMS representative and Waverley Council representative (Chair).*

*A Andrews addressed the meeting.*

**TC/CV ELECTORATES OF COOGEE AND VAUCLUSE**

**TC/CV.01/17.02 Multiple Streets - 'P DISABILITY ONLY' Zones (A02/0273-02)**

*Cr Kay relinquished the Chair for this item and Cr Mouroukas assumed the Chair.*

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs 6.7 m of 'P DISABILITY ONLY' parking outside 37 York Road, Queens Park.
2. Installs 5.5 m of 'P DISABILITY ONLY' parking outside 32 Tower Street, Vacluse.
3. Installs 6.7 m of 'P DISABILITY ONLY' parking outside 129 Hastings Parade, North Bondi.
4. Installs 5.5 m of 'P DISABILITY ONLY' parking outside 44 Military Road, North Bondi.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Coogee and Vaucluse, RMS representative and Waverley Council representative (Chair).*

**PLEASE NOTE****TC/TEACV.01/17.02    Multiple Streets - Installation of 'No Stopping' zones    (A02/0637-02)**

*This matter from Part 2 was dealt with by the Committee as a Part 1 matter as Council will have to exercise its traffic related functions delegated to it by the RMS to implement the 'No Stopping' zones. The recommendation made by the Committee will be submitted to Council for adoption.*

**PART 2 – TRAFFIC ENGINEERING ADVICE**

**NOTE:** *The matters listed under this part of the Agenda seek the advice of the WTC only and do not propose that Council exercise its delegated functions at this point in time (though they may or may not require it in the future).*

**TC/TEAC    STATE ELECTORATE OF COOGEE****TC/TEAC.01/17.02    Bronte Village Upgrade (A16/0755)****COUNCIL OFFICER'S PROPOSAL:**

That:

1. The Committee members receive and note the report
2. The members of the Committee and others wishing to comment on the proposal forward comments to the Executive Manager, Creating Waverley.

**WTC RECOMMENDATION (MAJORITY SUPPORT):**

That the Council Officer's Proposal be adopted subject to the deletion of clause 2.

*Voting members present for this item: Representative of the Member for Coogee, RMS representative and Waverley Council representative (Chair).*

**TC/TEAC.02/17.02    Bronte Road, Bondi Junction - Ebley Street to Birrell Street- Streetscape Enhancement (A14/0348)****COUNCIL OFFICER'S PROPOSAL:**

That:

1. The Committee members receive and note the report
2. The members of the Committee and others wishing to comment on the proposal forward comments to the Executive Manager, Creating Waverley.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal not be supported.

*Voting members present for this item: Representative of the Member for Coogee, RMS representative and Waverley Council representative (Chair).*

**TC/TEAV    STATE ELECTORATE OF VAUCLUSE****TC/TEAV.01/17.02      Bus Priority Improvement Program (BPIP) - Sydney Buses Routes 333, 380 and 389 (A03/0189)****COUNCIL OFFICER'S PROPOSAL:**

That the Committee:

1.    Receives and notes the report
2.    Raises no objections to the proposals contained in the "Final Scheme" documents for routes 333, 380 and 389 subject to the following:
  - (i)    Having regard to all JCDeCaux bus shelters being the subject of a current contract between that company and Council, any removal and/or relocation of a JCDeCaux shelter shall not take place until consultation has taken place with JCDeCaux and Council's Executive Manager Creating Waverley and agreement has been reached as to the terms and conditions relating to the removal and/or relocation of the shelter.
  - (ii)    The removal and /or relocation of bus shelters not subject to a contract with JCDeCaux (Council owned bus shelters) shall not take place without the prior consultation with and approval of Council's Executive Manager, Creating Waverley.
  - (iii)    The removal and/or relocation of JCDeCaux bus shelters shall be in accordance with JCDeCaux standards and specifications.
  - (iv)    The removal and/or relocation of Council owned bus shelters shall be in accordance with Council's standards and specifications.
  - (v)    A new concrete pathway being installed at no cost to Council on the eastern side of Portland Street, south of Lancaster Road to provide an all-weather link between the footpath in Lancaster Road and the new bus stop and shelter in Portland Street.
  - (vi)    The new bus stop in Portland Street being signposted with "Bus Zone 6am-9am Mon-Fri"
  - (vii)    The existing bus zone at location 24 TSN 203023 on Military Road, Vaucluse opposite Kimberley Street (outbound) being replaced with "No Stopping".
  - (viii)    All works associated with removing and or installing new bus stops and bus shelters shall be carried out at no cost and to the satisfaction of Council.
  - (ix)    Detailed engineering drawings of works to be undertaken at each of the bus stops shall be submitted to Council for the approval of the Executive Manager, Creating Waverley prior to works commencing at the stop.

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted, subject to the following amendments:

1.    The Council Officer's report be renamed "RMS On-time Running Program – Sydney Buses Routes 333,

380 and 389.”

2. The addition of clause (x) as follows:

- (x) The existing “Bus Zone” signs at location 9, TSN 202678, Wairoa Ave near Murriverie Rd (inbound) on removal being replaced with unrestricted parking, not “No Stopping” as proposed.

*Voting members present for this item: Representative of the Member for Vacluse, RMS representative and Waverley Council representative (Chair).*

**TC/TEACV ELECTORATES OF COOGEE AND VAUCLUSE**

**TC/TEACV.01/17.02 Multiple Streets - Installation of 'No Stopping' zones (A02/0637-02)**

*This matter was dealt with by the Committee as a Part 1 matter as Council will have to exercise its traffic related functions delegated to it by the RMS to implement the ‘No Stopping’ zones. The recommendation made by the Committee will be submitted to Council for adoption.*

**COUNCIL OFFICER’S PROPOSAL:**

That Council installs ‘No Stopping’ zones as follow:

1. Read Lane at Alfred Street, Bronte:
  - (a). 10m on the northern side of Read Lane, west of Alfred Street,
  - (b). 10m on the western side of Alfred Street, north of Read Lane,
  - (c). 10m on the western side of Alfred Street, south of Read Lane.
2. Read Street at Read Lane, Bronte:
  - (a). 10m on the western side of Read Lane, south of Read Street,
  - (b). 10m on the eastern side of Read Lane, south of Read Street.
3. Read Lane at Read Lane, Bronte:
  - (a). 5m on the northern side of Read Lane, east of Read Lane,
  - (b). 5m on the southern side of Read Lane, east of Read Lane,
  - (c). 10m on the eastern side of Read Lane, north of Read Lane,
  - (d). 10m on the eastern side of Read Lane, south of Read Lane.
4. Fitzgerald Street/ Cuthbert Street/ Bourke Lane, Queens Park:
  - (a). 10m on the eastern side of Fitzgerald Street, north of Cuthbert Street,



- (b). 10m on the northern side of Cuthbert Street, west of Fitzgerald Street,
- (c). Replaces the existing 'No Parking' zone with the statutory 'No Stopping' zone on Cuthbert Street, north side, east of Fitzgerald Street.
- (d). 10m on the southern side of Cuthbert Street, west of Bourke Lane,
- (e). Replaces the existing 'No Parking' zone with the statutory 'No Stopping' zone on Cuthbert Street, south side, east of Bourke Lane

**WTC RECOMMENDATION (UNANIMOUS SUPPORT):**

That the Council Officer's Proposal be adopted.

*Voting members present for this item: Representative of the Member for Coogee and Vaucluse, RMS representative and Waverley Council representative (Chair).*

**THE MEETING CLOSED AT 12.16PM**

**SIGNED AND CONFIRMED      MAYOR**  
**21 March 2017**

## REPORT

### TC/C.01/17.03



**Subject:** 6-8 Bronte Road, Bondi Junction - Work Zone/  
Construction Vehicle and Pedestrian Plan of Management

**TRIM No.:** DA-110/2016

**Author:** Sam Samadian, Traffic Engineer

**Director:** Emily Scott, Director, Waverley Renewal

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#### COUNCIL OFFICER'S PROPOSAL:

That Council

1. Installs a 15m long, fenced off, 'No Parking 7am-5pm Mon-Fri 8am-3pm Sat Council Authorised Vehicles Excepted' and 'No Stopping At Other Times' zone outside 6-8 Bronte Road incorporating 9m at the site and an additional 6m across the Tea Gardens Hotel frontage immediately to the north for a period of 13 weeks to allow the standing of trucks as approved in the Construction Vehicle and Pedestrian Plan of Management.
2. Delegates Authority to the Executive Manager, Creating Waverley to extend the duration of the Work Zone as necessary.
3. Approves the Construction Vehicle and Pedestrian Plan of Management (CVPPM), prepared by Traffix, dated 10 February 2017 ( Ref: 16.546r01v03) subject to the following:
  - a) The application for a Construction Certificate shall be taken as accepting these conditions of approval.
  - b) Occupants of neighbouring properties shall be advised of the demolition, excavation and construction activities.
  - c) Pedestrian access to neighbouring properties shall be maintained at all times.
  - d) No building materials shall be placed, dumped or left on any Council road or footpath area at any time.
  - e) The footpaths remaining in a safe condition for use by pedestrians at all times.
  - f) All vehicles, plant and equipment shall to be operated in accordance with the [NSW] *Road Rules 2008*.
  - g) Truck access to the site shall be via Syd Enfield Drive, then west on Oxford Street, then north on Adelaide Street, then west on Grafton Street and then north on Bronte Road.
  - h) Truck egress from the site shall be via south on Bronte Road, then west on Birrell Street, then north on York Road and then Syd Enfield Drive.
  - i) Truck servicing the site shall be limited in size to **8.8m** overall length. No approval is given for

the use of articulated vehicles (semi-trailers and truck/trailer combinations).

- j) Trucks, on arriving at the site, shall enter the fenced off loading /unloading area at the front of the site immediately. Trucks not able to enter the fenced off area immediately shall not wait on Bronte Road but drive on and wait remote from the site and enter Bronte Road only when the area is clear for access. The applicant shall where necessary co-ordinate truck movements with other nearby building activities.
- k) There shall be no blockage to through traffic on Bronte Road, other than for short periods of time when manoeuvring vehicles into and out of the approved fenced off loading/unloading zone.
- l) All traffic and pedestrian control shall be in accordance with the RMS's *Traffic Control at Work Sites* manual.
- m) No barricades or delineators – including bollards, witches hats, etc – shall be placed without the prior approval of Council.
- n) Separate approval is required from the Waverley Traffic Committee, Waverley Council, NSW Police Force and Roads and Maritime Service of NSW before any roads are closed to through traffic.
- o) Separate approval is required from the Waverley Traffic Committee and Waverley Council for:
  - (i) The use of any concrete pump.
  - (ii) Installation of a tower or other on site crane.
  - (iii) Crane or hoist over a footpath.
  - (iv) The erection of a hoarding.
- p) The builder being advised that during busy periods in the Bondi Junction business area such as at Christmas, Easter and mid-year sales when traffic volumes increase substantially, access to the site by trucks and other building related vehicles may be denied.
- q) Detailed drawings of the works to be undertaken on Council's road and footpath area to stand trucks in the fenced off area shall be submitted to Council for the approval of the Executive Manager, Creating Waverley prior to works commencing at the site. The detailed drawings shall include but be not limited to:
  - (i) The area of footpath paving to be removed.
  - (ii) The location, height and type of proposed fencing.
  - (iii) The location of the B class hoarding.
  - (iv) Any temporary materials to be installed on the road /footpath area to cater for the expected wheel loadings.
  - (v) All street furniture and garden beds to be temporarily removed.
  - (vi) Adjustments required to the existing Truck/Taxi zone to the north.

- (vii) Adjustments required to existing traffic signs.
- (viii) Details of all temporary street lighting to maintain lighting to the footpath area under the hoarding.
- (ix) Measures to be put in place to have pedestrians kept clear of the fenced off area of footpath when crossing Bronte Road.

## 1. Executive Summary

Council has received:

1. A Construction Vehicle and Pedestrian Plan of Management (CVPPM), prepared by TRAFFIX, traffic and transport planners and,
2. An application from the developers/builders to install a Work Zone, for the development site at 6-8 Bronte Road, Bondi junction.

Council will need to exercise its delegated functions to implement the proposal.

## 2. Introduction/Background

The site at 6-8 Bronte Road Bondi Junction is the former NAB building located on Bronte Road, east side, opposite Spring Street.



**Figure 1 – Aerial image of 6-8 Bronte Road, Bondi Junction**



**Figure2 – Street view showing site location**

Consent for DA-110/2016 at 6-8 Bronte Road, Bondi Junction for an eight (8) storey mixed use development without off street parking was granted by Council on 2 November 2016, subject to conditions.

Conditions 22 and 45 of the approval require the following:

## **22. CONSTRUCTION VEHICLE AND PEDESTRIAN PLAN OF MANAGEMENT**

*Prior to the issue of a Construction Certificate, the applicant shall submit a “Construction Vehicle and Pedestrian Plan of Management” (CVPPM) for the approval of the Executive Manager, Creating Waverley*

*The CVPPM shall provide, but not be limited to, details of the following:*

- a. *The proposed route to be taken by demolition/construction vehicles in the Council area when accessing and exiting the site.*
- b. *The type, size and number of demolition/construction vehicles for each of the separate phases of the development. Trucks with dog trailers and semi-trailers may not be approved for use if it is considered with the information provided that:*
  - *Such vehicles cannot adequately and safely gain access to and from the site or*
  - *Access into or out of the site may not be possible without the need to remove an unsatisfactory number of vehicles parked on the roadway adjacent to or opposite the site.*
- c. *The location of truck holding areas remote from the site should Council not give approval for demolition/construction vehicles to stand on the roadway in the vicinity of the site.*
- d. *Traffic control measures to be put in place when trucks, manoeuvring in the vicinity of the site, will interfere with the free flow of traffic.*
- e. *The location and materials of construction of temporary driveways providing access into and out of the site.*
- f. *The location and length of any proposed Works/Construction Zones. Note:*
  - *Works/Construction zones require the approval of the Waverley Traffic Committee and Council prior to installation.*

- *It is illegal to:*
  - i. *Park a vehicle exceeding 4.5 tonnes on a roadway for more than one (1) hour unless signs are installed to the contrary*
  - ii. *Barricade/reserve a section of roadway without the prior approval of Council*
- g. *The hours of operation of demolition/construction vehicles.*
- h. *The number of and where it is proposed to park light vehicles associated with staff/employees/contractors working on the site.*
- i. *How it is proposed to cater for the safe passage of pedestrians past the site. The details shall include:*
  - *The route required to be taken by pedestrians including signage and any other control measures that will need to be put in place to direct and keep pedestrians on the required route*
  - *Any obstructions such as street furniture, trees and bollards etc., that may interfere with the safe passage of pedestrians*
  - *The type(s) of material on which pedestrians will be required to walk*
  - *The width of the pathway on the route*
  - *The location and type of proposed hoardings*
  - *The location of existing street lighting*
- j. *Any bus zones, taxi zones etc., that may be affected/require temporary relocation as a result of development works.*

**NOTE: PRIOR TO THE PREPARATION OF THE CVPPM**, the applicant or his or her representative shall:

- *Make contact with Council's Senior Traffic Engineer to discuss information required at (a) to (j) above.*
- *Make contact with the operators of bus and taxi services whose zones may be affected/require temporary relocation as a result of development works*
- *Be aware that the full length of Oxford Street may not be approved as a route for trucks when travelling to or from the site*
- *Be aware swept wheel path drawings, prepared by a suitably qualified and experienced traffic engineering consultant, may need to be submitted to Council for all truck movements that are to take place within the Council area prior to an approval being issued for the CVPPM.*

*Be aware of and take into account the cumulative effect that truck movement from the development site and other development sites in the vicinity that are either under construction or have development approval will have on roads leading to and from the site.*

#### **45. CONSTRUCTION HOURS**

*Demolition and building work must only be undertaken between the hours of 7am and 5pm on Mondays to Fridays and 8am to 3pm on Saturdays with no work to be carried out on:*

- (a) *The Saturday (except minor renovation or refurbishment to a single dwelling construction) and Sunday which form part of public holiday weekends; and*
- (b) *Sundays and public holidays*

*Noise from construction activities shall comply with the Protection of the Environmental Operations (Noise Control) Regulation 2000.*

In accordance with the above, TRAFFIX, on behalf of the applicant, has submitted a CVPPM (attached), which covers the demolition, excavation and construction phases of development.

In accordance with standard practice, in Waverley Council, it is proposed that the Work Zone be signposted 'No Parking Council Authorised Vehicles Excepted' for the approved construction hours under the DA Consent. Council will then issue the applicant with a number of transferable permits to be used on their construction-related vehicles.

The Traffic Committee and Council generally only approve Work Zones at the same length as the frontage, regardless of the location of driveways, for a minimum of thirteen weeks. In this instance and due to the narrow, 9.2m wide block, the works zone will extend some 6m to the north and part way across the frontage of the Tea Gardens Hotel.

### 3. Technical Analysis

#### **SITE DESCRIPTION**

The development site has a frontage to Bronte Road only. The adjoining site to the north is the Tea Gardens Hotel and Westfield to the south and east. Bronte Road between Grafton Street and Ebley Street is a two-way regional road. Access to Bronte Road between Grafton Street and Spring Street is limited to buses, taxis, lorries and emergency vehicles. There is no vehicular access onto the site and parking outside is not permitted due to the presence of a road narrowing/kerb blister. To the south of the site are Bus Zones and No Stopping zones to Gray Street. To the north between the site and Oxford Street is "Truck Zone 6am-2pm Mon-Fri; Taxi Zone at Other Times".



**Kerb blister outside 6-8 Bronte Road, Bondi Junction and adjoining Westfields**

#### **STAGING OF WORKS**

The development involves demolition of existing building and construction of a new building of eight (8) floors over a period of some 18 months commencing in April, 2017.

##### **(A) Demolition Phase**

**Duration:** 6 weeks commencing April, 2017

**Vehicle Type and Size:** 8.8m long medium rigid vehicle (MRV)



**Number of Persons on Site:** 5

**Estimated Number of trucks per Day:** 3 in/3 out with 2 in/out during the peak

**Proposed Site Access Times:**

7.00am- 5.00pm Monday to Friday

7.00am- 3.00pm Saturdays

**Truck Routes:**

- (a) To the Site:** East on Syd Enfield Drive, west on Oxford Street, north on Adelaide Street, west on Grafton Street and then north on Bronte Road.
- (b) From the Site:** South on Bronte Road, then west on Birrell Street, then north on York Road and then Syd Enfield Drive.

**Traffic Control:** To facilitate trucks entering and exiting the Work Zone in a forward direction, RMS certified traffic controllers will be employed for the entire duration of the works.

**Construction/Works Zone:** A 15m long fenced off, one vehicle wide loading/unloading area zone is proposed in Bronte Road. **See work zone section below.**

*Comment by Council Traffic Engineer*

No objection on traffic and parking grounds to access to and from the site as proposed during the demolition works phase.

**(B) Construction/ Structure Phase**

**Duration:** 5 months commencing May 2017

**Vehicle Type and Size:** 8.8m long medium rigid vehicle (MRV).

**Estimated Number of trucks per Day:** 5 in/5 out with 1 in/1 out during the peak

**Number of Persons on Site:** 20

**Proposed Site Access Times:**

7.00am- 5.00pm Monday to Friday

7.00am- 3.00pm Saturdays

**Truck Routes:**

As per demolition and excavation phases above.

**Traffic Control:** To facilitate trucks entering and exiting the site in a forward direction, RMS certified traffic controllers will be employed for the entire duration of the works.

**Construction/Works Zone:** A 15m long fenced off, one vehicle wide loading/unloading area zone is proposed in Bronte Road. **See work zone section below.**



Comment by Council Traffic Engineer

No objection on traffic and parking grounds to truck access to and from the site as proposed during the construction phase.

**(D) Fit Out/Finishes**

**Duration:** 9 months commencing October 2017

**Vehicle Type and Size:** 8.8m long medium rigid vehicle (MRV)

**Estimated Number of trucks per Day:** 5 in/5 out with 3 in/3 out during the peak

**Number of Persons on Site:** 35

**Proposed Site Access Times:**

7.00am- 5.00pm Monday to Friday

7.00am- 3.00pm Saturdays

**Truck Routes:**

As per demolition, excavation and construction phases above

**Traffic Control:** To facilitate trucks entering and exiting the site in a forward direction, RMS certified traffic controllers will be employed for the entire duration of the works.

**Construction/Works Zone:** A 15m long fenced off, one vehicle wide loading/unloading area zone is proposed in Bronte Road. **See work zone section below.**

Comment by Council Traffic Engineer

No objection on traffic and parking grounds to access to and from the site as proposed during the fit out phase.

**WORK/ CONSTRUCTION ZONE**

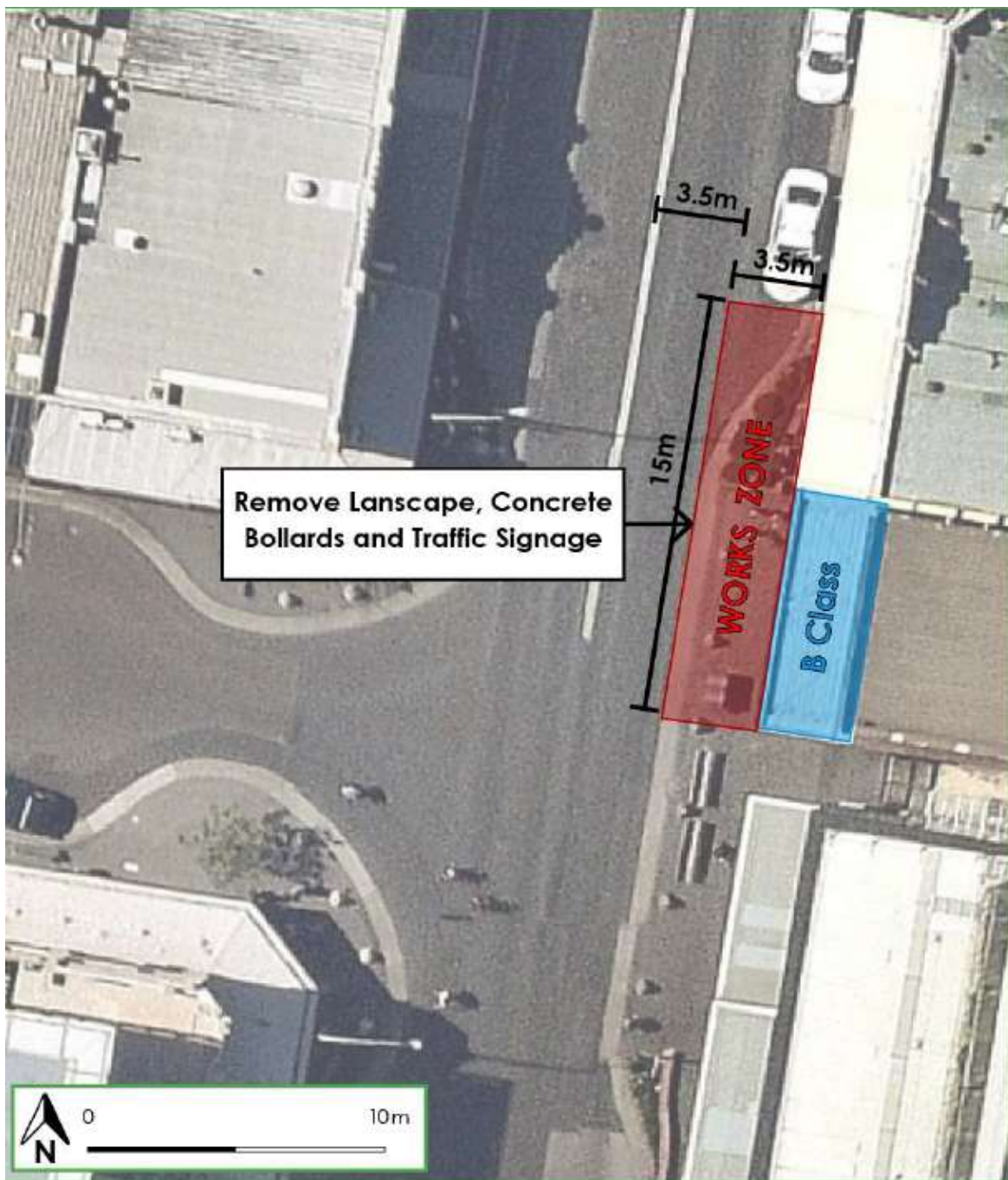
It is intended that all vehicle loading and unloading, including the removal of excavated material is to occur from the front of the site on Bronte Road by removing the street furniture and installing a fenced off, one vehicle wide loading/unloading work zone area. A tower crane will be installed early within the site and a B-class hoarding will be installed over the footpath with loading/ unloading taking place over it.

The proposed Work Zone on Bronte Road on what is the wider footpath/ blister area will allow trucks to stand outside the site whilst maintaining an effective traffic flow on the road. The details for the proposed Work Zone are outlined in *Table 1*, as follows:

*Table 1     6-8 Bronte Road, Bondi Junction – Work Zone details*

<b>Applicant</b>	MMD Construction Consultants
<b>Development Application</b>	DA-110/2016
<b>Works</b>	Demolition of existing building and construction of an eight storey commercial/retail development.
<b>Approved Hours of Construction</b>	7am-5pm Mon-Fri, 8am-3pm Sat
<b>Frontage Length</b>	9.2m
<b>Road</b>	Bronte Road
<b>Existing Parking</b>	No Stopping
<b>Requested Length</b>	15m
<b>Proposed Length</b>	15m
<b>Duration</b>	13 weeks minimum
<b>Beginning</b>	April 2017
<b>Fee Area</b>	Commercial Zone





**Figure3 – 6-8 Bronte Road - Work zone / Class B Hoarding**

Hours of activity at the site are complying with the standard development approval times 7am – 5pm Monday – Friday 8am – 3pm Saturday with no work to be carried out on:

- (a) The Saturday (except minor renovation or refurbishment to a single dwelling construction) and Sunday which form part of public holiday weekends; and
- (b) Sundays and public holidays

Noise from construction activities shall comply with the Protection of the Environmental Operations (Noise Control) Regulation 2000.

#### **TRUCK LAYOVER AREA**

A truck layover area to hold trucks prior to being called into the site is nominated as Oxford Street west of York Road opposite Centennial Park in the Woollahra Council area.

Comment by Council Traffic Engineer

No objections.

The section of roadway is a clearway from 3pm to 7pm Monday to Friday. Trucks will not be permitted to wait in the holding area after 3pm.

**PARKING FOR EMPLOYEE VEHICLES**

It is proposed that employees utilise public transport or park in a designated, off street parking areas.

Comment by Council Traffic Engineer

Parking arrangements considered satisfactory.

**PEDESTRIAN MANAGEMENT/HOARDINGS**

Pedestrian access surrounding the site is to be managed safely during all stages of development.

Hoardings are proposed as follows and are subject to a separate application to Council to Council's Building Waverley section.

**Demolition Phase**

Class "B" hoarding over the works/construction in Bronte Road.

**Excavation Phase**

As above.

**Construction/Fit Out**

As above.

Comment by Council Traffic Engineer

No objections to the installation of hoardings as proposed. Approval requires separate application to Council.

**TOWER CRANE INSTALLATION**

A tower crane will be installed to take materials into and out of the site during all phases of the development. Crane permits require separate application to Council.

Guidelines used:

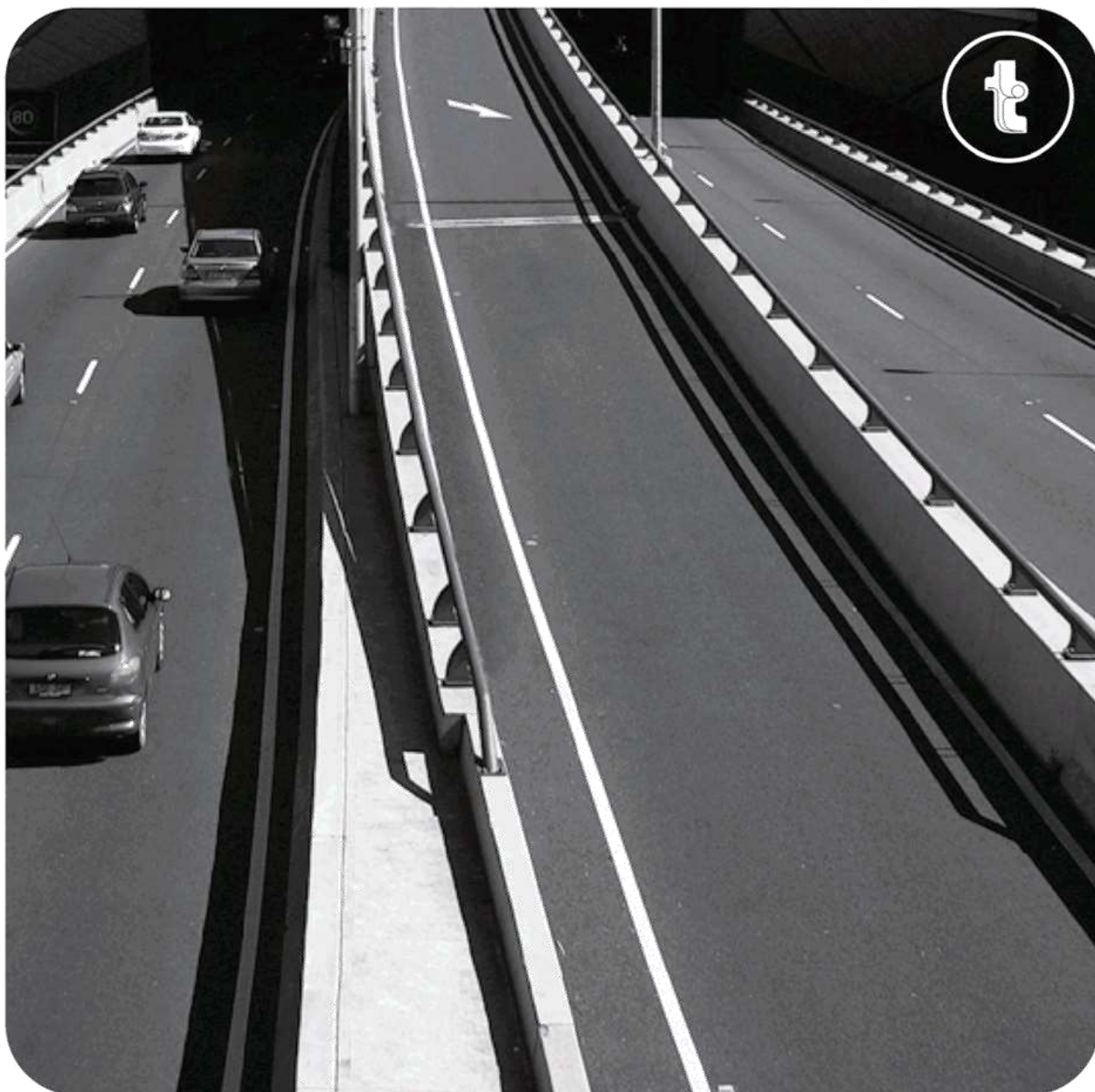
- *Transport Administration Act 1988*
- *RMS's Delegation to Councils; Regulation of Traffic*
- *RMS's Signs Database*
- *[NSW] Road Rules 2014.*

**4. Financial Information for Council's Consideration**

The applicant is responsible for the costs associated with implementing all aspects of the Construction Vehicle and Pedestrian Plan of Management including the work zone.

**5. Attachments:**

1. 16.546r01v03 TRAFFIX CVPPM Report; 6-8 Bronte Road, Bondi Junction



## Construction Vehicle and Pedestrian Plan of Management (CVPPM)

**Proposed Mixed-Use Development  
6-8 Bronte Road, Bondi Junction**

Reference: 16.546r01v03 TRAFFIX CVPPM Report  
Date: 10 February 2017

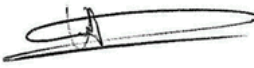
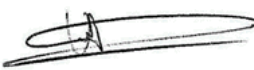
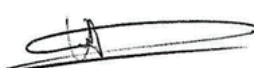
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## Document Verification

<b>Job Number:</b>	16.546			
<b>Project:</b>	6-8 Bronte Road, Bondi Junction			
<b>Client:</b>	MMD Construction Consultants			
Revision	Date	Prepared By	Approved By	Signature
v01-DRAFT	07/02/2017	Hadi Bagheri	Vince Doan	
v02	08/02/2017	Hadi Bagheri	Vince Doan	
v03	10/02/2017	Hadi Bagheri	Vince Doan	

## Traffic Control Plan Certificates

Prepare a Work Zone Traffic Management Plan			
<b>Name:</b>	Vince Doan	<b>Certificate No.</b>	0031360631
<b>Name:</b>	Hadi Bagheri	<b>Certificate No.</b>	0036198229

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Appendix A:	Construction Management Report
Appendix B:	Swept Path Analysis
Appendix C:	Traffic Control Plan
Appendix D:	Construction Management Plan





## 1. Introduction

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TRAFFIX has been commissioned by MMD Construction Consultants to prepare a *Construction Vehicle and Pedestrian Plan of Management* (CVPPM) report for the construction of a mixed-use development located at 6-8 Bronte Road, Bondi Junction. Conditions of Consent DA-110/2016 require a CVPPM to be submitted and approved by council prior to the issue of a Construction Certificate.

The site currently accommodates a single storey retail development and it lies within the sector bounded by Bronte Road to the west, Oxford Street to the north and Gray Street to the south. A Location Plan is presented in **Figure 1**, for ease of reference.

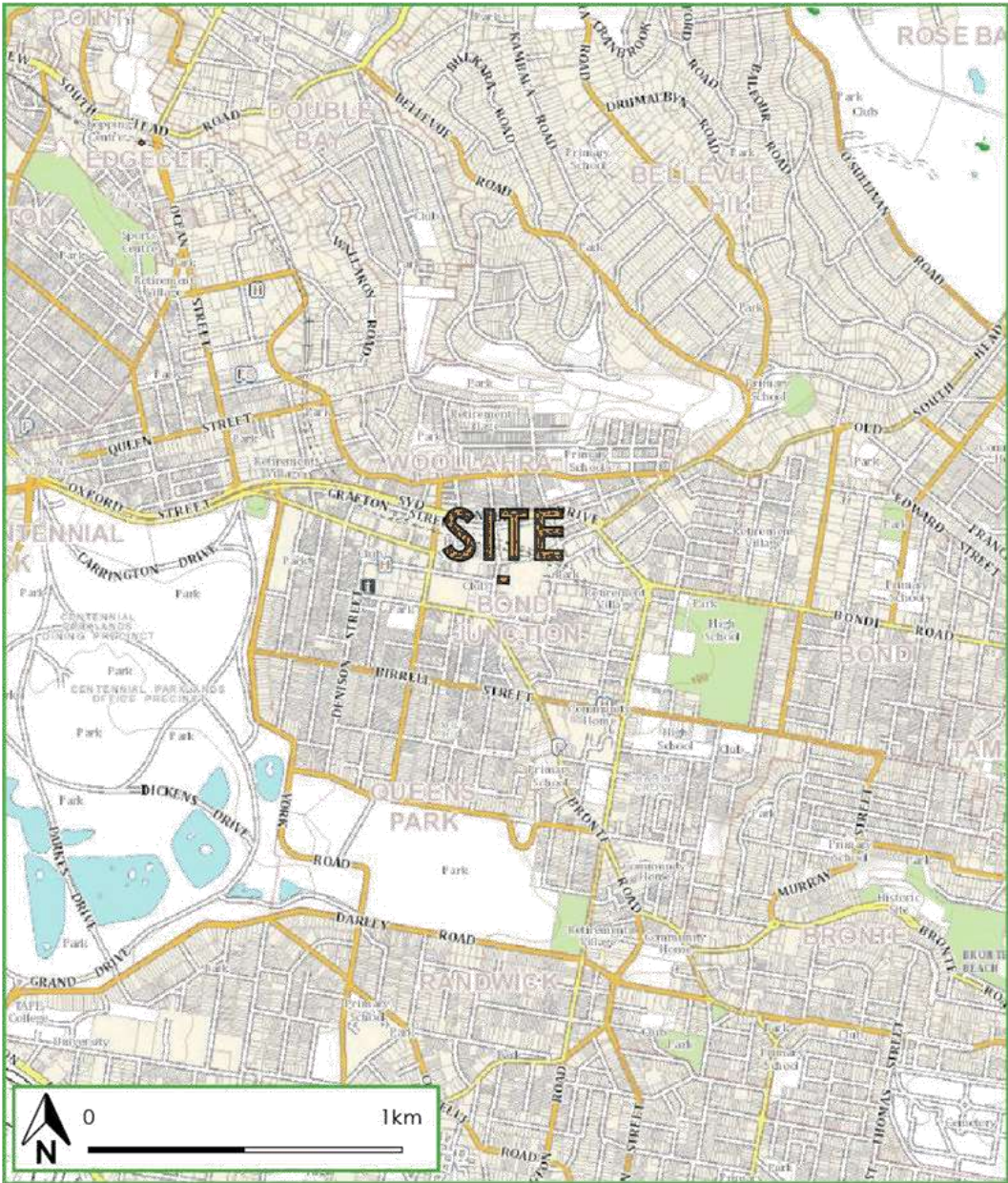


Figure 1: Location Plan



## 2. CVPPM Requirements

### 2.1 Traffic Control Plan

The Traffic Control Plan (TCP) that is included in this report, should be implemented taking due account of on-site conditions as will occur over the construction period. Accordingly, construction crew are expected to respond in a pro-active manner to ensure that this plan is implemented to maximum effect and with no obvious safety issues being overlooked. In particular, the following matters are considered noteworthy:

- All signs are to be placed where clear visibility is available.
- Installations should be checked intermittently during the course of the day/s, and
- A minimum of one Roads and Maritimes Services certified Traffic Controllers shall be on-site at all times during work hours to supervise truck movements.

It is noted that TRAFFIX is responsible for the preparation of this TCP only and not for their implementation, which is the responsibility of the project manager/builder.

### 2.2 CVPPM Requirements under DA Consent

In addition to the above, it is noted that Condition 22 of the DA Consent outlines a number of requirements which are to be adhered to for the CVPPM. Specifically, Condition 22 states the following:

*Prior to issue of a Construction Certificate, the applicant shall submit a "Construction Vehicle and Pedestrian Plan of Management" (CVPPM) for the approval of the Executive Manager, Creating Waverley Council. The CVPPM shall provide, but not limited to, details of the following:*

- (a) *The proposed route to be taken by demolition/construction vehicles in the Waverley Council area when accessing and exiting the site.*
- (b) *The type, size and number of demolition/construction vehicles for each of the separate phases of the development. Trucks with dog trailers and semi-trailers may not be approved for use if it is considered with the information provided that:*
  - *Such vehicles cannot adequately and safely gain access to and from the site or*



- *Access into or out of the site may not be possible without the need to remove an unsatisfactory number of vehicles parked on the roadway adjacent to or opposite the site.*
- (c) *The location of truck holding areas remote from the site should Council not give approval for demolition/construction vehicles to stand on the roadway in the vicinity of the site.*
- (d) *Traffic control measures to be put in place when trucks, manoeuvring in the vicinity of the site, will interfere with the free flow of traffic.*
- (e) *The location and materials of construction of temporary driveways providing access into and out of the site.*
- (f) *The location and length of any proposed Works/Construction Zones. Note:*
  - *Work/construction zones require the approval of the Waverley Traffic Committee and Council prior to installation.*
  - *It is illegal to:*
    - i. *Park a vehicle exceeding 4.5 tonnes on a roadway for more than one (1) hour unless signs are installed to the contrary*
    - ii. *Barricade/reserve a section of roadway without the prior approval of Council*
- (g) *The hours of operation of demolition/construction vehicles.*
- (h) *The number of and where it is proposed to park light vehicles associated with staff/employees/contractors working on the site.*
- (i) *How it is proposed to cater for the safe passage of pedestrians past the site. The details shall include:*
  - *the route required to be taken by pedestrians including signage and any other control measures that will need to be put in place to direct and keep pedestrians on the required route;*
  - *any obstructions such as street furniture, trees and bollards etc., that may interfere with the safe passage of pedestrians;*
  - *the type(s) of material on which pedestrians will be required to walk;*
  - *the width of the pathway on the route;*
  - *the location and type of proposed hoardings;*
  - *the location of existing street lighting.*
- (j) *Any bus zones, taxi zones etc., that may be affected/require temporary relocation as a result of development works.*





## 3. Existing Conditions

---

### 3.1 Location and Site

The site is situated approximately 130 metres south-west of Bondi Junction Railway Station and approximately 4.5 kilometres south-east of the Sydney CBD. More specifically, the site lies within the sector bounded by Bronte Road to the west, Oxford Street to the north and Gray Street to the south.

The site has rectangular configuration and currently accommodates a retail development with a total site area of approximately 298m<sup>2</sup>. It has a western frontage of 9.2 metres to Bronte Road, an eastern boundary of 7.9 metres, southern boundary of 33.0 metres neighbouring Westfield Shopping centre and northern boundary of 33.0 metres neighbouring Tea Garden Hotel.

It is noted that the subject development does not possess any existing vehicular access/driveway to the site.

A Site Plan is provided in **Figure 2** which provides a more detailed appreciation of the site in the context of neighbouring properties.

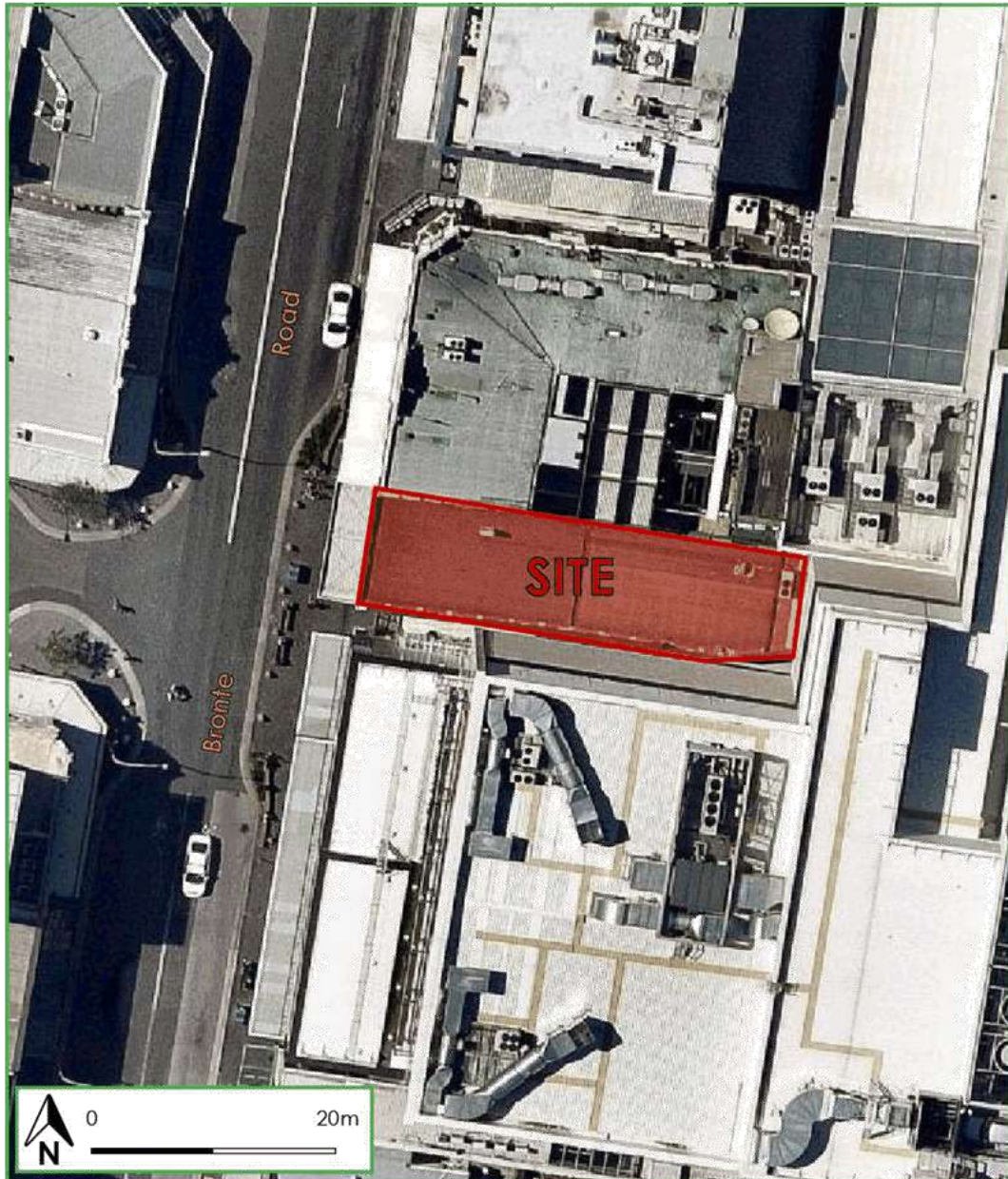


Figure 2: Site Plan



## 3.2 Road Network

The road hierarchy in the vicinity of the site is shown in **Figure 3** with the following roads of particular interest:

- Bronte Road: a Roads and Maritime Main Road (MR 340) that runs in a north east direction in the vicinity of the site. Bronte Road connects Oxford Street in the north to Macpherson Street in the south. It carries a single Lane of traffic in either direction and is subject to a 50km/h speed zoning in the vicinity of the site. Access for the northbound movements on Bronte Road has been restricted only to buses, taxis and emergency vehicles.
- Oxford Street: forms part of the Roads and Maritime Main Road (MR 172) that traverses in an east-west direction in the between Syd Einfeld Drive and Bondi Road in the east and Wentworth Avenue / College Street in the west. However, adjacent to the site, Oxford Street is a bus dedicated lane and parallel parking is not permitted along both kerbsides. Oxford Street carries a single lane of traffic in each direction within an undivided carriageway of width 12.5 metres. It is subject to 50km/h speed zoning in the vicinity of the site.
- Spring Street: a local road that traverses in east-west direction between Bronte Road in the east and Denison Street in the west. It is noted that Spring Street between Bronte Road and Newland Street, is a one-way road westbound. It is subject to 50km/h speed zoning and permits ticketed parallel parking along both kerbsides.
- Gray Street: a local one-way road that traverse westbound between Ann Street in the east and Bronte Road in the west. To the south of the Gray Street, ticketed angled parking is permitted along the kerbside however, adjacent a few parallel parking is also permitted to the northern kerbside of Gray Street. Gary Street is subject to a 50km/h speed zoning in the vicinity of the site.

It can be seen from Figure 3 that the site is conveniently located with respect to the arterial and local road systems serving the region. It is therefore able to effectively distribute traffic onto the wider road network, minimising traffic impacts.



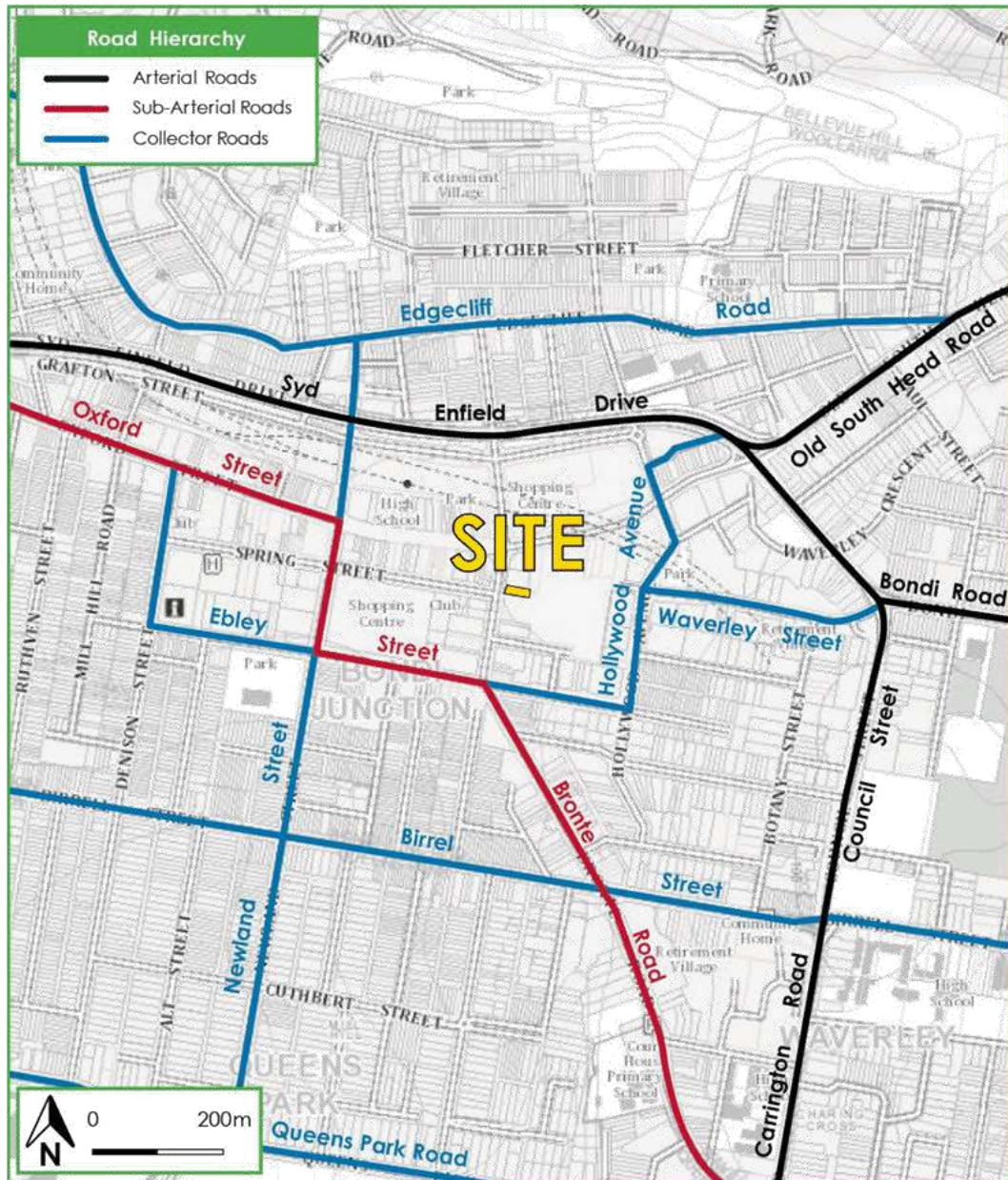


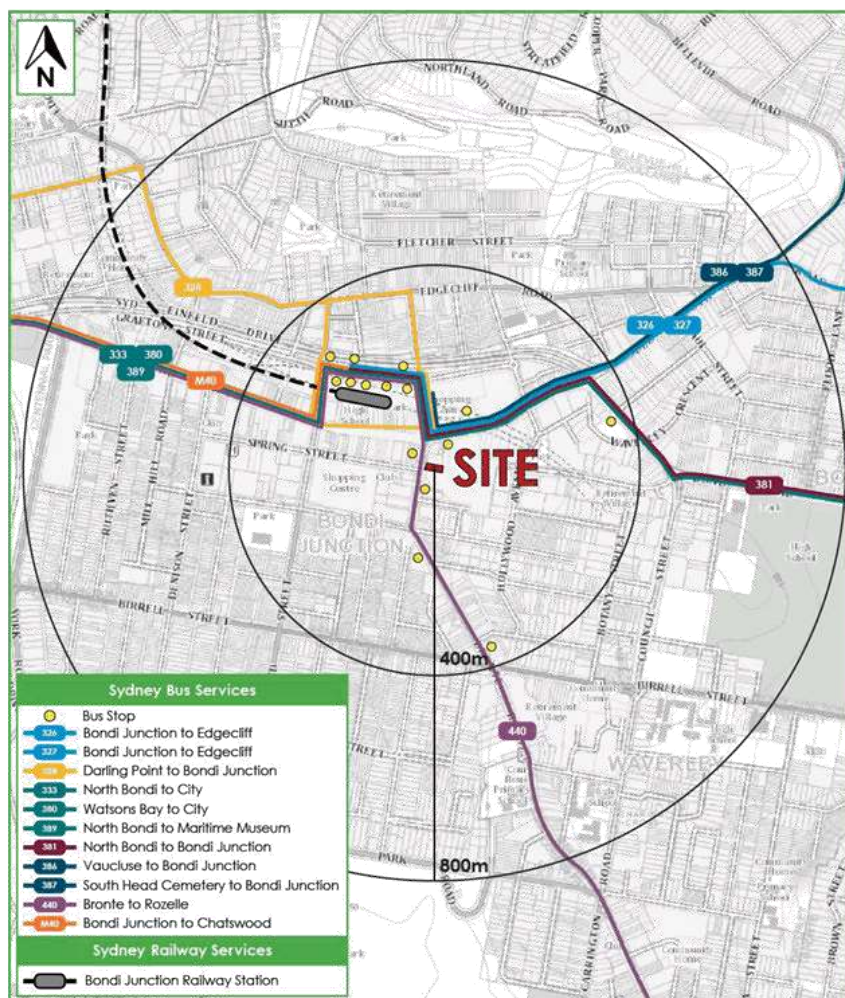
Figure 3: Road Hierarchy





### 3.3 Public Transport

The existing bus and rail services that operate in the locality are shown in **Figure 4**. It is evident that the site benefits from excellent public transport services being situated within less than 200 metres of Bondi Junction Bus and Rail Interchange. The rail services operate along the Western Suburbs & Illawarra Line, which provides connections to all major Sydney stations. In addition, frequent bus services operate out of the Bondi Junction Bus and Rail Interchange, providing connections to numerous centres as shown in Figure 4.



**Figure 4: Public Transport**



## 4. Overview of Construction Program

### 4.1 Times of Operation

The total construction period is expected to occur less than 16 months. The hours of operation will be in accordance with Condition 45 of the DA Conditions of Consent which states the following:

*Demolition and building work must only be undertaken between the hours of 7am and 5pm on Mondays to Fridays and 8am to 3pm on Saturdays with no work to be carried out on:*

- *The Saturday (except minor renovation or refurbishment to a single dwelling construction) and Sunday which form part of public holiday weekends; and*
- *Sundays and public holidays*

*Noise from construction activities shall comply with the Protection of the Environment Operations (Noise Control) Regulation 2000.*

This condition generally will be satisfied, however it is anticipated by the construction management team that inevitably one lane of Bronte Road is required to be occupied during the assembling / disassembling of tower crane by mobile crane for short-term. Having regards to the above, it is agreed between the representatives of State Transit Authority (STA) of NSW, Waverly Council, NSW Police and site's project manager that the tower crane erect and dismantle stages will occur during the night periods to mitigate any negative impact on NSW bus services. In addition, this is understood that the subject site is not located within a high or medium density residential and this strategy will consequently not create considerable negative impact on surrounding residents. These arrangements are discussed further in detail in Section 5.5.

### 4.2 Demolition and Detailed Site Clearance Stages

These stages will occur over a six (6) week period commencing in March 2017, pending approval of this CVPPM and will involve a maximum workforce of five (5) people on-site at any one time. The maximum sized truck to be utilised during these stages will be an 8.8m long medium rigid truck (MRV). It is proposed that all loading/unloading works will occur inside a proposed 15 metre long Works Zone. According to the MMD Construction Consultants Report (see **Appendix A**), trucks loading /unloading to



the Works Zone will be by the tower crane located within the lift shaft via a B-class hoarding. The works zone is proposed on the pedestrian footpath of Bronte Road. **Figure 5** overleaf illustrates schematic the location of the proposed work and is discussed in further detail in Section 4.3.

These stages will have an average of three (3) truck arrivals per day (3 in, 3 out) and a maximum of two (2) trucks arrival during the peak hour movements (2 in, 2 out). This is considered a minor volume and hence, the impact on the performance of key intersections in the vicinity of the site will be negligible.

All trucks will accommodate inside the proposed lay over area (discussed further in Section 5.2) and linked via CB radio and hands-free mobile and will only be called to the site when required and when there is spare capacity within the work zone to accommodate the truck. This management of loading / unloading or deliveries will be the same throughout all stages of construction to ensure that no trucks will be required to queue or park on-street. These arrangements will result in minimal disruption to the local traffic network and are therefore considered acceptable.



**Figure 5: Proposed Works Zone and Class-B Hoarding during Construction Stages**



### 4.3 Structure Stage

#### 4.3.1 Standard Structure Stage

This stage will occur over a five (5) month period (including Concrete Pouring), commencing in April 2017 and will involve a maximum workforce of 20 people on-site at any one time. The maximum sized truck to be utilised during this stage will be an 8.8m long MRV, with all loading / unloading to occur via the proposed Works Zone on Bronte Road with the use of B-class hoarding and on-site tower crane. The location of the proposed Works Zone is shown by **Figure 5** above.

This stage will have an average of five (5) truck arrivals per day (5 in, 5 out). This is again considered a minor volume and hence, the impact on the performance of key intersections in the vicinity of the site will be negligible.

#### 4.3.2 Concrete Pouring Stage

It is understood that pre-cast panels will be used to reduce the volume of concrete which is required to be delivered to the site. Therefore, only up to a maximum of seven (7) day period is required to complete this part of the structure stage. It is also proposed by project management team to bury the concrete pump and associated hose underneath of footpath to minimise the Works Zone size and the use of any pedestrian ramps. It has been advised by STA that truck movements regarding concrete pouring will be required to occur during the hours between certain hours of the day to reduce the impact of state buses. Accordingly, it has been agreed between STA, Waverly Council, NSW Police and site's project manager to manage truck movements during the concrete pouring stage between the hours of 9:00am to 3:00pm. Furthermore, a truck will be able to enter the Work Zone before 9:00am however, will only be able to egress during the period specified above.

It is estimated that this part of structure stage will require an average 3 truck arrivals per day (3 in, 3 out) during the proposed pouring period. This is considered a minor volume and out of state buses' peak period and hence, the impact on the performance of key intersections and traffic flow in the vicinity of the site will be manageable.

### 4.4 Fit-out & Finishes Stage

This will occur over a nine (9) month period, commencing in September 2017 and will involve a maximum workforce of 35 people on-site at any one time. The maximum sized truck to be utilised during this stage





will be an 8.8m long MRV, with all loading / unloading to occur via the proposed Works Zone on the pedestrian footpath of Bronte Road. These loading arrangements are shown by **Figure 5**.

This stage will have an average of five (5) truck arrivals per day (5 in, 5 out), with a maximum of three (3) trucks (3 in, 3 out) during the peak hour movements. This is again considered a minor volume and hence, the impact on the performance of key intersections in the vicinity of the site will be negligible.

## 4.5 Truck Standing / Works Zone Requirements

An appropriate truck standing/Works Zone is required to be proposed on Bronte Road (i.e. the only access of the site) to provide an access to the site. This access is required to provide safe passage for pedestrian during the construction of the project according to Condition 22.J. It has also been advised by STA that any long-term period blockage of Bronte Road would have direct impact on the efficiency of the buses and Bondi Junction interchange. Having regards to the above, two possible location of the Work Zone have been investigated and outcome are summarized in further details below:

### 4.5.1 On-Street Truck Zone at the frontage of Tea Garden Hotel

It has been discussed to propose existing on-street spaces along the western side of Bronte Road at the frontage of Tea Garden Hotel as Works Zone. 'Truck Zone' Sign has already been posted at the frontage of Tea Garden Hotel for a restricted-time. This area can be potentially an option for trucks to load/unload trucks, however, it is advised by MMD Construction Consultants that this Works Zone will raise safety issues for pedestrian passing this area if material requires to transfer via footpath and create safety / amenity issues for patrons / staff of Tea Garden Hotel if material requires to lift by tower crane. It is also advised by MMD Construction Consultants that the risk of falling objects to the public access area should be minimised according to Safe Work Australia guidelines and exclusion zone should be established including significant portion of Tea Garden Hotel. This option has been reviewed by the construction management team because of safety concerns for pedestrian and negative impacts on the business operation of Tea Garden Hotel. Further details of this assessment will be provided separately by MMD Construction Consultants.

### 4.5.2 Occupying a section of the footpath at the frontage of the Subject Site

It is decided by construction management team to propose a 15-metre Works Zone on a segment of footpath along the western side of Bronte Road at the frontage of the subject site as shown in **Figure 6**.



This Works Zone is required to be fenced during the whole construction stages and will be utilised for truck's loading / unloading with the use of an on-site tower crane.

It is proposed to erect a B-class hoarding for the length of the subject site (over the footpath) to ensure that pedestrian access is not impeded whilst the crane is utilised to load / unload trucks from the Works Zone. It has also been advised by STA that any fencing for the Works Zone along Bronte Road should not impede the visibility of bus driver to crossing pedestrians and vice versa.

The Works Zone will operate in accordance with the approved hours of construction except the proposed exemptions, as mentioned in Section 4.1.



**Figure 6: Proposed Work Zone and B-Class Hoarding**



## 5. Traffic Management Arrangements

### 5.1 Truck Routes

The proposed truck routes satisfy the requirements of Condition 22 of the DA Consent. These can be summarised as follows:

- Routes to the Works Zone: Trucks will arrive at the site via Syd Einfeld Drive, Oxford Street, Adelaide Street, Grafton Street, Grosvenor Street and Bronte Road.
- Routes from the Works Zone: Trucks will depart from the site via Bronte Road, Birrell Street, York Road and Oxford Street.

In addition to the above, truck routes are shown in Figure 7 overleaf. The below routes seek use of the arterial road network as much as possible with the use of local streets only where required. All truck drivers shall be provided with a copy of these routes prior to accessing the site.

Swept path analysis has been undertaken of the proposed truck routes for the critical entry and exit manoeuvres along the intersections of Syd Einfeld Drive / Oxford Street, Oxford Street / Adelaide Street, Adelaide Street / Grafton Street, Grafton Street / Grosvenor Street, Grosvenor Street / Bronte Road, Bronte Road / Birrell Street and Birrell Street / York Road with the use of an 8.8 metre MRV. These swept paths are provided in **Appendix B** and confirm that satisfactory access to the Works Zone can be achieved, in accordance with the requirements of AS 2890.2 (2002) and route requirements of Condition 22 of the DA Consent.



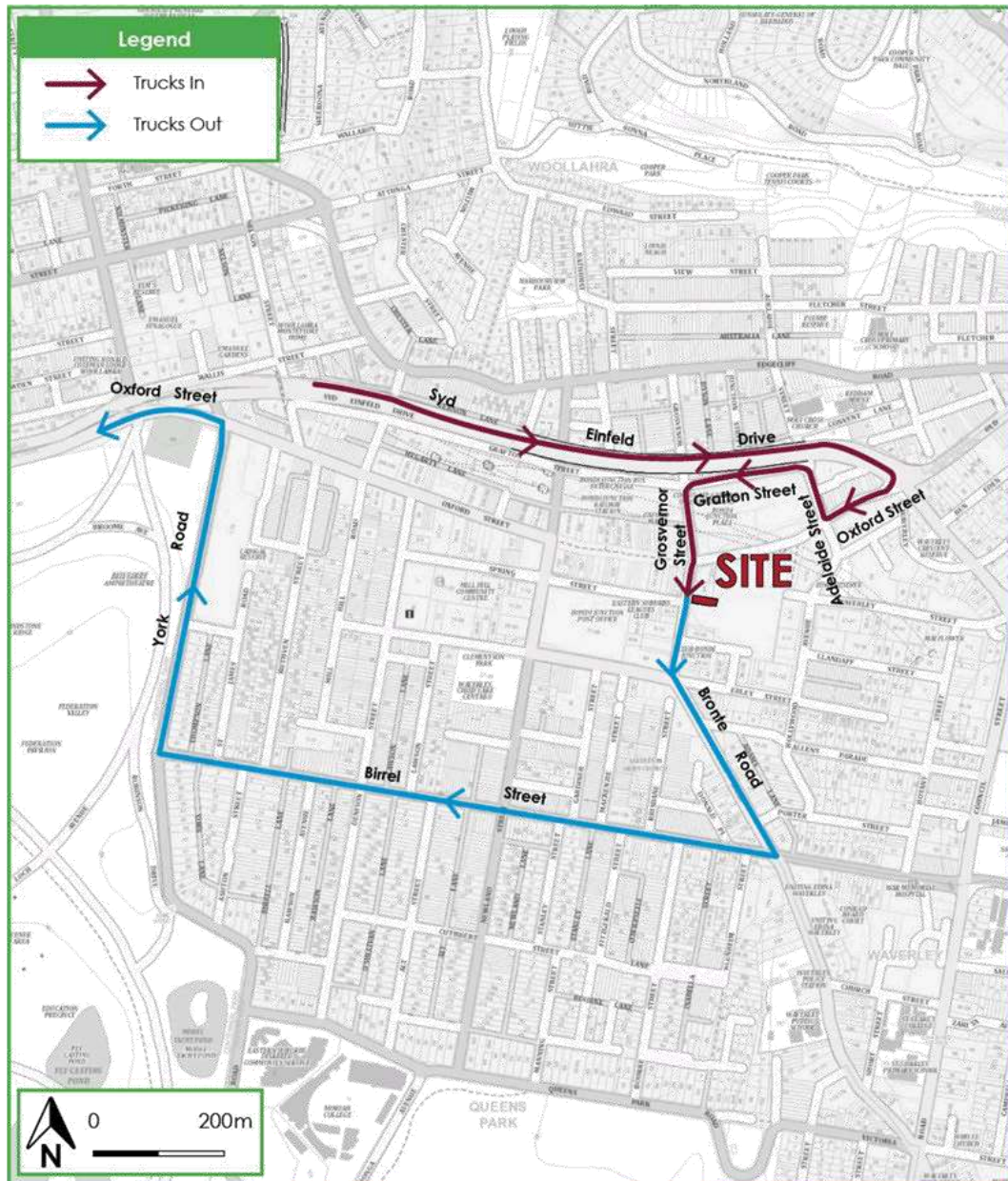


Figure 7: Truck Routes



## 5.2 Truck Layover Area

In addition to the above truck routes, a truck layover area has also been nominated along Oxford Street as shown by Figure 8 overleaf. Kerbside parking demands along this section of Oxford Street are generally minimal and hence, this zone will serve as a good layover area for truck drivers who are accessing the site but are required to briefly wait until being called to the site.

This layover area will ensure that no truck is required to queue/manoeuvring on-street near the site and will only be used on rare occasions, noting that the arrival of trucks will be strictly managed as discussed in Section 4.2.



Figure 8: Truck Stabling / Layover Area





### 5.3 Vehicular Access

Access to the subject site for any construction vehicle will not be practical during the construction stages of the project. Although Right of Way for any land at any time is applicable on the subject site, client has decided to not use it due to the safety concerns for pedestrian crossing this area.

### 5.4 Pedestrian Control

Pedestrian access surrounding the site will be managed safely during all construction stages. Reference should be made to the Traffic Control Plan in **Appendix C** which illustrates the location and type of the hoarding to be utilised. Class "B" hoarding shall be erected for the length of the sites frontage (over the footpath) to ensure that pedestrian access is not impeded whilst the crane is utilised to load / unload trucks from the Works Zone.

RMS certified Traffic Controller will be situated at the proposed construction vehicular accesses and Works Zone during work hours, to supervise all truck entry / exit manoeuvres and to ensure the safety of pedestrians is maintained when trucks are required to access the site or to stand on Bronte Road as illustrated in **Appendix C**. These arrangements are considered acceptable and will ensure that pedestrian safety is maintained at all times.

### 5.5 Tower Crane Erection and Dismantle Requirements

On-site tower cranes will be required during all construction stages, which will facilitate all loading / unloading of trucks. It has been advised by MMD Construction Consultants that the proposed tower crane will be located within the lift shaft. The proposed location of this tower crane is shown in the Construction Management Plan, prepared by MMD Construction Consultants, included in **Appendix D**.

Tower Crane erection and dismantle will require the preparation and approval of a separate Traffic Control Plan, which is to be prepared by the appointed crane operator. This installation would be the first part of structure stage and site establishment. It will approximately take 10-12 hour period to erect or dismantle a tower crane and it will occur from 8pm to 5am during summer and from 7:30pm to 5am during winter.



## 5.6 Traffic Control Plan

The Traffic Control Plan (TCP) included in **Appendix C** demonstrate the proposed signage / traffic management measures to be adopted for the following works:

### ➤ TCP No. 1 – Construction Stages

The TCP included in **Appendix C** will ensure that truck and pedestrian movements are managed safely and efficiently. This TCP has been designed in accordance with the requirements of the *Roads and Maritime Traffic Control at Work Sites Manual* and AS 1742.3 (2009) and are recommended for adoption.

## 5.7 Employee Vehicles

As illustrated in Figure 4, the site benefits from excellent access to public transport services, being situated less than 200 metres of Bondi Junction Railway Station. In addition, frequent bus services operate out of the Bondi Junction Bus and Rail Interchange, providing connections to numerous centres. This is expected to result in high levels of public transport usage by workers, thereby ensuring that the construction activities will generate minimal parking demands.

Should employees visit the site using private cars, they must park within available off-street public car parks in the locality to ensure that impacts on residents and tenants in the vicinity of the site are minimised wherever possible.

It is noted that at no time will contractors be allowed to park private vehicles within the proposed loading areas, which are for the sole purpose of the loading / unloading of materials and equipment.



## 6. Conclusions

---

The plan outlined above is considered satisfactory and will minimise any disruptions to residents / tenants of neighbouring developments, as well as pedestrians in the area. This plan meets all requirements of AS 2890.2, AS 1742.3, *Roads and Maritime Traffic Control at Work Sites Manual* and is recommended for adoption.



## Appendix A

### Construction Management Report



■ QUANTITY SURVEYORS ■ CONSTRUCTION ECONOMISTS ■ PROJECT MANAGERS

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**Melbourne**  
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E: michael@mmdcc.com.au

www.mmdcc.com.au

# Construction Management Plan

Prepared for

**Proposed Commercial Development**

**6 – 8 Bronte Rd Bondi Junction**



**CONSENT No. DA-110/2016**

**2<sup>nd</sup> February 2017**

## 1. Introduction

This proposed Construction Management Plan was prepared in consultation with 'TRAFFIX' the nominated Traffic Engineers. This Plan addresses the nature of the proposed development, likelihood of disruptions, impact on local amenity, dangers or risks involved, traffic management and other relevant issues.

The main elements of this Construction Management Plan are:

- Demolition & Site Clearance
- Structure
- Concrete pouring
- Fit out & Finishes
- Hoarding Plans
- Crane locations

## 2. Project Details

### a. General

Developing Firm	WLBY Pty Ltd
Company Business Address	
Company Contact Number	0481 118 481

### b. Onsite contact person responsible for compliance with this Construction Management Plan

Name	TBA
Contact Number	TBA
After Hours Contact Number	TBA

### c. Construction Stages

Construction works are proposed in two (2) main stages:

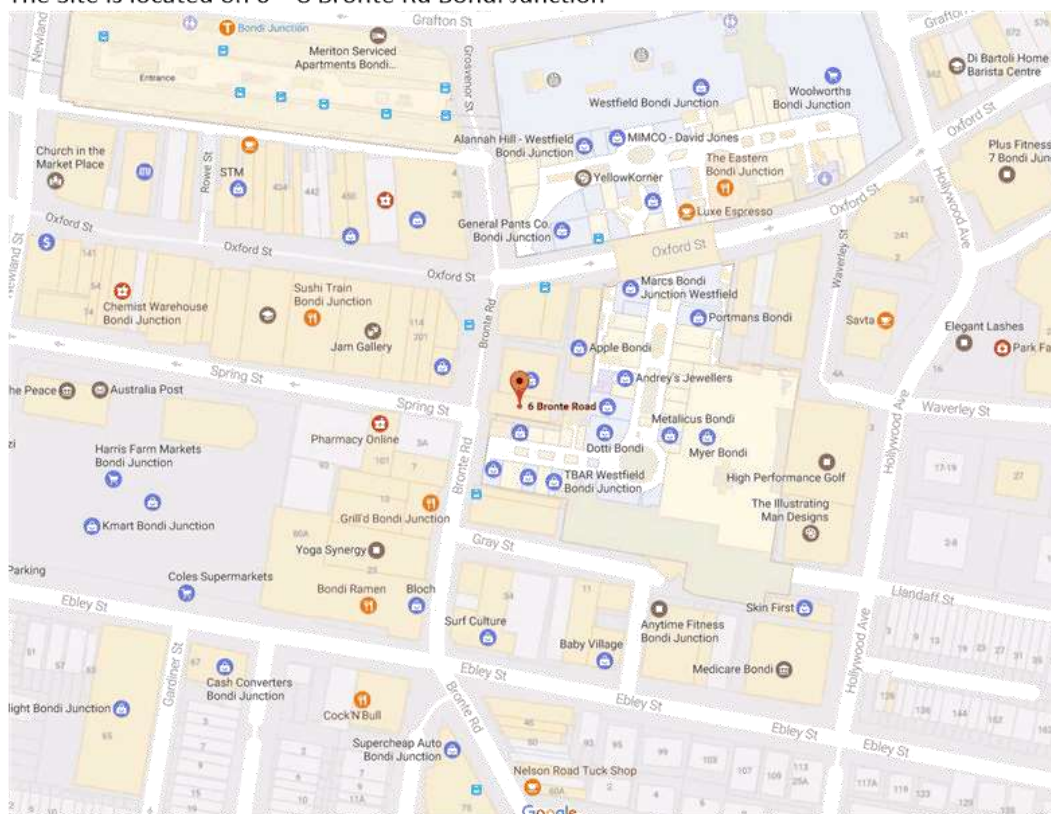
- Demolition / Excavation (Minor as there is no basements)
- Construction



### 3. Anticipated Impact

#### a. Local Traffic Routes

The Site is located on 6 – 8 Bronte Rd Bondi Junction



Locally, the anticipated impact of the demolition, and construction works shall be restricted to the site shown above. There will be almost no impact to the Bronte Rd Bus Corridor, as all construction works will be limited to the constraints of the site, and the proposed work zones in front.

All vehicles entering and existing the site will be limited to the same route, which is detailed within the construction traffic management plan.

#### b. On-street parking in the local area

As the proposed construction zone does not fit the parallel parking of two trucks, a truck waiting area should be located outside the Bondi Junction CBD, to minimise the impact on traffic. This system allows the trucks during construction to be relatively in near proximity of the site while not double parking or obstructing the traffic on Bronte Road.

### c. Pedestrian Circulation

Pedestrian circulation adjacent to the building site may be considered as minimal. The measures proposed to mitigate the impact of construction works and promote public safety are described herewith.

## 4. Demolition and Site Clearance

	Items	Details/Action
1	Site excavation	Following the demolition of the existing structure
2	Trucks	Truck and Dogs will not be used at this project. Smaller trucks are permissible pending Project Manager's approval.
4	Streets to be used	Bronte Rd is the only accessible street.
6	Truck storage area	Trucks will be strategically aligned, so that once a truck is in the construction zone outside the site's front, the second is waiting within a specifically defined zone outside the CBD. Once the truck has left the site, the second will be called to come to site.

## 5. Structure

The following describes the means proposed to manage construction works to minimise the anticipated impacts. Please refer to *Appendix A* for a combined Site Plan / Construction Traffic Management Plan (CTMP Drawing) showing the location of:

- Site sheds (No room for site sheds on this project)
- Anticipated use of tower crane and concrete pumps
- Area of Council property on which it is proposed to install a Works Zone (Construction Zone)

	Item	Details / Action
1	Construction Methods	As pouring of concrete is going to be limited to a stationary pump and a flexible hose, it is recommended to adapt construction methods which require minimal amount of concrete to be delivered to site. Suggestions as Ultra-floor solution and precast concrete panels are currently being developed by the Project Managers.

2	Pre-cast Panels	Precast Panels will be used where possible to reduce the construction period. The panels will be delivery to site by a truck which will be parked on the proposed construction zone (refer to CTMP attached) and then craned to site via a stationary tower crane.
3	Slab Sizing	As previously stated, the builder should endeavour to keep the volume of required concrete to a minimal value. This can be achieved by pursuing engineering solutions such as Ultra floor and Precast panels. This exercise is aimed at minimising the amount of concrete trucks entering and existing the site.
4	Construction Methods	At this stage of the project and as there is builder engaged to the project, the construction methods can not be finalised.

## 6. Concrete Pouring

	Items	Details/Action
1	Concrete Pump flexible hose	A certain part of the flexible concrete hose will be located beneath the ground of the footpath, in order to minimise the risk to pedestrians.
2	Concrete Pump Location	Due to the constraints of the site, and the proposed work zone, it is not possible to use a mobile pump. The only possible way to pump concrete to site is to use a stationary pump and a flexible hose which will be run underneath the footpath.

## 7. Fit out & Finishing

1	Please refer to the Architectural Plans for further details in regards to fit out and finishes.
---	---

## 8. Hoarding Plans

	Items	Details/Action
1	Hoarding Class	Type B Hoarding

2	Hoarding Location	Hoarding will be required to protect pedestrian walking on the footpath along Bronte Road.
3	Bins and Skips	5 m3 Bins may be located on site during construction
4	Hazardous Waste	

### 9. Crane Locations

	Items	Details/Action
1	Crane Type	Mobile Crane with an arm which can not reach Bronte road.
2	Crane Location	Within the lift shaft

This Preliminary Report was prepared by:

**Ali Badran**

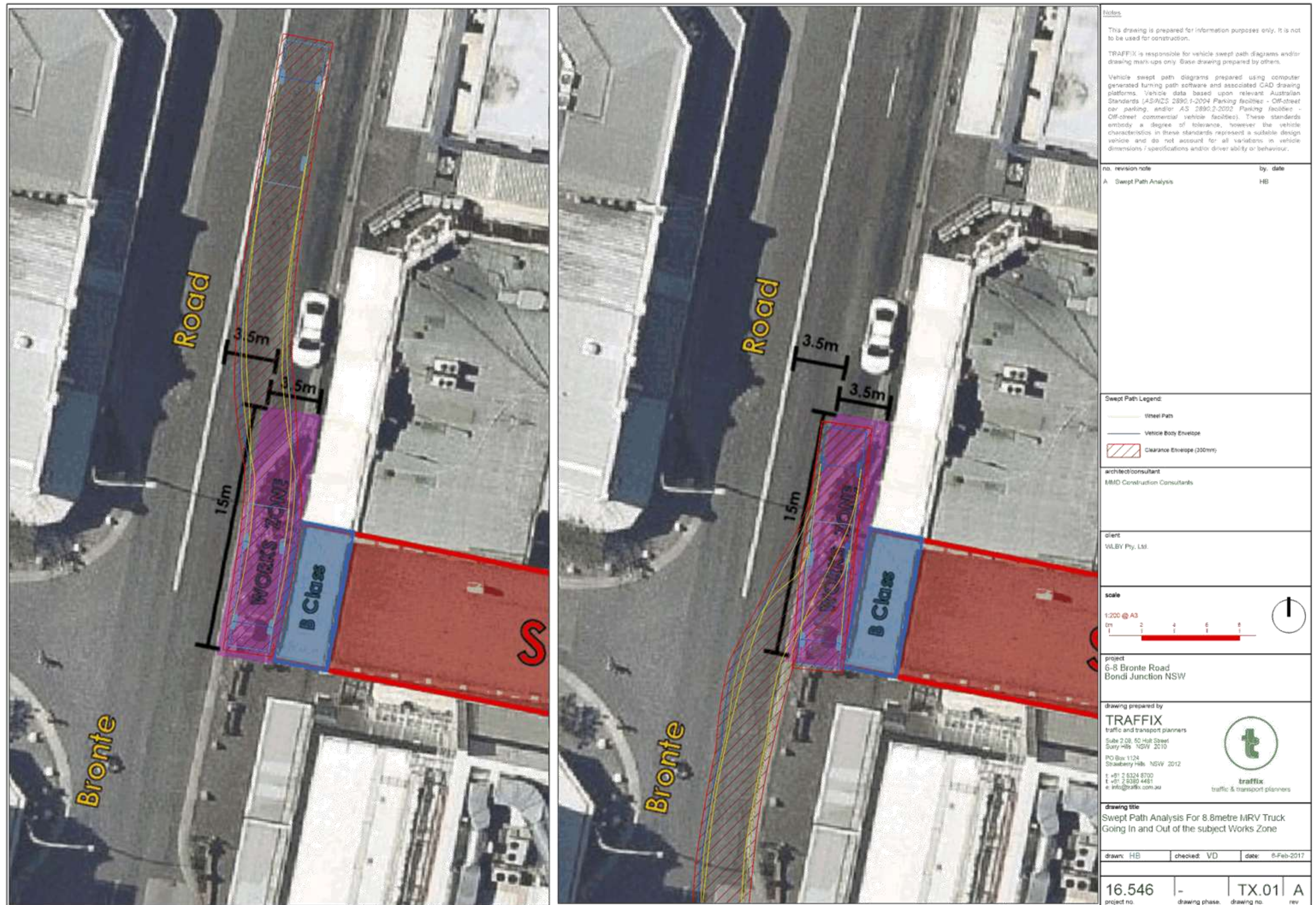
B.Engineering (AUB) | Master in Project Management (USYD)



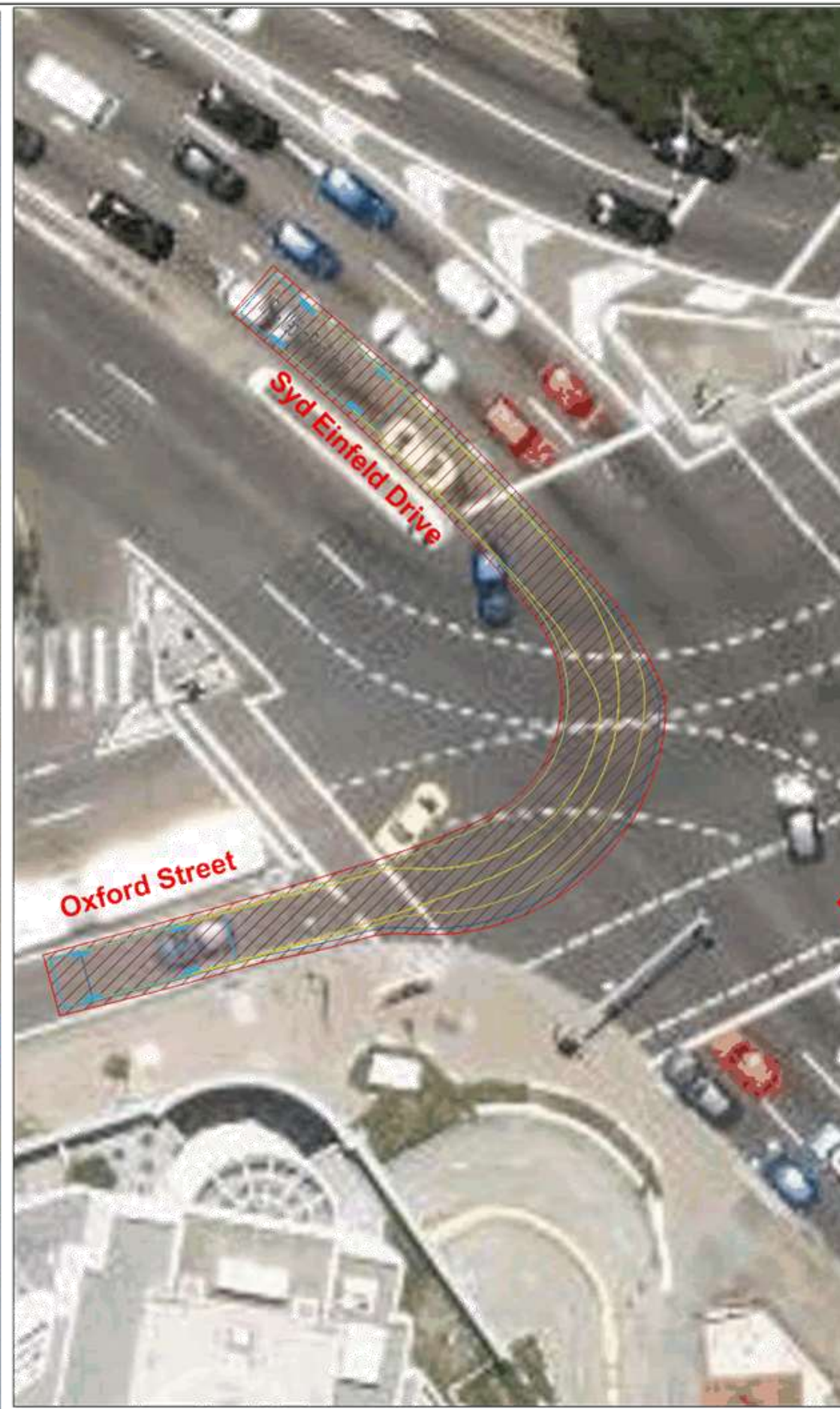
## Appendix B


### Swept Path Analysis



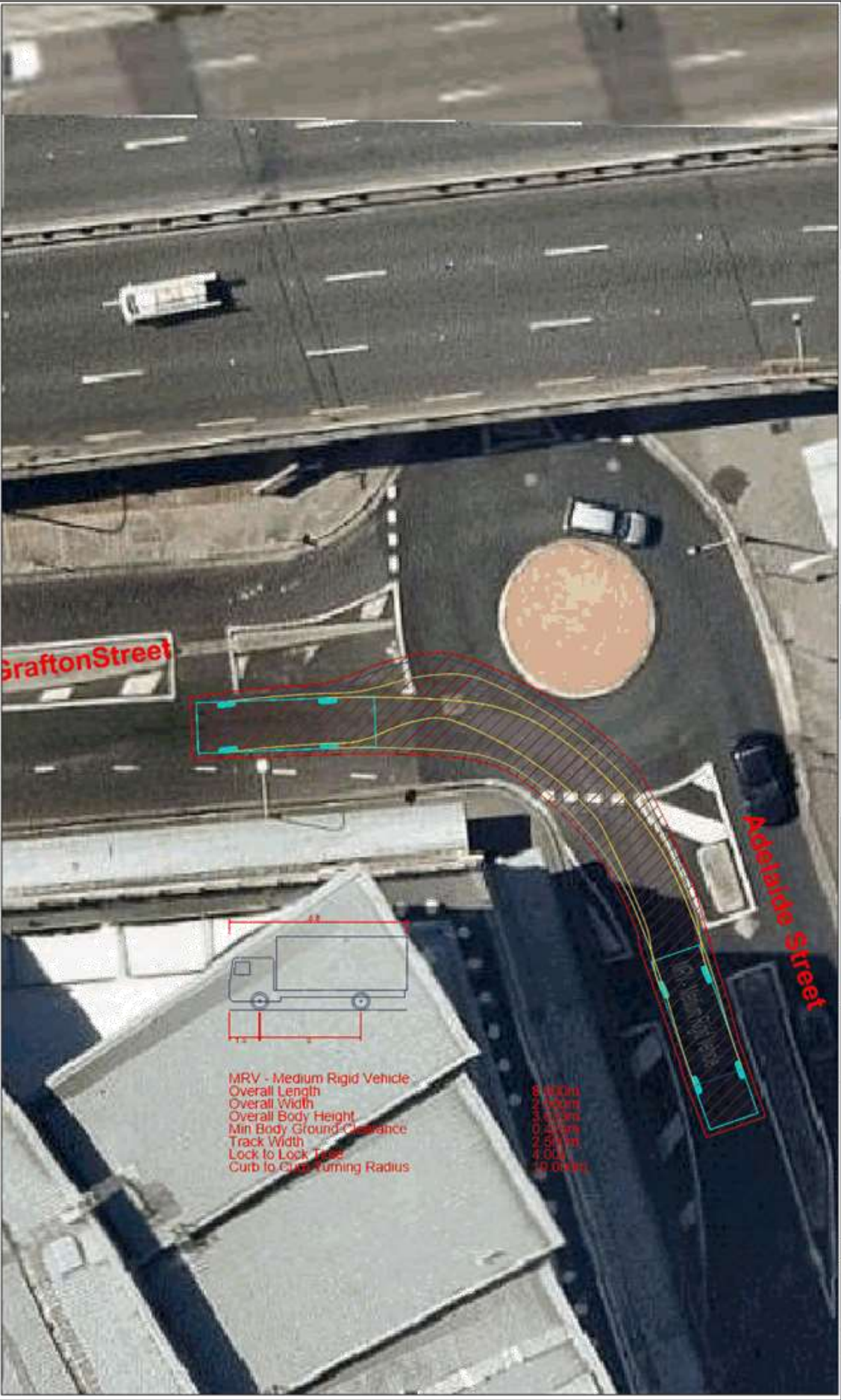






<b>Notes</b>		
This drawing is prepared for information purposes only. It is not to be used for construction.		
TRAFFIX is responsible for vehicle swept path diagrams and/or drawing mark-ups only. Base drawing prepared by others.		
Vehicle swept path diagrams prepared using computer generated turning path software and associated CAD drawing platforms. Vehicle data based upon relevant Australian Standards (AS/NZS 2890.1:2004 Parking facilities - Off-street car parking, and/or AS 2890.2:2002 Parking facilities - Off-street commercial vehicle facilities). These standards embody a degree of tolerance, however the vehicle characteristics in these standards represent a suitable design vehicle and do not account for all variations in vehicle dimensions / specifications and/or driver ability or behaviour.		
no.	revision note	by, date
A	Swept Path Analysis	HB
<b>Swept Path Legend:</b>		
Wheel Path		
Vehicle Body Envelope		
Clearance Envelope (300mm)		
architect/consultant		
IMMO Construction Consultants		
client		
VILBY Pty. Ltd.		
scale		
1:250 @ A3		
0m 1 2 3 4 5		
project		
6-8 Bronte Road Bondi Junction NSW		
drawing prepared by		
TRAFFIX traffic and transport planners Suite 2.08, 50 Holt Street Sunny Hills NSW 2010 PO Box 1124 Strawberry Hills NSW 2012 t: +61 2 8324 8700 f: +61 2 9380 4481 e: info@traffix.com.au		
		
drawing title		
Swept Path Analysis For 8.8metre MVRV Truck		
drawn:	checked:	date:
HB	VD	8-Feb-2017
16.546	-	TX.02
project no.	drawing phase.	drawing no.
		A





Notes

This drawing is prepared for information purposes only. It is not to be used for construction.

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no.	revision note	by	date
A	Swept Path Analysis	HB	

Swept Path Legend:

- Wheel Path
- Vehicle Body Envelope
- Clearance Envelope (300mm)

architect/consultant

MMD Construction Consultants

client

VALBY Pty. Ltd.

scale

1:250 @ A3

project

6-8 Bronte Road  
Bondi Junction NSW

drawing prepared by

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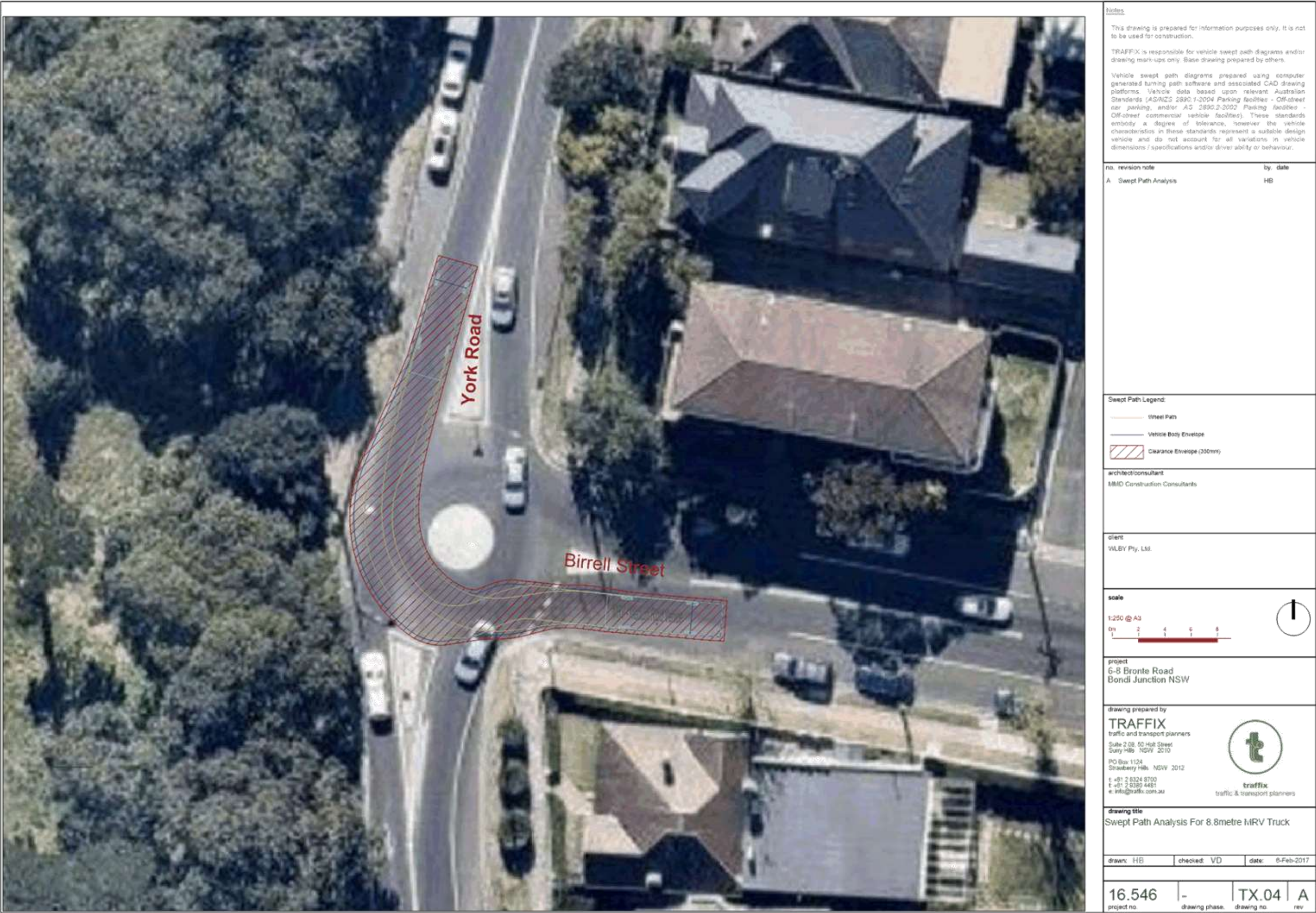
drawing title

Swept Path Analysis For 8.8metre MRV Truck

drawn: HB	checked: VD	date: 6-Feb-2017
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project no.	drawing phase	drawing no.	rev





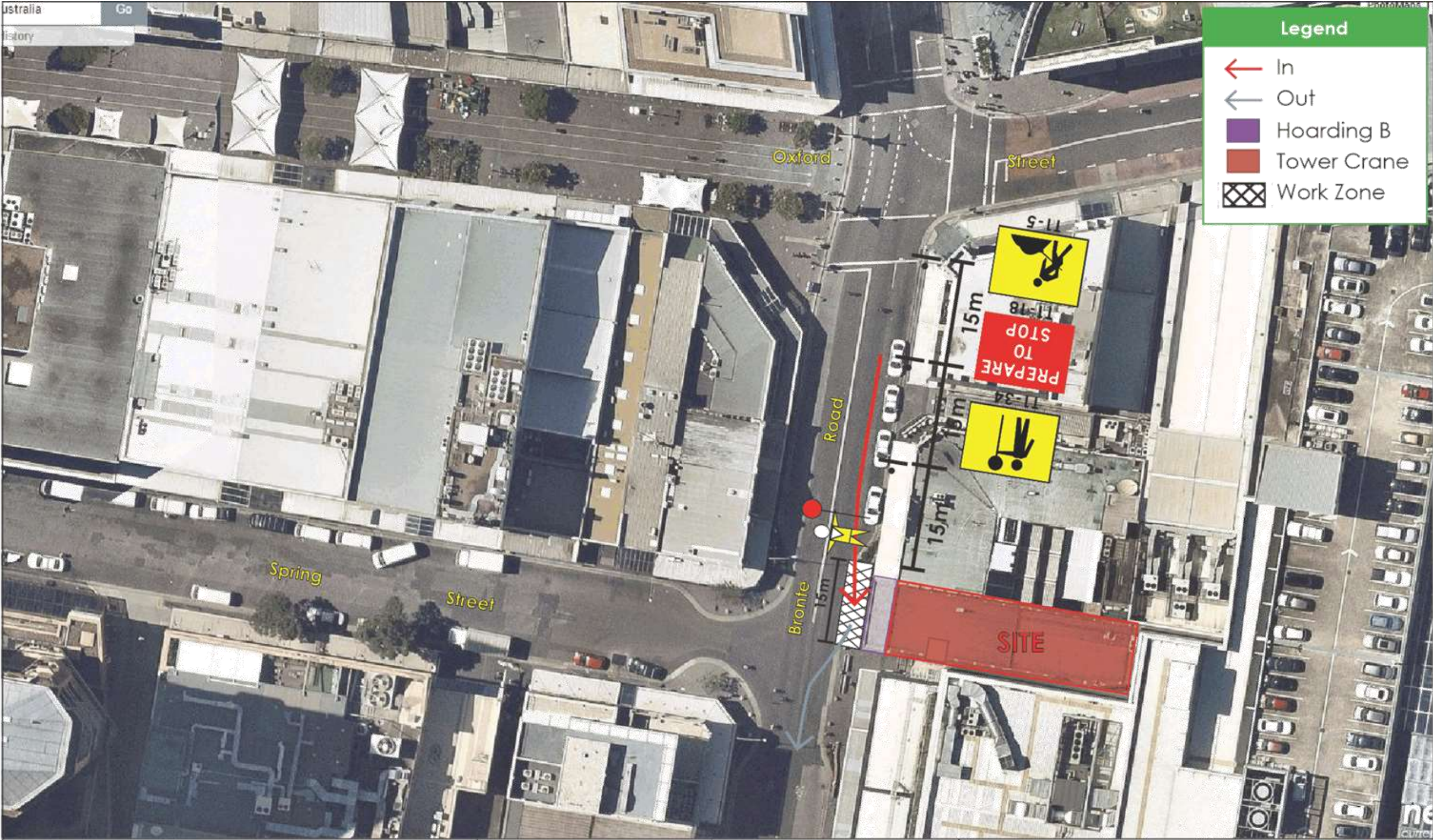


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## Appendix C

### Traffic Control Plan





Traffic Control Plan - TCP - Construction Stages	Date:	07.02.2017	TRAFFIX traffic and transport planners Suite 2/08, 50 Holt Street Surry Hills NSW 2011 PO Box 1124 Surry Hills NSW 1335 t: +61 2 8324 8700 f: +61 2 9380 4481 e: info@traffix.com.au	 traffix traffic & transport planners
Project: 16.546 6-8 Bronte Road, Bondi Junction	Prepared By:	Vince Doan		
Client: WLBY Pty Ltd	Certificate No.:	0031360631		



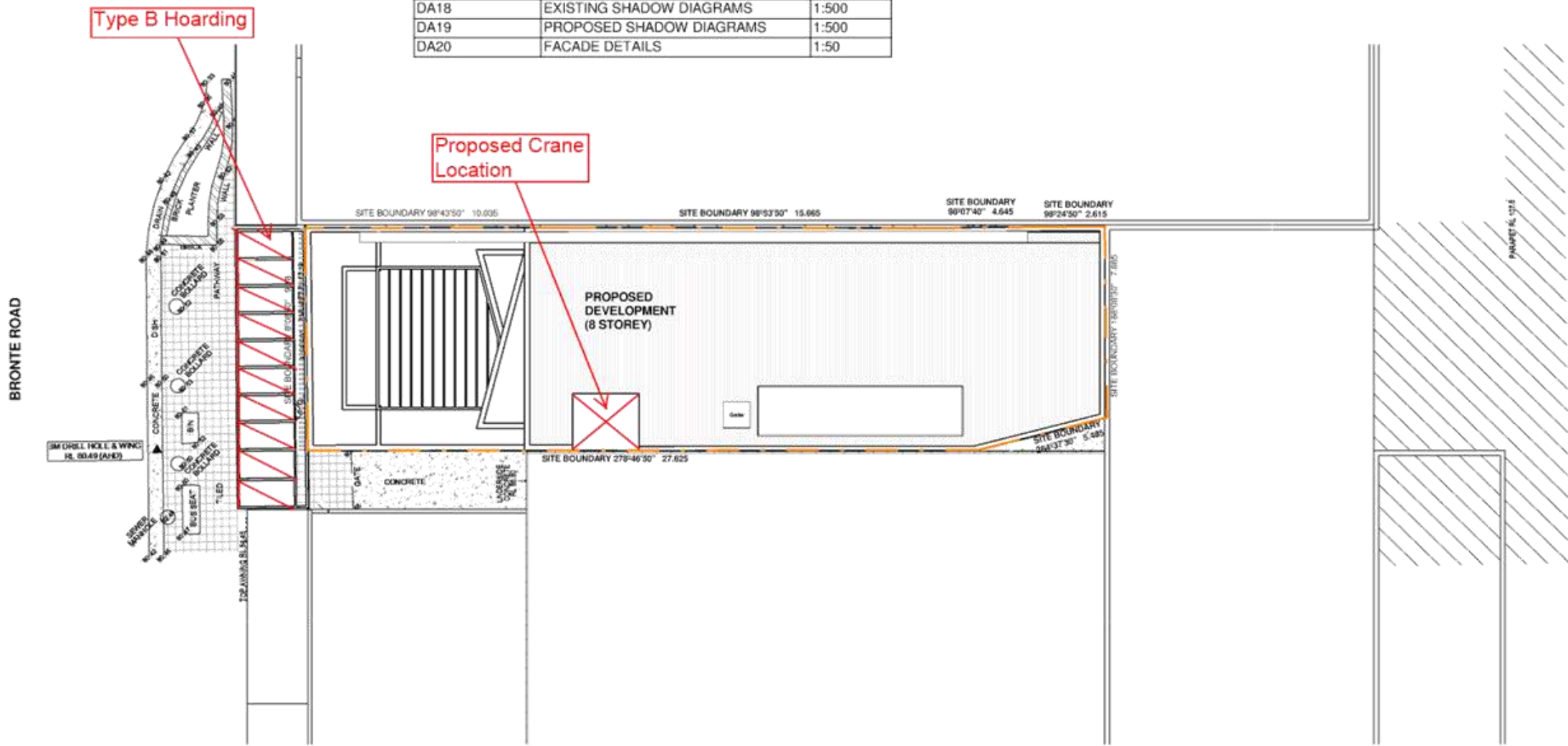


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## Appendix D

### Construction Management Plan

Drawing List		
Sheet Number	Sheet Name	Scale
DA01	DRAWING LIST AND SITE PLAN	1:200
DA02	SITE ANALYSIS	1:500
DA03	STREETSCAPE & CONTEXT	1:200
DA04	PROPOSED GROUND FLOOR PLAN	1:100
DA05	GROUND FLOOR PLAN (FUTURE ADAPTION)	1:100
DA06	PROPOSED LEVEL 1 PLAN	1:100
DA07	PROPOSED LEVEL 2 PLAN	1:100
DA08	PROPOSED LEVEL 3, 5 & 7	1:100
DA09	PROPOSED LEVEL 4 & 6 PLAN	1:100
DA10	PROPOSED ROOF PLAN	1:100
DA11	PROPOSED NORTH ELEVATION	1:150
DA12	PROPOSED EAST ELEVATION	1:150
DA13	PROPOSED SOUTH ELEVATION	1:150
DA14	PROPOSED WEST ELEVATION	1:150
DA15	SCHEDULE OF EXTERNAL FINISHES	
DA16	SECTION 1	1:150
DA17	SECTION 2	1:150
DA18	EXISTING SHADOW DIAGRAMS	1:500
DA19	PROPOSED SHADOW DIAGRAMS	1:500
DA20	FACADE DETAILS	1:50



## REPORT

### TC/C.02/17.03



**Subject:** Bayview Street, Bronte - Works Zone

**TRIM No.:** A03/2514-04

**Author:** Sam Samadian, Traffic Engineer

**Director:** Emily Scott, Director, Waverley Renewal

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#### COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 20m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone on the bend on the southern side of Bayview Street opposite No.29 subject to there being no blockage to through traffic on Bayview Street other than for short periods of time when manoeuvring vehicles into and out of the development site.
2. Delegate's authority to the Executive Manager, Creating Waverley, to extend the duration, length and location of the zone as necessary.

#### 1. Executive Summary

Council has received an application from the builder/developer at No.29 Bayview Street, Bronte for the installation of a construction zone on the opposite side of Bayview Street to cater for the swept wheel path of a medium rigid vehicle as it gains access to and from the site in the narrow carriageway street. Council will need to exercise its delegated function to implement the proposal.

#### 2. Introduction/Background

In accordance with standard practice in Waverley Council, it is proposed that the zone be signposted "No Parking - Council Authorised Vehicles Excepted" for the approved construction hours under the development consent. Council will then issue the applicant with a number of transferable permits to be used on their construction-related vehicles which can be parked in the zone whilst trucks are not entering or exiting the site.

#### 3. Technical Analysis

There is an existing NO STOPPING zone outside part of the site extending to the east to aid traffic movements around a bend in Bayview Street. It is proposed to install a 20m long Construction Zone opposite the site to accommodate truck manoeuvring into and out of the site as shown on the swept wheel path drawing (*Figure 2*). The zone then will be utilised to cater for construction activities during the later stages of building.



**NO STOPPING zone outside 29 and 27 Bayview Street, Bronte**

<b>Applicant</b>	Bronxx Construction Management
<b>Development Application</b>	DA-544/2015
<b>Works</b>	Demolition of existing dwelling and construction of a dual occupancy development with basement parking, including subdivision into 2 lots
<b>Approved Hours of Construction</b>	7am-5pm Mon-Fri, 8am-3pm Saturday
<b>Road</b>	Bayview Street
<b>Existing Parking</b>	No stopping outside Unrestricted south of Bayview Street
<b>Requested Length</b>	20 m
<b>Proposed Length</b>	20 m
<b>Duration</b>	13 weeks
<b>Beginning</b>	March
<b>Fee Area</b>	Residential, without metered parking
<b>Estimated Fees</b>	\$1160 per week





**Figure1** Aerial photograph of 29 Bayview Street, Bronte

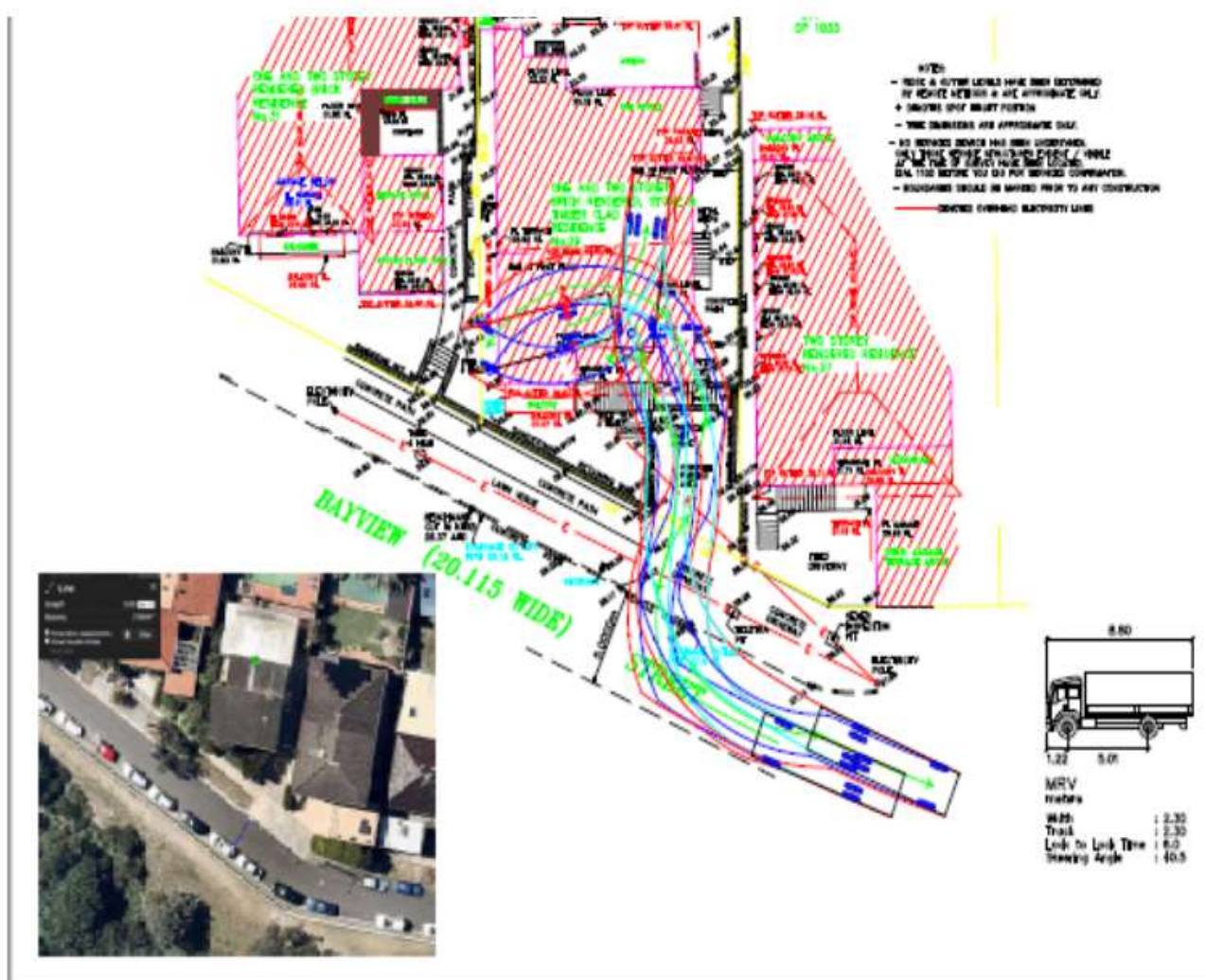


Figure2 Aerial photograph of 29 Bayview Street, Bronte – Swept wheel path of MRV

**4. Financial Information for Council's Consideration**

The applicant will be required to pay the approved application fee, which covers the cost of administration and signage.

The approved usage charges for the 2016/17 financial year are:

- \$58 per metre per week in a residential developments
- \$83per metre per week in a commercial & mixed-use developments

**5. Attachments:**

Nil

**REPORT**  
**TC/C.03/17.03**

**Subject:** Multiple Streets - Installation of 'No Stopping' Zones

**TRIM No.:** A02/0637-02

**Author:** Sam Samadian, Traffic Engineer

**Director:** Emily Scott, Director, Waverley Renewal

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**COUNCIL OFFICER'S PROPOSAL:**

That Council installs 'No Stopping' zones as follows:

1. Ashton Street at York Road, Queens Park:
  - (a). 10m on the eastern side of Ashton Street, north of York Road,
  - (b). 10m on the western side of Ashton Street, north of York Road,
  - (c). 10m on the northern side of York Road, west of Ashton Street,
  - (d). 10m on the northern side of York Road, east of Ashton Street.
2. Alt Street at Birrell Street, Queens Park:
  - (a). 10m on the eastern side of Alt Street, south of Birrell Street.

**1. Executive Summary**

Council has received representation from local residents and Council's Resource Recovery section requesting consideration be given to improving the sight distance and manoeuvrability for vehicles at the following intersections:

1. Ashton Street at York Road, Queens Park,
2. Alt Street at Birrell Street, Queens Park.

In response, it is proposed to install the statutory "NO STOPPING" restrictions at the above locations. Council will need to exercise its delegated functions to implement the proposal.

**2. Introduction/Background**

Residents and Council's Resource Recovery section have reported that sight distance and manoeuvrability is restricted by vehicles parked close to the intersections at several locations.

### 3. Technical Analysis

4.

#### 1. Ashton Street at York Road, Queens Park

There is currently no signposted parking restriction on the corners of Ashton Street and York Road, Queens Park.

A number of inspections have taken place during the day, revealing that vehicles are often parked close to this intersection. Vehicles parked illegally at these locations significantly reduce driver sight distances and block pedestrian access.

Therefore, since compliance is an issue at this location, it is recommended that the legislative requirements be signposted. This will not result in a net loss of legal on-street parking spaces.



**Figure 1.1** *Aerial photograph of intersection of Ashton Street and York Road, Queens Park*

#### 2. Alt Street at Birrell Street, Bondi Junction

There are currently 'No Stopping' restrictions:

1. on the southern side of Birrell Street, west of Alt Street,
2. on the southern side of Birrell Street, east of Alt Street,
3. on the western side of Alt Street, south of Birrell Street.

A number of inspections have taken place during the day, revealing that vehicles are often parked close to this intersection. Vehicles parked illegally at this location significantly reduce driver sight distances and hinder access.

Therefore, since compliance is an issue at this location, it is recommended that the legislative requirements be signposted. This will not result in a net loss of legal on-street parking spaces.





**Figure 2.1 Aerial photograph of intersection of Alt Street and Birrell Street, Queens Park**



**Vehicle parked in At Street within 10m of Birrell Street**

### Legislative Requirements

Under the [NSW] *Road Rules 2014*:

“A driver must not stop on a road within 10 metres from the nearest point of an intersecting road at an intersection without traffic lights, unless the driver stops ... at a place on a length of road, or in an area, to which a parking control sign applies and the driver is permitted to stop at that place under these Rules ...”.

Therefore, vehicles parked at these locations may be infringed without the installation of additional signage. However, the RMS's Technical Direction TDT 2002/12 *Stopping and Parking Restrictions at intersections and Crossings* addresses the installation of 'No Stopping' and 'No Parking' signs at intersections and states:

"It is not intended that signs be installed at all locations. Where signs are not installed the legislative restrictions will apply. Generally signposting of restrictions covered by legislative requirements is only required where there is adjoining signposting or compliance is an issue. **However, signposting adjacent to children's crossings, pedestrian crossings and marked footcrossings should always occur.** The relevant road authority is to determine where NO STOPPING and NO PARKING signs are to be erected.

Existing signs, with the exception of NO STANDING signs (see Technical Direction No 99/21), are not to be removed or replaced until required under maintenance. Where the post is required to be replaced, the sign should be relocated to the new location as specified in this *Technical Direction*."

Guidelines used:

- *Transport Administration Act 1988*
- *RMS's Delegation to Councils; Regulation of Traffic*
- *RMS's Signs Database*
- *[NSW] Road Rules 2014*
- *RMS's TDT 2002/12.*

## 5. Financial Information for Council's Consideration

If changes to signs and/or linemarking are approved, Council will undertake the installation of signs/lines from existing budgets.

## 6. Attachments:

Nil

**REPORT**  
**TC/C.04/17.03**

**Subject:** Birrell Street, Queens Park - Works Zone

**TRIM No.:** DA-621/2014

**Author:** Geoff Garnsey, Manager Traffic and Development

**Director:** Emily Scott, Director, Waverley Renewal

---

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 9m long, "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone on the Birrell Street frontage of 125 Bronte Road starting 10m from a "No Stopping" zone at Blenheim Street subject to there being no blockage to through traffic on Birrell Street other than for short periods of time when manoeuvring vehicles into and out of the Works Zone.
2. Delegate's authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone as necessary.

**1. Executive Summary**

The developer at No.125 Bronte Road, Queens Park is seeking approval for the installation of a works/construction zone on either the Birrell Street frontage or the Blenheim Street frontage of the property. This report recommends the installation of the zone on the Birrell Street frontage. Council will need to exercise its delegated function to implement the proposal.

**2. Introduction/Background**

Development Consent 621/2014 was issued by the Land and Environment Court of NSW on 20 January, 2016 for the demolition of the existing buildings on the site at 125 Bronte Road, Queens Park and the construction of a new, three (3) storey building containing a boarding house with 19 rooms, a manager's room and basement parking with access off Blenheim Street for the parking of 6 vehicles, bicycles and motorbikes.

The applicant is due to commence construction shortly and is seeking approval for a works/construction zone outside the site.

**3. Technical Analysis**

The site at 125 Bronte Road is located on the south-western corner of the signalised intersection of Bronte Road and Birrell Street, Queens Park.

The site has a frontages to Bronte Road in the east, Birrell Street to the north and Blenheim Street at the rear to the west.





***Aerial image of 125 Bronte Road Queens Park on the south-western corner of Bronte Road and Birrell Street.***

The Bronte Road frontage has full time “No Stopping” restrictions due to its proximity to the traffic signals. The Birrell Street frontage has a combination of “No Stopping” and “No Parking” restrictions and “No Stopping” restrictions exist across the full Blenheim Street frontage.



***Image of 125 Bronte Road, SW corner of Birrell Street (current Jaycar site)***



*Image of Birrell Street frontage with part “No Stopping” and part “No Parking” restrictions*



*Image of Blenheim Street frontage with “No Stopping” restrictions*

The applicant proposes two options for a works zone:

Option 1: On Birrell Street in the current “No Parking” zone (Preferred Option)

Option 2: In Blenheim Street at the rear with trucks occupying a fully closed footpath area

**Option 1 (Preferred Option)**

The RMS has indicated they raise no objections to a works zone being installed on the Birrell Street frontage provided a 10m long “No Stopping” zone is maintained to Blenheim Street to its west and “No Stopping” to the traffic signals to the east.

The width of travel lane between a works zone on Birrell Street and the BB centrelines is 3.7m which is adequate for the passing of heavy vehicles.

## **Option 2**

This option requires the full closure of the eastern side footpath in Blenheim Street with trucks parking on the footpath within 10m of Birrell Street.

This option is considered unsatisfactory on pedestrian and traffic safety grounds.

Pedestrians will be required to cross to the western side footpath to travel past the site and trucks when manoeuvring into and out of the footpath area will be doing so in close proximity to vehicles turning into Blenheim Street from Birrell Street.

<b>Applicant</b>	TBA
<b>Development Application</b>	DA-621/2014
<b>Works</b>	Demolition of all structures on the site and construction of a new 3 storey, 19 room boarding house with basement car parking.
<b>Approved Hours of Construction</b>	7am-5pm Mon-Fri, 8am-3pm Saturday
<b>Frontage Length</b>	25 m
<b>Road</b>	Birrell Street
<b>Existing Parking</b>	Combination of "No Stopping" and "No Parking"
<b>Requested Length</b>	9 m
<b>Proposed Length</b>	9 m
<b>Duration</b>	13 weeks min
<b>Beginning</b>	April/May
<b>Fee Area</b>	Zoned Residential R3, without metered parking
<b>Estimated Fees</b>	\$ 522 per week

In accordance with standard practice in Waverley Council, it is proposed that the zone be signposted "No Parking Council Authorised Vehicles Excepted" for the approved construction hours under the development consent. Council will then issue the applicant with a number of transferable permits to be used on their construction-related vehicles.

## **4. Financial Information for Council's Consideration**

The applicant will be required to pay the approved application fee, which covers the cost of administration and signage.

The approved usage charges for the 2016/17 financial year are:

- \$58 per metre per week in a residential developments
- \$83per metre per week in a commercial & mixed-use developments

**5. Attachments:**

1. Construction Traffic Management Plan- Stanbury Traffic Planning- March 2017- ref 16-054
2. Architectural drawings 1
3. Architectural drawings 2
4. Survey Plan



**STANBURY**  
TRAFFIC PLANNING  
TRAFFIC, PARKING & TRANSPORT CONSULTANTS

## CONSTRUCTION TRAFFIC MANAGEMENT PLAN

APPROVED BOARDING HOUSE DEVELOPMENT  
125 BRONTE ROAD  
QUEENS PARK

PREPARED FOR DRYVE CLEAN PTY. LTD.  
OUR REF: 16-054



**MARCH 2017**

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## Stanbury Traffic Planning

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- 5. Birrell Street Works Zone Swept Path Plan**
- 6. Blenheim Street Footpath Occupation Swept Path Plan**



## 1. INTRODUCTION

### 1.1 Scope of Assessment

Stanbury Traffic Planning has been commissioned by Dryve Clean Pty. Ltd. to prepare a Construction Vehicle and Pedestrian Plan of Management (CVPPM) for the construction of an approved boarding house development at 1256 Bronte Road, Queens Park.

The purpose of this Plan of Management is to provide details of the management of traffic and pedestrian movements to, from and adjoining the subject site associated with the construction stages of the abovementioned development. The Plan is required to be prepared in response to Conditions of Consent No. 23 of DA-621/2014.

This Plan provides the following scope of assessment:

- Section 2 provides a summary of the site location, details, existing and surrounding land-uses;
- Section 3 describes the existing traffic, parking and transport conditions surrounding and servicing the subject development site including a description of the surrounding road network, traffic demands, operational performance and available public transport infrastructure;
- Section 4 describes the approved development;
- Section 5 describes the planned internal and external management during construction;
- Section 6 describes management measures to be implemented to ensure safe and efficient site access / egress by construction vehicles;
- Section 7 describes the construction staging and traffic generation characteristics; and
- Section 8 describes the ability or otherwise of the surrounding road network to accommodate the additional demand associated with the site construction works.

This Plan should be read in conjunction with Construction Certificate plans prepared by Hill Thalys Architecture + Urban Projects Pty. Ltd.

## 2. SITE DETAILS

### 2.1 Site Location

The site is situated on the southern side of Birrell Street, bound by its junction with Blenheim Street in the west and Bronte Road in the east. The site location is illustrated overleaf within a local and aerial context by **Figure 1** and **Figure 2**, respectively.

### 2.2 Site Description

The subject land provides a street address of 125 Bronte Road, Queens Park and a real property description of Lot 1 DP120589. The site forms a predominantly trapezoidal shaped parcel of land, providing approximate frontages of 16m, 25m and 15m to Bronte Road, Birrell Street and Blenheim Street respectively. The total site area is in the order of 424m<sup>2</sup>.

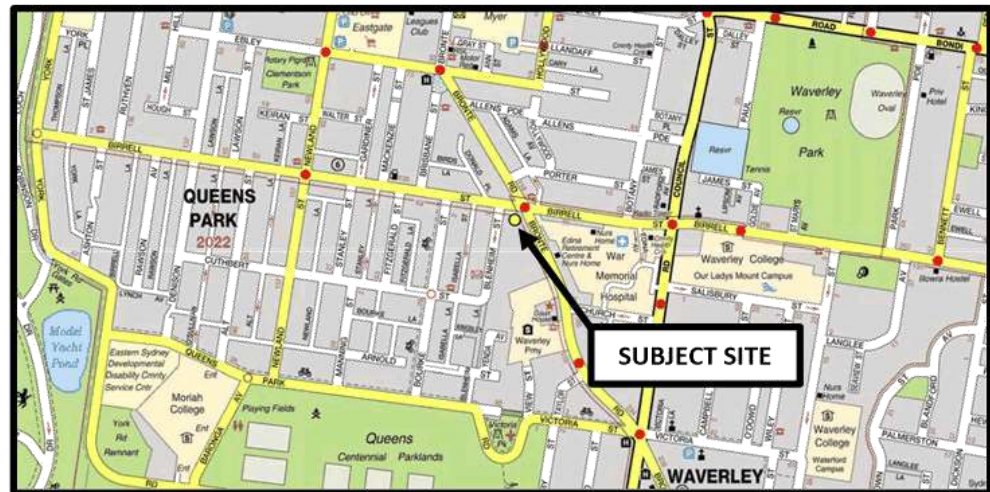
### 2.3 Existing Uses

The subject site is currently accommodating a retail electronics store (Jaycar Electronics) providing an approximate retail floor area of 118m<sup>2</sup>.

### 2.4 Surrounding Uses

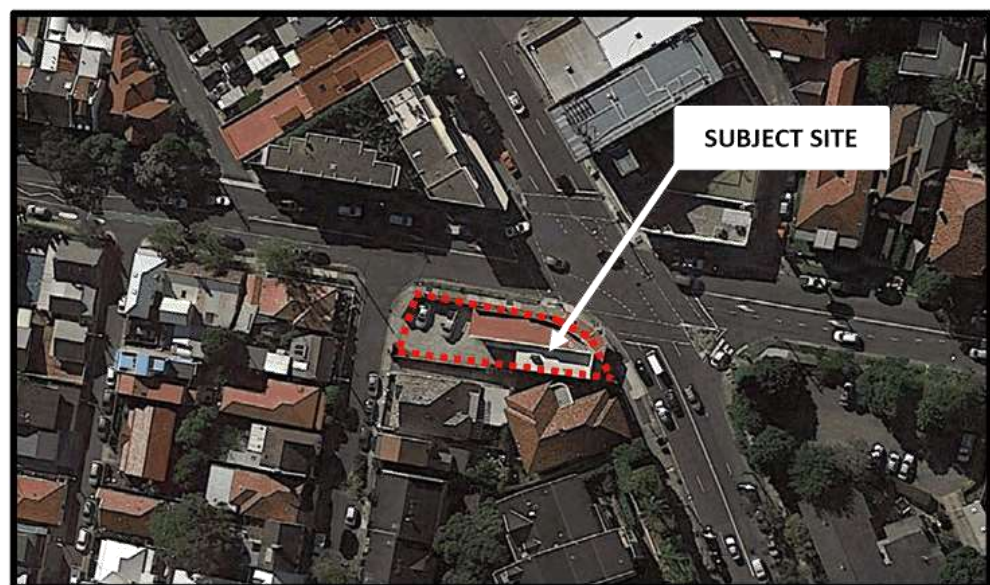
Land to the south and west generally accommodates low and medium density residential developments, fronting and accessed via Bronte Road, Blenheim Street and Birrell Street. Land to the north primarily accommodates commercial and mixed use development on approach to the Bondi Junction town centre.

**FIGURE 1**  
**SITE LOCATION WITHIN A LOCAL CONTEXT**



Source: UBD's Australian City Streets – Version 4

**FIGURE 2**  
**SITE LOCATION WITHIN AN AERIAL CONTEXT**



Source: Google Earth (accessed 07/03/17)

### 3. SURROUNDING ROAD NETWORK

#### 3.1 Road Network Function and Control

Bronte Road is a regional road under the administrative care and control of Waverley Municipal Council but partially funded by the Roads and Maritime Services. It performs a collector function, providing a north/south link between Oxford Street in the north with Bronte in the south-east. Bronte Road generally provides a 13m wide pavement, accommodating one through lane of traffic in each direction in conjunction with parallel parking along both kerb alignments.

Bronte Road forms an intersection with Birrell Street under traffic signal control, adjacent to the subject site. Parking restrictions apply within Bronte Road on approach to the signalised intersection to facilitate the provision of two approach lanes to facilitate turning movements. Traffic flow is governed by a sign posted speed limit of 50km/h however a 40km/h school zone speed limit applies to the south of Birrell Street during prescribed school start and finish times associated with Waverley Primary School.

Birrell Street performs a similar collector function to Bronte Road, providing an east/west connection between York Road in the west and Council Street / Carrington Road in the east, intersecting under single circulating roundabout and traffic signal control, respectively. Birrell Street continues to the east past Council Street / Carrington Road towards Tamarama.

Birrell Street provides a 13m wide carriageway, facilitating one through lane of traffic in each direction in addition to a designated parking lane adjoining both sides of the kerb. Kerb side parking restrictions apply on approach Bronte Road to facilitate the provision of exclusive right turn lanes in conjunction with combined through / left turn lanes on approach to the signalised intersection. Traffic flow is governed by a sign posted speed limit of 50km/h. A single 6.3m wide westbound travel lane is provided within Birrell Street adjacent to the subject site with kerb-side parking governed by a combination of 'No Stopping' on immediate departure from Bronte Road and approach to Blenheim Street, with 'No Parking' restrictions applying in-between.

Birrell Street intersects with Blenheim Street adjacent to the north-western corner of the site, under major / minor priority control, with Birrell Street forming the priority route. The right turn lane within Birrell Street servicing Bronte Road, also provides a secondary function servicing Blenheim Street access movements.

Blenheim Street provides a one-way (southbound) local access road function largely servicing abutting residential development, prior to curving to the west to connect with and form Cuthbert Street. Blenheim Street provides a pavement width of 6.5m accommodating a single southbound travel lane in conjunction with accommodate parallel parking along the eastern kerb alignment (governed by 2 hour restrictions during weekday business periods, with permit holders exempted). Traffic flow within Blenheim Street is governed by a sign posted 2 tonne load limit and a speed limit of 40km/h.



### 3.2 Existing Traffic Demands and Operation

Weekday peak hour traffic surveys were undertaken by others in December 2014 associated with the Development Application Traffic Impact Assessment. Whilst these surveys are approximately two years old, recent observations have indicated that the results remain valid, indicating the following:

- Directional traffic flow within Bronte Road is approximately 350 - 600 vehicles per hour during peak periods;
- Directional traffic flow within Birrell Street is approximately 400 - 600 vehicles per hour during peak periods;
- Directional traffic flow within Birrell Street is tidal during peak periods, with eastbound traffic dominating during the morning peak and westbound traffic dominating during the evening peak; and
- Traffic flows within Blenheim Street are low during peak periods, being less than 50 vehicles per hour.

SIDRA intersection traffic modelling undertaken in association with the abovementioned assessment indicated that the intersection of Bronte Road and Birrell Street currently operates at a Level of Service 'C' during both the morning and afternoon peak hour period, representing satisfactory conditions.

### 3.3 Public Transport

Sydney Buses operate a number of buses along Bronte Road, as follows:

- Routes 313 and 314 between Coogee and Bondi Junction via Waverley;
- Routes 316 and 317 between Eastgardens and Bondi Junction via South Maroubra, Maroubra Beach, South Coogee, Randwick Junction and Waverley;
- Route 348 between Wolli Creek and Bondi Junction via Tempe, Sydenham, St. Peters, Alexandria, Rosebery, UNSW, Randwick and Waverley;
- Route 353 between Bondi Junction and Eastgardens via Coogee; and
- Route 378 between Bronte and Railway Square via Waverley, Bondi Junction, Paddington and Darlinghurst.

The closest stops are located within 100m of the site to the north and south of Birrell Street.

Further to the above, the site is approximately 650m walking distance to the south of Bondi Junction Railway Station. Bondi Junction provides connectivity to regular and efficient services operating along the T4 (Eastern Suburbs & Illawarra) line. This line provides connectivity to the Sydney metropolitan rail network with interchanges at the City, Redfern, Sydney and Wolli Creek.



### 3.5 Pedestrian

Pedestrians are provided with the following access and mobility infrastructure in the immediate vicinity of the subject site:

- Footpaths are provided along both sides of Bronte Road, Birrell Street and Blenheim Street;
- Signalised pedestrian crossings are provided over all approaches at the intersection of Bronte Road and Birrell Street; and
- A raised marked pedestrian crossing is provided over Birrell Street approximately 175m to the west of the site.

#### 4. APPROVED DEVELOPMENT

##### 4.1 Built Form

Development consent DA-621/2014 was granted for the demolition of the existing retail development and the construction of a boarding house development.

The boarding house was approved to contain 19 boarding rooms in addition to a manager's room.

The development was approved to be serviced by a single level of undercroft parking containing six passenger vehicle parking spaces, four bicycle parking spaces and four motorcycle parking spaces, connecting with Blenheim Street via a 3.8m wide combined ingress / egress driveway located approximately 8m to the south of Birrell Street.

## 5. SITE MANAGEMENT

### 5.1 Internal Site Construction Works

#### 5.1.1 Construction Vehicle Accommodation Options

The scale of the development is such that the construction works will encompass a significant majority of the site. Construction vehicles larger than passenger vehicles (but limited to 8.8m long Medium Rigid Vehicles) are therefore not able to be wholly accommodated on-site. Construction vehicles servicing the site during the construction phases of the development will therefore need to be accommodated within the adjoining public road.

A meeting was convened on-site with Council's Traffic Engineer Geoff Garnsey on Thursday the 2<sup>nd</sup> of March 2017 to discuss the possible options to accommodate construction vehicles servicing the site in a safe and efficient manner. It was agreed during this meeting that the limited site frontage to Bronte Road and proximity of that frontage to the signalised intersection control at Birrell Street, results in Bronte Street not being an appropriate location for the establishment of a Works Zone.

The following was accordingly considered to be the remaining possible options for the accommodation of construction vehicles:

- Option 1 - a Works Zone could be implemented along the southern Birrell Street kerb alignment immediately adjacent to the site within the existing sign posted No Parking zone between Bronte Road and Blenheim Street; and / or
- Option 2 - Construction vehicles could be accommodated within the eastern Blenheim Street footway, to the south of Birrell Street.

A further option was discussed involving the implementation of a Works Zone within eastern Blenheim Street kerb alignment to the south of the site within the existing sign posted 2 Hour zone, commencing approximately 20m to the south of Birrell Street. This option was however discounted at the site meeting due to potential interactions of the Works Zone with abutting residences on the eastern side of Blenheim Street and difficulties in transporting construction materials between the Works Zone and the site as the Zone is off-set from the site frontage.

It is proposed that both the Birrell Street Works Zone option and the Blenheim Street footway occupation option (Options 1 and 2 respectively) be assessed by Council and approval be sought for one or both.

The following sub-sections of this Plan of Management provide discussion and details with respect to Options 1 and 2.

#### 5.1.1.1 Option 1 – Birrell Street Works Zone

Option 1 involves the implementation of a Works Zone along the southern Birrell Street kerb alignment immediately adjacent to the site within the existing sign posted No Parking zone between Bronte Road and Blenheim Street.

Some concerns were expressed by My Garnsey at the site meeting with respect to Option 1 due to the proximity of the Works Zone to the traffic signals at Bronte Road and the ability of the westbound Birrell Street travel lane to accommodate the Works Zone in conjunction with a westbound through traffic. This Practice subsequently discussed the Works Zone within Birrell Street with the Roads & Maritime Services Network & Safety Officer (and Local Traffic Committee representative) Mohamed Tita. Correspondence was received following this discussion indicating that Roads & Maritime Services would not object to the implementation of a Works Zone within Birrell Street under the following conditions (copies of this correspondence is attached as **Appendix 1**):

- The works zone provide a 10m separation from Blenheim Street, which is to be sign posted as “No Stopping”; and
- The area to the east of the works zone on approach to the Bronte Road traffic signals be provided with “No Stopping” sign posting.

A sign posting plan has been prepared detailing the proposed signage within Birrell Street associated with Option 1, in accordance with the requirements of Roads & Maritime Services, a copy of which is contained within **Appendix 2**. This plan illustrates that a 9m long Works Zone can be installed within Birrell Street immediately adjacent to the subject site whilst still maintaining a 3.7m wide westbound travel lane. Such a travel lane width, in combination with the single lane nature of westbound traffic past the site is such that the Works Zone is considered to be capable of being accommodated within Birrell Street in a safe and efficient manner.

A crane located approximately central to the site will transport construction materials between the construction vehicles within the Works Zone and the site. B Class Hoarding will be required to be implemented within the southern Birrell Street footway immediately adjacent to the Zone to protect pedestrians associated with this loading and unloading of materials. The location and extent of the Hoarding is illustrated within the sign posting plan contained within **Appendix 2**.

An application to Council will be made in relation to the Works Zone and Hoarding including the payment of appropriate fees associated with Option 1.

#### 5.1.1.2 Option 2 – Blenheim Street Footway Occupation

Option 2 involves the accommodation of construction vehicles within the eastern Blenheim Street footway, to the south of Birrell Street. This option does not necessitate the implementation of a formal Works Zone but will involve the full closure of the eastern Blenheim Street footpath for the full frontage of the subject site.

A plan detailing the extent of required footway accommodation and closure has been prepared by this Practice, a copy of which is contained within **Appendix 3**.

Option 2 requires pedestrian movements along the eastern Blenheim Street footpath to be redirected to the western Blenheim Street footpath. Appropriately qualified traffic controllers are to be provided to assist the movement of pedestrians across Blenheim Street where required. A Traffic Control Plan illustrating the traffic and pedestrian management measures to be implemented during the periods of the occupation of the eastern Blenheim Street footpath has been prepared by this Practice, a copy of which is attached as **Appendix 4**.

#### 5.2 External Public Domain Works

The development involves the removal of and reconstruction of the vehicular access driveway connecting with Blenheim Street adjacent to the western boundary, within the public road reserve:

The traffic and pedestrian control measures required to be implemented within Blenheim Street associated with the driveway reconstruction are the same as that proposed to be implemented and previously described for the construction vehicle accommodation. In this regard, the driveway works will necessitate the full closure of the eastern Blenheim Street footpath and the diversion of pedestrian movements to the western Blenheim Street footpath in the vicinity of the site. The traffic control plan contained within **Appendix 4** covers this scenario.

Any other unforeseen works which may necessitate the temporary occupation of the public roadway associated with the construction works will require separate formal approval from Council, via the preparation of a Traffic Control Plan in accordance with the Roads & Maritime Services *Traffic Control at Work Sites* manual.

#### 5.3 Parking Control

Prior to the construction of the undercroft parking area, all construction employee / tradesperson passenger vehicle parking is to be accommodated off-site. The surrounding on-street parking areas are not appropriate as they are primarily governed by 2 hour (or less) time restrictions.

All day parking is therefore to be sought within the various public off-street parking stations within and immediately surrounding the Bondi Junction town centre, until such time as the construction of the undercroft parking area is complete and accessible.



Notwithstanding this, construction workers / tradespersons are to be encouraged to do either of the following when travelling to the site in order to minimise the extent of parking demand:

- Utilise public transport to the site (the site is well serviced by previously presented bus and rail services operating within the subject vicinity); and / or
- Car pool with other construction workers.

The above transport options will form part of the conditions of commissioning when engaging the relevant site workers and as such form part of any site induction process.

#### 5.4 Hours of Operation

Construction works are to occur between 7:00am and 5:00pm Monday to Friday and between 8:00am and 3:00pm Saturday in accordance with Condition of Consent No. 47 of the consent.

No work is to occur on the Saturday of a long weekend, Sundays or public holidays.

## 6. SAFE INGRESS / EGRESS OF CONSTRUCTION TRAFFIC

### 6.1 Construction Vehicle Classification

Vehicles up to and including MRVs will service the subject site during all demolition and construction stages, under all construction vehicle accommodation options.

### 6.2 Construction Vehicle Access / Egress Management

It has previously been presented that construction vehicles larger than passenger vehicles will not access the subject site, rather all construction vehicles up to and including MRVs will service the site via a Works Zone within Birrell Street and / or the occupation of the eastern Blenheim Street footpath immediately adjacent to the subject site.

The following sub-sections provide a description of the construction vehicle access / egress management for the two construction vehicle accommodation options.

#### 6.2.1 Option 1 – Birrell Street Works Zone

Vehicles accessing the Birrell Street Works Zone will do so via a simple forward movement from the westbound Birrell Street Works Zone south-westbound Dibble Avenue travel lane. Similarly, construction vehicles will exit the Works Zone via a simple forward movement back to the westbound Birrell Street travel lane.

Swept path plans prepared by this Practice and contained within **Appendix 5** illustrate the simple forward ingress and egress movements of construction vehicles associated the Birrell Street Works Zone option.

All construction vehicle Works Zone access and egress movements are to be strictly controlled by appropriately qualified traffic controllers. Traffic controllers are not to stop traffic on the public street to allow trucks to enter or leave the Works Zone. They must wait until a suitable gap in traffic flows allows them to assist construction vehicles to enter or exit the Works Zone. The Roads Act does not give any special treatment for trucks leaving a Works Zone – the vehicles already on the road have right of way.

No queuing / marshalling of construction vehicles is to occur in any public road.

#### 6.2.2 Option 2 – Blenheim Street Footway Occupation

Vehicles accessing the Blenheim Street footway will do so via a simple forward right turn movement from the eastbound Birrell Street right turn lane into Blenheim Street and thence directly onto the footway, via a temporary construction gutter crossing. Similarly, construction vehicles will exit the footway via a simple forward movement back to the Blenheim Street southbound travel lane via a combination of a temporary construction gutter crossing fronting the

site and an existing driveway servicing a medium density residential development to the south of the site.

Swept path plans prepared by this Practice and contained within **Appendix 6** illustrate the forward ingress and egress movements of construction vehicles associated the Blenheim Street footway occupation option.

Similarly to that stated with respect to the Birrell Street Works Zone option, all construction area access and egress movements are to be strictly controlled by appropriately qualified traffic controllers. Traffic controllers are not to stop traffic on the public street to allow trucks to enter or leave the construction area. They must wait until a suitable gap in traffic flows allows them to assist construction vehicles to enter or exit the construction area. The Roads Act does not give any special treatment for trucks leaving a construction area – the vehicles already on the road have right of way.

No queuing / marshalling of construction vehicles is to occur in any public road.

#### 6.2.3 Passenger Vehicle Site Access during Construction

Further to the large construction vehicles described within Sections 6.2.1 and 6.2.3, construction employee / tradesperson passenger vehicles are to be accommodated within the undercroft parking area when constructed. Access / egress associated with this passenger vehicle parking function is to occur via a temporary construction gutter crossing connecting with Blenheim Street. These movements are to occur in a forward direction at all times.

### 6.3 Construction Vehicle Transport Routes

Construction vehicle transport routes will vary depending on the construction vehicle accommodation option utilised. The following sub-sections provide a description of the construction vehicle transit routes for the two construction vehicle accommodation options.

#### 6.3.1 Option 1 – Birrell Street Works Zone

##### Inbound Route

Eastern Distributor, South Dowling Street, right turn to Dacey Avenue, Alison Road, left turn to Avoca Street, right turn to Frenchmans Road, left turn to Carrington Road, left turn to Birrell Street and thence a forward movement to the Works Zone.

##### Outbound Route

Simple forward movement from the Works Zone to Birrell Street, right turn to York Street, left turn to Oxford Street, left turn to South Dowling Street and thence the Eastern Distributor.

### 6.3.2 Option 2 – Blenheim Street Footway Occupation

#### Inbound Route

Eastern Distributor, South Dowling Street, right turn to Oxford Street, right turn to York Road, left turn to Birrell Street, right turn to Blenheim Street and thence the eastern footway construction area.

#### Outbound Route

Simple forward movement from the construction area to the southbound Blenheim travel lane, right turn to Cuthbert Street, right turn to Bourke Street, left turn to Birrell Street, right turn to York Street, left turn to Oxford Street, left turn to South Dowling Street and thence the Eastern Distributor.

### 6.3.3 Discussion on Routes

The proximity of the site to the adjoining State Road network via Carrington Road to the east and Oxford Street to the north-west is such that construction vehicles are able to access and depart the site creating little disturbance to surrounding local road traffic flow or abutting residential amenity. The pavement width of the transit routes are such that heavy vehicle manoeuvring is able to occur without any unreasonable encroachment on opposing travel lanes, kerbs and / or parking lanes. Further, the positive intersection control at major junctions along the transit routes are such that construction vehicles are projected to be capable of manoeuvring between the State and local road networks in a safe and efficient manner.

Whilst it is noted that construction vehicles are required to manoeuvre along Blenheim Street, Cuthbert Street and Bourke Street under the Blenheim Street footway occupation option, these routes are capable of accommodating vehicles up to and including Medium Rigid Vehicles as they currently accommodate refuse collection vehicles.

## 7. PROJECT STAGING - TRAFFIC GENERATION

### 7.1 Construction Staging

Consultation with the developer in conjunction with the experience of this Practice with other developments of similar scale has indicated that the works are anticipated to be predominantly completed as follows within the five stages, as follows

- Stage 1 – Demolition – 3 weeks;
- Stage 2 – Structure – 18 weeks;
- Stage 3 – Roof, cladding and façade finishes – 10 weeks;
- Stage 4 – Internal services and finishes – 10 weeks; and
- Stage 5 – Public Domain – 3 week.

### 7.2 Construction Traffic Generation

The following provides a summary of the likely construction vehicle traffic generation of the proposed works during these stages:

- **Stage 1 – Demolition**  
Time period – 3 weeks  
Total traffic generation – 60 truck loads  
Average weekly traffic generation – 20 truck loads  
Maximum hourly traffic generation – 1 truck load
- **Stage 2 – Structure**  
Time period – 18 weeks  
Total traffic generation – 450 truck loads  
Average weekly traffic generation – 25 truck loads  
Maximum hourly traffic generation – 1 truck load
- **Stage 3 – Roof, cladding and façade finishes**  
Time period – 10 weeks  
Total traffic generation – 200 truck loads  
Average weekly traffic generation – 20 truck loads  
Maximum hourly traffic generation – 1 truck load
- **Stage 4 – Internal services and finishes**  
Time period – 10 weeks  
Total traffic generation – 200 truck loads  
Average weekly traffic generation – 20 truck loads  
Maximum hourly traffic generation – 1 truck load



- **Stage 5 – Public Domain**

Time period – 3 week

Total traffic generation – 30 truck loads

Average weekly traffic generation – 10 truck loads

Maximum hourly traffic generation – 1 truck load

It is acknowledged that during different stages of the construction that varying types of vehicles will access the site from MRVs through to employee passenger vehicles. Further, it is anticipated that there will occasionally be an overlap of one or more of the above phases with a combination of the above phase traffic generation travelling to and from the site during any one particular day or period. The above stage traffic generation summary indicates that the sites are projected to generate an average of up to one heavy vehicles per hour; that is one ingress movement and one egress movement.

During periods of more heavy construction vehicle generation (i.e. concrete pours) when it could be expected that the works could generate up to three trucks in any given hour, drivers will be instructed by radio when to arrive at the site to ensure that there is no vehicle queuing or parking within the surrounding local road network. This is to be strictly adhered.

## 8. IMPACTS ASSESSMENT

### 8.1 Traffic Impact

Recent observations have indicated that traffic demands within the surrounding regional and state road network are considerable during peak periods. The traffic signal control at the major intersections however provide motorists wishing to access / egress the precinct with safe and efficient connectivity.

Traffic demands within the local road network immediately servicing the subject site are significantly reduced, providing motorists with satisfactory level of service and allowing vehicles to access and vacate the site and immediate precinct with a good level of safety and efficiency.

Further, qualified traffic controllers are to assist all site and construction vehicle occupation area/s ingress and egress movements thereby minimising any vehicular conflicts with construction vehicles.

The previously presented traffic generation summary indicates that the site construction works are projected to generally generate up to an average of one heavy vehicle per hour, increasing to up to three heavy vehicles during concentrated site activity periods such as concrete pours. Such an additional traffic load is not anticipated to have an unreasonable impact on the safety and efficiency of the adjoining road network.

### 8.2 Impacts on Pedestrians

Pedestrian demands along Birrell Street are notable however pedestrian movements adjacent to the site along these roads are to occur in an unimpeded fashion during all periods of construction under both construction vehicle accommodation options. This is to be achieved through the provision of B Class Hoarding associated with the Birrell Street Works Zone.

Pedestrian demands along Blenheim Street are significantly reduced. Whilst the subject works involve the occupation of the eastern Blenheim Street footpath adjoining the site, pedestrian accessibility and mobility is to be maintained at all times through the diversion of pedestrian movements to the existing western Blenheim Street footpath and appropriate ancillary measures illustrated within the traffic control plan attached as **Appendix 4**.

Appropriately qualified traffic controllers are to assist these roadway crossing movements where necessary. The closure of the eastern Blenheim Street footpath is therefore most unlikely to result in any unreasonable impacts on the amenity of pedestrian movements.

Unimpeded pedestrian access to adjoining developments will be maintained at all times. Site boundary fencing will protect pedestrians from dust and debris.

No unreasonable impacts on the safety or mobility of pedestrians are therefore anticipated during the construction works associated with the subject development.

## 9. SUMMARY AND CERTIFICATION STATEMENT

This Construction Vehicle and Pedestrian Plan of Management provides a description of the traffic and pedestrian management measures to be incorporated during the construction of an approved boarding house development at 125 Bronte Road, Queens Park. Having regard to this Plan, the following summary is provided:

- The construction works will necessitate the establishment of one or both of the following off-site construction vehicle occupation options:
  - Option 1 - a Works Zone could be implemented along the southern Birrell Street kerb alignment immediately adjacent to the site within the existing sign posted No Parking zone between Bronte Road and Blenheim Street; and / or
  - Option 2 - Construction vehicles could be accommodated within the eastern Blenheim Street footway, to the south of Birrell Street.
- Plans detailing the location and extent of the abovementioned off-site construction vehicle occupation measures are contained within **Appendices 2 and 3**;
- The implementation of a Works Zone within Birrell Street will also require the implementation of B Class Hoarding within the southern Birrell Street footpath to protect pedestrians and maintain accessibility adjoining the site;
- Heavy construction vehicles, up to and including MRVs, are to access the Birrell Street Works Zone and or the Blenheim Street footpath construction area via simple forward movements, under qualified traffic control supervision;
- The abovementioned qualified traffic controllers are also to supervise the required diversion of pedestrians onto the western Blenheim Street footpath associated with the Blenheim Street footpath occupation option;
- A Traffic Control Plan detailing the traffic and pedestrian management measures to be implemented during the occupation of the Blenheim Street footpath is contained within **Appendix 4**;
- Any other unforeseen works which may necessitate the temporary occupation of the public roadway associated with the construction works will require separate formal approval from Council, via the preparation of a Traffic Control Plan in accordance with the Roads & Maritime Services *Traffic Control at Work Sites* manual;
- Passenger vehicle parking associated with the construction works is largely to occur within nearby public off-street parking stations, until such time as the undercroft car parking level is available, following which some parking is to occur on-site;

- Construction transit routes are to be in accordance with that contained within Section 6.3 of this Plan, whereby routes are limited to State Roads as much as is practicable;
- The construction works are to occur over approximately 44 weeks;
- The peak traffic generation of the construction works associated with the development is projected to be three heavy vehicles per hour; and
- The surrounding road network is considered to be suitably capable of accommodating the construction traffic generation, incorporating the traffic management measures proposed.

I certify that this traffic and pedestrian management measures proposed to be implemented by this Construction Vehicle and Pedestrian Plan of Management are appropriate to satisfy the local traffic and pedestrian demands associated with the construction of the subject boarding house development.



Morgan Stanbury  
**Transport Planner & Road Safety Auditor**

9 March 2017

## **APPENDIX 1**





Morgan Stanbury &lt;morgan@stanburytraffic.com.au&gt;

**125 Bronte Road, Queens Park - Works Zone in Birrell Street**

23 messages

TITA Mohamed <Mohamed.TITA@rms.nsw.gov.au>  
 To: "morgan@stanburytraffic.com.au" <morgan@stanburytraffic.com.au>  
 Cc: GeoffG <Geoff.Garnsey@waverley.nsw.gov.au>

Thu, Mar 2, 2017 at 3:11 PM

Good Afternoon,

As mentioned in our telephone conversation. Roads and Maritime Services will not have any objections to the works zone proposed on Birrell Street, Queens Park.

As requested, please send me a copy of the propose location of the works zone in Birrell Street, Queens Park and RMS will work with Council and yourself to achieve a suitable solution.

Kind Regards,

Mohamed Tita  
 Network & Safety Officer  
 Traffic Management | Journey Management  
 T: 02 8849 2821 M: 0476 833 251  
[www.rms.nsw.gov.au](http://www.rms.nsw.gov.au)  
 Every journey matters

Roads and Maritime Services  
 Level 5, 27 Argyle Street Parramatta NSW 2150



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Morgan Stanbury <morgan@stanburytraffic.com.au>  
 To: TITA Mohamed <Mohamed.TITA@rms.nsw.gov.au>  
 Cc: GeoffG <Geoff.Garnsey@waverley.nsw.gov.au>

Fri, Mar 3, 2017 at 7:36 AM

Hi Mohamed,

Thanks for your time on the phone yesterday. As discussed, please find attached an aerial sketch with the proposed Works Zone location. The Works Zone is proposed to encompass the western-most 9m of the existing 14m long 'No Parking' zone on the southern side of Birrell Street between Blenheim Street and Bronte Road.

The Birrell Street westbound travel lane is 6.3m between kerb and BB line. A 2.6m wide Works Zone will therefore leave a 3.7m westbound travel lane for through traffic.

Please don't hesitate to call to discuss as required.

Kind Regards

Morgan Stanbury  
 Director

9 Tordeth Road  
 Glebe NSW 2037

p 02 9680 6394 m 0410 561 848  
 e [morgan@stanburytraffic.com.au](mailto:morgan@stanburytraffic.com.au)  
 w [www.stanburytraffic.com.au](http://www.stanburytraffic.com.au)



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Works Zone.pdf  
 221K

TITA Mohamed <Mohamed.TITA@rms.nsw.gov.au>  
To: Morgan Stanbury <morgan@stanburytraffic.com.au>  
Cc: GeoffG <Geoff.Garnsey@waverley.nsw.gov.au>

Fri, Mar 3, 2017 at 8:48 AM

Hi Morgan,

Thank you for the information you have provided.

Council does have the final say on this matter as Birrell St is under the care of Council and may have additional requirements for the installation of the Works Zone. Roads and Maritime only can make comments on the close proximity from the signals.

In saying that, with the proposed location of the works zone being close to the traffic signals at the intersections of Bronte Rd and Birrell St, Roads and Maritime Services would no objections with the existing "No Parking" being used as a works zone subject to:

- The works zone be installed 10m from Blenheim St. This 10m be made "No Stopping"
- That the remaining area after the proposed works zone towards the traffic lights be made "No Stopping"

This will still allow suitable traffic flow through the intersection.

Please don't hesitate giving me a call if you wish to further discuss any of my comments.

Regards,

Mohamed Tita  
Network & Safety Officer  
T: 02 8849 2821 M: 0476 833 251  
www.rms.nsw.gov.au

From: Morgan Stanbury [mailto:morgan@stanburytraffic.com.au]  
Sent: Friday, 3 March 2017 7:37 AM  
To: TITA Mohamed  
Cc: GeoffG  
Subject: Re: 125 Bronte Road, Queens Park - Works Zone in Birrell Street

[Quoted text hidden]  
[Quoted text hidden]

Morgan Stanbury <morgan@stanburytraffic.com.au>  
To: TITA Mohamed <Mohamed.TITA@rms.nsw.gov.au>  
Cc: GeoffG <Geoff.Garnsey@waverley.nsw.gov.au>

Fri, Mar 3, 2017 at 8:51 AM

Hi Mohamed,

Thanks for your comments. We'll put together a Construction Traffic Management Plan for formal consideration of the Traffic Committee.

Kind Regards

Morgan Stanbury  
Director

9 Toxteth Road  
Glebe NSW 2037

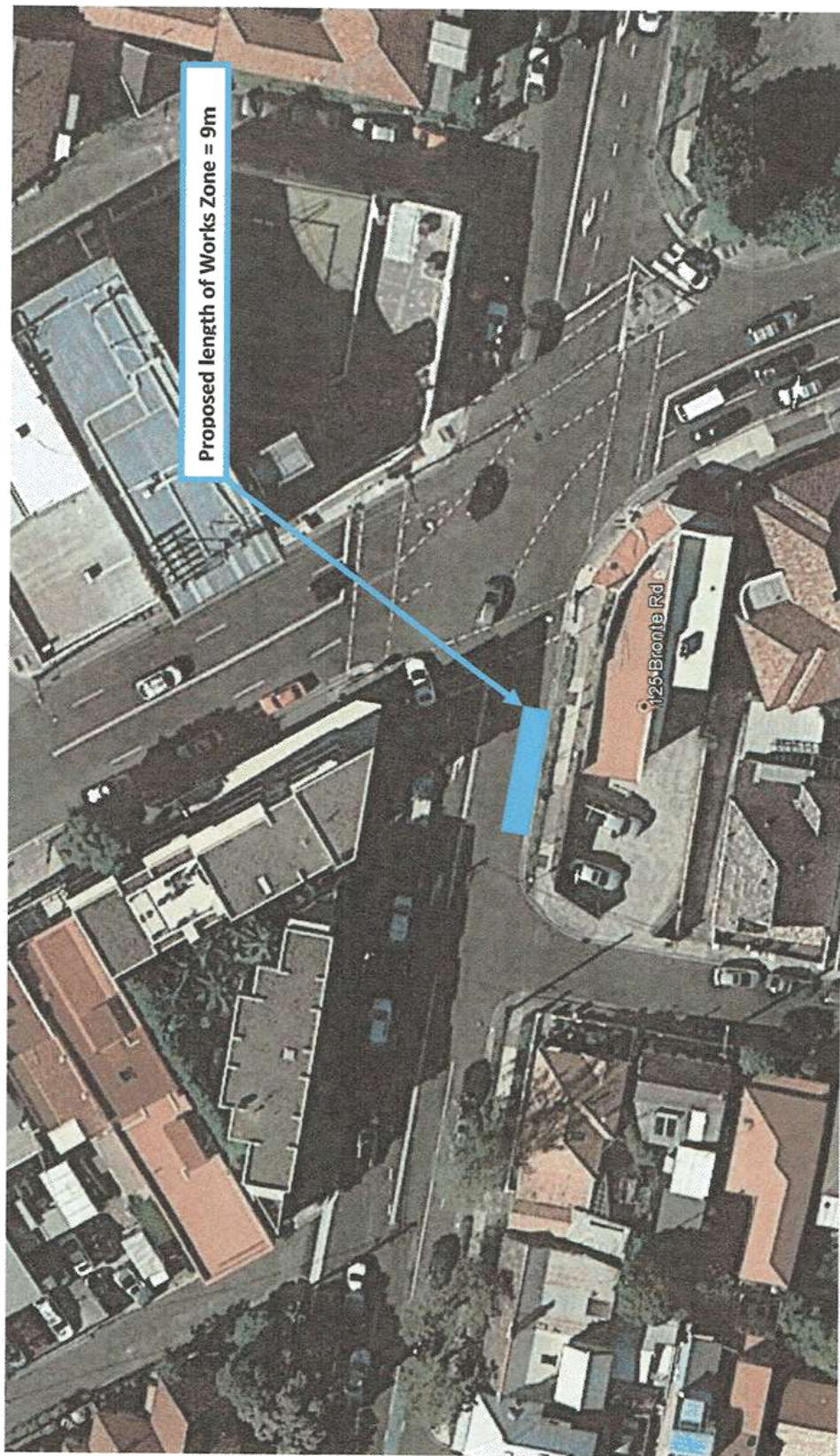
p 02 9660 6394 m 0410 561 848  
e morgan@stanburytraffic.com.au  
w www.stanburytraffic.com.au



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LOCATION AND EXTENT OF WORKS ZONE  
BIRRELL STREET, BETWEEN BRONTE ROAD & BLENHEIM STREET



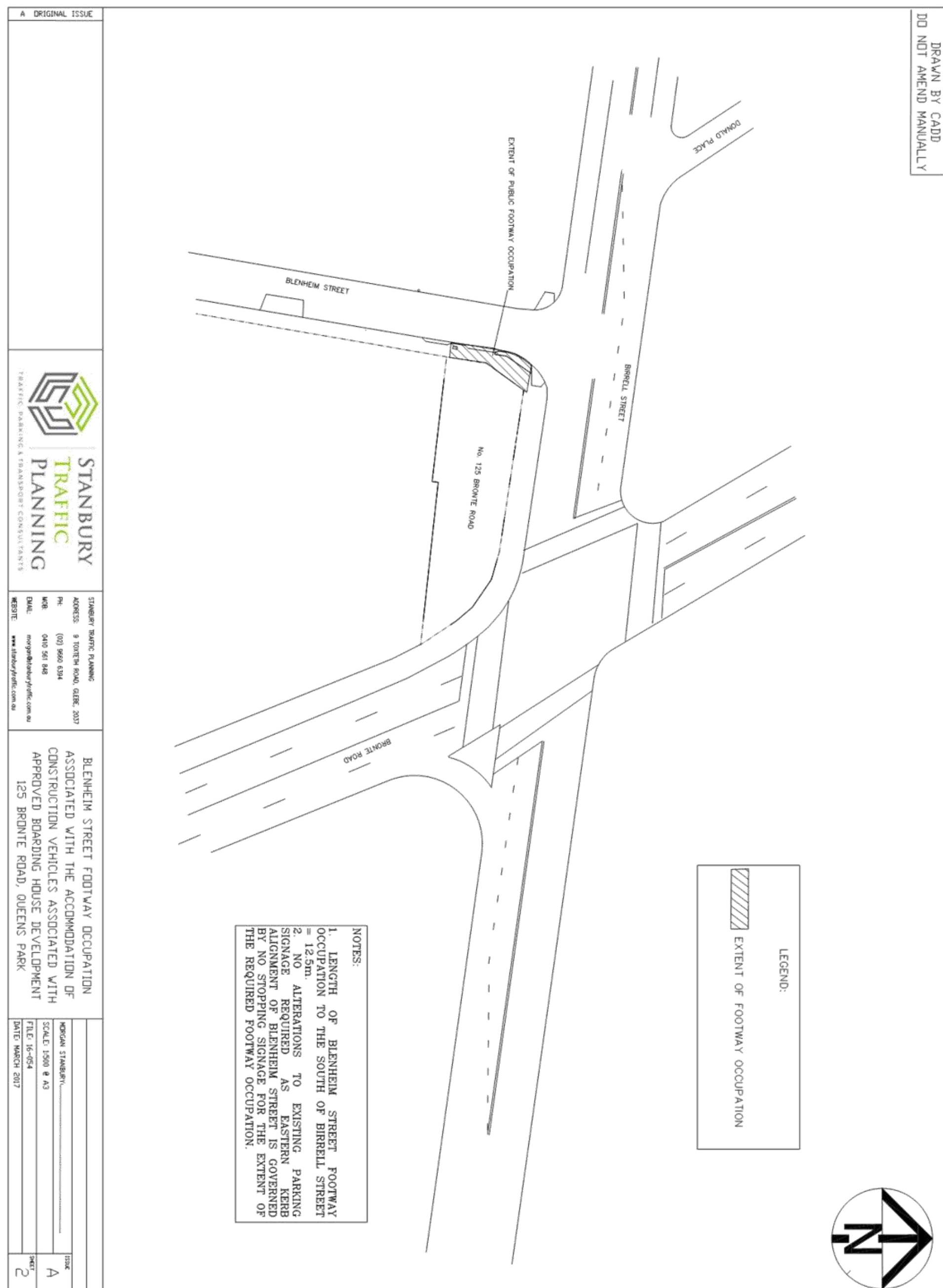
## **APPENDIX 2**



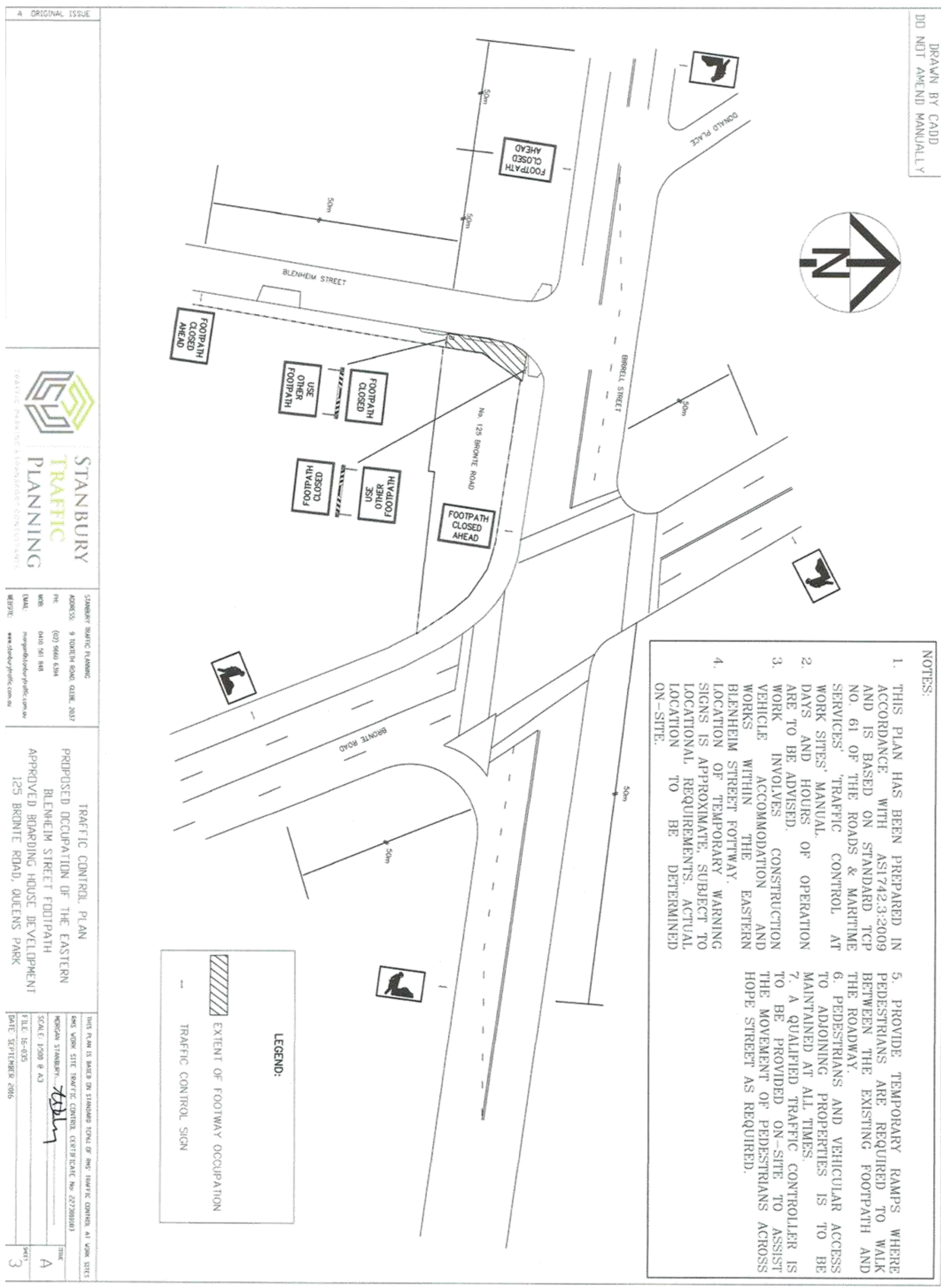




## **APPENDIX 3**



## **APPENDIX 4**



## **APPENDIX 5**



**NOTES:**

1. THE SWEEP PATH PROVIDED IN THIS PLAN HAS BEEN GENERATED UTILISING AUTOTURN AND MEDIUM RIGID VEHICLE TURNING SPECIFICATIONS PROVIDED BY AUSTRROADS.

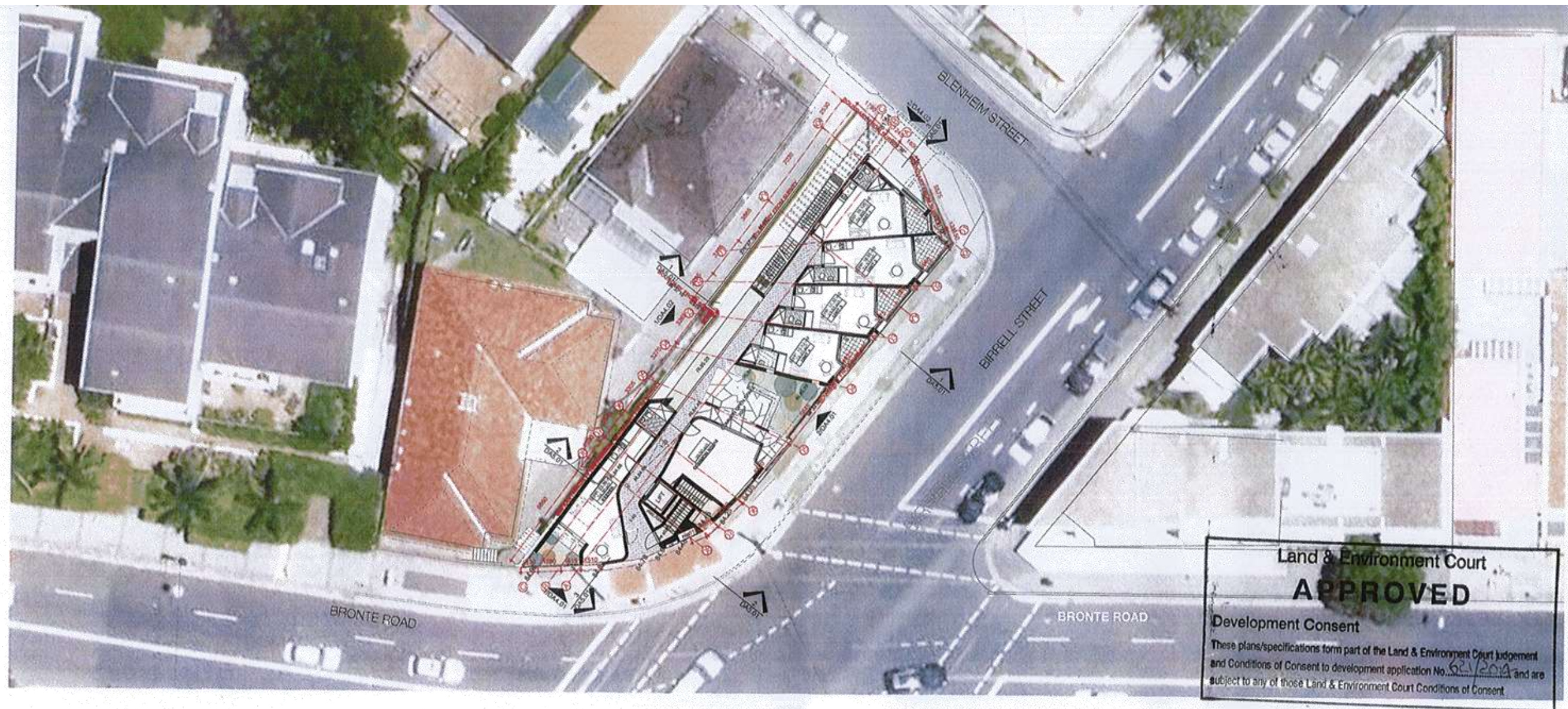


## **APPENDIX 6**



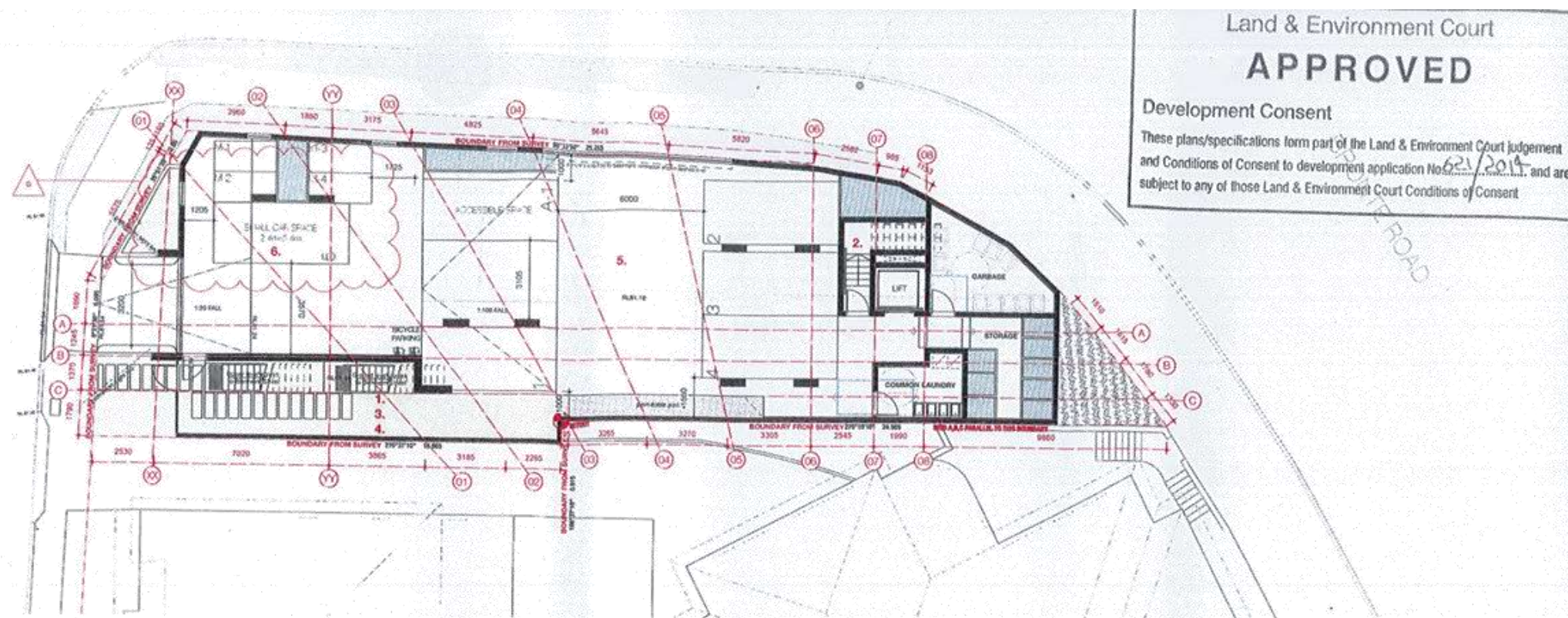








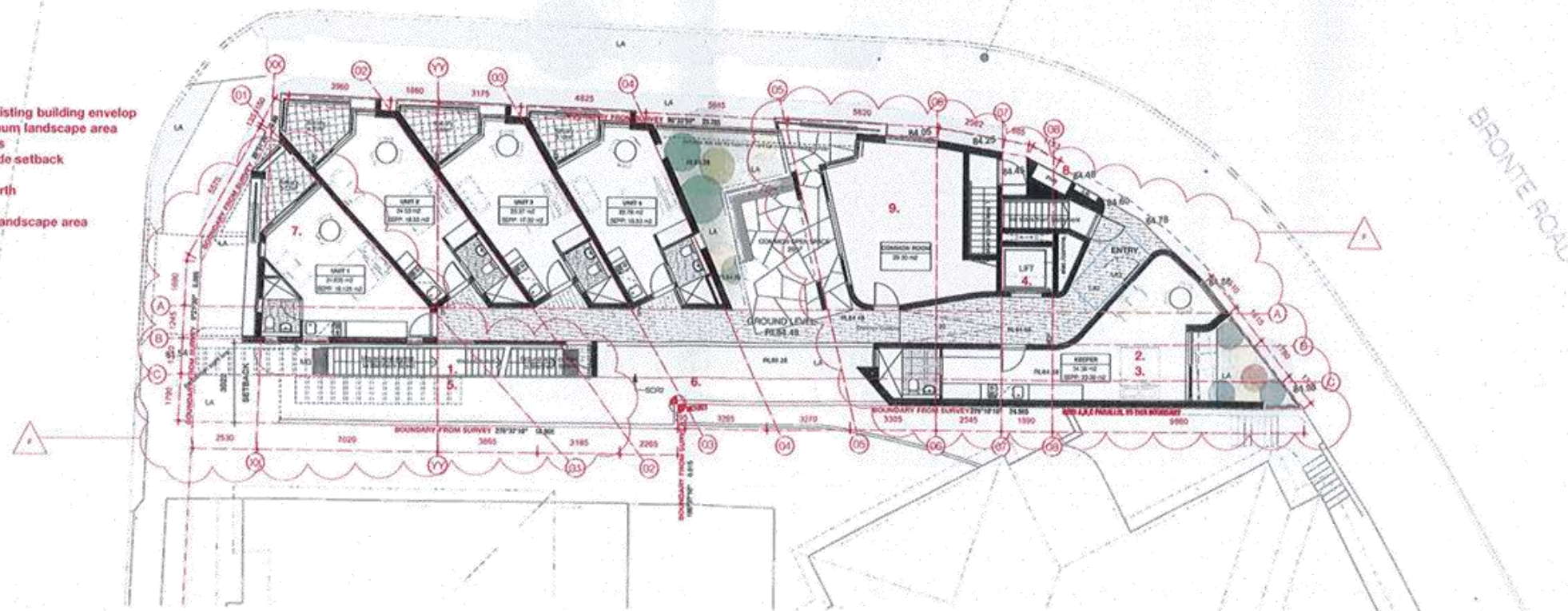
1. 3m southern side setback
2. Relocation of lift and eastern fire-isolated stairs
3. Consolidated western staircases to improve side setback
4. Increased landscape deep soil area
5. reconfigured basement layout
6. small car space



1 Basement Plan  
Scale: 1:100

#### AMENDMENTS

1. 3m southern side setback
2. South-eastern bulk reduced; compliant with existing building envelop
3. keeper unit at ground level with required minimum landscape area
4. Relocation of lift and eastern fire-isolated stairs
5. Consolidated western staircases to improve side setback
6. planted screenwall along corridor removed
7. western unit reconfigured; balcony oriented north
8. FHB + LB and services consolidated
9. reduced common room; increased communal landscape area



2 Ground Level Plan  
Scale: 1:100

NOT FOR CONSTRUCTION		KEY		REV		DATE		BY		AMENDMENTS		MATERIALS LEGEND:		LANDSCAPE ARCHITECTURE		HYDRAULIC/STORMWATER		ARCHITECT		PROJEC		OWNER		ENGINEER		CHECKED		DATE		REVISION		SCALE		SHEET			
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1084		1085		1086		1087		1088																													



Land &amp; Environment Court

**APPROVED**

Development Consent

These plans/specifications form part of the Land & Environment Court judgement and Conditions of Consent to development application No. 63/2014 and are subject to any of those Land & Environment Court Conditions of Consent

- 3m southern side setback
- South-eastern bulk reduced; compliant with existing building envelop
- Relocation of lift and eastern fire-isolated stairs
- Consolidated western staircases to improve side setback
- planted screenwall along corridor removed
- western unit reconfigured; balcony oriented north
- higher ceiling to unit 11 to improve amenity

1 Level 1 Plan  
Scale: 1:100

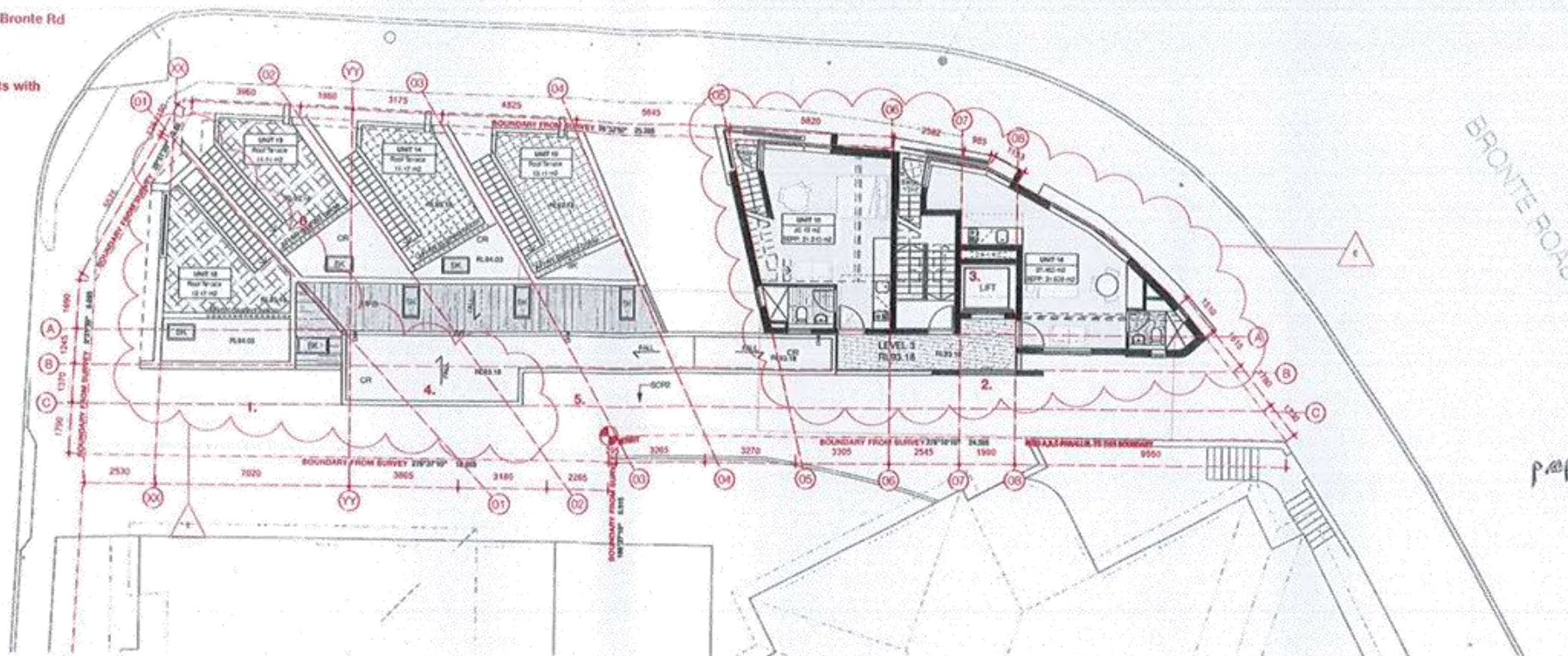
**AMENDMENTS**

- 3m southern side setback
- South-eastern bulk reduced; compliant with existing building envelop
- observed glass block wall at lift landing to improve privacy to 127 Bronte Rd
- Relocation of lift and eastern fire-isolated stairs
- Consolidated western staircases to improve side setback
- planted screenwall along corridor removed
- western unit reconfigured; balcony oriented north
- high level windows shown on level 2, improving amenity to units with roof terraces

2 Level 2 Plan  
Scale: 1:100

<b>NOT FOR CONSTRUCTION</b>		<b>KEY</b>		<b>REV</b>		<b>DATE</b>		<b>AMENDMENTS</b>		<b>MATERIALS LEGEND</b>		<b>LANDSCAPE ARCHITECTURE</b>		<b>HYDRAULIC/STORMWATER</b>		<b>ARCHITECTS</b>		<b>PROJECT</b>		<b>DRAWING</b>		<b>LEVEL 1 + LEVEL 2 PLAN</b>	
				1		15/07/24		Initial Design		AGG Aluminium framed double glass window		20 Rain Pipe		20 Rain Pipe		hill thalis		125 Bronte Road		DRAWN		14.47	
				2		15/07/24		Revised Design		AGG Aluminium framed double glass window		20 Rain Pipe		20 Rain Pipe		hill thalis		125 Bronte Road		CHECKED		14.47	
				3		15/07/24		Revised Design		AGG Aluminium framed double glass window		20 Rain Pipe		20 Rain Pipe		hill thalis		125 Bronte Road		DATE		14.47	
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				6		15/07/24		Revised Design		AGG Aluminium framed double glass window		20 Rain Pipe		20 Rain Pipe		hill thalis		125 Bronte Road		DATE		14.47	
				7		15/07/24		Revised Design		AGG Aluminium framed double glass window		20 Rain Pipe		20 Rain Pipe		hill thalis		125 Bronte Road		DATE		14.47	
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				11		15/07/24		Revised Design		AGG Aluminium framed double glass window		20 Rain Pipe		20 Rain Pipe		hill thalis		125 Bronte Road		DATE		14.47	
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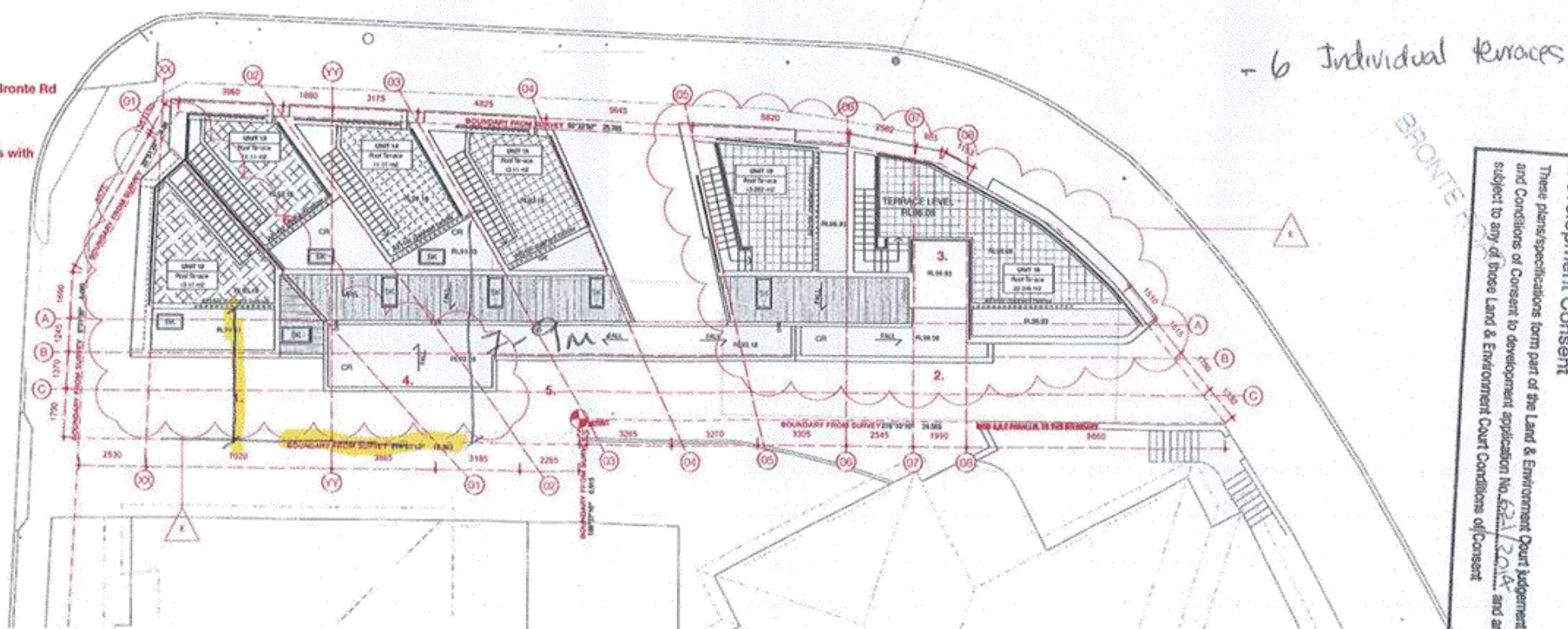




Parking  
 $19 \times 0.2$   
 $= 3.8$   
 $+ 1$  worker  
 proposed = 5-6

1 Level 3 Plan  
Scale: 1:100

- 3m southern side setback
- obscured glass block wall at lift landing to improve privacy to 127 Bronte Rd
- Relocation of lift and eastern fire-isolated stairs
- Consolidated western staircases to improve side setback
- planted screenwall along corridor removed
- high level windows shown on level 2 + 3, improving amenity to units with roof terraces



- 6 Individual terraces

Laid & Environment Court  
**APPROVED**  
Development Consent

These plans/specifications form part of the Land & Environment Court Judgement and Conditions of Consent to development application No. 621/2014 and are subject to any of those Land & Environment Court Conditions of Consent

2 Roof Plan  
Scale: 1:100

### NOT FOR CONSTRUCTION

• Use Imperial dimensions only.  
 • Do not scale.  
 • Comply with local and Authority requirements.  
 • Comply with the Building Code of Australia.  
 • Comply with all relevant Australian Standards.  
 • Except as indicated, all dimensions are given in Imperial units. All dimensions are given in Imperial units. All dimensions are given in Imperial units.

KEY	SYMBOL	REMARKS
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2	2	2
3	3	3
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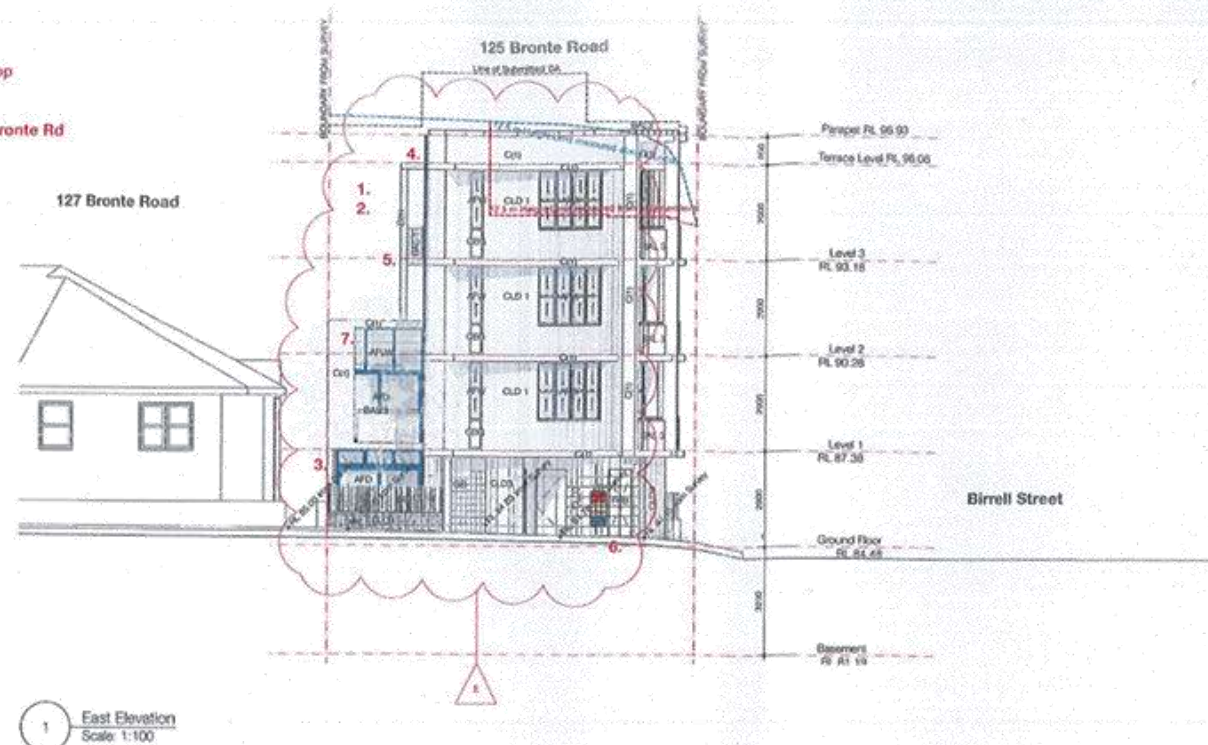
### MATERIALS LEGEND

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16	16	16
17	17	17
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19	19	19
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21	21	21
22	22	22
23	23	23



## AMENDMENTS

1. 3m southern side setback
2. South-eastern bulk reduced; compliant with existing building envelop
3. keeper unit at ground level with required minimum landscape area
4. Relocation of lift and eastern fire-isolated stairs
5. obscured glass block wall at lift landing to improve privacy to 127 Bronte Rd
6. FHB + LB and services consolidated
7. higher ceiling to unit 11 to improve amenity



Land & Environment Court  
**APPROVED**

## Development Consent

These plans/specifications form part of the Land & Environment Court judgement and Conditions of Consent to development application No. 63/2015, and are subject to any of those Land & Environment Court Conditions of Consent

## AMENDMENTS

1. keeper unit at ground level with required minimum landscape area
2. Relocation of lift and eastern fire-isolated stairs
3. FHB + LB and services consolidated
4. reduced common room; increased communal landscape area



NOT FOR CONSTRUCTION		KEY	REV	DATE	BY	AMENDMENTS	MATERIALS LEGEND	CLD 1	CLD 2	CLD 3	CLD 4	CLD 5	CLD 6	CLD 7	CLD 8	CLD 9	CLD 10	CLD 11	CLD 12	CLD 13	CLD 14	CLD 15	CLD 16	CLD 17	CLD 18	CLD 19	CLD 20	CLD 21	CLD 22	CLD 23	CLD 24	CLD 25	CLD 26	CLD 27	CLD 28	CLD 29	CLD 30	CLD 31	CLD 32	CLD 33	CLD 34	CLD 35	CLD 36	CLD 37	CLD 38	CLD 39	CLD 40	CLD 41	CLD 42	CLD 43	CLD 44	CLD 45	CLD 46	CLD 47	CLD 48	CLD 49	CLD 50	CLD 51	CLD 52	CLD 53	CLD 54	CLD 55	CLD 56	CLD 57	CLD 58	CLD 59	CLD 60	CLD 61	CLD 62	CLD 63	CLD 64	CLD 65	CLD 66	CLD 67	CLD 68	CLD 69	CLD 70	CLD 71	CLD 72	CLD 73	CLD 74	CLD 75	CLD 76	CLD 77	CLD 78	CLD 79	CLD 80	CLD 81	CLD 82	CLD 83	CLD 84	CLD 85	CLD 86	CLD 87	CLD 88	CLD 89	CLD 90	CLD 91	CLD 92	CLD 93	CLD 94	CLD 95	CLD 96	CLD 97	CLD 98	CLD 99	CLD 100	CLD 101	CLD 102	CLD 103	CLD 104	CLD 105	CLD 106	CLD 107	CLD 108	CLD 109	CLD 110	CLD 111	CLD 112	CLD 113	CLD 114	CLD 115	CLD 116	CLD 117	CLD 118	CLD 119	CLD 120	CLD 121	CLD 122	CLD 123	CLD 124	CLD 125	CLD 126	CLD 127	CLD 128	CLD 129	CLD 130	CLD 131	CLD 132	CLD 133	CLD 134	CLD 135	CLD 136	CLD 137	CLD 138	CLD 139	CLD 140	CLD 141	CLD 142	CLD 143	CLD 144	CLD 145	CLD 146	CLD 147	CLD 148	CLD 149	CLD 150	CLD 151	CLD 152	CLD 153	CLD 154	CLD 155	CLD 156	CLD 157	CLD 158	CLD 159	CLD 160	CLD 161	CLD 162	CLD 163	CLD 164	CLD 165	CLD 166	CLD 167	CLD 168	CLD 169	CLD 170	CLD 171	CLD 172	CLD 173	CLD 174	CLD 175	CLD 176	CLD 177	CLD 178	CLD 179	CLD 180	CLD 181	CLD 182	CLD 183	CLD 184	CLD 185	CLD 186	CLD 187	CLD 188	CLD 189	CLD 190	CLD 191	CLD 192	CLD 193	CLD 194	CLD 195	CLD 196	CLD 197	CLD 198	CLD 199	CLD 200	CLD 201	CLD 202	CLD 203	CLD 204	CLD 205	CLD 206	CLD 207	CLD 208	CLD 209	CLD 210	CLD 211	CLD 212	CLD 213	CLD 214	CLD 215	CLD 216	CLD 217	CLD 218	CLD 219	CLD 220	CLD 221	CLD 222	CLD 223	CLD 224	CLD 225	CLD 226	CLD 227	CLD 228	CLD 229	CLD 230	CLD 231	CLD 232	CLD 233	CLD 234	CLD 235	CLD 236	CLD 237	CLD 238	CLD 239	CLD 240	CLD 241	CLD 242	CLD 243	CLD 244	CLD 245	CLD 246	CLD 247	CLD 248	CLD 249	CLD 250	CLD 251	CLD 252	CLD 253	CLD 254	CLD 255	CLD 256	CLD 257	CLD 258	CLD 259	CLD 260	CLD 261	CLD 262	CLD 263	CLD 264	CLD 265	CLD 266	CLD 267	CLD 268	CLD 269	CLD 270	CLD 271	CLD 272	CLD 273	CLD 274	CLD 275	CLD 276	CLD 277	CLD 278	CLD 279	CLD 280	CLD 281	CLD 282	CLD 283	CLD 284	CLD 285	CLD 286	CLD 287	CLD 288	CLD 289	CLD 290	CLD 291	CLD 292	CLD 293	CLD 294	CLD 295	CLD 296	CLD 297	CLD 298	CLD 299	CLD 300	CLD 301	CLD 302	CLD 303	CLD 304	CLD 305	CLD 306	CLD 307	CLD 308	CLD 309	CLD 310	CLD 311	CLD 312	CLD 313	CLD 314	CLD 315	CLD 316	CLD 317	CLD 318	CLD 319	CLD 320	CLD 321	CLD 322	CLD 323	CLD 324	CLD 325	CLD 326	CLD 327	CLD 328	CLD 329	CLD 330	CLD 331	CLD 332	CLD 333	CLD 334	CLD 335	CLD 336	CLD 337	CLD 338	CLD 339	CLD 340	CLD 341	CLD 342	CLD 343	CLD 344	CLD 345	CLD 346	CLD 347	CLD 348	CLD 349	CLD 350	CLD 351	CLD 352	CLD 353	CLD 354	CLD 355	CLD 356	CLD 357	CLD 358	CLD 359	CLD 360	CLD 361	CLD 362	CLD 363	CLD 364	CLD 365	CLD 366	CLD 367	CLD 368	CLD 369	CLD 370	CLD 371	CLD 372	CLD 373	CLD 374	CLD 375	CLD 376	CLD 377	CLD 378	CLD 379	CLD 380	CLD 381	CLD 382	CLD 383	CLD 384	CLD 385	CLD 386	CLD 387	CLD 388	CLD 389	CLD 390	CLD 391	CLD 392	CLD 393	CLD 394	CLD 395	CLD 396	CLD 397	CLD 398	CLD 399	CLD 400	CLD 401	CLD 402	CLD 403	CLD 404	CLD 405	CLD 406	CLD 407	CLD 408	CLD 409	CLD 410	CLD 411	CLD 412	CLD 413	CLD 414	CLD 415	CLD 416	CLD 417	CLD 418	CLD 419	CLD 420	CLD 421	CLD 422	CLD 423	CLD 424	CLD 425	CLD 426	CLD 427	CLD 428	CLD 429	CLD 430	CLD 431	CLD 432	CLD 433	CLD 434	CLD 435	CLD 436	CLD 437	CLD 438	CLD 439	CLD 440	CLD 441	CLD 442	CLD 443	CLD 444	CLD 445	CLD 446	CLD 447	CLD 448	CLD 449	CLD 450	CLD 451	CLD 452	CLD 453	CLD 454	CLD 455	CLD 456	CLD 457	CLD 458	CLD 459	CLD 460	CLD 461	CLD 462	CLD 463	CLD 464	CLD 465	CLD 466	CLD 467	CLD 468	CLD 469	CLD 470	CLD 471	CLD 472	CLD 473	CLD 474	CLD 475	CLD 476	CLD 477	CLD 478	CLD 479	CLD 480	CLD 481	CLD 482	CLD 483	CLD 484	CLD 485	CLD 486	CLD 487	CLD 488	CLD 489	CLD 490	CLD 491	CLD 492	CLD 493	CLD 494	CLD 495	CLD 496	CLD 497	CLD 498	CLD 499	CLD 500	CLD 501	CLD 502	CLD 503	CLD 504	CLD 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630	CLD 631	CLD 632	CLD 633	CLD 634	CLD 635	CLD 636	CLD 637	CLD 638	CLD 639	CLD 640	CLD 641	CLD 642	CLD 643	CLD 644	CLD 645	CLD 646	CLD 647	CLD 648	CLD 649	CLD 650	CLD 651	CLD 652	CLD 653	CLD 654	CLD 655	CLD 656	CLD 657	CLD 658	CLD 659	CLD 660	CLD 661	CLD 662	CLD 663	CLD 664	CLD 665	CLD 666	CLD 667	CLD 668	CLD 669	CLD 670	CLD 671	CLD 672	CLD 673	CLD 674	CLD 675	CLD 676	CLD 677	CLD 678	CLD 679	CLD 680	CLD 681	CLD 682	CLD 683	CLD 684	CLD 685	CLD 686	CLD 687	CLD 688	CLD 689	CLD 690	CLD 691	CLD 692	CLD 693	CLD 694	CLD 695	CLD 696	CLD 697	CLD 698	CLD 699	CLD 700	CLD 701	CLD 702	CLD 703	CLD 704	CLD 705	CLD 706	CLD 707	CLD 708	CLD 709	CLD 710	CLD 711	CLD 712	CLD 713	CLD 714	CLD 715	CLD 716	CLD 717	CLD 718	CLD 719	CLD 720	CLD 721	CLD 722	CLD 723	CLD 724	CLD 725	CLD 726	CLD 727	CLD 728	CLD 729	CLD 730	CLD 731	CLD 732	CLD 733	CLD 734	CLD 735	CLD 736	CLD 737	CLD 738	CLD 739	CLD 740	CLD 741	CLD 742	CLD 743	CLD 744	CLD 745	CLD 746	CLD 747	CLD 748	CLD 749	CLD 750	CLD 751	CLD 752	CLD 753	CLD 754	CLD 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1115	CLD 1116	CLD 1117	CLD 1118	CLD 1119	CLD 1
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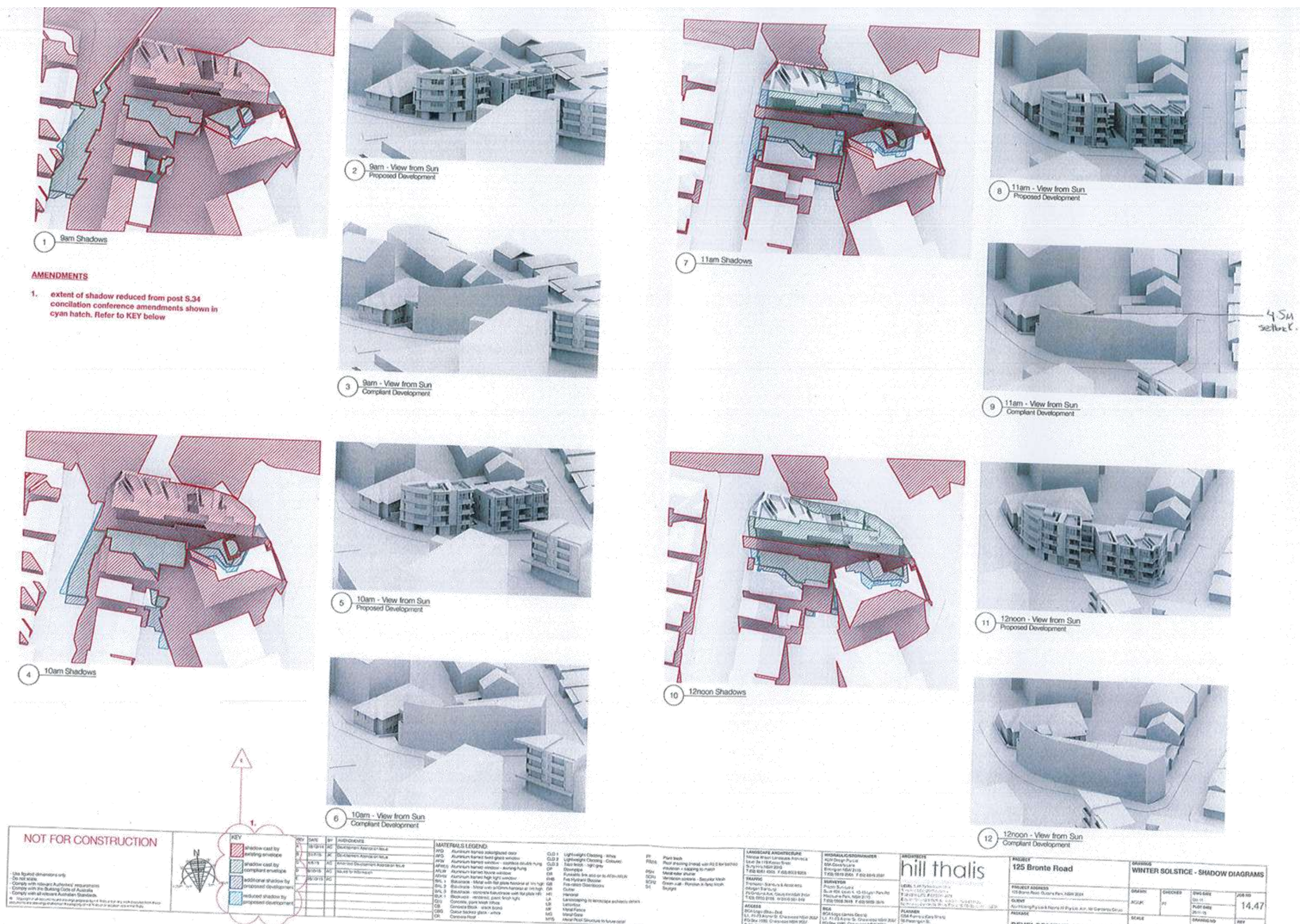








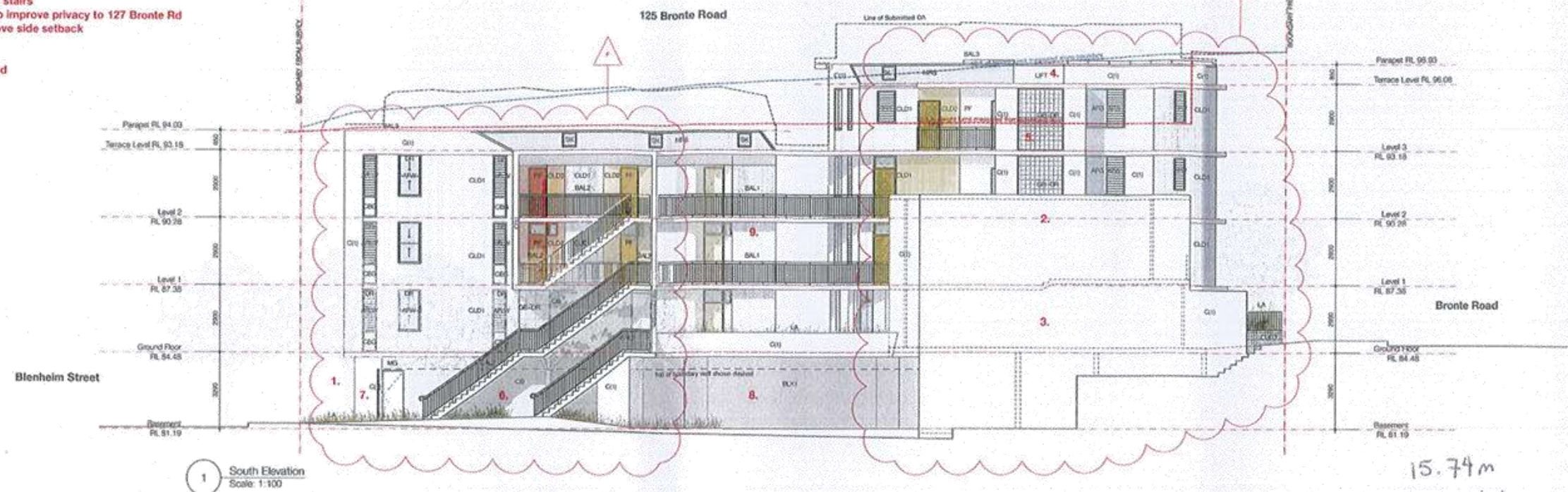






## AMENDMENTS

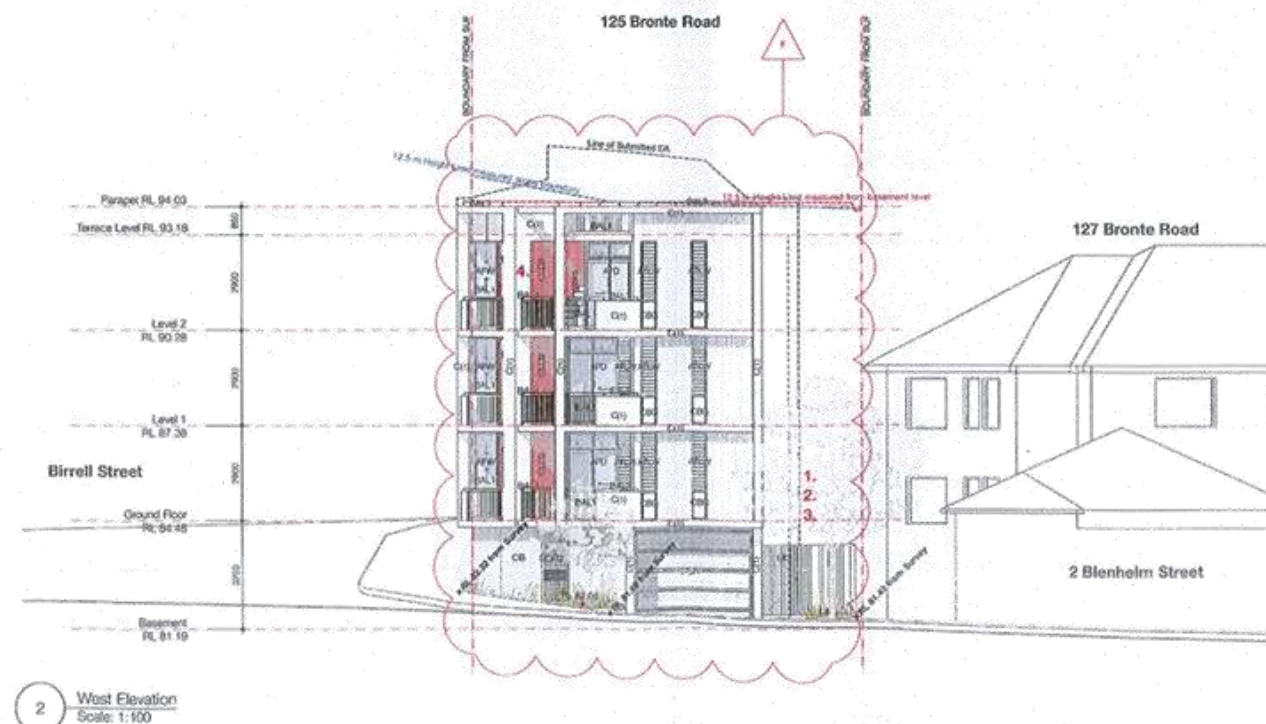
1. 3m southern side setback
2. South-eastern bulk reduced; compliant with existing building envelop
3. keeper unit at ground level with required minimum landscape area
4. Relocation of lift and eastern fire-isolated stairs
5. obscured glass block wall at lift landing to improve privacy to 127 Bronte Rd
6. Consolidated western staircases to improve side setback
7. Increased landscape deep soil area
8. reconfigured basement layout
9. planted screenwall along corridor removed



15.74m  
Basement to  
parapet.

## AMENDMENTS

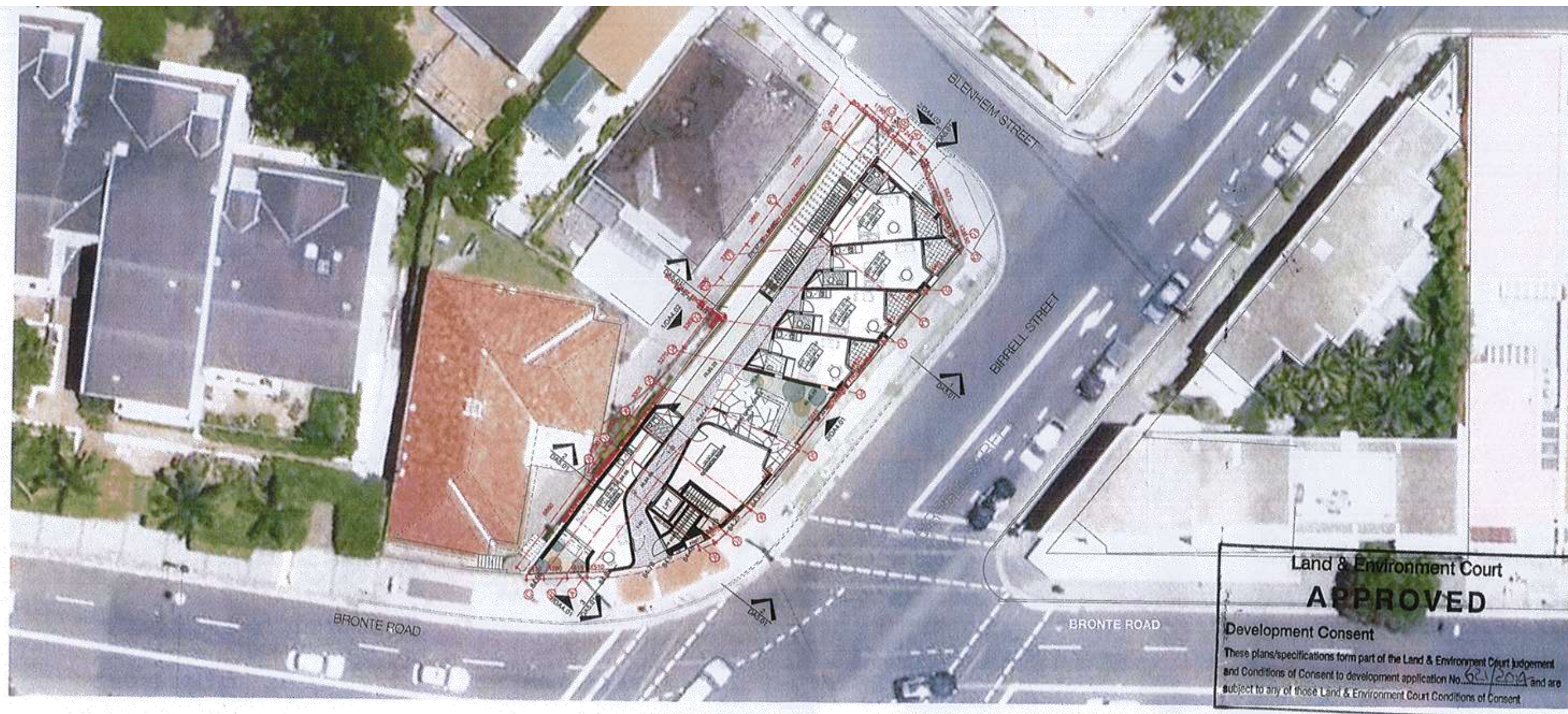
1. 3m southern side setback
2. Consolidated western staircases to improve side setback
3. Increased landscape deep soil area
4. western unit reconfigured; balcony oriented north



approved/  
experienced

NOT FOR CONSTRUCTION		KEY	REV	DATE	BY	AMENDMENTS	MATERIALS LEGEND	CLD-1	CLD-2	CLD-3	CLD-4	CLD-5	CLD-6	CLD-7	CLD-8	CLD-9	CLD-10	CLD-11	CLD-12	CLD-13	CLD-14	CLD-15	CLD-16	CLD-17	CLD-18	CLD-19	CLD-20	CLD-21	CLD-22	CLD-23	CLD-24	CLD-25	CLD-26	CLD-27	CLD-28	CLD-29	CLD-30	CLD-31	CLD-32	CLD-33	CLD-34	CLD-35	CLD-36	CLD-37	CLD-38	CLD-39	CLD-40	CLD-41	CLD-42	CLD-43	CLD-44	CLD-45	CLD-46	CLD-47	CLD-48	CLD-49	CLD-50	CLD-51	CLD-52	CLD-53	CLD-54	CLD-55	CLD-56	CLD-57	CLD-58	CLD-59	CLD-60	CLD-61	CLD-62	CLD-63	CLD-64	CLD-65	CLD-66	CLD-67	CLD-68	CLD-69	CLD-70	CLD-71	CLD-72	CLD-73	CLD-74	CLD-75	CLD-76	CLD-77	CLD-78	CLD-79	CLD-80	CLD-81	CLD-82	CLD-83	CLD-84	CLD-85	CLD-86	CLD-87	CLD-88	CLD-89	CLD-90	CLD-91	CLD-92	CLD-93	CLD-94	CLD-95	CLD-96	CLD-97	CLD-98	CLD-99	CLD-100	CLD-101	CLD-102	CLD-103	CLD-104	CLD-105	CLD-106	CLD-107	CLD-108	CLD-109	CLD-110	CLD-111	CLD-112	CLD-113	CLD-114	CLD-115	CLD-116	CLD-117	CLD-118	CLD-119	CLD-120	CLD-121	CLD-122	CLD-123	CLD-124	CLD-125	CLD-126	CLD-127	CLD-128	CLD-129	CLD-130	CLD-131	CLD-132	CLD-133	CLD-134	CLD-135	CLD-136	CLD-137	CLD-138	CLD-139	CLD-140	CLD-141	CLD-142	CLD-143	CLD-144	CLD-145	CLD-146	CLD-147	CLD-148	CLD-149	CLD-150	CLD-151	CLD-152	CLD-153	CLD-154	CLD-155	CLD-156	CLD-157	CLD-158	CLD-159	CLD-160	CLD-161	CLD-162	CLD-163	CLD-164	CLD-165	CLD-166	CLD-167	CLD-168	CLD-169	CLD-170	CLD-171	CLD-172	CLD-173	CLD-174	CLD-175	CLD-176	CLD-177	CLD-178	CLD-179	CLD-180	CLD-181	CLD-182	CLD-183	CLD-184	CLD-185	CLD-186	CLD-187	CLD-188	CLD-189	CLD-190	CLD-191	CLD-192	CLD-193	CLD-194	CLD-195	CLD-196	CLD-197	CLD-198	CLD-199	CLD-200	CLD-201	CLD-202	CLD-203	CLD-204	CLD-205	CLD-206	CLD-207	CLD-208	CLD-209	CLD-210	CLD-211	CLD-212	CLD-213	CLD-214	CLD-215	CLD-216	CLD-217	CLD-218	CLD-219	CLD-220	CLD-221	CLD-222	CLD-223	CLD-224	CLD-225	CLD-226	CLD-227	CLD-228	CLD-229	CLD-230	CLD-231	CLD-232	CLD-233	CLD-234	CLD-235	CLD-236	CLD-237	CLD-238	CLD-239	CLD-240	CLD-241	CLD-242	CLD-243	CLD-244	CLD-245	CLD-246	CLD-247	CLD-248	CLD-249	CLD-250	CLD-251	CLD-252	CLD-253	CLD-254	CLD-255	CLD-256	CLD-257	CLD-258	CLD-259	CLD-260	CLD-261	CLD-262	CLD-263	CLD-264	CLD-265	CLD-266	CLD-267	CLD-268	CLD-269	CLD-270	CLD-271	CLD-272	CLD-273	CLD-274	CLD-275	CLD-276	CLD-277	CLD-278	CLD-279	CLD-280	CLD-281	CLD-282	CLD-283	CLD-284	CLD-285	CLD-286	CLD-287	CLD-288	CLD-289	CLD-290	CLD-291	CLD-292	CLD-293	CLD-294	CLD-295	CLD-296	CLD-297	CLD-298	CLD-299	CLD-300	CLD-301	CLD-302	CLD-303	CLD-304	CLD-305	CLD-306	CLD-307	CLD-308	CLD-309	CLD-310	CLD-311	CLD-312	CLD-313	CLD-314	CLD-315	CLD-316	CLD-317	CLD-318	CLD-319	CLD-320	CLD-321	CLD-322	CLD-323	CLD-324	CLD-325	CLD-326	CLD-327	CLD-328	CLD-329	CLD-330	CLD-331	CLD-332	CLD-333	CLD-334	CLD-335	CLD-336	CLD-337	CLD-338	CLD-339	CLD-340	CLD-341	CLD-342	CLD-343	CLD-344	CLD-345	CLD-346	CLD-347	CLD-348	CLD-349	CLD-350	CLD-351	CLD-352	CLD-353	CLD-354	CLD-355	CLD-356	CLD-357	CLD-358	CLD-359	CLD-360	CLD-361	CLD-362	CLD-363	CLD-364	CLD-365	CLD-366	CLD-367	CLD-368	CLD-369	CLD-370	CLD-371	CLD-372	CLD-373	CLD-374	CLD-375	CLD-376	CLD-377	CLD-378	CLD-379	CLD-380	CLD-381	CLD-382	CLD-383	CLD-384	CLD-385	CLD-386	CLD-387	CLD-388	CLD-389	CLD-390	CLD-391	CLD-392	CLD-393	CLD-394	CLD-395	CLD-396	CLD-397	CLD-398	CLD-399	CLD-400	CLD-401	CLD-402	CLD-403	CLD-404	CLD-405	CLD-406	CLD-407	CLD-408	CLD-409	CLD-410	CLD-411	CLD-412	CLD-413	CLD-414	CLD-415	CLD-416	CLD-417	CLD-418	CLD-419	CLD-420	CLD-421	CLD-422	CLD-423	CLD-424	CLD-425	CLD-426	CLD-427	CLD-428	CLD-429	CLD-430	CLD-431	CLD-432	CLD-433	CLD-434	CLD-435	CLD-436	CLD-437	CLD-438	CLD-439	CLD-440	CLD-441	CLD-442	CLD-443	CLD-444	CLD-445	CLD-446	CLD-447	CLD-448	CLD-449	CLD-450	CLD-451	CLD-452	CLD-453	CLD-454	CLD-455	CLD-456	CLD-457	CLD-458	CLD-459	CLD-460	CLD-461	CLD-462	CLD-463	CLD-464	CLD-465	CLD-466	CLD-467	CLD-468	CLD-469	CLD-470	CLD-471	CLD-472	CLD-473	CLD-474	CLD-475	CLD-476	CLD-477	CLD-478	CLD-479	CLD-480	CLD-481	CLD-482	CLD-483	CLD-484	CLD-485	CLD-486	CLD-487	CLD-488	CLD-489	CLD-490	CLD-491	CLD-492	CLD-493	CLD-494	CLD-495	CLD-496	CLD-497	CLD-498	CLD-499	CLD-500	CLD-501	CLD-502	CLD-503	CLD-504	CLD-505	CLD-506	CLD-507	CLD-508	CLD-509	CLD-510	CLD-511	CLD-512	CLD-513	CLD-514	CLD-515	CLD-516	CLD-517	CLD-518	CLD-519	CLD-520	CLD-521	CLD-522	CLD-523	CLD-524	CLD-525	CLD-526	CLD-527	CLD-528	CLD-529	CLD-530	CLD-531	CLD-532	CLD-533	CLD-534	CLD-535	CLD-536	CLD-537	CLD-538	CLD-539	CLD-540	CLD-541	CLD-542	CLD-543	CLD-544	CLD-545	CLD-546	CLD-547	CLD-548	CLD-549	CLD-550	CLD-551	CLD-552	CLD-553	CLD-554	CLD-555	CLD-556	CLD-557	CLD-558	CLD-559	CLD-560	CLD-561	CLD-562	CLD-563	CLD-564	CLD-565	CLD-566	CLD-567	CLD-568	CLD-569	CLD-570	CLD-571	CLD-572	CLD-573	CLD-574	CLD-575	CLD-576	CLD-577	CLD-578	CLD-579	CLD-580	CLD-581	CLD-582	CLD-583	CLD-584	CLD-585	CLD-586	CLD-587	CLD-588	CLD-589	CLD-590	CLD-591	CLD-592	CLD-593	CLD-594	CLD-595	CLD-596	CLD-597	CLD-598	CLD-599	CLD-600	CLD-601	CLD-602	CLD-603	CLD-604	CLD-605	CLD-606	CLD-607	CLD-608	CLD-609	CLD-610	CLD-611	CLD-612	CLD-613	CLD-614	CLD-615	CLD-616	CLD-617	CLD-618	CLD-619	CLD-620	CLD-621	CLD-622	CLD-623	CLD-624	CLD-625	CLD-626	CLD-627	CLD-628	CLD-629	CLD-630	CLD-631	CLD-632	CLD-633	CLD-634	CLD-635	CLD-636	CLD-637	CLD-638	CLD-639	CLD-640	CLD-641	CLD-642	CLD-643	CLD-644	CLD-645	CLD-646	CLD-647	CLD-648	CLD-649	CLD-650	CLD-651	CLD-652	CLD-653	CLD-654	CLD-655	CLD-656	CLD-657	CLD-658	CLD-659	CLD-660	CLD-661	CLD-662	CLD-663	CLD-664	CLD-665	CLD-666	CLD-667	CLD-668	CLD-669	CLD-670	CLD-671	CLD-672	CLD-673	CLD-674	CLD-675	CLD-676	CLD-677	CLD-678	CLD-679	CLD-680	CLD-681	CLD-682	CLD-683	CLD-684	CLD-685	CLD-686	CLD-687	CLD-688	CLD-689	CLD-690	CLD-691	CLD-692	CLD-693	CLD-694	CLD-695	CLD-696	CLD-697	CLD-698	CLD-699	CLD-700	CLD-701	CLD-702	CLD-703	CLD-704	CLD-705	CLD-706	CLD-707	CLD-708	CLD-709	CLD-710	CLD-711	CLD-712	CLD-713	CLD-714	CLD-715	CLD-716	CLD-717	CLD-718	CLD-719	CLD-720	CLD-721	CLD-722	CLD-723	CLD-724	CLD-725	CLD-726	CLD-727	CLD-728	CLD-729	CLD-730	CLD-731	CLD-732	CLD-733	CLD-734	CLD-735	CLD-736	CLD-737	CLD-738	CLD-739	CLD-740	CLD-741	CLD-742	CLD-743	CLD-744	CLD-745	CLD-746	CLD-747	CLD-748	CLD-749	CLD-750	CLD-751	CLD-752	CLD-753	CLD-754	CLD-755	CLD-756	CLD-757	CLD-758	CLD-759	CLD-760	CLD-761	CLD-762	CLD-763	CLD-764	CLD-765	CLD-766	CLD-767	CLD-768	CLD-769	CLD-770	CLD-771	CLD-772	CLD-773	CLD-774	CLD-775	CLD-776	CLD-777	CLD-778	CLD-779	CLD-780	CLD-781	CLD-782	CLD-783	CLD-784	CLD-785	CLD-786	CLD-787	CLD-788	CLD-789	CLD-790	CLD-791	CLD-792	CLD-793	CLD-794	CLD-795	CLD-796	CLD-797	CLD-798	CLD-799	CLD-800	CLD-801	CLD-802	CLD-803	CLD-804	CLD-805	CLD-806	CLD-807	CLD-808	CLD-809	CLD-810	CLD-811	CLD-812	CLD-813	CLD-814	CLD-815	CLD-816	CLD-817	CLD-818	CLD-819	CLD-820	CLD-821	CLD-822	CLD-823	CLD-824	CLD-825	CLD-826	CLD-827	CLD-828	CLD-829	CLD-830	CLD-831	CLD-832	CLD-833	CLD-834	CLD-835	CLD-836	CLD-837	CLD-838	CLD-839	CLD-840	CLD-841	CLD-842	CLD-843	CLD-844	CLD-845	CLD-846	CLD-847	CLD-848	CLD-849	CLD-850	CLD-851	CLD-852	CLD-853	CLD-854	CLD-855	CLD-856	CLD-857	CLD-858	CLD-859	CLD-860	CLD-861	CLD-862	CLD-863	CLD-864	CLD-865	CLD-866	CLD-867	CLD-868	CLD-869	CLD-870	CLD-871	CLD-872	CLD-873	CLD-874	CLD-875	CLD-876	CLD-877	CLD-878	CLD-879	CLD-880	CLD-881	CLD-882	CLD-883	CLD-884	CLD-885	CLD-886	CLD-887	CLD-888	CLD-889	CLD-890	CLD-891	CLD-892	CLD-893	CLD-894	CLD-895	CLD-896	CLD-897	CLD-898	CLD-899	CLD-900	CLD-901	CLD-902	CLD-903	CLD-904	CLD-905	CLD-906	CLD-907	CLD-908	CLD-909	CLD-910	CLD-911	CLD-912	CLD-913	CLD-914	CLD-915	CLD-916	CLD-917	CLD-918	CLD-919	CLD-920	CLD-921	CLD-922	CLD-923	CLD-924	CLD-925	CLD-926	CLD-927	CLD-928	CLD-929	CLD-930	CLD-931	CLD-932	CLD-933	CLD-934	CLD-935	CLD-936	CLD-937	CLD-938	CLD-939	CLD-940	CLD-941	CLD-942	CLD-943	CLD-944	CLD-945	CLD-946	CLD-947	CLD-948	CLD-949	CLD-950	CLD-951	CLD-952	CLD-953	CLD-954	CLD-955	CLD-956	CLD-957	CLD-958	CLD-959	CLD-960	CLD-961	CLD-962	CLD-963	CLD-964	CLD-965	CLD-966	CLD-967	CLD-968	CLD-969	CLD-970	CLD-971	CLD-972	CLD-973	CLD-974	CLD-975	CLD-976	CLD-977	CLD-978	CLD-979	CLD-980	CLD-981	CLD-982	CLD-983	CLD-984	CLD-985	CLD-986	CLD-987	CLD-988	CLD-989	CLD-990	CLD-991	CLD-992	CLD-993	CLD-994	CLD-995	CLD-996	CLD-997	CLD-998	CLD-999	CLD-1000	CLD-1001	CLD-1002	CLD-1003	CLD-1004	CLD-1005	CLD-1006	CLD-1007	CLD-1008	CLD-1009	CLD-1010	CLD-1011	CLD-1012	CLD-1013	CLD-1014	CLD-1015	CLD-1016	CLD-1017	CLD-1018	CLD-1019	CLD-1020	CLD-1021	CLD-1022	CLD-1023	CLD-1024	CLD-1025	CLD-1026	CLD-1027	CLD-1028	CLD-1029	CLD-1030	CLD-1031	CLD-1032	CLD-1033	CLD-1034	CLD-1035	CLD-1036	CLD-1037	CLD-1038	CLD-1039	CLD-1040	CLD-1041	CLD-1042	CLD-1043	CLD-1044	CLD-1045	CLD-1046	CLD-1047	CLD-1048	CLD-1049	CLD-1050	CLD-1051	CLD-1052	CLD-1053	CLD-1054	CLD-1055	CLD-1056	CLD-1057	CLD-1058	CLD-1059	CLD-1060	CLD-1061	CLD-1062	CLD-1063	CLD-1064	CLD-1065	CLD-1066	CLD-1067	CLD-1068	CLD-1069	CLD-1070	CLD-1071	CLD-1072	CLD-1073	CLD-1074	CLD-1075	CLD-1076	CLD-1077	CLD-1078	CLD-1079	CLD-1080	CLD-1081	CLD-1082	CLD-1083	CLD-1084	CLD-1085	CLD-1086	CLD-1087	CLD-1088	CLD-1089	CLD-1090	CLD-1091	CLD-1092	CLD-1093	CLD-1094	CLD-1095	CLD-1096	CLD-1097	CLD-1098	CLD-1099	CLD-1100	CLD-1101	CLD-1102	CLD-1103	CLD-1104	CLD-1105	CLD-1106	CLD-1107	CLD-1108	CLD-1109	CLD-1110	CLD-1111	CLD-1112	CLD-1113	CLD-1114	CLD-1115	CLD-1116	CLD-1117	CLD-1118	CLD-1119	CLD-1
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1. 3m southern side setback
2. Relocation of lift and eastern fire-isolated stairs
3. Consolidated western staircases to improve side setback
4. Increased landscape deep soil area
5. reconfigured basement layout
6. small car space

Land &amp; Environment Court

**APPROVED**

Development Consent

These plans/specifications form part of the Land & Environment Court judgement and Conditions of Consent to development application No. 621/2014, and are subject to any of those Land & Environment Court Conditions of Consent

1 Basement Plan  
Scale: 1:100

**AMENDMENTS**

1. 3m southern side setback
2. South-eastern bulk reduced; compliant with existing building envelop
3. keeper unit at ground level with required minimum landscape area
4. Relocation of lift and eastern fire-isolated stairs
5. Consolidated western staircases to improve side setback
6. planted screenwall along corridor removed
7. western unit reconfigured; balcony oriented north
8. FHB + LB and services consolidated
9. reduced common room; increased communal landscape area

2 Ground Level Plan  
Scale: 1:100

<b>NOT FOR CONSTRUCTION</b>		<b>KEY</b>		<b>REV</b>		<b>DATE</b>		<b>AMENDMENTS</b>		<b>MATERIALS LEGEND</b>		<b>CLD</b>		<b>CLD 1</b>		<b>CLD 2</b>		<b>CLD 3</b>		<b>CLD 4</b>		<b>CLD 5</b>		<b>CLD 6</b>		<b>CLD 7</b>		<b>CLD 8</b>		<b>CLD 9</b>		<b>CLD 10</b>		<b>CLD 11</b>		<b>CLD 12</b>		<b>CLD 13</b>		<b>CLD 14</b>		<b>CLD 15</b>		<b>CLD 16</b>		<b>CLD 17</b>		<b>CLD 18</b>		<b>CLD 19</b>		<b>CLD 20</b>		<b>CLD 21</b>		<b>CLD 22</b>		<b>CLD 23</b>		<b>CLD 24</b>		<b>CLD 25</b>		<b>CLD 26</b>		<b>CLD 27</b>		<b>CLD 28</b>		<b>CLD 29</b>		<b>CLD 30</b>		<b>CLD 31</b>		<b>CLD 32</b>		<b>CLD 33</b>		<b>CLD 34</b>		<b>CLD 35</b>		<b>CLD 36</b>		<b>CLD 37</b>		<b>CLD 38</b>		<b>CLD 39</b>		<b>CLD 40</b>		<b>CLD 41</b>		<b>CLD 42</b>		<b>CLD 43</b>		<b>CLD 44</b>		<b>CLD 45</b>		<b>CLD 46</b>		<b>CLD 47</b>		<b>CLD 48</b>		<b>CLD 49</b>		<b>CLD 50</b>		<b>CLD 51</b>		<b>CLD 52</b>		<b>CLD 53</b>		<b>CLD 54</b>		<b>CLD 55</b>		<b>CLD 56</b>		<b>CLD 57</b>		<b>CLD 58</b>		<b>CLD 59</b>		<b>CLD 60</b>		<b>CLD 61</b>		<b>CLD 62</b>		<b>CLD 63</b>		<b>CLD 64</b>		<b>CLD 65</b>		<b>CLD 66</b>		<b>CLD 67</b>		<b>CLD 68</b>		<b>CLD 69</b>		<b>CLD 70</b>		<b>CLD 71</b>		<b>CLD 72</b>		<b>CLD 73</b>		<b>CLD 74</b>		<b>CLD 75</b>		<b>CLD 76</b>		<b>CLD 77</b>		<b>CLD 78</b>		<b>CLD 79</b>		<b>CLD 80</b>		<b>CLD 81</b>		<b>CLD 82</b>		<b>CLD 83</b>		<b>CLD 84</b>		<b>CLD 85</b>		<b>CLD 86</b>		<b>CLD 87</b>		<b>CLD 88</b>		<b>CLD 89</b>		<b>CLD 90</b>		<b>CLD 91</b>		<b>CLD 92</b>		<b>CLD 93</b>		<b>CLD 94</b>		<b>CLD 95</b>		<b>CLD 96</b>		<b>CLD 97</b>		<b>CLD 98</b>		<b>CLD 99</b>		<b>CLD 100</b>		<b>CLD 101</b>		<b>CLD 102</b>		<b>CLD 103</b>		<b>CLD 104</b>		<b>CLD 105</b>		<b>CLD 106</b>		<b>CLD 107</b>		<b>CLD 108</b>		<b>CLD 109</b>		<b>CLD 110</b>		<b>CLD 111</b>		<b>CLD 112</b>		<b>CLD 113</b>		<b>CLD 114</b>		<b>CLD 115</b>		<b>CLD 116</b>		<b>CLD 117</b>		<b>CLD 118</b>		<b>CLD 119</b>		<b>CLD 120</b>		<b>CLD 121</b>		<b>CLD 122</b>		<b>CLD 123</b>		<b>CLD 124</b>		<b>CLD 125</b>		<b>CLD 126</b>		<b>CLD 127</b>		<b>CLD 128</b>		<b>CLD 129</b>		<b>CLD 130</b>		<b>CLD 131</b>		<b>CLD 132</b>		<b>CLD 133</b>		<b>CLD 134</b>		<b>CLD 135</b>		<b>CLD 136</b>		<b>CLD 137</b>		<b>CLD 138</b>		<b>CLD 139</b>		<b>CLD 140</b>		<b>CLD 141</b>		<b>CLD 142</b>		<b>CLD 143</b>		<b>CLD 144</b>		<b>CLD 145</b>		<b>CLD 146</b>		<b>CLD 147</b>		<b>CLD 148</b>		<b>CLD 149</b>		<b>CLD 150</b>		<b>CLD 151</b>		<b>CLD 152</b>		<b>CLD 153</b>		<b>CLD 154</b>		<b>CLD 155</b>		<b>CLD 156</b>		<b>CLD 157</b>		<b>CLD 158</b>		<b>CLD 159</b>		<b>CLD 160</b>		<b>CLD 161</b>		<b>CLD 162</b>		<b>CLD 163</b>		<b>CLD 164</b>		<b>CLD 165</b>		<b>CLD 166</b>		<b>CLD 167</b>		<b>CLD 168</b>		<b>CLD 169</b>		<b>CLD 170</b>		<b>CLD 171</b>		<b>CLD 172</b>		<b>CLD 173</b>		<b>CLD 174</b>		<b>CLD 175</b>		<b>CLD 176</b>		<b>CLD 177</b>		<b>CLD 178</b>		<b>CLD 179</b>		<b>CLD 180</b>		<b>CLD 181</b>		<b>CLD 182</b>		<b>CLD 183</b>		<b>CLD 184</b>		<b>CLD 185</b>		<b>CLD 186</b>		<b>CLD 187</b>		<b>CLD 188</b>		<b>CLD 189</b>		<b>CLD 190</b>		<b>CLD 191</b>		<b>CLD 192</b>		<b>CLD 193</b>		<b>CLD 194</b>		<b>CLD 195</b>		<b>CLD 196</b>		<b>CLD 197</b>		<b>CLD 198</b>		<b>CLD 199</b>		<b>CLD 200</b>		<b>CLD 201</b>		<b>CLD 202</b>		<b>CLD 203</b>		<b>CLD 204</b>		<b>CLD 205</b>		<b>CLD 206</b>		<b>CLD 207</b>		<b>CLD 208</b>		<b>CLD 209</b>		<b>CLD 210</b>		<b>CLD 211</b>		<b>CLD 212</b>		<b>CLD 213</b>		<b>CLD 214</b>		<b>CLD 215</b>		<b>CLD 216</b>		<b>CLD 217</b>		<b>CLD 218</b>		<b>CLD 219</b>		<b>CLD 220</b>		<b>CLD 221</b>		<b>CLD 222</b>		<b>CLD 223</b>		<b>CLD 224</b>		<b>CLD 225</b>		<b>CLD 226</b>		<b>CLD 227</b>		<b>CLD 228</b>		<b>CLD 229</b>		<b>CLD 230</b>		<b>CLD 231</b>		<b>CLD 232</b>		<b>CLD 233</b>		<b>CLD 234</b>		<b>CLD 235</b>		<b>CLD 236</b>		<b>CLD 237</b>		<b>CLD 238</b>		<b>CLD 239</b>		<b>CLD 240</b>		<b>CLD 241</b>		<b>CLD 242</b>		<b>CLD 243</b>		<b>CLD 244</b>		<b>CLD 245</b>		<b>CLD 246</b>		<b>CLD 247</b>		<b>CLD 248</b>		<b>CLD 249</b>		<b>CLD 250</b>		<b>CLD 251</b>		<b>CLD 252</b>		<b>CLD 253</b>		<b>CLD 254</b>		<b>CLD 255</b>		<b>CLD 256</b>		<b>CLD 257</b>		<b>CLD 258</b>		<b>CLD 259</b>		<b>CLD 260</b>		<b>CLD 261</b>		<b>CLD 262</b>		<b>CLD 263</b>		<b>CLD 264</b>		<b>CLD 265</b>		<b>CLD 266</b>		<b>CLD 267</b>		<b>CLD 268</b>		<b>CLD 269</b>		<b>CLD 270</b>		<b>CLD 271</b>		<b>CLD 272</b>		<b>CLD 273</b>		<b>CLD 274</b>		<b>CLD 275</b>		<b>CLD 276</b>		<b>CLD 277</b>		<b>CLD 278</b>		<b>CLD 279</b>		<b>CLD 280</b>		<b>CLD 281</b>		<b>CLD 282</b>		<b>CLD 283</b>		<b>CLD 284</b>		<b>CLD 285</b>		<b>CLD 286</b>		<b>CLD 287</b>		<b>CLD 288</b>		<b>CLD 289</b>		<b>CLD 290</b>		<b>CLD 291</b>		<b>CLD 292</b>		<b>CLD 293</b>		<b>CLD 294</b>		<b>CLD 295</b>		<b>CLD 296</b>		<b>CLD 297</b>		<b>CLD 298</b>		<b>CLD 299</b>		<b>CLD 300</b>		<b>CLD 301</b>		<b>CLD 302</b>		<b>CLD 303</b>		<b>CLD 304</b>		<b>CLD 305</b>		<b>CLD 306</b>		<b>CLD 307</b>		<b>CLD 308</b>		<b>CLD 309</b>		<b>CLD 310</b>		<b>CLD 311</b>		<b>CLD 312</b>		<b>CLD 313</b>		<b>CLD 314</b>		<b>CLD 315</b>		<b>CLD 316</b>		<b>CLD 317</b>		<b>CLD 318</b>		<b>CLD 319</b>		<b>CLD 320</b>		<b>CLD 321</b>		<b>CLD 322</b>		<b>CLD 323</b>		<b>CLD 324</b>		<b>CLD 325</b>		<b>CLD 326</b>		<b>CLD 327</b>		<b>CLD 328</b>		<b>CLD 329</b>		<b>CLD 330</b>		<b>CLD 331</b>		<b>CLD 332</b>		<b>CLD 333</b>		<b>CLD 334</b>		<b>CLD 335</b>		<b>CLD 336</b>		<b>CLD 337</b>		<b>CLD 338</b>		<b>CLD 339</b>		<b>CLD 340</b>		<b>CLD 341</b>		<b>CLD 342</b>		<b>CLD 343</b>		<b>CLD 344</b>		<b>CLD 345</b>		<b>CLD 346</b>		<b>CLD 347</b>		<b>CLD 348</b>		<b>CLD 349</b>		<b>CLD 350</b>	



Land &amp; Environment Court

**APPROVED**

Development Consent

These plans/specifications form part of the Land & Environment Court judgement and Conditions of Consent to development application No. 631/2014 and are subject to any of those Land & Environment Court Conditions of Consent

- 3m southern side setback
- South-eastern bulk reduced; compliant with existing building envelop
- Relocation of lift and eastern fire-isolated stairs
- Consolidated western staircases to improve side setback
- planted screenwall along corridor removed
- western unit reconfigured; balcony oriented north
- higher ceiling to unit 11 to improve amenity

1 Level 1 Plan  
Scale: 1:100

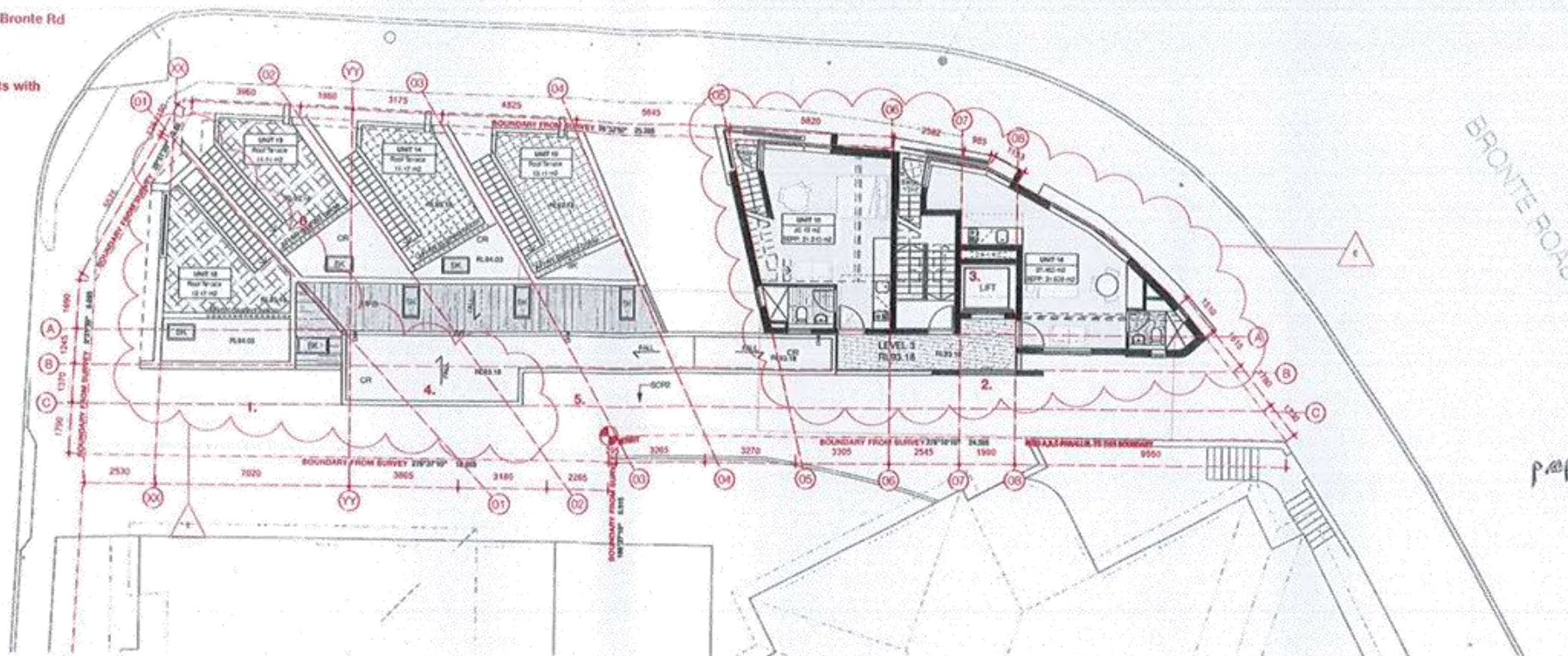
**AMENDMENTS**

- 3m southern side setback
- South-eastern bulk reduced; compliant with existing building envelop
- observed glass block wall at lift landing to improve privacy to 127 Bronte Rd
- Relocation of lift and eastern fire-isolated stairs
- Consolidated western staircases to improve side setback
- planted screenwall along corridor removed
- western unit reconfigured; balcony oriented north
- high level windows shown on level 2, improving amenity to units with roof terraces

2 Level 2 Plan  
Scale: 1:100

<div>NOT FOR CONSTRUCTION</div>		<div>KEY</div>		<div>REV</div>		<div>DATE</div>		<div>AMENDMENTS</div>		<div>MATERIALS LEGEND</div>		<div>LANDSCAPE ARCHITECTURE</div>		<div>HYDRAULIC/STRUCTURAL</div>		<div>ARCHITECTS</div>		<div>PROJECT</div>		<div>DRAWING</div>		<div>LEVEL 1 + LEVEL 2 PLAN</div>			
<div>Use specified dimensions only. Do not scale. Comply with relevant Australian Instruments. Comply with the Building Code of Australia. Comply with all relevant Australian Standards. Do not copy or reproduce without the written consent of the author. Do not use or copy without the written consent of the author.</div>		<div></div>		<div>A 18/10/14 AC A 18/10/14 AC A 18/10/14 AC A 18/10/14 AC A 18/10/14 AC A 18/10/14 AC</div>		<div>18/10/14 18/10/14 18/10/14 18/10/14 18/10/14 18/10/14</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>		<div>Approved Approved Approved Approved Approved Approved</div>	

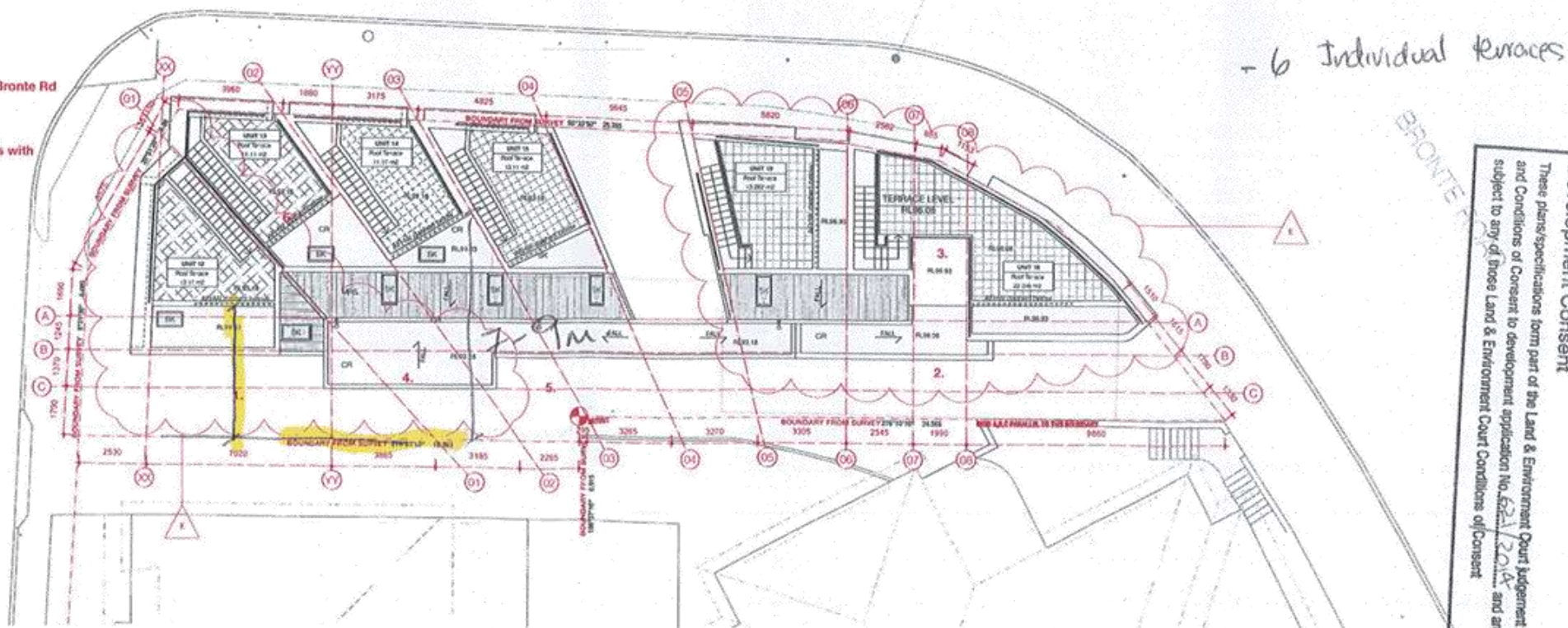




Parking  
 $19 \times 0.2$   
 $= 3.8$  | 4  
 + 1 worker  
 proposed = 5 - 6

1 Level 3 Plan  
Scale: 1:100

- 3m southern side setback
- obscured glass block wall at lift landing to improve privacy to 127 Bronte Rd
- Relocation of lift and eastern fire-isolated stairs
- Consolidated western staircases to improve side setback
- planted screenwall along corridor removed
- high level windows shown on level 2 + 3, improving amenity to units with roof terraces



- 6 Individual terraces

Land & Environment Court  
**APPROVED**  
Development Consent

These plans/specifications form part of the Land & Environment Court Judgement and Conditions of Consent to development application No. 621/2014 and are subject to any of those Land & Environment Court Conditions of Consent

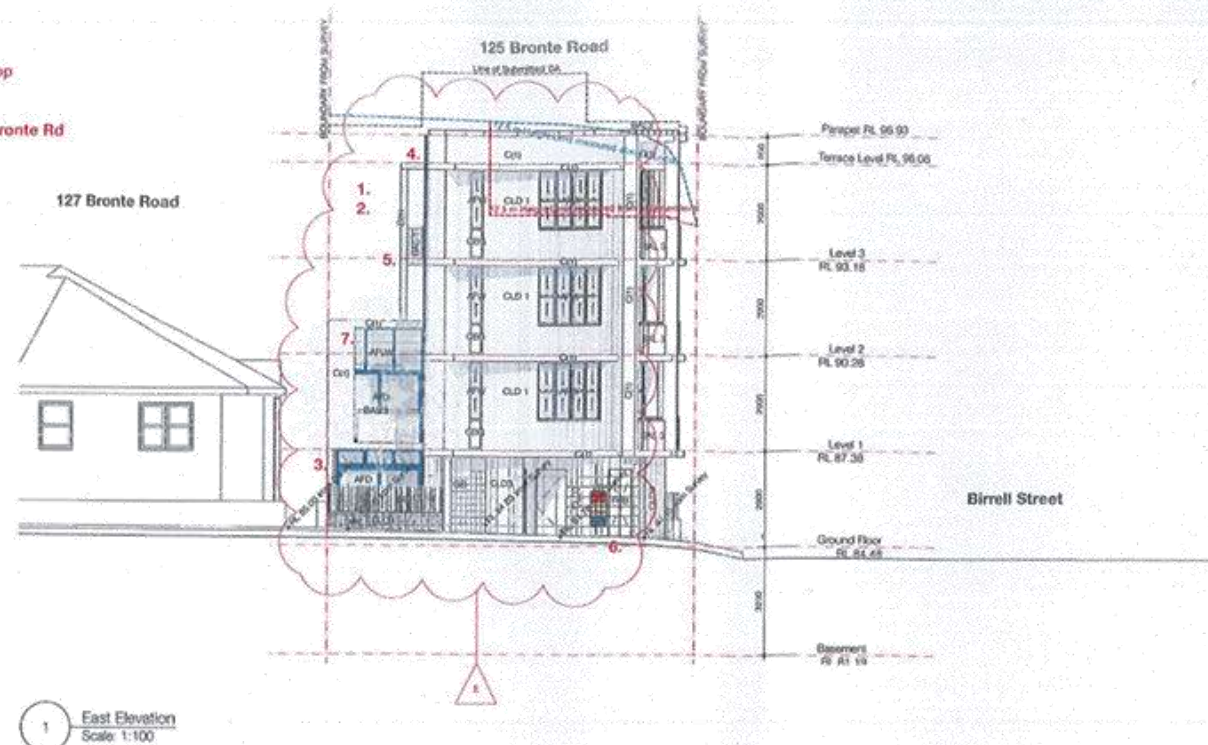
2 Roof Plan  
Scale: 1:100

[illegible]



## AMENDMENTS

1. 3m southern side setback
2. South-eastern bulk reduced; compliant with existing building envelop
3. keeper unit at ground level with required minimum landscape area
4. Relocation of lift and eastern fire-isolated stairs
5. obscured glass block wall at lift landing to improve privacy to 127 Bronte Rd
6. FHB + LB and services consolidated
7. higher ceiling to unit 11 to improve amenity



Land & Environment Court  
**APPROVED**

## Development Consent

These plans/specifications form part of the Land & Environment Court judgement and Conditions of Consent to development application No. 63/2015, and are subject to any of those Land & Environment Court Conditions of Consent

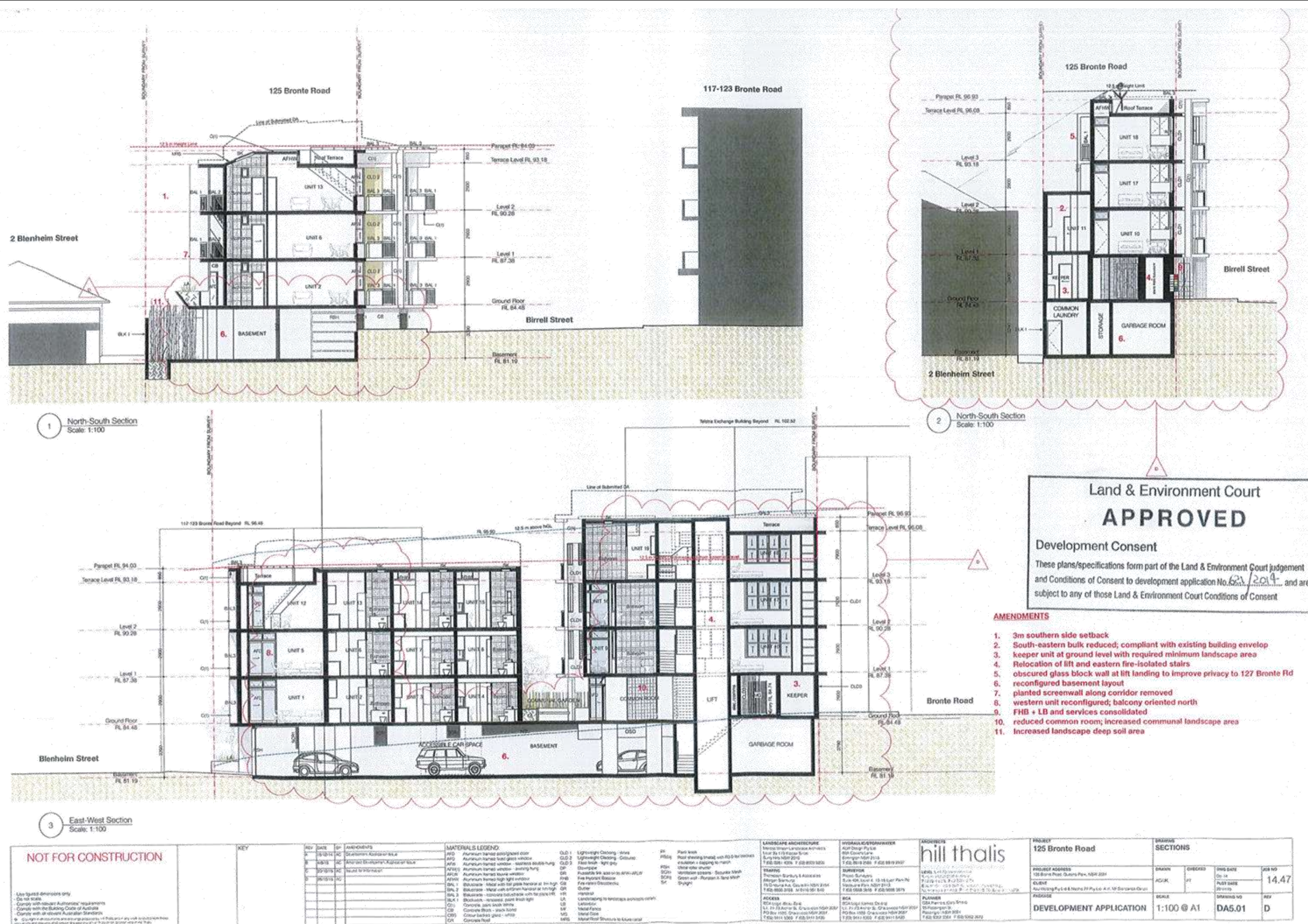
## AMENDMENTS

1. keeper unit at ground level with required minimum landscape area
2. Relocation of lift and eastern fire-isolated stairs
3. FHB + LB and services consolidated
4. reduced common room; increased communal landscape area

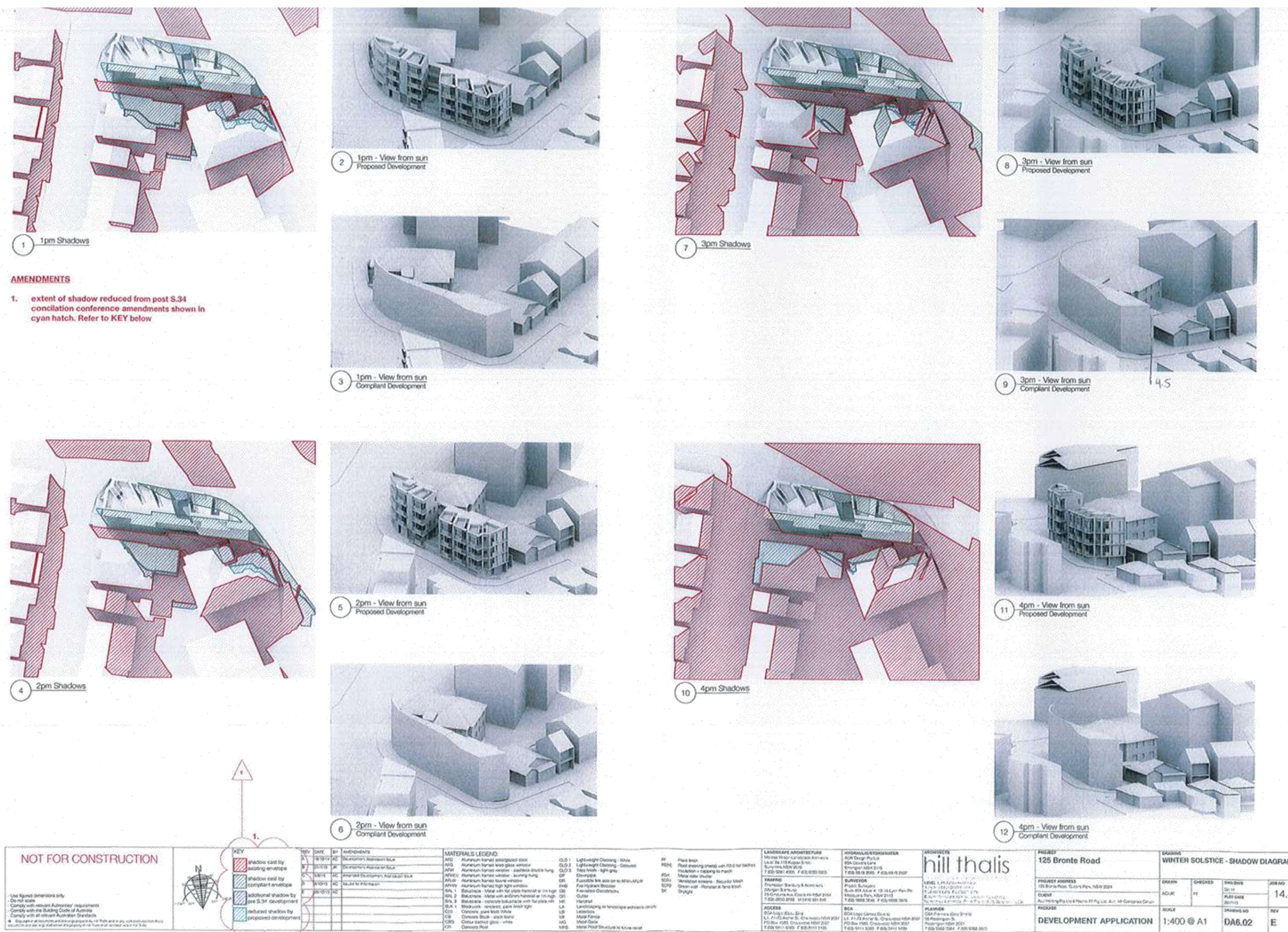


NOT FOR CONSTRUCTION		KEY	REV	DATE	BY	AMENDMENTS	MATERIALS LEGEND	CLD 1	CLD 2	CLD 3	CLD 4	CLD 5	CLD 6	CLD 7	CLD 8	CLD 9	CLD 10	CLD 11	CLD 12	CLD 13	CLD 14	CLD 15	CLD 16	CLD 17	CLD 18	CLD 19	CLD 20	CLD 21	CLD 22	CLD 23	CLD 24	CLD 25	CLD 26	CLD 27	CLD 28	CLD 29	CLD 30	CLD 31	CLD 32	CLD 33	CLD 34	CLD 35	CLD 36	CLD 37	CLD 38	CLD 39	CLD 40	CLD 41	CLD 42	CLD 43	CLD 44	CLD 45	CLD 46	CLD 47	CLD 48	CLD 49	CLD 50	CLD 51	CLD 52	CLD 53	CLD 54	CLD 55	CLD 56	CLD 57	CLD 58	CLD 59	CLD 60	CLD 61	CLD 62	CLD 63	CLD 64	CLD 65	CLD 66	CLD 67	CLD 68	CLD 69	CLD 70	CLD 71	CLD 72	CLD 73	CLD 74	CLD 75	CLD 76	CLD 77	CLD 78	CLD 79	CLD 80	CLD 81	CLD 82	CLD 83	CLD 84	CLD 85	CLD 86	CLD 87	CLD 88	CLD 89	CLD 90	CLD 91	CLD 92	CLD 93	CLD 94	CLD 95	CLD 96	CLD 97	CLD 98	CLD 99	CLD 100	CLD 101	CLD 102	CLD 103	CLD 104	CLD 105	CLD 106	CLD 107	CLD 108	CLD 109	CLD 110	CLD 111	CLD 112	CLD 113	CLD 114	CLD 115	CLD 116	CLD 117	CLD 118	CLD 119	CLD 120	CLD 121	CLD 122	CLD 123	CLD 124	CLD 125	CLD 126	CLD 127	CLD 128	CLD 129	CLD 130	CLD 131	CLD 132	CLD 133	CLD 134	CLD 135	CLD 136	CLD 137	CLD 138	CLD 139	CLD 140	CLD 141	CLD 142	CLD 143	CLD 144	CLD 145	CLD 146	CLD 147	CLD 148	CLD 149	CLD 150	CLD 151	CLD 152	CLD 153	CLD 154	CLD 155	CLD 156	CLD 157	CLD 158	CLD 159	CLD 160	CLD 161	CLD 162	CLD 163	CLD 164	CLD 165	CLD 166	CLD 167	CLD 168	CLD 169	CLD 170	CLD 171	CLD 172	CLD 173	CLD 174	CLD 175	CLD 176	CLD 177	CLD 178	CLD 179	CLD 180	CLD 181	CLD 182	CLD 183	CLD 184	CLD 185	CLD 186	CLD 187	CLD 188	CLD 189	CLD 190	CLD 191	CLD 192	CLD 193	CLD 194	CLD 195	CLD 196	CLD 197	CLD 198	CLD 199	CLD 200	CLD 201	CLD 202	CLD 203	CLD 204	CLD 205	CLD 206	CLD 207	CLD 208	CLD 209	CLD 210	CLD 211	CLD 212	CLD 213	CLD 214	CLD 215	CLD 216	CLD 217	CLD 218	CLD 219	CLD 220	CLD 221	CLD 222	CLD 223	CLD 224	CLD 225	CLD 226	CLD 227	CLD 228	CLD 229	CLD 230	CLD 231	CLD 232	CLD 233	CLD 234	CLD 235	CLD 236	CLD 237	CLD 238	CLD 239	CLD 240	CLD 241	CLD 242	CLD 243	CLD 244	CLD 245	CLD 246	CLD 247	CLD 248	CLD 249	CLD 250	CLD 251	CLD 252	CLD 253	CLD 254	CLD 255	CLD 256	CLD 257	CLD 258	CLD 259	CLD 260	CLD 261	CLD 262	CLD 263	CLD 264	CLD 265	CLD 266	CLD 267	CLD 268	CLD 269	CLD 270	CLD 271	CLD 272	CLD 273	CLD 274	CLD 275	CLD 276	CLD 277	CLD 278	CLD 279	CLD 280	CLD 281	CLD 282	CLD 283	CLD 284	CLD 285	CLD 286	CLD 287	CLD 288	CLD 289	CLD 290	CLD 291	CLD 292	CLD 293	CLD 294	CLD 295	CLD 296	CLD 297	CLD 298	CLD 299	CLD 300	CLD 301	CLD 302	CLD 303	CLD 304	CLD 305	CLD 306	CLD 307	CLD 308	CLD 309	CLD 310	CLD 311	CLD 312	CLD 313	CLD 314	CLD 315	CLD 316	CLD 317	CLD 318	CLD 319	CLD 320	CLD 321	CLD 322	CLD 323	CLD 324	CLD 325	CLD 326	CLD 327	CLD 328	CLD 329	CLD 330	CLD 331	CLD 332	CLD 333	CLD 334	CLD 335	CLD 336	CLD 337	CLD 338	CLD 339	CLD 340	CLD 341	CLD 342	CLD 343	CLD 344	CLD 345	CLD 346	CLD 347	CLD 348	CLD 349	CLD 350	CLD 351	CLD 352	CLD 353	CLD 354	CLD 355	CLD 356	CLD 357	CLD 358	CLD 359	CLD 360	CLD 361	CLD 362	CLD 363	CLD 364	CLD 365	CLD 366	CLD 367	CLD 368	CLD 369	CLD 370	CLD 371	CLD 372	CLD 373	CLD 374	CLD 375	CLD 376	CLD 377	CLD 378	CLD 379	CLD 380	CLD 381	CLD 382	CLD 383	CLD 384	CLD 385	CLD 386	CLD 387	CLD 388	CLD 389	CLD 390	CLD 391	CLD 392	CLD 393	CLD 394	CLD 395	CLD 396	CLD 397	CLD 398	CLD 399	CLD 400	CLD 401	CLD 402	CLD 403	CLD 404	CLD 405	CLD 406	CLD 407	CLD 408	CLD 409	CLD 410	CLD 411	CLD 412	CLD 413	CLD 414	CLD 415	CLD 416	CLD 417	CLD 418	CLD 419	CLD 420	CLD 421	CLD 422	CLD 423	CLD 424	CLD 425	CLD 426	CLD 427	CLD 428	CLD 429	CLD 430	CLD 431	CLD 432	CLD 433	CLD 434	CLD 435	CLD 436	CLD 437	CLD 438	CLD 439	CLD 440	CLD 441	CLD 442	CLD 443	CLD 444	CLD 445	CLD 446	CLD 447	CLD 448	CLD 449	CLD 450	CLD 451	CLD 452	CLD 453	CLD 454	CLD 455	CLD 456	CLD 457	CLD 458	CLD 459	CLD 460	CLD 461	CLD 462	CLD 463	CLD 464	CLD 465	CLD 466	CLD 467	CLD 468	CLD 469	CLD 470	CLD 471	CLD 472	CLD 473	CLD 474	CLD 475	CLD 476	CLD 477	CLD 478	CLD 479	CLD 480	CLD 481	CLD 482	CLD 483	CLD 484	CLD 485	CLD 486	CLD 487	CLD 488	CLD 489	CLD 490	CLD 491	CLD 492	CLD 493	CLD 494	CLD 495	CLD 496	CLD 497	CLD 498	CLD 499	CLD 500	CLD 501	CLD 502	CLD 503	CLD 504	CLD 505	CLD 506	CLD 507	CLD 508	CLD 509	CLD 510	CLD 511	CLD 512	CLD 513	CLD 514	CLD 515	CLD 516	CLD 517	CLD 518	CLD 519	CLD 520	CLD 521	CLD 522	CLD 523	CLD 524	CLD 525	CLD 526	CLD 527	CLD 528	CLD 529	CLD 530	CLD 531	CLD 532	CLD 533	CLD 534	CLD 535	CLD 536	CLD 537	CLD 538	CLD 539	CLD 540	CLD 541	CLD 542	CLD 543	CLD 544	CLD 545	CLD 546	CLD 547	CLD 548	CLD 549	CLD 550	CLD 551	CLD 552	CLD 553	CLD 554	CLD 555	CLD 556	CLD 557	CLD 558	CLD 559	CLD 560	CLD 561	CLD 562	CLD 563	CLD 564	CLD 565	CLD 566	CLD 567	CLD 568	CLD 569	CLD 570	CLD 571	CLD 572	CLD 573	CLD 574	CLD 575	CLD 576	CLD 577	CLD 578	CLD 579	CLD 580	CLD 581	CLD 582	CLD 583	CLD 584	CLD 585	CLD 586	CLD 587	CLD 588	CLD 589	CLD 590	CLD 591	CLD 592	CLD 593	CLD 594	CLD 595	CLD 596	CLD 597	CLD 598	CLD 599	CLD 600	CLD 601	CLD 602	CLD 603	CLD 604	CLD 605	CLD 606	CLD 607	CLD 608	CLD 609	CLD 610	CLD 611	CLD 612	CLD 613	CLD 614	CLD 615	CLD 616	CLD 617	CLD 618	CLD 619	CLD 620	CLD 621	CLD 622	CLD 623	CLD 624	CLD 625	CLD 626	CLD 627	CLD 628	CLD 629	CLD 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755	CLD 756	CLD 757	CLD 758	CLD 759	CLD 760	CLD 761	CLD 762	CLD 763	CLD 764	CLD 765	CLD 766	CLD 767	CLD 768	CLD 769	CLD 770	CLD 771	CLD 772	CLD 773	CLD 774	CLD 775	CLD 776	CLD 777	CLD 778	CLD 779	CLD 780	CLD 781	CLD 782	CLD 783	CLD 784	CLD 785	CLD 786	CLD 787	CLD 788	CLD 789	CLD 790	CLD 791	CLD 792	CLD 793	CLD 794	CLD 795	CLD 796	CLD 797	CLD 798	CLD 799	CLD 800	CLD 801	CLD 802	CLD 803	CLD 804	CLD 805	CLD 806	CLD 807	CLD 808	CLD 809	CLD 810	CLD 811	CLD 812	CLD 813	CLD 814	CLD 815	CLD 816	CLD 817	CLD 818	CLD 819	CLD 820	CLD 821	CLD 822	CLD 823	CLD 824	CLD 825	CLD 826	CLD 827	CLD 828	CLD 829	CLD 830	CLD 831	CLD 832	CLD 833	CLD 834	CLD 835	CLD 836	CLD 837	CLD 838	CLD 839	CLD 840	CLD 841	CLD 842	CLD 843	CLD 844	CLD 845	CLD 846	CLD 847	CLD 848	CLD 849	CLD 850	CLD 851	CLD 852	CLD 853	CLD 854	CLD 855	CLD 856	CLD 857	CLD 858	CLD 859	CLD 860	CLD 861	CLD 862	CLD 863	CLD 864	CLD 865	CLD 866	CLD 867	CLD 868	CLD 869	CLD 870	CLD 871	CLD 872	CLD 873	CLD 874	CLD 875	CLD 876	CLD 877	CLD 878	CLD 879	CLD 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1004	CLD 1005	CLD 1006	CLD 1007	CLD 1008	CLD 1009	CLD 1010	CLD 1011	CLD 1012	CLD 1013	CLD 1014	CLD 1015	CLD 1016	CLD 1017	CLD 1018	CLD 1019	CLD 1020	CLD 1021	CLD 1022	CLD 1023	CLD 1024	CLD 1025	CLD 1026	CLD 1027	CLD 1028	CLD 1029	CLD 1030	CLD 1031	CLD 1032	CLD 1033	CLD 1034	CLD 1035	CLD 1036	CLD 1037	CLD 1038	CLD 1039	CLD 1040	CLD 1041	CLD 1042	CLD 1043	CLD 1044	CLD 1045	CLD 1046	CLD 1047	CLD 1048	CLD 1049	CLD 1050	CLD 1051	CLD 1052	CLD 1053	CLD 1054	CLD 1055	CLD 1056	CLD 1057	CLD 1058	CLD 1059	CLD 1060	CLD 1061	CLD 1062	CLD 1063	CLD 1064	CLD 1065	CLD 1066	CLD 1067	CLD 1068	CLD 1069	CLD 1070	CLD 1071	CLD 1072	CLD 1073	CLD 1074	CLD 1075	CLD 1076	CLD 1077	CLD 1078	CLD 1079	CLD 1080	CLD 1081	CLD 1082	CLD 1083	CLD 1084	CLD 1085	CLD 1086	CLD 1087	CLD 1088	CLD 1089	CLD 1090	CLD 1091	CLD 1092	CLD 1093	CLD 1094	CLD 1095	CLD 1096	CLD 1097	CLD 1098	CLD 1099	CLD 1100	CLD 1101	CLD 1102	CLD 1103	CLD 1104	CLD 1105	CLD 1106	CLD 1107	CLD 1108	CLD 1109	CLD 1110	CLD 1111	CLD 1112	CLD 1113	CLD 1114	CLD 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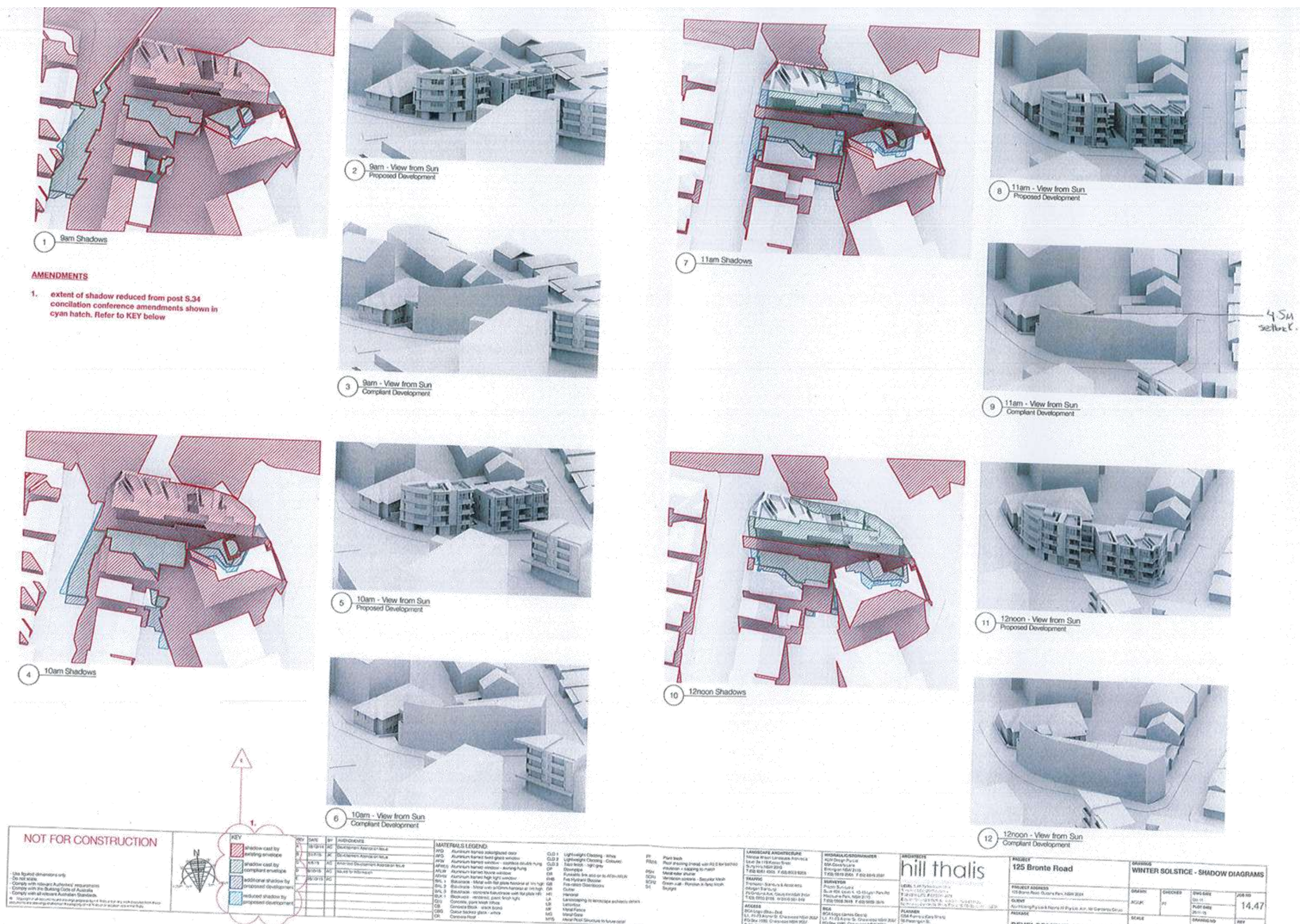






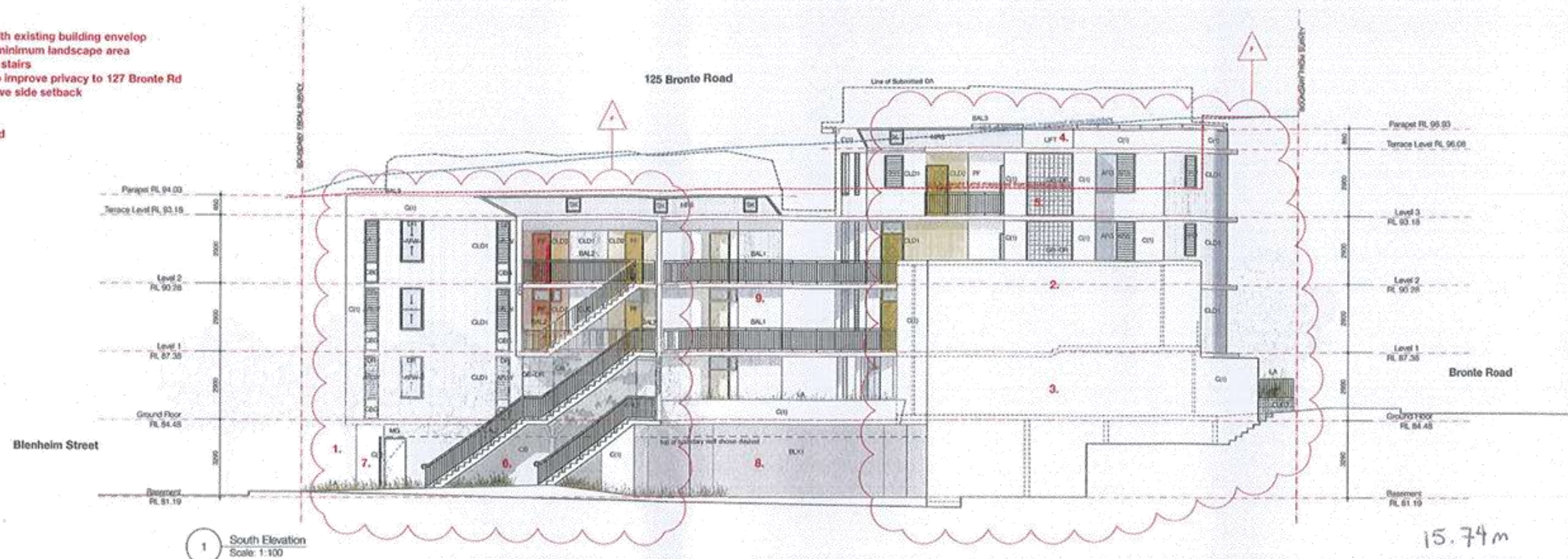






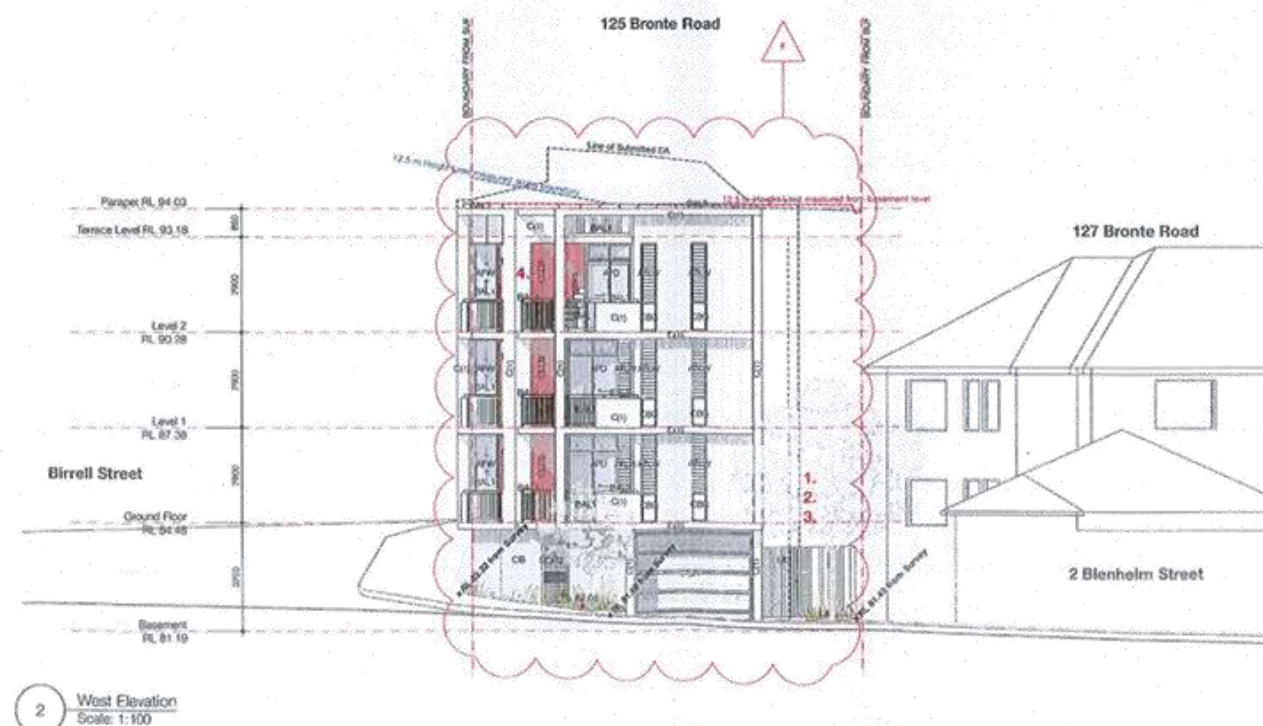


1. 3m southern side setback
2. South-eastern bulk reduced; compliant with existing building envelop
3. keeper unit at ground level with required minimum landscape area
4. Relocation of lift and eastern fire-isolated stairs
5. obscured glass block wall at lift landing to improve privacy to 127 Bronte Rd
6. Consolidated western staircases to improve side setback
7. Increased landscape deep soil area
8. reconfigured basement layout
9. planted screenwall along corridor removed



15.74m  
Basement to  
parapet.

1. 3m southern side setback
2. Consolidated western staircases to improve side setback
3. Increased landscape deep soil area
4. western unit reconfigured; balcony oriented north



Land & Environment Court  
**APPROVED**

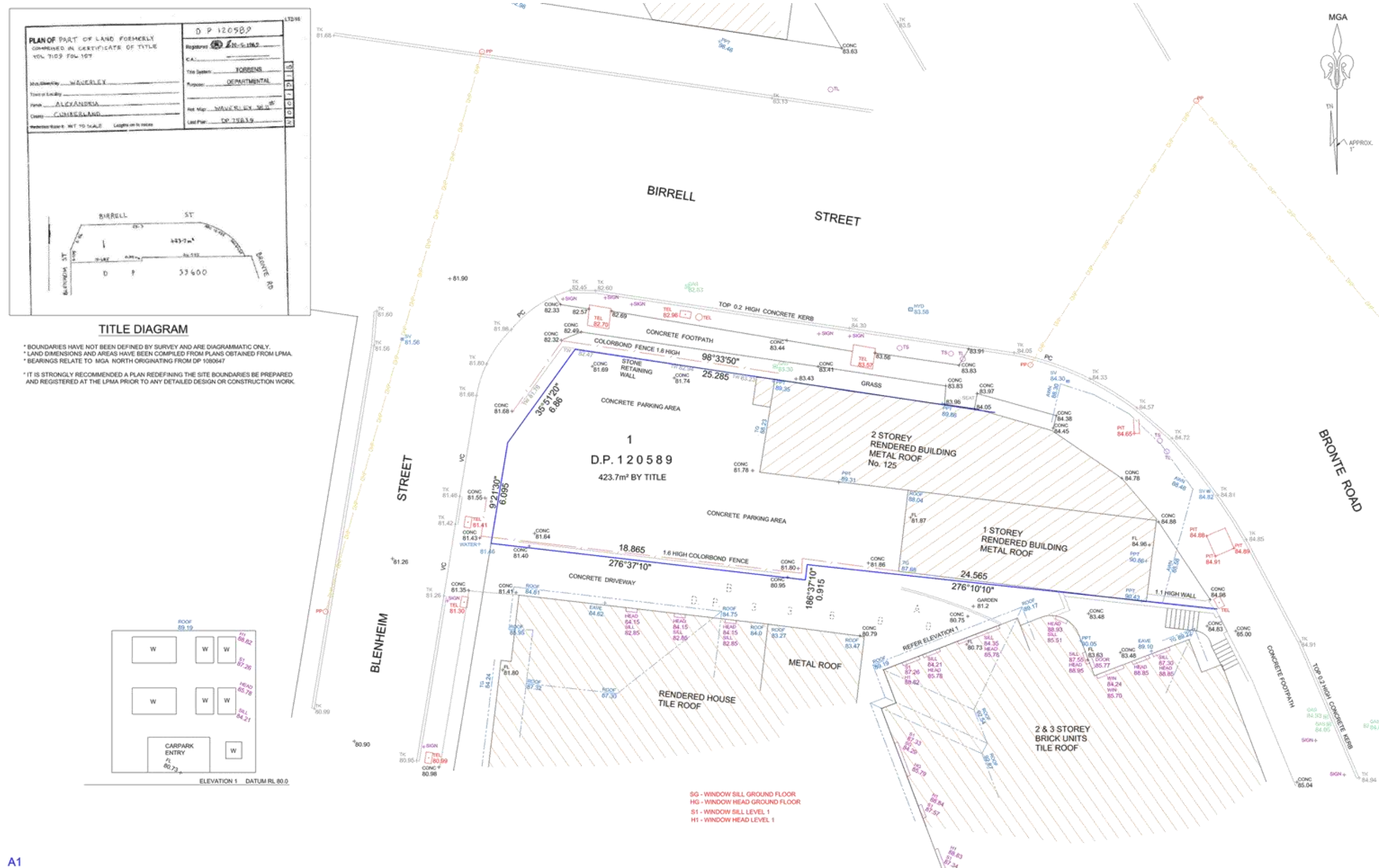
Development Consent

These plans/specifications form part of the Land & Environment Court judgement and Conditions of Consent to development application No. 629/2014 and are subject to any of those Land & Environment Court Conditions of Consent

appreciated /  
experience

[illegible]





A1

## NOTES:

- \* BOUNDARIES HAVE NOT BEEN DEFINED BY SURVEY AND ARE DIAGRAMMATIC ONLY.
- \* LAND DIMENSIONS AND AREAS HAVE BEEN COMPILED FROM PLANS OBTAINED FROM LPMA.
- \* BEARINGS RELATE TO MGA NORTH ORIGINATING FROM DP 1080647
- \* LEVEL DATUM IS AND ORIGINATING FROM SSM 70767 RL 82.843 LOCATED ON BRONTE ROAD
- \* VISIBLE, ACCESSIBLE SERVICES ONLY HAVE BEEN LOCATED.
- \* THE EXISTENCE OF UNDERGROUND SERVICES HAS NOT BEEN INVESTIGATED. IF THESE ARE CRITICAL IT IS ESSENTIAL THE APPROPRIATE AUTHORITIES BE NOTIFIED AND THE SERVICES LOCATED BY UNDERGROUND DETECTION METHODS OR POT-HOLES AND THEN SURVEYED.
- \* NEIGHBOURING HOUSES, WINDOWS AND ROOF POSITIONS ARE APPROXIMATE ONLY.
- \* FLOOR LEVELS GENERALLY SURVEYED AT DOOR THRESHOLDS. INTERNAL ROOMS NOT SURVEYED.
- \* CONTOURS SHOWN ARE INDICATIVE OF LAND FORM. SPOT LEVELS SHOULD TAKE PRECEDENCE.
- \* REFER TO FACE OF PLAN FOR SUBJECT TITLE NOTATIONS.
- \* THIS TITLEBLOCK IS AN INTEGRAL PART OF THIS DRAWING AND SHOULD NOT BE REMOVED.

## LEGEND

- SV - STOP VALVE
- HYD - HYDRANT
- WM - WATER METER
- SP - SIGN POST
- PC - PRAM CROSSING
- VC - VEHICLE CROSSING
- L - STORMWATER LINTEL
- MH - MANHOLE
- SW - STORMWATER
- PP - POWER POLE
- ELP - ELECTRIC LIGHT POLE
- TEL - TELSTRA PIT
- SIP - SEWER INSPECTION POINT
- SMH - SEWER MANHOLE



REV	AMENDMENTS	DATE
B	EXTRA DETAIL	16/10/14
A	ADJ WINDOWS AND BUILDINGS TO NORTH	30/09/14

CLIENT: CERRUTTI

PLAN OF: 125 BRONTE ROAD BONDJUNCTION

BEING: LOT 1 IN DP 120589

SHOWING: GENERAL DETAIL AND SITE LEVELS

PURPOSE: ARCHITECTURAL DESIGN COUNCIL SUBMISSION

**SHEET 1 OF 2**

SCALE 1:100

0 1 2 3 4 5 6 7 8 9 10

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SURVEYOR: SD

DATUM: AUSTRALIAN HEIGHT DATUM

JOB REF: B1723

DRAWING No: B1723-1

DATE OF SURVEY: AUGUST 2014

REDUCTION RATIO: 1:100

**MACQUARIE PARK**

PO Box 4004  
MACQUARIE CENTRE NSW 2113

SUITE 404, LEVEL 4  
No 13-15 LYON PARK ROAD,  
MACQUARIE PARK NSW 2113

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INCORPORATING GARVIN MORGAN PTY LTD

**PROJECT SURVEYORS**

Professional Innovative... Results.

**REPORT**  
**TC/V.01/17.03**

**Subject:** Ocean Street, Bondi - Works Zone

**TRIM No.:** A03/2514-04

**Author:** Sam Samadian, Traffic Engineer

**Director:** Emily Scott, Director, Waverley Renewal

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**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs an 18m long "No Parking 7am-5pm Monday-Friday 8am-3pm Saturday Authorised Council Vehicles Excepted" zone across the frontage of 21-23 Ocean Street North, subject to there being no blockage to through traffic on Ocean Street other than for short periods of time when manoeuvring vehicles into and out of the 'Construction Zone'.
2. Delegate's authority to the Executive Manager, Creating Waverley, to extend the duration of the construction zone, as necessary.

**1. Executive Summary**

Council has received an application from the builder/developer at No.21-23 Ocean Street, Bondi for the installation of a construction zone across the site frontage. Council will need to exercise its delegated function to implement the proposal.

**2. Introduction/Background**

In accordance with standard practice in Waverley Council, it is proposed that the zone be signposted "No Parking Council Authorised Vehicles Excepted" for the approved construction hours under the development consent. Council will then issue the applicant with a number of transferable permits to be used on their construction-related vehicles.

The Traffic Committee and Council generally only approve construction zones at the same length as the frontage, regardless of the location of driveways, for a minimum of thirteen weeks.



### 3. Technical Analysis

Table 1 21-23 Ocean Street, Bondi

<b>Applicant</b>	Combined Australia Pty Ltd
<b>Development Application</b>	DA-287/2013
<b>Works</b>	Partial demolition of the rear of existing dwellings including alterations and additions and erection of a 9 storey residential flat building to the rear comprising 17 units and subdivision.
<b>Approved Hours of Construction</b>	7am-5pm Mon-Fri, 8am-3pm Saturday
<b>Frontage Length</b>	18 m
<b>Road</b>	Ocean Street
<b>Existing Parking</b>	Unrestricted and parallel outside the site
<b>Requested Length</b>	18 m
<b>Proposed Length</b>	18 m
<b>Duration</b>	13 weeks
<b>Beginning</b>	April
<b>Fee Area</b>	Residential, without metered parking
<b>Estimated Fees</b>	\$1044 per week

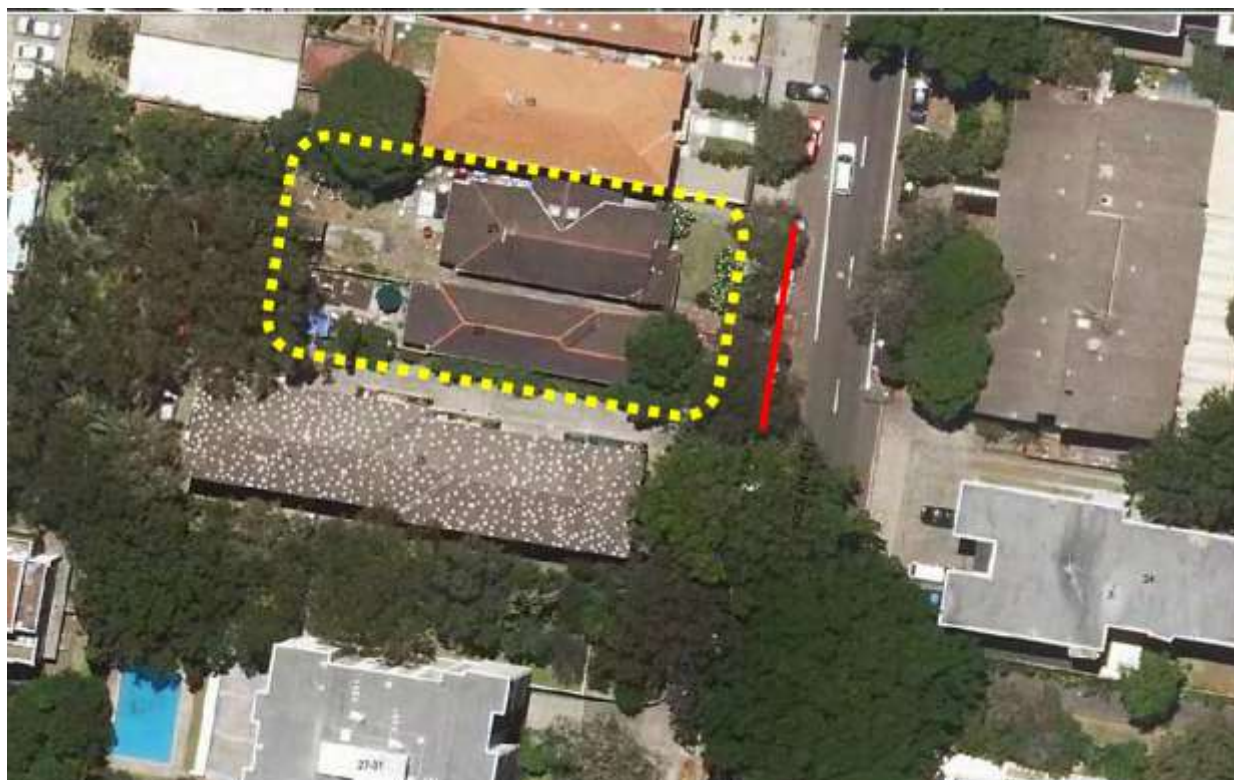


Figure1 Aerial photograph of 21-23 Ocean Street, Bondi



***Two single storey dwellings on left of image at 21-23 Ocean Street North***

#### **4. Financial Information for Council's Consideration**

The applicant will be required to pay the approved application fee, which covers the cost of administration and signage.

The approved usage charges for the 2016/17 financial year are:

- \$58 per metre per week in a residential developments
- \$83per metre per week in a commercial & mixed-use developments

#### **5. Attachments:**

Nil

## REPORT

### TC/V.02/17.03



**Subject:** Ocean Street North, Bondi - No Parking Zone

**TRIM No.:** A02/0637-02

**Author:** Geoff Garnsey, Manager Traffic and Development

**Director:** Emily Scott, Director, Waverley Renewal

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#### COUNCIL OFFICER'S PROPOSAL:

That Council installs "No Parking" signs to align with the driveway lines at the northern driveway to 27-31 Ocean Street North, Bondi.

#### 1. Executive Summary

Council has received a request from a resident/representative of the Body Corporate at the unit block at 27-31 Ocean Street North, Bondi, for consideration to be given to the installation of "No Parking" signs on either side of the second and northernmost driveway to the property to improve driver sightlines when exiting the block. The resident informs drivers of vehicles frequently park beyond white driveway lines installed either side of the driveway. Council will be required to exercise its delegated function to implement the proposal.



***Aerial image showing 27-31 Ocean Street North, Bondi***

#### 2. Introduction/Background



Driveway lines were installed at the northernmost driveway some years ago in an attempt to keep vehicles parked at an angle to the roadway sufficiently clear of the driveway in order to improve driver sightlines when exiting the property. Drivers frequently park beyond the lines resulting in residents of the unit block when exiting the property to the north having to drive well into the southbound lanes on Ocean Street. The southernmost driveway to the property has “No Parking” signs across it when similar problems arose in the past.

### 3. Technical Analysis

Property 27-31 Ocean Street North, Bondi is located on the western side of Ocean Street North some 200m north of Bondi Road. The unit block is large, containing forty (40) units.

The property has two vehicular access points with one driveway adjacent to the southern boundary and a second some 12m to the north serving the main basement car park area.

The southernmost driveway has driveway lines as well as “No Parking” signs with the latter being installed following drivers frequently parking cars beyond the driveway lines



***Southern Driveway at 27 Ocean Street with driveway lines and “No Parking” signs***

The driveway lines at the southern and northern driveway have been installed beyond the driveway wings in excess of the standard in order to aid left (north) turns onto Ocean Street. The street has a reduced travel lane width due to the presence of angled parking on the western side in the vicinity of No. 27-31. Installing driveway lines at the standard point at the driveway wings would result in drivers exiting the property to the left (north) having to travel well into the southbound lane.

Having regard to there being “No Parking” signs at the southern driveway to 27-31 as well as at another driveway immediately to the south outside the unit block at 33-37 and at properties to the north, it is recommended in this instance that “No Parking” signs be installed at the northern driveway to No. 27-31.



***Images of vehicles parking beyond driveway lines at northern driveway to 27 Ocean Street North.***



***Vehicle parked well beyond white edge line located on its left***

**4. Financial Information for Council's Consideration**

The installation of the new signs and adjustment to existing signs can be carried out from existing budgets.

**5. Attachments:**

Nil



## REPORT



**Subject:** Anzac Day - Ramsgate Avenue and Campbell Parade,  
North Bondi – Temporary Road Closure

**TRIM No.:** A02/0216

**Author:** Sam Samadian, Traffic Engineer

**Director:** Emily Scott, Director, Waverley Renewal

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### COUNCIL OFFICER'S PROPOSAL:

That Council approves the Transport Management Plan for the temporary closure of Ramsgate Avenue and Campbell Parade, North Bondi on 25 April 2017 between 4am and 8am as per the submitted Transport Control Plans (TCP's), subject to the following:

1. The applicant undertakes the following:
  - a) Submits the Transport Management Plan to the Roads and Maritime Services for the approval of the Traffic Management Centre.
  - b) Provides Public Liability Insurance for the event.
  - c) Obtains NSW Police Force approval and assessment of the event classification.
  - d) Notifies the State Transit Authority, NSW Ambulance Service and NSW Fire & Rescue (Bondi, Woollahra and Randwick Fire Stations) not less than seven (7) days prior to the event.
  - e) Notifies local residents and businesses at least seven (7) days prior to the event.
  - f) Uses only RMS-accredited Traffic Controllers to regulate traffic.
  - g) Covers all costs associated with traffic control.
  - h) Submits a copy of the approved R.O.L to the Executive Manager, Creating Waverley prior to the event taking place.
2. The Executive Manager, Creating Waverley or his representatives being delegated authority to audit the TMP and cancel the approval if required.

### 1. Executive Summary

Council has received an application on behalf of North Bondi RSL Club requesting consideration be given to approving the temporary closure of Ramsgate Avenue and Campbell Parade, North Bondi in order to hold an ANZAC Day March and Dawn Service on Tuesday 25 April, 2017. Council will need to exercise its delegated functions to implement the proposal.

### 2. Introduction/Background

The only change from previous years is to extend the temporary closure to Campbell Parade between Brighton Boulevard and Queen Elizabeth Drive, Bondi as detailed on the attached TMP. This change has been requested following representations made to Council by NSW Police to minimise the localised congestion and improve the security of crowds attending the event.

This is considered to be a class 2 Special Event in accordance with the RMS's *Guide to Traffic and Transport Management for Special Events*.

This event will require an approval from the Transport Management Centre (TMC) as a temporary full closure is proposed. The applicant will be advised to contact the TMC to obtain the required approval.

Once all approvals have been received and all conditions have been met, Council's Executive Manager, Creating Waverley can issue a signed copy of the TMP for implementation.

### 3. Technical Analysis

The attached TMP proposes the event be undertaken in the manner detailed in *Table 1*, below.

*Table 1 Proposed closure schedule*

Date	Time	Activity
Monday 25 April 2017	4.00 am	Implement temporary closure of Ramsgate Avenue and Campbell Parade. Access for emergency vehicles and pedestrians to remain.
	5.00 am	Participants to form up in Ramsgate Avenue.
	5.30 am	Participants & spectators assemble in Ramsgate Avenue, adjacent to North Bondi War Memorial.
	6.00 am	Dawn Service & Wreath Laying Ceremony commences.
	6.45 am	Participants & spectators invited to RSL Club
	8.00 am	Ramsgate Avenue and Campbell parade opens and all normal traffic conditions to resume.

Guidelines used:

- [NSW] *Road Rules 2014*
- Australian Standard AS 1742.3
- RMS's *Traffic Control at Work Sites*
- RMS's *Guide to Traffic and Transport Management for Special Events*

### 4. Financial Information for Council's Consideration

The applicant will be required to meet the cost of all traffic control, rubbish removal and damage restoration.

### 5. Attachments:

1. Anzac Day- North Bondi RSL 2017- TMP

## **TRAFFIC MANAGEMENT PLAN**



**NORTH BONDI RSL SUB-BRANCH**

**ANZAC DAY COMMEMORATION**

**25 April 2017**



**TMP Details**

Event Date: **Tuesday 25<sup>th</sup> April 2017**

Event Start Time: **0600 Hrs**

Event Finish Time: **0700 Hrs**

Location: **Ramsgate Avenue, North Bondi**

Additional Closures: **Campbell Parade, North Bondi**  
**Between Brighton Boulevard & Queen Elizabeth Drive**

Parking: **Queen Elizabeth Drive, Bondi Beach**

## 1 EVENT DETAILS

### 1.1 Event Summary

**Event Organiser:** North Bondi RSL Sub-Branch

**TMP Version:** 01

**Issue Date:** 14 March 2017

**Document Author:** Sharon Haines  
PWZTMP #0033639570  
Expiry Date: 23 August 2019

#### **This Traffic Management Plan is approved by:**

New South Wales Police: Date:

Roads & Maritime Services: Date:

Waverly Council: Date:

#### **Authority of the TMP**

This Traffic Management Plan (TMP) when approved by the relevant authorities becomes the prime document detailing the traffic, transport and pedestrian arrangements under which the 2017 ANZAC Day Dawn Service operate.

In case of emergencies, or for the management of incidents, the NSW Police is not subject to the conditions of this TMP but should endeavour to inform other agencies of the nature of the incident and the Police response.

### 1.2 Contact Names

#### **North Bondi RSL Sub-Branch**

Kate Cass  
Event Manager  
Mobile: 0414 615 807  
Email: [kate@tobrukhouse.com](mailto:kate@tobrukhouse.com)

#### **North Bondi RSL Sub-Branch**

David Sims  
Event Manager  
Mobile: 0448 727 882  
Email: [simsdss@gmail.com](mailto:simsdss@gmail.com)





**2.3 Police Approval**

Waiting on written police approval

**2.3 Fire Brigades & Ambulance**

See Appendix D

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**3 TRAFFIC AND TRANSPORT MANAGEMENT****3.1 Route & Location**

See Appendix E

**3.2 Parking**

See Appendix E

**3.3 Construction, traffic calming and traffic generating developments**

There are no construction activities, traffic calming devices or traffic-generating developments at the location/route or on the detour routes.

**3.4 Trusts, authorities or Government enterprises**

This event does not use a facility managed by a trust, authority or enterprise.

**3.5 Impact on/of Public Transport**

Public Transport will be impacted as a result of the event and the impact of this has been reduced through consultation with Sydney Buses.

**3.6 Reopening roads after moving events**

This is a non-moving event.

**3.7 Traffic management requirements unique to this event**

See Appendix E

**3.8 Contingency plans**

Nil

**3.9 Heavy Vehicle Impacts**

This event does not impact heavy vehicles

**3.10 Special Event Clearways**

Special event clearways not required

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#### **4 MINIMISING IMPACT ON NON-EVENT COMMUNITY & EMERGENCY SERVICES**

##### **4.1 Access for local residents, businesses, hospitals and emergency vehicles**

Plans to minimise impact on non-event community attached

##### **4.2 Advertise traffic management arrangements**

Road Closures or Restrictions – advertising medium and copy of proposed advertisements attached

##### **4.3 Special Event Warning Signs**

Special Event information signs are described in the Traffic Control Plan/s

##### **4.4 Permanent Variable Message Signs**

This event does not use permanent Variable Message Signs

##### **4.5 Portable Variable Message Signs**

The proposed messages and locations for portable VMS are attached

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#### **5 PRIVACY NOTICE**

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The "Personal Information" contained in the completed Transport Management Plan may be collected and held by the NSW Police, the NSW Roads and Traffic Authority (RTA), or Local Government.

I declare that the details in this application are true and complete. I understand that:

- The "personal information" is being collected for submission of the Transport Management Plan for the event described in Section 1 of this document.
- I must supply the information under the Road Transport Legislation (as defined in the *Road Transport (General) Act 1999*) and the *Roads Act 1993*.
- Failure to supply full details and to sign or confirm this declaration can result in the event not proceeding.
- The "personal information" being supplied is either my own or I have the approval of the person concerned to provide his/her "personal information".
- The "personal information" held by the Police, RTA or Local Government may be disclosed inside and outside of NSW to event managers or any other person or organisation required to manage or provide resources required to conduct the event or to any business, road user or resident who may be impacted by the event.
- The person to whom the "personal information" relates has a right to access or correct it in accordance with the provisions of the relevant privacy legislation.

- The person to whom the "personal information" relates has a right to access or correct it in accordance with the provisions of the relevant privacy legislation.

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**6 APPROVAL**

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TMP Approved by:  Event Organiser 9.3.17 Date

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**7 AUTHORISATION TO \*REGULATE TRAFFIC**

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Council's traffic management requirements have been met. Regulation of traffic is therefore authorised for all non-classified roads described in the risk management plans attached to this TMP.

Regulation of traffic authorised by: \_\_\_\_\_ Council \_\_\_\_\_ Date

The RTA's traffic management requirements have been met. Regulation of traffic is therefore authorised for all classified roads described in the risk management plans attached to this TMP.

Regulation of traffic authorised by: \_\_\_\_\_ RTA \_\_\_\_\_ Date

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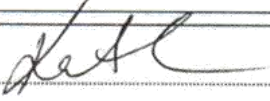
\* 'Regulate traffic' means restrict or prohibit the passage along a road of persons, vehicles or animals (Roads Act, 1993). Council and RTA require traffic to be regulated as described in the risk management plans with the layouts installed under the direction of a qualified person.



**Schedule I Form - Notice of Intention to Hold a Public Assembly****SUMMARY OFFENCES ACT 1988 - Sec 23**

To the Commissioner of Police


1	<p>I, <u>Kate Cass</u> ..... (name) of <u>118-120 Ramsgate Ave, North Bondi, NSW, 2026</u> ..... (address) on behalf of <u>North Bondi RSL Sub-Branch</u> ..... (organisation) notify the Commissioner of Police that on the <u>25th</u> (day) of <u>April</u> (month), <u>2017</u> (year), it is intended to hold  <i>either:</i>  (a) a public assembly, not being a procession, of approximately <u>10,000</u> (number) persons, which will assemble at <u>North Bondi RSL Memorial</u> ..... (Place) at approximately <u>6</u> am/pm, and disperse at approximately <u>7</u> am/pm  <i>or</i>  (b) a public assembly, being a procession of approximately ..... (number) persons, which will assemble at approximately ..... am/pm, and at approximately ..... am/pm the procession will commence and shall proceed  ..... ..... <i>(Specify route, any stopping places and the approximate duration of any stop; and the approximate time of termination. A diagram may be attached.)</i></p>
2	<p>The purpose of the proposed assembly is <u>to conduct the annual North Bondi RSL Sub-Branch ANZAC Day Service</u> ..... ..... ..... ..... ..... .....</p>

3	<p>The following special characteristics associated with the assembly would be useful for the Commissioner of Police to be aware of in regulating the flow of traffic or in regulating the assembly <i>(strike out whichever is not applicable)</i>:</p> <p>(i) There will be <u>up to 14</u> (number) of vehicles and/or <u>0</u> (number) of floats involved.</p> <p>The type and dimensions are as follows:  <u>1 x Bus, 2 x Garbage Trucks, 2 x Police cars, 3 x Traffic Control Vehicles, 4 x Sedans</u>  <u>1 x Ambulance, 1 x Additional Stationary Truck</u></p> <p>(ii) There will be <u>3</u> (number) of bands, musicians, entertainers, etc. which will entertain or address the assembly.</p> <p>(iii) The following number and type of animals will be involved in the assembly:  <u>NIL</u></p> <p>(iv) Other special characteristics of the proposed assembly are as follows:  <u>NIL</u></p>
4	<p>I take responsibility for organising and conducting the proposed assembly.</p>
5	<p>Notices for the purposes of the <i>Summary Offences Act 1988</i> may be served upon me at the following address:</p> <p><u>118-120 Ramsgate Ave,</u>  <u>North Bondi, NSW,</u>  <u>Postcode. 2026</u>  <u>Telephone No. 9365 1896</u></p>
6	<p>Signed <u></u></p> <p>Capacity/Title <u>AZNAC Day Event Manager - North Bondi RSL Sub-Branch</u></p> <p>Date <u>08 March 2017</u></p>

## **APPENDIX A**

### **Traffic Management Safe Work Method Statement**



Date SWMS developed: 20-01-2017 SWMS designed by: Daniel Muller  Signature: _____ Managed by: Daniel Muller  Signature: _____ SWMS Number 20/01 SWMS Revision number 02 Supervisory staff responsible for inducting & training personnel in SWMS 01: Daniel Muller, Method of training: In House delivery.	Principle Contractor: North Bondi RSL Sub-Branch  Project Name: ANZAC Day Commemoration  Project Number: 2017  Project Location: Ramsgate Ave, North Bondi	Unit 1B/7 PIONEER DRIVE, BELLAMBI, 2518 Office: (02) 4285 3900 Mobile 0410 981 798 accounts@sstc.com.au   ABN 87 152 059 820
SUPERVISOR	LOCATION:	
DESCRIPTION OF WORKS	Traffic Control	
Hierarchy of Control:- Eliminate, Substitute, Isolate, Engineering, Administration, PPE		
<b>Training/Qualification requirements for personnel</b>		
General WHS construction induction certificate and site induction. T/C's to have "traffic controller" card (Blue) & "Implement Traffic Control Plans" (Yellow) Ticket. T/L to have "Prepare Work Zone Traffic Management Plans" (Red&Orange) Ticket First Aider to be onsite	WHS ACT 2011 WHS REGULATION 2011 AS 1742.3-2009 Traffic control for works on roads RTA Traffic control at work sites version 4.0 Manual Handling	<b>Relevant documentation, legislation, standards and codes</b> Traffic control at worksites codes of practice Risk Assessment
<b>Personal Protective Requirements</b>		
Minimum PPE requirements: High VIS long sleeve shirt with reflective stripes/ High VIS vest with reflective stripes. Long pants Hard hat with wide brim. Steel cap safety boots. Australian standards approved safety glasses/gloves	<b>Plant Required</b> STOP/SLOW bats Traffic control vehicles TMA Traffic cones Traffic control signs and other devices. Two-way radios.	<b>Corrective Actions</b> Task observations, Daily Risk Assessment, Audits, Pre-Start talk, ToolBox Meetings, and Inspections By Team Leader.
<b>Personnel consulted for SWMS development.</b> Daniel Muller, Allan Smith, Sharon Haines. Andrew Traill, Simon Stevens		
<b>Maintenance</b> Visual inspections/ongoing checks per TCP		

## RISK MATRIX

LIKELIHOOD (L)		CONSEQUENCE (C)			RISK RATING (R)					RISK RATING - SUMMARY
		1	2	3	4	5	1	2	3	
A	Almost Certain Expected to be the most likely outcome.	1	2	3	4	5	A	3	4	4
B	Likely Not unusual perhaps 50-50 chance.	2	3	4	5	6	B	2	3	4
C	Possible Might occur at some time.	3	4	5	6	7	C	1	2	3
D	Unlikely Could occur, but would not be expected.	4	5	6	7	8	D	1	2	3
E	Rare Rare that this would occur. No previous occurrence in similar circumstances.	5	6	7	8	9	E	1	2	3

**RISK TREATMENT OPTIONS**

**Best** • Eliminate the risk (e.g. design changes).  
 • Reduce the likelihood of the risk occurrence (e.g. substitute an engineered control).  
 • Transfer the risk (e.g. to operation & maintenance).  
 • Accept and manage the risk (e.g. operational procedure and training).

**Worst**

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
1.	-Traffic Controllers arriving for a shift.  -Conducting prestart checks on vehicles, job packs, equipment etc.	-Traffic controllers turning up for a shift without correct PPE and tickets.	2	-Traffic Controllers to arrive with correct PPE including yellow High VIS vest, white High VIS night overalls, steel cap boots, safety glasses, wet weather gear, night wand and Hard hat if required. -Traffic controllers to ensure they are carrying their tickets on them at all times.	1	Supervisor  Team Leader  All PCB
		-Staff presenting to work fatigued.	4	-Allocator to ensure employees have suitable breaks between shifts (10 hours). -Team leader to question all team members about fatigue prior to leaving the depot. -Team members who have not had a 10 hour break but are still within acceptable fatigue levels are to be monitored by the Team leader at hourly intervals. These checks are to be documented. -Any person who is fatigued will be replaced for that shift.	3	Allocator  Supervisor  Team Leader  All PCB
		-Radios not working or charged.	2	-Traffic controllers to ensure their radio is in working order and fully charged the night before a shift. -If a traffic controller discovers that their radio is defective when rostered for a shift, they should contact management immediately.	1	Supervisor  Team Leader  All PCB



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
1. Cont'	-Traffic Controllers arriving for a shift. -Conducting prestart checks on vehicles, job packs, equipment etc.	-Job packs or Kit bags not available. -No Plan or missing information.	2	-Allocator to ensure job packs are ready prior to the start of the shift. -Allocator to ensure job packs contain all relevant permits and documentation. -Team Leader to ensure Kit bag has all relevant docket books, SWMS, TC@WS manual. -Replenish any missing items from stock in the shed. -Contact management if something is missing from your Job pack or Kit bag that cannot be replenished from the shed.	1	Allocator Team Leader
		-Leaving the depot with incorrect plans, information or equipment (including faulty equipment or vehicle). -Site setup cannot be implemented as per the TCP or not enough equipment to make the site safe. -Manual handling injuries from handling equipment or loading/unloading vehicle.	2	-Team Leaders to ensure they have the correct Job pack allocated to them, their vehicle and the team they have been provided. -Ensure correct signage and equipment is on or in the vehicle. -Ensure that all signage and equipment is in good working order. -Replace defective equipment. -Conduct vehicle inspection prior to leaving the depot and complete vehicle inspection docket. -Always use safe manual handling techniques. -Ensure you assess the load and request assistance if required.	1	Allocator Supervisor Team Leader All PCB

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
1. Cont'	-Traffic Controllers arriving for a shift.	-Arrow Board, VMS or flashing beacons not working	4	-Check the accessory battery is connected and in working order. -Check that no leads or plugs have been disconnected. -Contact Management to discuss a replacement vehicle	1	Supervisor Team Leader
	-Conducting prestart checks on vehicles, job packs, equipment etc.					
2.	-Drive to Job Site	-Signs and equipment falling from vehicle causing damage to other vehicles or injury or death to pedestrians	3	-Ensure all equipment has been well secured to the vehicle. -Team Leader to conduct final check on secured equipment prior to leaving the depot.	1	Supervisor Team Leader All PCB
		-Motor Vehicle Accident resulting in damage, injury or death.	4	-All drivers must have a valid Driver's Licence to a class suitable for the vehicle they will be driving. -All road rules MUST be obeyed. -All occupants of the vehicle MUST be wearing a properly fitted seatbelt. -Drivers are not to be under the influence of alcohol or drugs. -Drivers must not be fatigued.	1	Supervisor Team Leader Drivers All PCB
		-Arrow Board, VMS or Light bar detach from vehicle causing accident resulting in damage, injury or death	4	-Ensure that attachments are secure prior to leaving the depot. -Maintain a suitable speed for the vehicle (and attachments) that you are driving. -Listen/feel for unusual noises, rattles or shakes that could indicate a loose vehicle attachment.	1	Supervisor Team Leader Drivers All PCB

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
2. Cont'	-Drive to Job Site	-Hazardous weather conditions including heavy rain or fog, strong winds, poor light.	4	-Drivers must always drive to the weather and road conditions. -Reduce speed where necessary. -Use Headlights -Use Hazard lights if required. -Pull off the road if the conditions worsen. -Contact Management if the weather conditions are going to delay your arrival to site.	2	Supervisor Team Leader Drivers All PCB
		-Work in Fire Zones or in extreme weather conditions that may increase the risk of fire	4	-Stay alert to the environment and conditions that you are working in. -Be aware of changes to that environment including the smell of smoke or reduced visibility due to smoke in the area. -Contact Team Leader if these issues arise.	2	Supervisor Team Leader All PCB
		-Fire Hazard due to smoking or the incorrect disposal of cigarette butts.	4	-Smoking is not permitted in company vehicles. -All cigarette butts are to be extinguished in full prior to disposal. -Cigarette butts are to be disposed of in an appropriate disposal receptacle ie. Butt Bin, Garbage Bin. -DO NOT throw cigarette butts into the ground or into the bush. THIS IS LITTERING and poor representation of Stop Slow.	2	All PCB



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
3.	-Arrive at Job site	-Impact with pedestrian or another vehicle.  -Parking or pulling into Job site causes traffic congestion.  -Slips, trips or falls when exiting the vehicle.	4	-Use correct indicators and lights when pulling off the road and/or parking vehicles. -Use a spotter where possible. -Ensure parked vehicles don't overhang into live traffic lanes. -Park in designated parking areas. -Do Not park on private property. -Do Not park in NO GO ZONES or areas deemed to be unsafe. -Check that ground is stable before stepping out of the vehicle.	1	Supervisor  Team Leader  Drivers  All PCB
	-Team Leader to meet with site contact to confirm scope of works and conduct Toolbox and Site Risk Assessment.	-Traffic controllers not briefed on the scope of the works. -Traffic controllers not visible on site.  -Commercial/residential driveways  -Work location  -Road condition  -Post speed  -Difficult sites  -Exposure to sun light	4	-Team Leader to meet with site contact to confirm scope of works. -All Traffic Controllers must be site inducted. -Team Leader to conduct toolbox and brief all Traffic Controllers on their individual roles & responsibilities. -Identify/record/communicate all hazards & risks. -All employees must read & sign off on SWMS. -Nominate emergency meeting point. -Nominate vehicle movement spotter. -Conduct daily pre-start checklist. -Wear High VIS yellow vest, safety glasses, hats/visor.	1	Supervisor  Team Leader  All PCB

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
3. Cont'	-Team Leader to meet with site contact to confirm scope of works and conduct Toolbox and Site Risk Assessment.	-Traffic Controllers/Clients/Contractors not aware of risks/hazards associated with the scope of works and no controls implemented.	4	-Team Leader to ensure client understands 'communicating all movements' rules. Clients needs to understand that Stop Slow will not be held accountable for incidents arising from uncommunicated vehicle movements. -Noncompliance to communicate vehicle movements will be reported to management as a near miss.	1	Supervisor Team Leader All PCB
4.	-Setting up signs & devices specified by TCP	-Traffic Controllers set up site without approval resulting in unnecessary traffic congestion and increased risk on a traffic incident occurring	4	-Ensure approvals have been received and are included in the Job pack. -Ensure times and conditions on ROLs and other approvals are adhered to.	1	Allocator Supervisor Team Leader
		-Motor Vehicle Accident resulting in damage, injury or death.	4	-All drivers must have a valid Driver's Licence to a class suitable for the vehicle they will be driving. -All road rules MUST be obeyed. -All occupants of the vehicle MUST be wearing a properly fitted seatbelt. -Drivers are not to be under the influence of alcohol or drugs. -Drivers must not be fatigued.	1	Supervisor Team Leader Drivers All PCB

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
4. Cont'	-Setting up signs & devices specified by TCP	-Slips, trips, falls, sprains & strains -Cut /abrasions, pinch points -Client entering worksite prior to setup being completed.	4	-Set up signs/devices in TCP in the following order. 1. Advance warning & regulatory signs 2. All intermediate advance warning & regulatory signs/devices required in advance of taper. 3. All delineating devices required to form a taper including flashing arrow signs or temp hazard markers where required. 4. Delineation of work area or side track. 5. All other warning & regulatory signs, including termination & end of temporary speed zone. -Ensure that signs & devices are installed so that they are visible to oncoming traffic. -Travel in direction of normal traffic flow when setting up. -Position traffic control vehicle with rotating beacons on, between PCB and approaching traffic, setting up signs on kerb side. -Maximum 5 cones, or, 2 signs to be carried at one time. -Employees to be made aware of slippery or unstable ground conditions, watch your step.	2	Team Leader Driver All PCB



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
<b>4. Cont'</b>	-Setting up signs & devices specified by TCP	-Impact with Pedestrian or other vehicle. -Slips, trips, falls, sprains & strains -Cut /abrasions, pinch points -Client entering worksite prior to setup being completed.	<b>4</b>	-Traffic Controllers to use care when moving through the job site. -Install signs on one side of the road at a time -Place cones next to signs. -Weigh signs down. Sand bags/Bollard bases. -Communicate with all T/C's -Client is not to enter site until setup complete and all clear given by Team Leader. -Only yellow ticketed PCB permitted to erect signs/devices. -Ensure traffic has a minimum 3mtrs lane width to travel on. -Refer to section <b>4+</b> for additional notes on the taper setup.	<b>2</b>	Team Leader Driver All PCB
<b>4+.</b>	-Setting up signs & devices specified by TCP ( <b>Southern Taper - Slow Lane</b> )	-Impact with Pedestrian or other vehicle. -Slips, trips, falls, sprains & strains -Cut /abrasions, pinch points -Client entering worksite prior to setup being completed.	<b>4</b>	-Note all Control Measures listed in 4a in addition to 4b. -Stop traffic in the lane you are going to set up taper. -2 x advanced warning vehicles to occupy slow lane with hazard lights, beacon lights and right arrow flashing. -TMA next in line indicating speed reduction and flashing right arrow. -Set up vehicle in front of TMA. -Only yellow ticketed PCB permitted to erect signs/devices.	<b>2</b>	Team Leader Driver TMA Driver Advance Warning Drivers All PCB

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
4. Cont'	-Setting up signs & devices specified by TCP	-Traffic Controller being struck by a passing vehicle or plant during setup	4	<ul style="list-style-type: none"> <li>-Park vehicle on shoulder to remove signs.</li> <li>-Always remove signs from the passenger side of the vehicle.</li> <li>-Do Not stand a live lane while unloading or erecting signs.</li> <li>-Do Not turn your back to the traffic.</li> <li>-Ensure you have a clear and safe escape route.</li> <li>-If signage is being duplicated on the median this will require a second loop of the site.</li> <li>-NEVER run across Highways or Motorways to duplicate signage.</li> <li>-Park vehicle on the median shoulder where it is wide enough to maintain clearance from the live lane and place duplicate signage.</li> <li>-If a safe area cannot be located to park then a shadow vehicle with attenuator must be used.</li> <li>-If a shadow vehicle and attenuator cannot be used than duplicate signage cannot be used.</li> <li>-Ensure clear radio communication during the setup process.</li> </ul>	2	Supervisor Team Leader Drivers All PCB

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
4. Cont'	-Setting up signs & devices specified by TCP	-Manual handling injuries from handling equipment or loading/unloading vehicle.	3	-Always use safe manual handling techniques. -Ensure you assess the load and request assistance if required.	1	Team Leader All PCB
		-Conflicting signage or setup is unclear and confusing to motorists.	4	-Set up signage in accordance with the TCP. -Stand back and assess the site looking for anything that may be confusing to motorists. -If changes need to be made, call the office to discuss changes. -Only ticketed Red or Orange carders to make amendments to plan.	2	Team Leader All PCB
		-Hazardous weather conditions including heavy rain or fog, strong winds, poor light.	4	-Drivers must always drive to the weather and road conditions. -Reduce speed where necessary. -Use Headlights -Use Hazard lights if required. -Pull over if the conditions worsen. -Dress appropriately for the conditions ie. Wet weather gear, layers of warm clothing if required.	1	Supervisor Team Leader Drivers All PCB
		-Work in Fire Zones or in extreme weather conditions that may increase the risk of fire	4	-Stay alert to the environment and conditions that you are working in. -Be aware of changes to that environment including the smell of smoke or reduced visibility due to smoke in the area. -Contact Team Leader if these issues arise.	2	All PCB



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
4. Cont'	-Setting up signs & devices specified by TCP	-Fire Hazard due to smoking or the incorrect disposal of cigarette butts.	4	<ul style="list-style-type: none"> <li>-Smoking is not permitted when on the bat.</li> <li>-All cigarette butts are to be extinguished in full prior to disposal.</li> <li>-Cigarette butts are to be disposed of in an appropriate disposal receptacle ie. Butt Bin, Garbage Bin.</li> <li>-DO NOT throw cigarette butts into the ground or into the bush.</li> <li>THIS IS LITTERING and poor representation of Stop Slow.</li> </ul>	2	All PCB
5.	-Providing access to vehicles, mobile plant & machinery to worksite. Vehicle Movement Spotter (VMS)	<ul style="list-style-type: none"> <li>-Vehicles/motorists</li> <li>-Machinery/mobile plant</li> <li>-Open trench/pits</li> <li>-Slips, trips &amp; falls</li> </ul>	4	<ul style="list-style-type: none"> <li>-Nominate VMS at toolbox talk</li> <li>-Keep good communication with all mobile/machine operators.</li> <li>-Use 2 ways when necessary</li> <li>-Stop the lane of traffic affected when vehicles, mobile plant &amp; machinery want to enter/exit the zone of influence.</li> <li>-Avoid open trenches/pits.</li> <li>-Isolate trenches/pits with barricading/ danger tape.</li> <li>-Position yourself so that you are visible to traffic &amp; plant operators.</li> <li>-Avoid impact zone.</li> <li>-Always remain in contact with VMS</li> <li>-VMS to always be in a visible position to all PCB</li> </ul>	2	

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
6.	Escorting mobile plant/machinery	-Vehicles/motorists -Pedestrians	4	-Use an approved traffic control vehicle with rotating beacons -Communicate with plant operator -Traffic controllers stay clear from moving plant & halting pedestrians until danger is clear.	2	
7.	Guiding pedestrians & creating a safe footpath.	-Vehicles/motorists -Machinery/mobile plant -Materials -Open trenches/pits -Slips, trips & falls -Young children, elderly persons, handicapped & intoxicated persons.	3	-Keep pedestrians isolated from zone of influence. -Clearly define the work area using plastic barricading and danger tape. -Ensure good housekeeping. -Pedestrian diversion shall be adequately sign posted to indicate the direction of footway. -Pedestrians shall be instructed to wait by traffic controllers whilst construction vehicles enter/exit worksite.	1	
8.	Maintaining the Job site	-Traffic Controllers unable to be seen or in a location where they may be struck by a moving vehicle or have no escape route.	4	-Pedestrians are not permitted to walk past work area on the road. -Traffic Controllers must be visible to oncoming traffic (1.5D is the recommendation). -Never turn your back to the traffic -Have a clear escape route. -Do not step onto the live lane.	2	

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Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
8. Cont'	Maintaining the Job site	-Signage or equipment knocked over	4	-Ensure signage is stable and weighted. -Conduct regular sign checks -Use additional weights or sandbags. -Replace or repair any damaged signage. -If signage is to be moved, the TCP is to be amended by a ticketed Red or Orange Carder.	2	
		-Unauthorised vehicles entering the worksite	4	-Safely attempt to stop any unauthorised people entering the site. -If unauthorised vehicle cannot be stopped, radio details through to other crew on site. -Ensure your radio has been heard and other workers are aware of the situation.	2	
		-Hydration -Fatigue -Heat exhaustion -Radiant heat	4	-Maintain hydration (preferred method – water) -avoid the overconsumption of energy drinks. These will reduce kidney function if dehydrated. -monitor urine colour (Light = hydrated) (Dark = dehydrated) -Traffic controllers to ensure they are well rested for each shift. -Inform Team leader if feeling fatigued or dehydrated.	2	



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
8. Cont'	Maintaining the Job site	<ul style="list-style-type: none"> <li>-Hydration</li> <li>-Fatigue</li> <li>-Heat exhaustion</li> <li>-Radiant heat</li> </ul>	4	<ul style="list-style-type: none"> <li>-Monitor temperature, humidity &amp; PCB physical response to environmental conditions. PCB showing signs of heat stress shall stop work immediately.</li> <li>Signs:               <ul style="list-style-type: none"> <li>Dizziness, Muscle weakness, Nausea, Vomiting, Thirst</li> </ul> </li> <li>-Provide frequent rest breaks, rotate duties.</li> <li>-Wear company issued PPE</li> <li>Long sleeve shirt (sleeves down)</li> <li>Wide brim hat/brim for helmet</li> <li>Sunscreen</li> </ul>	2	
		<ul style="list-style-type: none"> <li>-Hazardous weather conditions including heavy rain or fog, strong winds, poor light.</li> <li>-Reduced braking distance</li> <li>-Reduced visibility</li> <li>-Slippery road surface</li> <li>-Malfunction of 2 ways</li> </ul>	4	<ul style="list-style-type: none"> <li>-Drivers must always drive to the weather and road conditions.</li> <li>-Reduce speed where necessary.</li> <li>-Use Headlights</li> <li>-Use Hazard lights if required.</li> <li>-Pull over if the conditions worsen.</li> <li>-Dress appropriately for the conditions ie. Wet weather gear, layers of warm clothing if required.</li> <li>-Reduce posted speed limit</li> <li>-Keep 2 ways out of rain</li> <li>-Extend buffer zone taper</li> <li>-Temporarily cease work in extreme wet conditions.</li> </ul>	2	
		<ul style="list-style-type: none"> <li>-Work in Fire Zones or in extreme weather conditions that may increase the risk of fire</li> </ul>	4	<ul style="list-style-type: none"> <li>-Stay alert to the environment and conditions that you are working in.</li> </ul>	2	

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
8. Cont'	Maintaining the Job site	-Work in Fire Zones or in extreme weather conditions that may increase the risk of fire.  -Fire Hazard due to smoking or the incorrect disposal of cigarette butts.  -Fire Hazard due to smoking or the incorrect disposal of cigarette butts.	4	-Be aware of changes to that environment including the smell of smoke or reduced visibility due to smoke in the area. -Contact Team Leader if these issues arise.	2	
			4	-Smoking is not permitted when on the bat. -All cigarette butts are to be extinguished in full prior to disposal. -Cigarette butts are to be disposed of in an appropriate disposal receptacle ie. Butt Bin, Garbage Bin. -DO NOT throw cigarette butts into the ground or into the bush. THIS IS LITTERING and poor representation of Stop Slow.	2	
9.	Team Leader to confirm with client that work is complete for the day.	Traffic controllers packing up the site before work is complete or without confirmation from the Team Leader resulting in an unprotected site and possible traffic congestion or incidents/accidents.	4	-Traffic controllers only to pack up site after receiving confirmation from the Team leader. -Team Leader ensure client is aware of ROL cut off times. -Team leader to monitor works to ensure pack up will be completed inside ROL cut off times.	2	

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
10.	Pack up Signs and devices	<ul style="list-style-type: none"> <li>-Impact with Pedestrian or other vehicle.</li> <li>-Slips, trips, falls, sprains &amp; strains</li> <li>-Cut /abrasions, pinch points</li> </ul>	4	<ul style="list-style-type: none"> <li>-Removal of signs/devices shall be undertaken in reverse order of erection</li> <li>-Ensure client is off the road prior to packing up</li> <li>-Always travel in direction of normal traffic flow when packing up</li> <li>-Position traffic control vehicle with rotating beacons on, between the PCB and approaching traffic, placing signs on vehicle from kerb side.</li> <li>-PCB to be made aware of slippery conditions, watch your step.</li> <li>-Use gloves</li> <li>-Only yellow ticketed PCB to conduct sign removal.</li> <li>-Travel in direction of normal traffic flow when setting up.</li> <li>-Position traffic control vehicle with rotating beacons on, between PCB and approaching traffic, setting up signs on kerb side.</li> <li>-Maximum 5 cones, or, 2 signs to be carried at one time.</li> <li>-Employees to be made aware of slippery or unstable ground conditions, watch your step.</li> <li>-Communicate with all T/C's</li> </ul>	2	



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
10+	-Pack up signs & devices (Southern Taper - Slow Lane)	-Impact with Pedestrian or other vehicle.  -Slips, trips, falls, sprains & strains  -Cut /abrasions, pinch points	4	-Note all Control Measures listed in 4a in addition to 4b. -Stop traffic in the lane you are going to pack up taper. -2 x advanced warning vehicles to occupy slow lane with hazard lights, beacon lights and right arrow flashing. -TMA next in line indicating speed reduction and flashing right arrow. -Pack up vehicle in front of TMA. -Only yellow ticketed PCB permitted to erect signs/devices.	2	
10. Cont'	Pack up Signs and devices	-Traffic Controller being struck by a passing vehicle or plant during pack up	4	-Park vehicle on shoulder. -Always load signs from the passenger side of the vehicle. -Do Not stand a live lane while packing up or loading signs. -Do Not turn your back to the traffic. -Ensure you have a clear and safe escape route. -If signage has been duplicated on the median this will require a second loop of the site. -NEVER run across Highways or Motorways to duplicate signage. -Park vehicle on the median shoulder where it is wide enough to maintain clearance from the live lane and pick up duplicate signage	2	

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
10. Cont'	Pack up Signs and devices	-Traffic Controller being struck by a passing vehicle or plant during pack up	4	-Use a shadow vehicle with attenuator if required. -Ensure clear radio communication during the pack up process.	2	
		-Manual handling injuries from handling equipment or loading/unloading vehicle.	3	-Always use safe manual handling techniques. -Ensure you assess the load and request assistance if required.	1	Team Leader All PCB
		-Hazardous weather conditions including heavy rain or fog, strong winds, poor light.	4	-Drivers must always drive to the weather and road conditions. -Reduce speed where necessary. -Use Headlights -Use Hazard lights if required. -Pull over if the conditions worsen. -Dress appropriately for the conditions ie. Wet weather gear, layers of warm clothing if required.	2	Supervisor Team Leader Drivers All PCB
		-Work in Fire Zones or in extreme weather conditions that may increase the risk of fire	4	-Stay alert to the environment and conditions that you are working in. -Be aware of changes to that environment including the smell of smoke or reduced visibility due to smoke in the area. -Contact Team Leader if these issues arise.	2	All PCB
		-Fire Hazard due to smoking or the incorrect disposal of cigarette butts.	4	-Smoking is not permitted when on the bat. -All cigarette butts are to be extinguished in full prior to disposal.	2	

Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible
12.	Job Process training, implementing, monitoring and improving	-Traffic controllers not familiar with or compliant with the SWMS requirements	4	<ul style="list-style-type: none"> <li>-Team leader to review SWMS with the team prior to commencing work.</li> <li>-Team members to ask questions if they are unsure about any points outlined in the SWMS.</li> <li>-All team members to sign off on the SWMS. This is an acknowledgment that you have read and understand the SWMS.</li> <li>-Team leaders to continually monitor the site.</li> <li>-Team members to inform the team leader of any change in the conditions or environment that may alter or affect the requirements of the SWMS.</li> </ul>	1	



Step	Job Activity	Hazard/Risk Identified	Risk Score	Control Measure	Res'd Risk	PCB Responsible

**Emergency Response Procedure**  
 (Example: Site Specific Safety Management will reflect appropriate CHANGES)  
 1) Emergency Numbers

In the event of an Emergency, the following telephone numbers apply:

CONTACTS	NAME	PHONE NUMBER
State Operations Manager	Daniel Muller	0410 981 798
Head Office	Bellambi	(02) 42 853 900
Site Office		
<b>Emergency services</b>		
Nearest Hospital:	Wollongong	Crown Street Wollongong NSW 2500 (02)4222 5000
Nearest Medical Centre:	Bellambi Medical Centre	u4/53-55 Pioneer Road Bellambi NSW 2518 (02) 4283 7373
Ambulance and fire	000	
Nearest Police Station	Corrimal Police 412-416 Princes Highway Towradgi NSW 2518 (02) 4283 0200	
Poisons Info Centre	13 11 26	
Electricity	13 10 03	
Gas	13 19 09	
Water	13 20 90	
Telstra	Dial Before You Dig Faults	1100 13 22 03

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## **APPENDIX B**

### **Event Risk Assessment and Emergency Management and Crowd Control Plan**

LIKELIHOOD	RISK RANKING MATRIX				
HIGH	5	10	15	20	25
SIGNIFICANT	4	8	12	16	20
MODERATE	3	6	6	12	15
LOW	2	4	6	8	10
NEGLIGIBLE	1	2	3	4	5
CONSEQUENCE	NEGLIGIBLE	LOW	MODERATE	MAJOR	CATASTROPHIC

LIKELIHOOD DEFINITIONS	
A <b>high</b> likelihood	<ul style="list-style-type: none"> <li>It is expected to occur in most circumstances</li> <li>There is a strong likelihood of the hazards reoccurring</li> </ul>
A <b>significant</b> likelihood	<ul style="list-style-type: none"> <li>Similar hazards have been recorded on a regular basis</li> <li>Considered that it is likely that the hazard could occur</li> </ul>
A <b>moderate</b> likelihood	<ul style="list-style-type: none"> <li>Incidents or hazards have occurred infrequently in the past</li> </ul>
A <b>low</b> likelihood	<ul style="list-style-type: none"> <li>Very few known incidents of occurrence</li> <li>Has not occurred yet, but it could occur sometime</li> </ul>
A <b>negligible</b> likelihood	<ul style="list-style-type: none"> <li>No known or recorded incidents of occurrence</li> <li>Remote chance, may only occur in exceptional circumstance</li> </ul>

CONSEQUENCE DEFINITIONS	
<b>Catastrophic</b>	<ul style="list-style-type: none"> <li>Multiple of single death</li> <li>Costs to Event of up to \$5 million</li> <li>International and National Media outrage</li> </ul>
<b>Major</b>	<ul style="list-style-type: none"> <li>Serious health impacts on multiple or single persons or permanent disability.</li> <li>Costs to Event between \$2.5 – \$5 million</li> <li>National media outrage</li> </ul>
<b>Moderate</b>	<ul style="list-style-type: none"> <li>More than 10 days rehabilitation required for injured persons</li> <li>Costs to Event between \$200,000 and \$2.5 million</li> <li>Local media and community concern</li> </ul>
<b>Low</b>	<ul style="list-style-type: none"> <li>Injury to person resulting in lost time and claims</li> <li>Costs to Event between \$50,000 and \$200,000</li> <li>Minor isolated concerns raised by stakeholders, customers</li> </ul>
<b>Negligible</b>	<ul style="list-style-type: none"> <li>Persons requiring first aid</li> <li>Costs to Event up to \$50,000</li> <li>Minimum impact to reputation</li> </ul>

**ANZAC DAY NORTH BONDI RISK ASSESSMENT – 2016**

RISK ASSESSMENT			
Name of Event: ANZAC DAY NORTH BONDI		Risk Management Team: North Bondi RSL Sub Branch Committee	
Date of Event: Saturday 25 April 2016			
Location of Event: North Bondi War Memorial			
Hazards	Risk rank	Control / Actions	Responsibility
Event located near the ocean and a member of the crowd enters the water and has difficulties returning to shore	8	Controlled by advising Waverley Bondi beach Life Guards of the event and presence of trained life savers at North Bondi Surf Club	James Isbell
Vehicles driving on public areas causing damage to the site or a collision with a person.	8	Controlled by presence of Stop Slow Traffic Management providing road closures and facilitating access. Supported by on-site security guard overnight to help facilitate early deliveries and set up and prevent unauthorised access by members of the public	Kate Cass
Inadequate public toilets causing queuing and complaints	1	Controlled by keeping the event time to approximately one hour enabling the crowd to depart to use other facilities nearby	David Sims
Inadequate free drinking water supply causing dehydration	6	Controlled by the presence of public drinking fountains along the boardwalk, drinking water available at designated BBQ areas, from first aid offices and available at the RSL Club throughout the service	Kate Cass
Erection of large screen and generator an injury to contractor or member of the public.	6	Control the hazard by cordoning off the area during set up. Site supervisor/s to monitor the area and ensure that the public are not entering the work space.  Provision of overnight security Guard to help facilitate up and prevent unauthorised access by members of the public	Contractor



Public or staff member receives sun burn, heat stroke or dehydration.	6	Controlled by keeping the event time to approximately one hour enabling the crowd to depart to move into shaded areas.  Shaded spaces with seating available for more elderly Club members during the service within the RSL Club area	David Sims
Child is separated from their guardian.	4	Lost Children will be handed to local police on site and if required an announcement made over the PA system to identify the parents	NSW Police
Crowd crush injury if site was evacuated in an emergency.	12	Controlled via clear announcements over the PA system of any requirement to evacuate, identifying the most suitable routes and requesting the crowd remain calm. Supported by Local Life Savers, NSW Police, Traffic Controllers, North Bondi RSL Committee members and volunteers.	David Sims James Isbell Volunteer Co-ordinator NSW Police
Person on site requires an ambulance but the ambulance is not able to access the site.	8	In the event that an evacuation is required where it is possible to do so the patient will be transported to the North Bondi Surf Club and an ambulance will be called to this location. The First Aid Co-Ordinator supported by Traffic Controllers and volunteers will enable passage through the crowd for the St John's personnel and if required for evacuation. If required the service will be provided and guidance given to the crowd, over the PA, to help facilitate rapid access to a patient and where required their evacuation. The Bondi Council Life Guards will also be contacted where required to provide advanced first aid (8362 3450)	James Isbell
Person receives a minor injury on site	4	Controlled by the presence of St John First Aid staff and first aid trained volunteers, security, police and life savers. During the opening of the service the crowd will be	James Isbell

		notified of the location of the St John First Aiders located next to the North Bondi Surf Club.	
Small fire on site causes damage to equipment	4	There are adequate fire extinguishers located in nearby facilities that can be utilised to tackle any small fire. In the event of a larger fire 000 will immediately be called and appropriate evacuation procedures utilised.	Nick de Bont
Inappropriate behaviour from intoxicated person	3	Controlled by ensuring responsible service of alcohol, presence of NSW Police, Licensed Security officers at liquor service locations and no service of alcohol prior or during the service	Steve Lovet
Vehicle born or personal born attack	10	<p>Traffic Management strategies in association with NSW Police have been implemented preventing any and all vehicles from entering the vicinity of the service. Vehicle mitigation strategies have been implemented and NSW Police will be on site to respond and reinforce the road closures along with Traffic Controllers.</p> <p>Vehicles contained within the road closures will be 'screened' and assessed by NSW Police prior to the ingress of patrons for the service. Any identified at risk vehicles will be dealt with accordingly by NSW Police.</p>	<p>NSW Police</p> <p>Stop Slow</p> <p>Traffic Control</p>

## 2016 - ANZAC DAY NORTH BONDI

### Emergency Management and Crowd Control Plan

Whilst responsibility for certain actions is designated throughout this plan at any time responsibility should be transferred to emergency service personnel present and able to assist.

#### Summary of Roles and Responsibilities

Name	Positions	Responsibilities
James Isbell	First Aid Co-coordinator	<ul style="list-style-type: none"> <li>• Liaise with North Bondi Surf Club to enable access to first aid room</li> <li>• Ensure St John's volunteers are positioned prominently in vicinity of North Bondi Surf Club</li> <li>• In the event of a medical incident co-ordinate access to the patient by advising David Sims of any public announcement requirements</li> <li>• Where required during a first aid incident contact Bondi Beach Waverley Life Guards (8362 3450) and 000</li> </ul>
James Isbell	Extreme Weather Manager	<ul style="list-style-type: none"> <li>• Where required make an appropriate assessment on the requirement to cancel the event due to extreme weather. Where appropriate liaise with North Bondi Surf Club and Waverley Life Guards during the assessment. Once this determination has been made advise David Sims of any public announcement requirements.</li> </ul>
David Sims	Emergency Management Announcer	<ul style="list-style-type: none"> <li>• Where required make appropriate public announcements utilising the PA system to co-ordinate any evacuation or crowd control measures throughout the service</li> <li>• Prior to the commencement of the service make an announcement highlighting you will advise the crowd of any requirement to evacuate and provide directions at the time of the event. Additionally that any member requiring first aid should move to the North Bondi Surf Club where they will be provided with assistance.</li> </ul>
Volunteer Co-ordinator	Crowd Control Manager	<ul style="list-style-type: none"> <li>• In all non-emergency circumstances provide guidance to volunteers with regards to managing the crowd and providing direction to the public on suitable places to ensure the crowd does not become over crowded.</li> </ul>
Kate Cass	Traffic Manager	<ul style="list-style-type: none"> <li>• Provide a briefing and guidance to Stop Slow Traffic Control PTY LTD staff on their roll in controlling traffic, providing guidance to late arrivals of members of the public and help facilitate access in an emergency.</li> </ul>
Fire Manager	Nick de Bont	<ul style="list-style-type: none"> <li>• Where required, provide direction to volunteers to utilise firefighting equipment located in nearby premises and call 000.</li> </ul>



**Crowd Management**

1. Crowd management will be controlled by the North Bondi RSL Sub-Branch Committee members led by the Volunteer Co-ordinator and David Sims. Support provided by NSW Police, North Bondi Club PTY LTD security staff and Stop Slow Traffic Controllers.
2. During the event any committee member who feels that there is overcrowding in a section will direct attendees to other areas.
3. The Volunteer Co-ordinator will be in charge of directing volunteers to advise arriving attendees of areas they can move to that are not overcrowded. Those volunteers handing out programs will be briefed prior to the event on how to appropriately direct the public during their arrival. The volunteers will also be supported by staff members of Stop Traffic Control who will control traffic as well as advising the crowds on appropriate locations to move to.
4. North Bondi RSL Club PTY LTD will have four crowd controllers present throughout the day in order to control patrons within the RSL premises. These crowd controllers will also enable access up and down the stairs between Campbell Pde and Ramsgate Ave throughout the service and at the conclusion of the service. The management of these crowd controllers will be undertaken by North Bondi RSL Club PTY LTD as per their standard club operating guidelines.
5. If at any time there is concern about overcrowding and a failure for the public to follow directions David Sims will utilise the PA system to make a public announcement and attending NSW Police Officers will be requested to assist if required.

**Traffic Management**

1. Traffic management will be managed by Stop Slow Traffic Control PTY LTD. Stop Slow will have five staff present who will operate the closure of the car park and Ramsgate Ave during the service.
2. The Stop Slow Traffic staff will be managed by Kate Cass and will also provide guidance to the late arrivals of the crowd on appropriate locations to observe/hear the service.
3. Kate Cass will brief the staff on ensuring they provide appropriate control in an emergency situation and if required help facilitate the access of emergency services.

**First Aid**

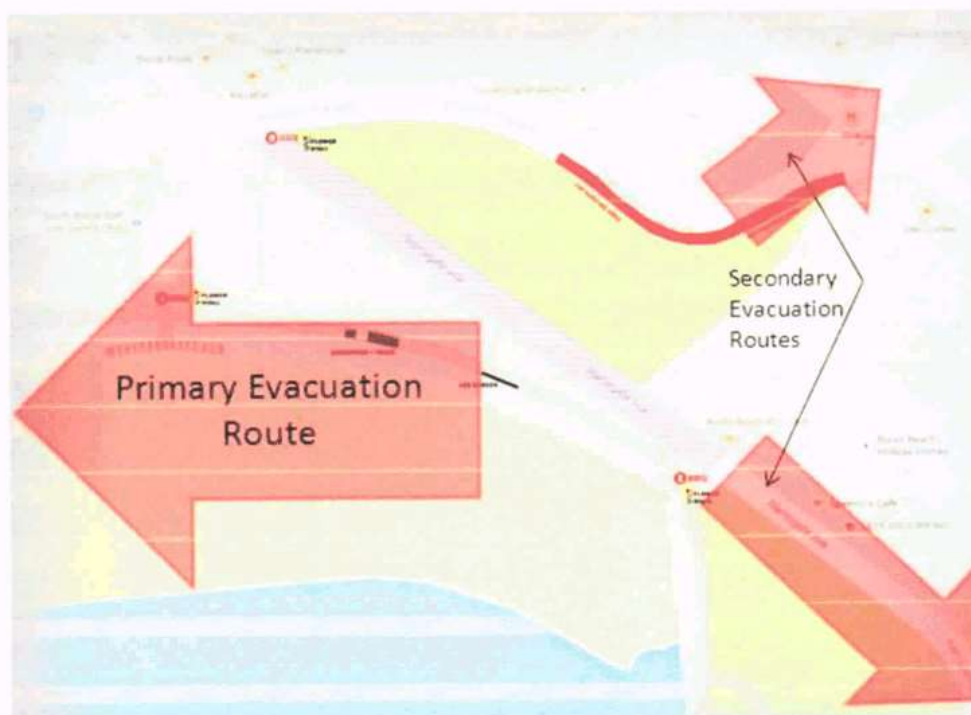
4. James Isbell will be the first aid co-coordinator for the event.
5. In attendance will be volunteer St John's Ambulance staff who will take the lead in providing support to any member of the public who requires medical assistance. St John's Ambulance staff will position themselves at the North Bondi Surf Club. David Sims will advise members of the public that if they require medical assistance during the service they should move to the North Bondi Surf Club to seek assistance from St John's staff.
6. North Bondi Surf Club members will be in attendance and will provide assistance to St John's if required. James Isbell will liaise with North Bondi Surf Club to enable access to their first aid room if required.
7. In the event of an incident James Isbell will co-ordinate passage through the crowd for the St John's personnel and if required for evacuation. If required David Sims will interrupt the service and provide guidance to the crowd, over the PA, to help facilitate rapid access to a

patient and their evacuation. The Bondi Council Life Guards will also be contacted where required to provide advanced first aid (8362 3450).

8. The North Bondi RSL Club facility and the North Bondi Surf Life Saving Club will be able to provide water and a seat if required for minor medical incidents. Additionally elderly club patrons will be provided with a seat within the North Bondi RSL for the duration of the service.
9. In the event that an evacuation is required where it is possible to do so the patient will be transported to the North Bondi Surf Club and an ambulance will be called to this location.
10. Ambulance staff will determine the best medical facility to transport the patient.

#### Evacuation

11. Prior to the commencement of the service David Sims will advise the attendees that in the event of an emergency he will make an announcement and advise the public if they should move North or South.
12. The authority to order an evacuation rests with any member of the North Bondi RSL sub-branch and the North Bondi RSL sub-branch trustees.
13. If an evacuation is deemed necessary this information will be immediately conveyed to David Sims who will utilise the PA system to make a public announcement.
14. In the unlikely event of an emergency to the south of the North Bondi RSL Club building members of the public will be directed to move north along Campbell Parade and Ramsgate Avenue.
15. Any other event that requires emergency evacuation members of the public will be asked to move in an orderly fashion south along the beach and the boardwalk.



**Fire**

16. All commercial premises within vicinity of the memorial have firefighting equipment. This equipment is to be utilised to control small fires. In the event of a larger fire 000 will immediately be called and appropriate evacuation procedures utilised.
17. Nick de Bont is the fire response manager and will, where required, provide direction to volunteers to utilise firefighting equipment and call 000.

**Extreme Weather**

18. The event is an outdoor event and will proceed despite inclement weather. However if a severe thunder storm or strong winds pose a safety risk the event will be cancelled. This determination will be made by James Isbell in consultation with North Bondi Surf Life Savers and Bondi Beach Life Guards. If this determination is made David Sims will make an announcement across the PA to members of the public who will be advised to disperse from the area in order to seek appropriate shelter. Crowds will be asked to slowly exit the area utilising Campbell Pde and Ramsgate Ave.



## **APPENDIX C**

### **Public Liability Insurance**



CGU Insurance Ltd  
388 George Street  
Sydney NSW 2000

ABN: 27 004 478 371

### CERTIFICATE OF CURRENCY

Date: 9<sup>th</sup> March 2017

To Whom It May Concern:

The Policy referred to below is current as at the date of this certificate. Whilst due date has been indicated it should be noted that this policy may be cancelled in the future.


Policy Number:	10M 7431902	
Policy Type:	General and Products Liability	
Insured:	RSL of Australia Sub Branches and Women's Auxiliaries including North Bondi RSL Sub-Branch	
Period of Insurance:	From: 31 May 2016	at 4:00pm local time at the place of issue
	To: 31 May 2017	at 4:00m local time at the place of issue
Public Liability	\$20,000,000	any one Occurrence
Products Liability	\$20,000,000	any one Occurrence and in the aggregate
Advertising Liability	\$20,000,000	any one Occurrence and in the aggregate
Interested Party:	Waverley Council are noted for their respective rights and interests in respect of the ANZAC Day March, 25 <sup>th</sup> April 2017	

This is a Policy summary only. Full details of this Insurance appear on the Policy Document

**CGU**  
CGU INSURANCE LIMITED  
ABN 27 004 478 371

CGU Insurance Limited – Certificate of Currency

www.asruw.com.au





## Certificate of Currency

This certificate of insurance confirms that cover has been issued subject to Policy Terms and Conditions.

<b>Date of Issue:</b>	Tuesday, October 11, 2016
<b>Policy Type:</b>	Public and Product Liability Insurance
<b>Insurer:</b>	Certain Underwriters at Lloyd's Of London
<b>Policy Number:</b>	AT141608N36555 ASR/EXC/NOV951
<b>The Insured:</b>	Stop Slow Traffic Control (NSW) Pty Ltd
<b>Situation To &amp; From:</b>	37 Braidwood Rd, Goulburn NSW 2580 2/24 Railway St, Bomaderry NSW 2541
<b>Period of Insurance:</b>	From 30 Sep 2016 To 30 Sep 2017 Both Dates at 16.00 LST
<b>Business Descriptions:</b>	Traffic Control including Traffic Management, install road markers and road site clean up.
<b>Coverage/Limit</b>	
Public Liability	\$30,000,000 Any One Occurrence
Product Liability	\$30,000,000 In Aggregate
<b>Territorial Limits:</b>	Worldwide Excluding USA & Canada
<b>Jurisdictional Limits</b>	Australia Wide

This certificate is furnished at the request of the Insured as a matter of information only and does not constitute an insurance contract upon which claims can be made.

For and on behalf of ASR Underwriting Agencies Pty Ltd,

**Melissa Fox**  
Liability Underwriter





Chubb Insurance Australia Limited  
 ABN: 23 001 642 020 AFSL: 239687  
 Grosvenor Place, Level 38, 225 George Street, Sydney NSW 2000  
 Australia  
 O (02) 9335 3200  
 F (02) 9335 3411  
[www.chubb.com/au](http://www.chubb.com/au)

## Certificate of Currency

This Certificate of Currency confirms the following policy is current at the date stated below.  
 Please refer to the policy document for full terms and conditions.

<b>Insured:</b>	WaleyCorp Pty Ltd		
<b>Policy Number:</b>	93395803		
<b>Policy Type:</b>	Information Technology Liability		
<b>Limits of Insurance:</b>			
<b>Coverage 1</b>			
<b>Financial Injury (Professional Liability)</b>	\$10,000,000	each act and	
	\$10,000,000	in the aggregate	
<b>Coverage 2</b>			
<b>Public Liability</b>	\$20,000,000	each occurrence	
<b>Products Liability</b>	\$20,000,000	each occurrence and	in the aggregate
<b>Policy Period:</b>	From: 22/12/2016		
	To: 22/12/2017		

This Certificate is furnished as matter of information only and does not constitute an insurance contract upon which claims can be made.

The insurance afforded by the policies described herein is subject to all terms, exclusions and conditions of such policies.

19 December 2016

\_\_\_\_\_  
 Authorised Representative

\_\_\_\_\_  
 Date



## CERTIFICATE OF CURRENCY



STOP SLOW TRAFFIC CONTROL (NSW) PTY LTD  
6A/7 Pioneer Road  
BELLAMBI 2518

Date of Letter: 10/08/2016

Dear Sir/Madam,

### 1. STATEMENT OF COVERAGE

The following policy of insurance covers the full amount of the employer's liability under the *Workers Compensation Act 1987*.

**This Certificate is valid from 30/06/2016 - 30/06/2017**

The information provided in this Certificate of Currency is correct at: 10/08/2016

### 2. EMPLOYERS INFORMATION

POLICY NUMBER WGB110711079122

LEGAL NAME STOP SLOW TRAFFIC CONTROL (NSW) PTY LTD

TRADING NAME Stop Slow Traffic Control (NSW) Pty Ltd

ABN 87152059820

TRUST NAME

TRUST ABN

WorkCover Industry Classification Number (WIC)	Industry	Numbers of Workers*	Wages+ / Units
425940	Construction Services nec	70	\$2,000,000.00

\* Number of workers includes contractors/deemed workers

+ Total wages estimated for the current period

### 3. IMPORTANT INFORMATION

Principals relying on this certificate should ensure it is accompanied by a statement under section 175B of the *Workers Compensation Act 1987*. Principals should also check and satisfy themselves that the information is correct and ensure that the proper workers compensation insurance is in place, ie. compare the number of employees on site to the average number of employees estimated; ensure that the wages are reasonable to cover the labour component of the work being performed; and confirmed that the description of the industry/industries noted is appropriate.

A principal contractor may become liable for any outstanding premium of the sub-contractor if the principal has failed to obtain a statement or has accepted a statement where there was reason to believe it was false.

Yours Faithfully,

Duncan Struthers  
Team Manager



# Certificate of Currency

Aon Risk Services Australia Limited  
ABN 17 000 434 720 AFSL 241141

St John Ambulance Australia (NSW) Inc  
9 Deane St  
Burwood NSW 2134

In our capacity as Insurance Brokers to St John Ambulance Australia (NSW) Inc, we hereby certify that the under mentioned insurance policy is current.

## As at Date

15 December 2016

## Policy Information

### Class of Insurance

Public and Products Liability

### Insurer (Lead)

AAI LIMITED TRADING AS VERO INSURANCE ABN:  
48 005 297 807

### Policy Number(s)

LCB014002716

### Expiry Date

31/12/2017

### Insured

St John Ambulance Australia (NSW) Inc

### Interest Insured

Principally property owners and occupiers, charitable organisation, ambulance services, paramedic services, Community Care services, first aid providers and trainers, importer and distributor of first aid manuals and supplies and any other activities incidental thereto.

### Situation of Risk

Anywhere in the world except United States of America and Canada other than in respect of  
(a) Products sent to the United States of America and/or Canada without the knowledge of the Insured or  
(b) Commercial visits by directors and non manual employees normally resident outside USA or Canada

### Sum Insured

\$50,000,000 any one occurrence or in respect of Products and Pollution, in the aggregate

## Important notes

- This certificate is a summary of cover only. Please refer to the Policy Wording and Schedule for its full terms and conditions.
- Aon does not guarantee that the insurance outlined in this Certificate will continue to remain in force for the period referred to as the Policy may be cancelled or altered by either party to the contract, at any time, in accordance with the terms of the Policy and the Insurance Contracts Act 1984 (Cth).
- Aon accepts no responsibility or liability to advise any party who may be relying on this Certificate of such alteration to or cancellation of the Policy.
- This Certificate does not
  - represent an insurance contract or confer rights to the recipient, or
  - amend, extend or alter the Policy.

## Contact Us

### CRM

Kirsty Teer

### Telephone

612 61024900

### Email

kirsty.teer@aon.com

### Branch

Level 10, 15 London Circuit  
CANBERRA ACT 2601



## **APPENDIX D**

### **Notice to Fire and Ambulance**



**THE RETURNED & SERVICES LEAGUE OF AUSTRALIA  
NORTH BONDI RSL SUB-BRANCH**

118-120 Ramsgate Avenue

North Bondi, NSW 2026

Telephone: (02) 9365 1896 • Fax: (02) 9365 4398

[info@tobrukhouse.com](mailto:info@tobrukhouse.com) • [www.tobrukhouse.com](http://www.tobrukhouse.com)

ABN 69 219 779 044

**Patron: Dave Griffiths DCM**

**President: Robert de Graaf**

**Honorary Secretary: James Isbell**

**Trustees: Frederick D. Alderman**

**James Isbell**

**David S. Sims**

9 March 2017

Transit & Emergency Services  
North Bondi Beach/Waverley Municipality

To Whom It May Concern,

RE: Road Closure – North Bondi RSL Sub Branch ANZAC Day Dawn Service

Please be advised that The Returned & Services League of Australia at North Bondi Sub-Branch has applied to Waverley Council and been granted a temporary road closure, from 4:00am to 7:00am, of Ramsgate Avenue and parts of Campbell Pde. (between the NB SLSC & Brighton Blvd) for the Annual ANZAC Day Dawn Service to be held on Monday 25<sup>th</sup> April 2014 at 6:00am.

This letter serves as notice as required by Waverley Council as a condition of the application approval.

Local residents and business have been advised of the above closure and we have contracted Stop Slow Traffic Control NSW as the RMS accredited Traffic Controllers for the event. Please also see the attached the traffic control plan.

Should you have any questions please contact us on 0414 615 807 or by email [info@tobrukhouse.com](mailto:info@tobrukhouse.com)

Sincerely

Robert de Graaf  
President

transitemergencyservice\_noticeofroadclosure\_anzacday\_2017

*Tobruk House – since 1944  
No Surrender*

## **APPENDIX E**

### **Traffic and Transport Management Traffic Control Plans**





**THE RETURNED & SERVICES LEAGUE OF AUSTRALIA  
NORTH BONDI RSL SUB-BRANCH**

118-120 Ramsgate Avenue

North Bondi, NSW 2026

Telephone: (02) 9365 1896 • Fax: (02) 9365 4398

[info@tobrukhouse.com](mailto:info@tobrukhouse.com) • [www.tobrukhouse.com](http://www.tobrukhouse.com)

ABN 69 219 779 044

**Patron: Dave Griffiths DCM**

**President: Robert deGraaf**

**Honorary Secretary: James Isbell**

**Trustees: Frederick D. Alderman**

**James Isbell**

**David S. Sims**

9 March 2017

Local Residents

North Bondi

Please be advised that The Returned & Services League of Australia at North Bondi Sub-Branch applied to the Waverley Council Traffic Committee and was granted a temporary road closure of Ramsgate Ave and part of Campbell Parade for the Annual ANZAC Day Dawn Service.

The temporary road closure will be enforced by our contracted traffic management company. Please do not leave your vehicle parked in any of the closed areas designated on the back of this notice on the eve of the 24 April 2016.

Road Closure:

Date: 25 April 2017

Time: 12am until 7am (or until Police authorise the road opening)

We apologise for any inconvenience this may cause.

Should you have any questions please contact us on (02) 9365 1896 or by email

[info@tobrukhouse.com](mailto:info@tobrukhouse.com)

Sincerely

Robert de Graaf  
President

*Tobruk House – since 1944  
No Surrender*

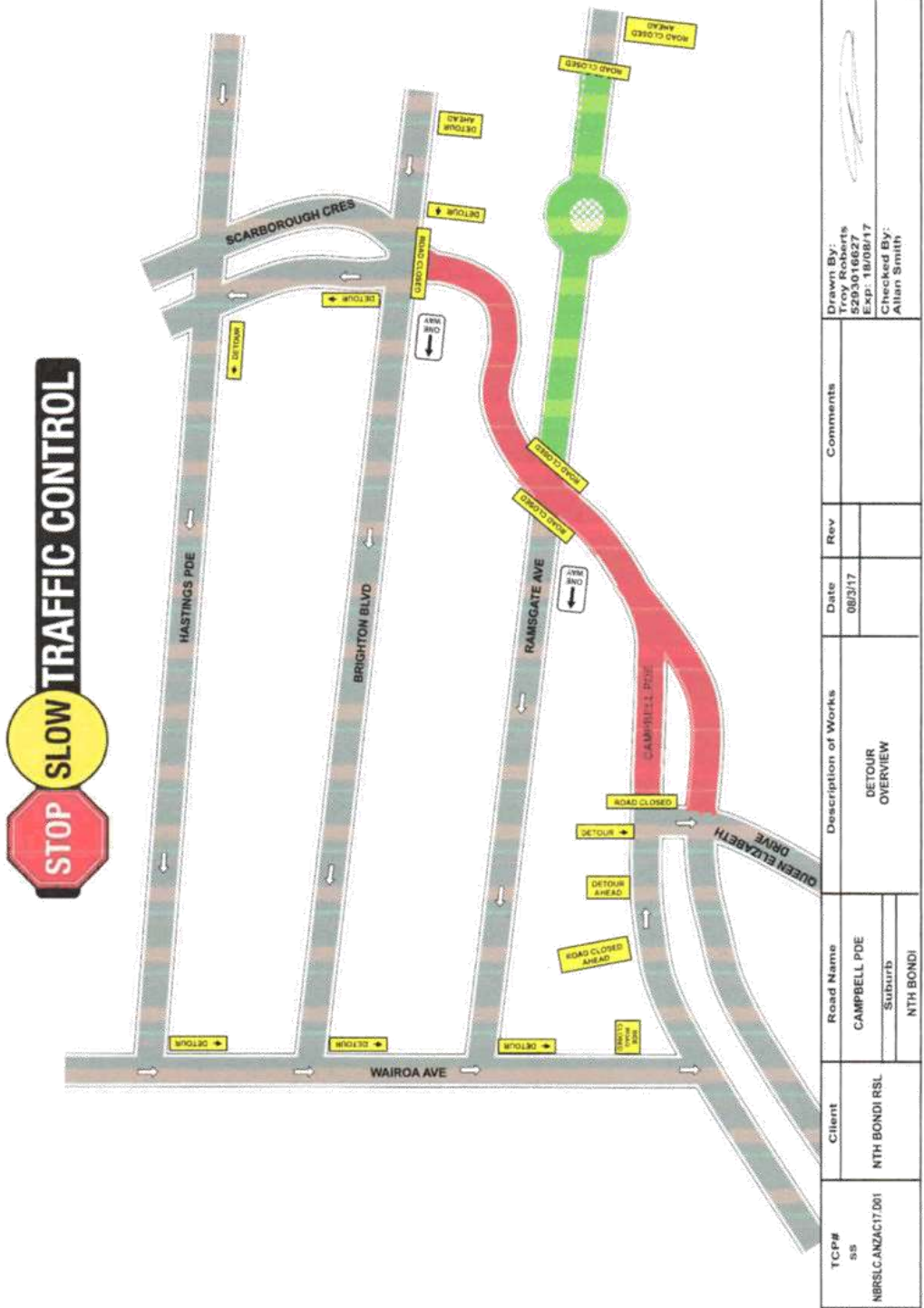








www.rivision.com





## **APPENDIX F**

### **Run Sheet and Contact List**



MASTER RUNSHEET				
NORTH BONDI RSL ANZAC DAY DAWN SERVICE 2016				
NAME	Anzac Day Dawn Service			
DATE	Monday 25th April 2016			
TIME	06:00 - 07:00			
LOCATION	North Bondi RSL, Ramsgate Ave			
CONTACT LIST				
COMPANY	NAME	ROLE	CONTACT	NOTES
NBRSL SB	Kate Cass	Management/Event team	0414 615 807	
NBRSL SB	Dave Sims	Management/MC	0448 727 882	
NBRSL SB	Robert deGraaf	Sub Branch President	0419 435 276	
NBRSL SB	James Isbell	Management/Crowd	0412 733 376	
NBRSL	Steve Lovat	Club Manager	0412 327 606	
North Bondi SLSC	Melissa Suriano	Surf Club Liason	0412 330 860	
NSW Police	Luke Barrett	ES LAC Traffic Services	0428 560 047	
Big Screen Projects	Toby Waley	AV/Camera Technicians	0414 979 990	
Freelance	Alan Boxsell	Sound/Mics	0435 125 400	
St Johns Ambulance	Wendy Yao	2IC - Operations Officer	0481 100 667	
Stop Slow Traffic Control	Dan Muller	Traffic Control	0410 981 798	
Gold Crest Security	Grant Heanes	Security	0410 546 783	
Galilee Public School	TBC	BBQ Vendor	TBC	
North Bondi SLSC	Melissa Suriano	BBQ Vendor	0412 330 860	
TIME	TASK	RESPONSIBILITY	SUPPLIER	DURATION
MONDAY 24th APRIL				
10:00	Memorial Clean	HMAS Kuttabul	Tony Froome	2 hours
14:00	Rehearsal	All	All	TBC
16:00	Big screen & AV equipment delivered	Toby Waley	Big Screen Projects	
17:00	Speakers/Lectern etc. delivered	Alan Boxsell	N/A	
21:00	Overnight security guard on site	Grant Heanes	Security	Site closed & secured overnight
TUESDAY 25TH APRIL				
4:00	BUMP IN			
4:00	Event team on site	Kate/Rob/Dave/James	Event Team	2 hours
4:00	VIP Seating & Red Carpet/Programs set up	TBC/Event team	Equipment set up	30 minutes
4:30	Volunteers on site & briefed	Volunteers/Event team	Volunteers/Event Team	15 minutes
4:45	Programs/Rosemary distributed	Volunteers/Event team/Legatees	Volunteers/Event Team	15 minutes
5:00	St John's Ambulance on site & set up	St Johns	St John	15 mins
5:00	BBQ Vendors in place & set up	BBQ Vendors x 2	BBQ Vendors	30 minutes
5:15	All participants (Sub Branch & SOER) muster at memorial for final checks and confirmation of roles	All	All	5 minutes
5:20	SOER/Volunteers on standby for VIP arrival	SOER/Volunteers	SOER/Volunteers	
5:30	VIP, sub branch members & general public arrival	TBC to distribute VIP wristbands	Guests	20-30 minutes
5:50	Flag orderly sets ANF to half mast	Flag orderly	NBRSL SB	5 minutes
5:55	MC gives Safety Briefing	David Sims		
6:00	DAWN SERVICE COMMENCES			
6:00	MC Welcome	David Sims	MC	2 minutes
6:02	MC Introduces Catafalque Party	David Sims	MC	10 seconds
6:02:10	Guard commander mounts Catafalque Party	SOER	Guard Commander	3 minutes
6:05:10	MC Introduces Chaplain to give the opening prayer	David Sims	MC	10 seconds
6:05:20	Chaplain gives opening prayer	Kim Kuchal	Chaplain	1 minute
6:06:20	MC Introduces first hymn	David Sims	MC	10 seconds
6:06:30	Hymn: The Recessional is sung	All	All	2 minutes
6:08:30	MC Introduces Chaplain to give scripture reading and provide thoughts for reflection	David Sims	MC	10 seconds
6:06:40	Chaplain gives scripture reading and thoughts for reflection	TBC	Chaplain	3 minutes
6:09:40	MC Introduces second hymn	David Sims	MC	10 seconds
6:09:50	Hymn: Abide With Me is sung	All	All	2 minutes
6:11:50	MC Introduces ANZAC Day Address	David Sims	MC	30 seconds
6:12:20	ANZAD Day Address	Commodore Lee Goddard	Guest Speaker	5 Minutes
6:17:20	MC invites VIP's and others to lay wreaths	David Sims/VIP guests	MC/VIP Guests	10 minutes
6:27:20	MC speaks on behalf of Sub Branch, Introduces the Ode, Last Post,Reveille, National Anthems	David Sims	MC	4 minutes
6:31:40	Last post played, one minute silence, Reveille (flag to Masthead)	TBC	Bugler/Piper/Flag Orderly	2 minutes

6:32:40	New Zealand national anthem	TBC	TBC	1 minute
6:33:40	Australian national anthem	TBC	TBC	1 minute
6:34:40	MC Introduce Chaplain to give blessing and grace	David Sims	MC	10 seconds
6:34:50	Chaplain gives blessing and grace	TBC	Chaplain	2 minutes
6:36:50	MC Introduces Guard Commander to dismount Catafalque Party	David Sims	MC	10 seconds
6:37:00	Guard Commander dismounts Catafalque Party	TBC	Guard Commander	3 minutes
6:40:00	MC closes service with thanks and admin instructions	David Sims	MC	2 minutes
<b>6:42:00</b>	<b>DAWN SERVICE CONCLUDES</b>			
	VIP's/Sub Branch Committee Members + Guests/Military Personnel commence entry into NBRSL Club			
<b>7:00</b>	<b>BUMP OUT (RAMSGATE AV)</b>			
7:00	All suppliers to commence bump out (Ramsgate Av)	All suppliers	All Suppliers	
7:00	Club open - VIP's, Sub Branch Committee Members/Guests & Military Personnel entry to club	Kate/Volunteer TBC	Event team	Event team to distribute wristbands on entry
7:00	Bar Open	Steve Lovat	Club Ltd.	All wristbands
7:00	Breakfast commences	Steve Lovat	Club Ltd.	All wristbands
8:00	Bump out concludes (Ramsgate Av)	All suppliers	All Suppliers	
<b>8:00:00 AM TBC</b>	<b>SITE CLOSED (RAMSGATE AV)</b>			
10:00	Bar closed for Military Personnel	Steve Lovat	Club Ltd.	TBC

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**REPORT**  
**TC/CV.01/17.03**

**Subject:** Multiple Streets - 'P DISABILITY ONLY' Zones

**TRIM No.:** A02/0273-02

**Author:** Sam Samadian, Traffic Engineer

**Director:** Emily Scott, Director, Waverley Renewal

---

**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs 5.5m of 'P DISABILITY ONLY' parking outside No. 3 Birrell Street, Queens Park immediately to the west of the "No Stopping" zone at Alt Lane.
2. Extends the length of the existing 'P DISABILITY ONLY' parking by 6.5 m to occupy the full frontage of the unit block at No. 7 Brae Street, Bronte.
3. Installs 6.5m of 'P DISABILITY ONLY' parking outside No. 13 Barracluff Avenue, Bondi Beach.
4. Installs 5.5m of 'P DISABILITY ONLY' parking outside No. 49 Glenayr Avenue, Bondi.
5. Installs 6.5m of 'P DISABILITY ONLY' parking outside No. 25 Park Parade, Bondi.
6. Removes the existing 'P Disability Only' zone outside No. 26 Knowles Avenue, North Bondi.

**1. Executive Summary**

Council has received representations from residents requesting consideration be given to the installation of 'P DISABILITY ONLY' parking zones to improve access at:

1. 3 Birrell Street, Queens Park,
2. 7 Brae Street, Bronte,
3. 13 Barracluff Avenue, Bondi Beach,
4. 49 Glenayr Avenue, Bondi,
5. 25 Park Parade, Bondi.

Council has also been advised the existing 'P DISABILITY ONLY' zones are no longer required at:

1. 26 Knowles Avenue, North Bondi,

Council will need to exercise its delegated functions to implement the proposal.

**2. Introduction/Background**

At the Waverley Traffic Committee meeting, held on 10 November 2005, the issue of installing 'P Disability Only' parking was discussed. Subsequently, at the Council meeting, held on 13 December 2005, it was resolved:

*That the following guidelines for assessing applications for a mobility parking space on a residential street be adopted:*

1. *Together with proof of their mobility-parking permit, applicants for a mobility parking space must have a vehicle registered to their address. They do not need to be the driver of the vehicle.*
2. *Applicants who have access to off-street parking will not be entitled to a mobility parking space.*
3. *At least three site visits will be carried out of a morning and evening for each application. These site visits will also be carried out annually. The level of parking within 50m either side of the property must be at or above 85% capacity on each separate occasion.*
4. *Letters will be sent to residents annually to determine if the mobility parking space is still required. Residents will also be requested to again provide proof of current registration and their mobility-parking permit.*

### 3. Technical Analysis

Table 1. Requests for new 'P DISABILITY ONLY' zones

Address	MPS Card	Off-Street Parking Available	On-street Parking Occupancy	Length of Zone	Recommend Installation	Comments
3 Birrell Street, Queens Park	Yes	Yes	90% in the afternoon	5.5 m	Yes	The garage width is 2.4m which is too narrow for the resident to park and alight from the vehicle
7 Brae Street, Bronte	Yes	No	95% in the afternoon	6.5 m	Yes	There is an existing disabled parking outside this property which is in use by another eligible resident.
13 Barracluff Avenue, Bondi Beach	Yes	No	90% in the afternoon	6.5 m	Yes	
49 Glenayr Avenue, Bondi	Yes	No	95% in the afternoon	6.5 m	Yes	Awaiting evidence of MPS card, vehicle registration.
25 Park Parade, Bondi	Yes	No	95% in the afternoon	6.5 m	Yes	Awaiting evidence of MPS card, vehicle registration.

Table 2. 'P DISABILITY ONLY' zones no longer required

Address	WTC Approval	Surrounding Parking Restrictions	Recommend Removal	Comments
26 Knowles Avenue, North Bondi	April 2015	Unrestricted	Yes	

Under the [NSW] *Road Rules 2014*, Mobility Parking Scheme card holders are provided with the following concessions:

- Parking is permitted in 'P DISABILITY ONLY' zones.

- When parking in a metered, coupon or ticket parking area, no charge is applied
- Where parking is limited by a sign to more than 30 minutes, the vehicles can park for an unlimited time.
- Where parking is limited by a sign to 30 minutes, the vehicle can park for 2 hours.
- Where parking is limited by a sign to less than 30 minutes, the vehicle can park for a maximum of 30 minutes.
- When stopping in a 'No Parking' zone, the vehicle can drop off or pick up passengers or goods for a period of 5 minutes.

Guidelines used:

- *Transport Administration Act 1988*
- *RMS's Delegation to Councils; Regulation of Traffic*
- *RMS's Signs Database*
- *[NSW] Road Rules 2014.*

#### **4. Financial Information for Council's Consideration**

If changes to signs and/or linemarking are approved, Council will supply and install the signs and/or linemarking and remove existing signs with funds from existing budgets.

#### **5. Attachments:**

Nil