



WAVERLEY TRAFFIC COMMITTEE MEETING

A meeting of the WAVERLEY TRAFFIC COMMITTEE will be held by video conference at:

10.00 AM, THURSDAY 25 MARCH 2021

Waverley Council
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AGENDA

Apologies

Declarations of Interests

Adoption of previous Minutes by Council - 25 February 2021 9

The recommendations contained in Part 1 - Matters Proposing That Council Exercise Its Delegated Functions - of the Minutes of the Waverley Traffic Committee meeting held on 25 February 2021 were adopted by Council at its meeting on 16 March 2021, with the following changes:

1. TC/C.02/21.02 - 40km/h Speed Limit Changes – Traffic Control Devices – Park Parade, Bondi

Council adopted the recommendations of the Traffic Committee subject to it being amended to read as follows:

‘That Council:

1. Approves the design of the traffic calming devices consisting of 75 mm rubber cushions as shown in Attachment 1 (Option 3) of the report, including a 12-month trial of the rubber speed cushions outside 10 and 32 Park Parade.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.’

2. TC/C.04/21.02 - 40km/h Speed Limit Changes – Review of Treatments on Bourke Street, Bennett Street and Botany Street

Council adopted the recommendations of the Traffic Committee subject to it being amended to read as follows:

‘That Council:

1. Installs the following traffic calming treatments:
 - (a) A mountable roundabout at the intersection of Botany Street and Allens Parade, Bondi Junction, as shown in Figure 2 of the report.
 - (b) 75 mm rubber speed cushions at 27 Bennett Street, Bondi, as shown in Figure 3 of the report, for a trial for 12 months.
2. Notes the submissions from residents of Bourke Street and does not install rubber speed cushions at 13 Bourke Street, Queens Park.’

PART 1 – MATTERS PROPOSING THAT COUNCIL EXERCISE ITS DELEGATED FUNCTIONS

NOTE: *The matters listed under this part of the agenda propose that Council either does or does not exercise the traffic related functions delegated to it by TfNSW. The recommendations made by the Committee under this part of the agenda will be submitted to Council for adoption.*

TC/C STATE ELECTORATE OF COOGEE**TC/C.01/21.03 Bronte Cutting Safety Upgrade (A20/0015)..... 24****COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Endorses the 100% detailed design for Bronte Cutting Safety Upgrade, on Calga Place, between Bronte Road and Macpherson Street, attached to this report (Attachment 1).
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the design should circumstances arise during the delivery of the project.

TC/C.02/21.03 York Road and Baronga Avenue - Slip Lane (SF21/1014)..... 55**COUNCIL OFFICER'S PROPOSAL:**

That Council approves the construction of a left-turn slip lane on York Road at intersection with Baronga Avenue and associated lines and signs as shown in Attachment 1.

TC/C.03/21.03 40 km/h Speed Limit Changes - Traffic Control Devices - Park Parade and Dickson Street at Birrell Street, Bondi (A18/0579) 63**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Approves the installation of centre line marking with kerb extensions at the intersection of Park Parade and Birrell Street as shown in Figure 2.
2. Approves the installation of a pedestrian refuge island with kerb extensions at the intersection of Dickson Street and Birrell Street as shown in Figure 3.
3. Approves the installation of a 'No Right Turn, 8am-9:30am and 2:30pm-4pm, Monday to Friday' restriction on the southbound approach to Birrell Street at the intersection of Park Parade and Birrell Street.
4. Approves the installation of a 'No Right Turn, 8am-9:30am and 2:30pm-4pm, Monday to Friday' restriction on the northbound approach to Birrell Street at the intersection of Dickson Street and Birrell Street.
5. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

TC/C.04/21.03	49 Blair Street, Bondi Beach - 'P Disability only' and 'Motor Bikes Only' Zones (A02/0273)	71
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COUNCIL OFFICER'S PROPOSAL:

That Council installs:

1. A 5.4 metre 'P Disability Only' parking zone outside 49 Blair Street, Bondi Beach.
2. A 3.6 metre 'P Motor Bikes Only' zone between the driveway of 47 Mitchell Street, Bondi Beach and the new 'P Disability Only' zone.

TC/C.05/21.03	40 km/h Speed Limit Changes - Traffic Control Devices - Tamarama Marine Drive (A18/0579)	75
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COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the detailed design of the pedestrian crossing and relocation of bus stops on Tamarama Marine Drive, Tamarama as shown in Attachment 1.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

TC/V **STATE ELECTORATE OF VAUCLUSE**

TC/V.01/21.03	Old South Head Road and Diamond Bay Road, Vaucluse - Installation of Double Centre (BB) Line Marking (A14/0145)	81
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COUNCIL OFFICER'S PROPOSAL:

That Council installs double centre (BB) line marking on Diamond Bay Road at the intersection with Old South Head Road, Vaucluse, as shown in Attachment 1 of the report.

TC/V.02/21.03	32 Tower Street, Vaucluse - Removal of 'P Disability Only' Zone (A20/0534).....	87
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COUNCIL OFFICER'S PROPOSAL:

That Council removes the existing 5.5 metre long 'P Disability Only' zone outside 32 Tower Street, Vaucluse.

TC/V.03/21.03 4 MacDonald Street, Vaucluse - Construction Zone (A03/2514-04)..... 89**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 10.9 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 4 MacDonald Street, Vaucluse.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

TC/V.04/21.03 Old South Head Road, Bondi Junction- Bondi Road to Bon Accord Avenue - Shared Path Upgrade (SF19/3435) 93**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs shared pathing including continuous footpath treatments at the intersections of Old South Head Road with Paul Street and Old south Head Road with Bon Accord Avenue as shown in Attachment 1.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

TC/V.05/21.03 Barracluff Avenue, Bondi Beach - Traffic Calming Scheme (A03/0042-04)..... 102**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Approves the traffic calming scheme for Barracluff Avenue as shown in Attachment 1.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

TC/V.06/21.03 2-4 Jaques Avenue, Bondi Beach - Construction Zone (A03/2514-04)..... 109**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 9 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 2-4 Jaques Avenue, Bondi Beach.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

TC/V.07/21.03 43-45 Hall Street, Bondi Beach - Construction Zone (A03/2514-04)..... 113**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 18.5 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 43-45 Hall Street, Bondi Beach.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

TC/V.08/21.03 Francis Street south of Wellington Street, Bondi Beach - Proposed Double Centre (BB) Line Marking (A03/0042-04) 117**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs 10 metres of B-B double centre line marking on Francis Street south of Wellington Street, Bondi Beach, as shown in Attachment 1 of the report.
2. Extends the existing statutory 'No Stopping' restrictions on Francis Street south of Wellington Street, Bondi Beach, to meet the minimum 10 metre distance requirement as shown in Attachment 1 of the report.

TC/CV ELECTORATES OF COOGEE AND VAUCLUSE**TC/CV.01/21.03 Bondi to Bronte Coastal Walk - One Way System (A20/0500)..... 123****COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Does not implement a permanent one-way road system for Gaerloch Avenue and Dellview Street.
2. Undertakes a detailed assessment of appropriate crossing treatments at:
 - (a) Wilga Street on the eastern side of Sandridge Street - a continuous footpath treatment.
 - (b) Fletcher Street on the eastern side of Sandridge Street - a marked pedestrian crossing.
 - (c) Kenneth Street on the eastern side of Alexander Street - a marked pedestrian crossing with kerb extensions.
 - (d) Southeast of Gaerloch Avenue at 22 Gaerloch Avenue (the 90-degree bend in the road) - a continuous footpath treatment.
3. Delegates authority to the Executive Manager, Infrastructure Services, to investigate alternative designs as part of the detailed design assessment.

TC/CV.02/21.03	Flood Street and Evans Street linemarking - Resheeting and Linemarking (A18/0579)	132
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COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the line-marking of the pavement within:
 - (a) Flood Street, Bondi (Attachment 1).
 - (b) Evans Street, Bronte (Attachment 2).
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

PART 2 – TRAFFIC ENGINEERING ADVICE

NOTE: The matters listed under this part of the agenda seek the advice of the WTC only and do not propose that Council exercise its delegated functions at this point in time (though they may or may not require it in the future).

TC/TEAC STATE ELECTORATE OF COOGEE

Nil

TC/TEAV STATE ELECTORATE OF VAUCLUSE

Nil

TC/TEACV ELECTORATES OF COOGEE AND VAUCLUSE

Nil

**MINUTES OF THE WAVERLEY TRAFFIC COMMITTEE
MEETING HELD BY VIDEO CONFERENCE ON
THURSDAY, 25 FEBRUARY 2021**



Voting Members Present:

Cr P Masselos	Waverley Council – Chair
S/Cst A Birchansky	NSW Police – Eastern Suburbs Police Area Command – Traffic Services
Mr M Carruthers	Transport for NSW – Network and Safety Officer
Mr P Pearce	Representing Marjorie O’Neill, MP, Member for Coogee
Ms J Zin	Representing Gabrielle Upton, MP, Member for Vaucluse

Also Present:

Mr B Gidiess	State Transit – Traffic and Services Manager (Eastern Region)
Cr T Kay	Waverley Council – Alternate Chair
Mr D Joannides	Waverley Council – Executive Manager, Infrastructure Services
Mr C Hutcheson	Waverley Council – Service Manager, Traffic and Transport
Mr M Almuhanha	Waverley Council – Senior Traffic Engineer

At the commencement of proceedings at 10.00 am, those present were as listed above, with the exception of Ms J Zin, who arrived at 10.06 am.

At 10.40 am, Mr P Pearce left the meeting and did not return.

Apologies

There were no apologies.

Declarations of Pecuniary and Non-Pecuniary Interests

The Chair called for declarations of interest and none were received.

Adoption of Previous Minutes by Council - 26 November 2020

The recommendations contained in Part 1 – Matters Proposing that Council Exercise its Delegated Functions – of the minutes of the Waverley Traffic Committee meeting held on 26 November 2020 were adopted by Council at its meeting on 16 February 2021.

ITEMS BY EXCEPTION

The following items on the agenda were dealt with together and the Council Officer's Proposal for each item was unanimously supported by the Committee:

TC/C.01/21.02	20 Scott Street, Bronte - Temporary Road Closures for Modular Housing Delivery.
TC/C.03/21.02	40 km/h Speed Limit Changes - Traffic Control Devices (Line Marking Group).
TC/C.05/21.02	Denison Street, Bondi Junction - Community Bus and Taxi Parking.
TC/C.06/21.02	Birrell Street and Dickson Street Intersection, Bronte - Installation of C3 Line Marking.
TC/C.07/21.02	Carlisle Street, Tamarama - Addition of Parking Space.
TC/C.08/21.02	11 Pacific Avenue, Tamarama - 'P Motor Bikes Only' Parking.
TC/C.09/21.02	85 Birrell Street, Queens Park - 'P Disability Only' Zone.
TC/C.10/21.02	3 Palmerston Avenue, Bronte - 'P Disability Only' Zone.
TC/C.11/21.02	292 Birrell Street, Bondi - No Stopping Zone.
TC/C.13/21.02	Ruthven Lane, Bondi Junction - Modification of No Parking Zone.
TC/C.14/21.02	5 Pembroke Street, Bronte - No Parking Zone.
TC/C.15/21.02	14 Fletcher Street, Bondi - Construction Zone.
TC/V.02/21.02	92 Ramsgate Avenue, Bondi Beach - Construction Zone.
TC/V.03/21.02	3 Lancaster Road, Dover Heights - Construction Zone.
TC/V.04/21.02	116-118 Brighton Boulevard, North Bondi - Construction Zone.
TC/V.05/21.02	45-47 Ramsgate Avenue, Bondi Beach - Construction Zone.
TC/V.06/21.02	39 Beach Road, Bondi Beach - Construction Zone.
TC/V.07/21.02	Mons Street and Tower Street Intersection, Vaucluse - No Stopping Zones.
TC/V.10/21.02	Yeshiva College - Modification of Pick-up/Drop-off Zone Restrictions.
TC/V.11/21.02	Notts Avenue, Bondi Beach - Changes to Loading Zone Signage.
TC/V.12/21.02	43 Mitchell Street, North Bondi (O'Donnell Street Frontage) - 'P Disability Only' Zone.

TC/V.13/21.02 92 O'Brien Street, Bondi Beach - 'P Disability Only' Zone.

PART 1 – MATTERS PROPOSING THAT COUNCIL EXERCISE ITS DELEGATED FUNCTIONS

NOTE: The matters listed under this part of the agenda propose that Council either does or does not exercise the traffic related functions delegated to it by TfNSW. The recommendations made by the Committee under this part of the agenda will be submitted to Council for adoption.

TC/C STATE ELECTORATE OF COOGEE

TC/C.01/21.02 20 Scott Street, Bronte - Temporary Road Closures for Modular Housing Delivery (DA-194/2019)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Temporarily closes Scott Street between Marroo Street and Pacific Lane, and Marroo Street between Boundary Street and Chesterfield Parade, Bronte, over two days during a working week (7.00 am on day 1 to 11.30 pm on day 2) on dates to be agreed with Council, in accordance with the Construction Traffic and People Plan of Management attached to the report, subject to the applicant:
 - (a) Notifying NSW Police and Transport for NSW and obtaining relevant approvals.
 - (b) Notifying the State Transit Authority, NSW Ambulance Service and NSW Fire and Rescue.
 - (c) Notifying affected residents and businesses at least five working days prior to the event, with a follow-up notification on the day prior to day 1 of the closure.
 - (d) Obtaining relevant approvals from the National Heavy Vehicle Regulator (NHVR).
 - (e) Abiding by the attached conditioned approval from Randwick City Council to occupy parking spaces on roads within the Randwick local government area.
 - (f) Obtaining approval of any tree pruning from Council's tree management team.
 - (g) Ensuring access to affected residents and liaising with other residents, schools and businesses during the duration of the temporary closure.
 - (h) Using Transport-for-NSW-accredited traffic controllers.
 - (i) Covering all costs associated with closing the road, including traffic control.
 - (j) Completing all works by 11.30 pm on day 2 when full access to Scott Street and Marroo Street will be restored.
 - (k) Obtaining written approval from Randwick City Council in relation to implementation of parking restrictions on Randwick Council roads.
2. Delegates authority to the Executive Manager, Infrastructure Services, to approve the dates that the operation will occur.

3. Requires a Council compliance officer to be on-site during the day of the modular housing crane lift at the applicant's expense.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.02/21.02 40 km/h Speed Limit Changes - Traffic Control Devices - Park Parade, Bondi (A18/0579)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the design of the rubber hump traffic control devices as shown in Attachment 1 (Option 3) of the report, including a 12-month trial of the rubber speed humps outside 10 and 32 Park Parade.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted subject to clause 1 being amended to read as follows:

'Approves the design of 75 mm rubber hump traffic control devices as shown in Attachment 1 (Option 3) of the report, including a 12-month trial of the rubber speed humps outside 10 and 32 Park Parade.'

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.03/21.02 40 km/h Speed Limit Changes - Traffic Control Devices (Line Marking Group) (A18/0579)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the designs of the 18 signage and line marking traffic control treatments attached to this report for construction at:
 - (a) Alfred Street.
 - (b) Birrell Street between Bronte Road and Carrington Road.
 - (c) Birrell Street between Goldie Avenue and Henrietta Street.

- (d) Bronte Road between Birrell Street and Church Street.
 - (e) Bronte Road between Birrell Street and Ebley Street.
 - (f) Bronte Road between Victoria Street and Church Street.
 - (g) Bronte Road (near Bronte House).
 - (h) Chesterfield Parade between Arden Street and St Thomas Street.
 - (i) Fletcher Street Between Denham Street and Sandridge Street.
 - (j) Gipps Street between Henrietta Street and Bronte Road.
 - (k) Grafton Street Between Leswell Street and Nelson Street.
 - (l) Grafton Street between Newland Street and Leswell Street.
 - (m) Langlee Avenue (east of Seaview Street).
 - (n) Macpherson Street between Arden Street and Evans Street.
 - (o) Macpherson Street between Evans Street and St Thomas Street.
 - (p) Ocean Street.
 - (q) Pacific Street between Gardyne Street and Bronte Road.
 - (r) Waverley Street between Botany Street and Hollywood Avenue.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.04/21.02 40 km/h Speed Limit Changes - Review of Treatments on Bourke Street, Bennett Street and Botany Street (A18/0579)

COUNCIL OFFICER'S PROPOSAL:

That Council installs the following traffic calming treatments:

- 1. Rubber speed cushions at 13 Bourke Street, Queens Park, as shown in Figure 1 of this report.
- 2. A mountable roundabout at the intersection of Botany Street and Allens Parade, Bondi Junction, as shown in Figure 2 of this report.

3. Rubber speed cushions at 27 Bennett Street, Bondi, as shown in Figure 3 of this report.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted subject to being amended to read as follows:

That Council:

1. Installs the following traffic calming treatments:
 - (a) A mountable roundabout at the intersection of Botany Street and Allens Parade, Bondi Junction, as shown in Figure 2 of the report.
 - (b) 75 mm rubber speed cushions at 27 Bennett Street, Bondi, as shown in Figure 3 of the report.
2. Notes the submissions from residents of Bourke Street and does not install rubber speed cushions at 13 Bourke Street, Queens Park.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

A and N Margolis, S Hovell, B Fitzpatrick, G and D Blackwood, A Gray, and N and C Gottheiner addressed the meeting.

TC/C.05/21.02 Denison Street, Bondi Junction - Community Bus and Taxi Parking (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council installs an 11.4 metre 'No Stopping, Authorised Vehicles and Taxis Excepted, 10 Min Only' zone on the eastern side of Denison Street adjacent to the intersection of Spring Street.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.06/21.02 Birrell Street and Dickson Street Intersection, Bronte - Installation of C3 Line Marking (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council installs golden yellow C3 line marking along the existing 'No Stopping' zone on the south-east corner of the intersection of Birrell Street with Dickson Street for 10 metres on either side.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.07/21.02 Carlisle Street, Tamarama - Addition of Parking Space (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council reduces the length of the existing No Stopping line-marking around the cul-de-sac on upper Carlisle Street to provide 5 extra metres for car parking at the retaining wall (adjacent to 31 Carlisle Street).

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.08/21.02 11 Pacific Avenue, Tamarama - 'P Motor Bikes Only' Parking (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 'P Motor Bikes Only' zone between the driveways of 11 and 12 Pacific Avenue, Tamarama.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the 'P Motor Bikes Only' parking zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.09/21.02 85 Birrell Street, Queens Park - 'P Disability Only' Zone (A18/0719)

COUNCIL OFFICER'S PROPOSAL:

That Council installs a 6 metre 'P Disability Only' parking zone outside 85 Birrell Street, Queens Park.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.10/21.02 3 Palmerston Avenue, Bronte - 'P Disability Only' Zone (A18/0719)**COUNCIL OFFICER'S PROPOSAL:**

That Council installs a 6 metre 'P Disability Only' parking zone outside 3 Palmerston Avenue, Bronte.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.11/21.02 292 Birrell Street, Bondi - No Stopping Zone (A14/0145)**COUNCIL OFFICER'S PROPOSAL:**

That Council installs a 10 metre 'No Stopping' zone on the northern side of Birrell Street adjacent to 292 Birrell Street.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.12/21.02 Albert Street and St Thomas Street Intersection, Bronte - No Stopping Zones (A14/0145)**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a No Stopping zone at the intersection of St Thomas Street and Albert Street for:
 - (a) 8 metres on the western side of St Thomas Street north of Albert Street.
 - (b) 8 metres on the northern side of Albert Street west of St Thomas Street.
2. Installs unbroken yellow C3 lines along the 'No Stopping' zone.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That:

1. The Council Officer's Proposal not be adopted.
2. Council consults the community on introducing 8 metre signposting rather than 10 metre statutory signposting at the intersection of St Thomas Street and Albert Street, with an explanation of the

rationale.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.13/21.02 Ruthven Lane, Bondi Junction - Modification of No Parking Zone (A02/0637)

COUNCIL OFFICER'S PROPOSAL:

That Council modifies the existing 'No Parking: 8 am–9 am Thursday' zones in Ruthven Lane, Bondi Junction, to 'No Parking 6 am–10 am Thursday' and '1 Hour Parking (Permit Holders Excepted)' at other times between 8 am and 6 pm, Monday to Saturday.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.14/21.02 5 Pembroke Street, Bronte - No Parking Zone (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council installs a 'No Parking' sign on the eastern side of Pembroke Street opposite 5 Pembroke Street as shown in Figures 1 and 2 of the report.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/C.15/21.02 14 Fletcher Street, Bondi - Construction Zone (A03/2514-04)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 23 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone fronting 14 Fletcher Street on Glen Street, Bondi.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/V STATE ELECTORATE OF VAUCLUSE

TC/V.01/21.02 Bondi Pavilion Restoration and Conservation Project (SF21/617)

COUNCIL OFFICER'S PROPOSAL:

That Council approves the proposal for Buildcorp to undertake construction forecourt works in two stages on Queen Elizabeth Drive (QED) as part of the Bondi Pavilion Restoration and Conservation project, as shown in Attachment 1 to the report.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Vacluse, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/V.02/21.02 92 Ramsgate Avenue, Bondi Beach - Construction Zone (A03/2514-04)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 9 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 92 Ramsgate Avenue, Bondi Beach.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vacluse was not present for the vote on this item.

TC/V.03/21.02 3 Lancaster Road, Dover Heights - Construction Zone (A03/2514-04)**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 14.5 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone at 3 Lancaster Road, Dover Heights.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vaucluse was not present for the vote on this item.

TC/V.04/21.02 116-118 Brighton Boulevard, North Bondi - Construction Zone (A03/2514-04)**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs an 18 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 116-118 Brighton Boulevard, North Bondi.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vaucluse was not present for the vote on this item.

TC/V.05/21.02 45-47 Ramsgate Avenue, Bondi Beach - Construction Zone (A03/2514-04)**COUNCIL OFFICER'S PROPOSAL:**

That Council:

1. Installs a 20 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles

Excepted' construction zone along the frontage of 45-47 Ramsgate Avenue, Bondi Beach.

2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vacluse was not present for the vote on this item.

TC/V.06/21.02 39 Beach Road, Bondi Beach - Construction Zone (A03/2514-04)

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 15 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone fronting 39 Beach Road, Bondi Beach.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vacluse was not present for the vote on this item.

TC/V.07/21.02 Mons Street and Tower Street Intersection, Vacluse - No Stopping Zones (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council installs 10 metre 'No Stopping' zones on the south-eastern and south western corners of the intersection of Mons Street and Tower Street, Vacluse.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vaucluse was not present for the vote on this item.

TC/V.08/21.02 59 Gould Street, Bondi Beach - Reduction of No Stopping Zone (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council reduces the existing No Stopping Zone on the southern side of Glasgow Avenue at Gould Street from 26 metres to 10 metres.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Vaucluse, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/V.09/21.02 Turner Street, Bronte - Installation of C3 Line Marking along No Stopping Zones (A14/0145)

This item was listed on the agenda under the Electorate of Vaucluse in error. The item relates to the Electorate of Coogee.

COUNCIL OFFICER'S PROPOSAL:

That Council installs C3 line marking along the existing statutory 'No Stopping' zones at the intersection of Turner Street and Ashley Street, Bronte, as shown in Figure 3 of this report.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Coogee, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

TC/V.10/21.02 Yeshiva College - Modification of Pick-up/Drop-off Zone Restrictions (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council modifies the existing 'No Parking, 7.30 am–9 am Mon–Fri, 3.30 pm–6 pm Mon–Thu, 2.30 pm–3.30 pm Fri, School Days' (pick-up/drop-off) zone in front of Yeshiva College on Flood Street, Bondi, to 'No Parking, 7.30 am–9 am Mon–Fri, 3 pm–5.30 pm Mon–Thu, 1.30 am–2.30 pm FRI.'

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vaucluse was not present for the vote on this item.

TC/V.11/21.02 Notts Avenue, Bondi Beach - Changes to Loading Zone Signage (A14/0145)

COUNCIL OFFICER'S PROPOSAL:

That Council replaces the existing loading zone signs at the lower end of Notts Avenue, Bondi with 'Loading Zone, 8am-1pm' plus '2P, Meter Registration 1 pm-10pm, Permit Holders Excepted Area 8' signs.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vaucluse was not present for the vote on this item.

TC/V.12/21.02 43 Mitchell Street, North Bondi (O'Donnell Street Frontage) - 'P Disability Only' Zone (A18/0719)

COUNCIL OFFICER'S PROPOSAL:

That Council installs a 5.4 metre 'P Disability Only' parking zone outside 43 Mitchell Street, North Bondi (O'Donnell Street frontage).

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vaucluse was not present for the vote on this item.

TC/V.13/21.02 92 O'Brien Street, Bondi Beach - 'P Disability Only' Zone (A18/0719)

COUNCIL OFFICER'S PROPOSAL:

That Council installs a 5.4 metre 'P Disability Only' parking zone outside 92 O'Brien Street, Bondi Beach.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

The representative of the Member for Vacluse was not present for the vote on this item.

TC/CV ELECTORATES OF COOGEE AND VAUCLUSE

TC/CV.01/21.02 Queen Elizabeth Drive - 'Electric Vehicle Excepted Only While Charging' Zone (A17/0477)

This item was listed on the agenda under the Electorates of Coogee and Vacluse in error. The item only relates to the Electorate of Vacluse.

COUNCIL OFFICER'S PROPOSAL:

That Council installs a 'No Parking Electric Vehicles Excepted Only While Charging' zone for one space adjacent to an existing space with the same restrictions on Queen Elizabeth Drive.

WTC RECOMMENDATION (UNANIMOUS SUPPORT):

That the Council Officer's Proposal be adopted.

Voting members present for this item: Representative of the Member for Vacluse, NSW Police representative, TfNSW representative and Waverley Council representative (Chair).

THE MEETING CLOSED AT 10.49 AM.

.....
SIGNED AND CONFIRMED
MAYOR
16 MARCH 2021

REPORT
TC/C.01/21.03

Subject: Bronte Cutting Safety Upgrade

TRIM No: A20/0015

Author: Amanda Tipping, Project Manager, Major Projects
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Endorses the 100% detailed design for Bronte Cutting Safety Upgrade, on Calga Place, between Bronte Road and Macpherson Street, attached to this report (Attachment 1).
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the design should circumstances arise during the delivery of the project.

1. Executive Summary

This report seeks Council endorsement of the 100% detailed design for improvements to Bronte Cutting and Calga Place, between Bronte Road and Macpherson Street, Bronte Beach, as detailed in Attachment 1.

The Bronte Cutting safety upgrade project has largely focused on improving pedestrian and vehicle safety on Calga Place through Bronte Cutting.

The 100% detailed design follows a concept design which was considered at the November 2020 Council Meeting, placed on exhibition for community consultation from 23 November 2020 to 21 December 2020 and reported back to the February 2021 Council Meeting on 16 where it was approved to proceed to detailed design phase with minor changes and investigations.

Figure 1 presents the site location. The 100% design is attached.



Figure 1. Area of investigation – Calga Place, between Bronte Road and Macpherson Street.

2. Introduction/Background

Bronte Cutting forms part of one of the most significant coastal walks in the world. The Bondi to Bronte coastal walk attracts an estimated one million walkers per year and during peak times (such as when the Sculpture by the Sea event is held in Spring each year), as many as 500 pedestrians per hour.

The component of the coastal walk along Bronte Cutting currently has a temporary footpath within the roadway for around 140 metres and no footpath for around 110 metres resulting in conflicts between pedestrians and cars. The proposed works will result in a dedicated footpath along these sections.

The pedestrian conflict is referenced in several Council strategic documents and other studies, such as the Bronte Plan of Management (PoM), Waverley People Movement and Places (WPMP), Bronte Park Universal Access Study, as well as in Council resolutions. The local community has also identified the problem in public consultation for the Bronte PoM and for the Bronte Access Study. During previous consultation with the community, safety concerns were raised for pedestrians through the Cutting due to conflict between traffic flow, parking and pedestrians walking on the road. A continuous dedicated pedestrian walkway was requested, linking Bronte Park and the Coastal Walk.

In March 2018, Council requested Council officers to report to the Strategic Planning and Development Committee with options for the improved pedestrian connection in the Bronte Cutting, consistent with the recommendations contained in WPMP and Bronte PoM, including a stakeholder consultation strategy. As a result, in September 2018 Council endorsed a pilot project to pedestrianize part of the Bronte Cutting by temporarily removing 21 car spaces in order to connect the existing footpath with Calga Reserve and the Coastal Walk. The pilot project was installed in November 2018.

While the pilot project was in place, Council consulted with the community and other relevant stakeholders to communicate options for the future and seek feedback on the pilot project. For this, Council organised intercept surveys, an information session, and a community workshop. The consultation reached over 500 people with a high number of responses expressing support for the temporary footpath solution.

The key concerns about a future solution from the ideas presented to the community were mainly related to the impact on the heritage significance of the Bronte Cutting, impacts on biodiversity and loss of parking.

The majority of the engaged community want to preserve the natural landscape and heritage of Bronte Cutting, as well as to not impact local biodiversity.

In April 2019, a meeting was held with the Mayor, Councillors and Council officers to clarify the scope of works for the project in conjunction with approval of funding requirements.

Council engaged GHD Pty Ltd in May 2020 to carry out an options analysis study and concept and detailed design package for Calga Place, including Bronte Cutting.

At the August 2020 Council meeting, Council noted the outcome of the options analysis study, which investigated footpath and shared zone options and approved to proceed with the concept development of the footpath, while investigating other options.

At the November 2020 Council meeting, the footpath concept design was approved to proceed to community consultation. Community consultation took place from 23 November 2020 to 21 December 2021 where both the concept design and the Review of Environmental Factors (REF) were on display. Following community consultation, feedback and Council officer's recommendations associated with the project were reported to Council Meeting on 16 February 2021. Council approved the recommendations and to proceed to detailed design.

Table 1. Previous recommendation and Council decision.

Meeting and Date	Minute No.	Recommendation/decision
Council 16 February 2021	CM/7.12/21.02	<p>That Council:</p> <ol style="list-style-type: none"> 1. Receives and notes the summary of community feedback attached to the report. 2. Endorses the recommended changes to the concept design proposal set out in the report, including: <ol style="list-style-type: none"> (a) Investigating options and the design of existing speed bumps to allow a gap to improve cyclist accessibility along Calga Place. (b) Undertaking tube counts to monitor speed along Calga Place to determine if additional traffic calming treatments are required to slow traffic. (c) Investigating and determining whether a pedestrian crossing at the steps from Bronte Road is warranted. (d) Not proceeding with any lighting along Calga Place. 3. Proceeds to detailed design of the footpath, with a report to be considered by the Waverley Traffic Committee before proceeding to procurement for the construction phase.
Council 17 November 2020	CM/7.14/20.11	<p>That Council:</p> <ol style="list-style-type: none"> 1. Receives and notes the outcome of the feasibility study for the Bronte Cutting Project. 2. Endorses Option C (loss of one Bronte SLSC Parking space, converted to motorbike parking) as the preferred concept option for Bronte Cutting Project, as detailed in the attachments to this report.

		<p>3. Publicly exhibits the concept design, summary feasibility study report and review of environmental factors for 28 days, and consults with impacted residents, and the Bronte Beach and Bronte precinct committees and any other interested precincts.</p> <p>4. Notes that, following the public exhibition period, a further report will be prepared for Council summarising the consultation process, key feedback and recommending revision to concept design, as necessary.</p> <p>5. Approves the allocation of an additional \$550,000 as part of the Q2 budget amendment to enable the finalisation of the design and commencement of construction in Q4.</p> <p>6. Notes that the remaining construction budget will be included in the 2021–22 Capital Works Program.</p> <p>7. Notes that:</p> <p>(a) The cost of rebuilding the Bronte Road to Calga Place steps has not been budgeted.</p> <p>(b) The cost of repairing the Bronte Road to Calga Place steps has been budgeted.</p> <p>8. Notes the sandstone heritage wall will not be altered or impacted by the Bronte Cutting Project.</p>
Council 18 August 2020	CM/7.10/20.08	<p>That Council:</p> <p>1. Receives and notes the outcome of the options analysis study for the Bronte Cutting Project.</p> <p>2. Endorses Option 1B (2.5–3 m wide footpath, Calga Place) as the preferred option for Bronte Cutting Project, as detailed in the attachment to this report, subject to:</p> <p>(a) Bronte SLSC feedback during the concept design phase, including assurance that parking movements are acceptable when occupancy is high.</p> <p>(b) Investigating widening the 50 m section of footpath on the eastern side of Calga Place near the steps from Bronte Road instead of a realignment of the grass verge and impacts to the steps and heritage sandstone wall.</p> <p>3. Develops Option 1B, informed by the outcomes of the investigations, to a concept design suitable for consulting with the community.</p> <p>4. Notes that the developed Option 1B will be reported back to Council prior to consulting with the community, including the</p>

		<p>Bronte SLSC.</p> <p>5. Keeps the heritage items within the Bronte Cutting intact.</p> <p>6. Investigates a widening of the coastal walk and stairs from Calga Place to the Bronte Community centre.</p> <p>7. Ensures that the creative and safety lighting of the project is sympathetic to the environment and does not adversely impact on adjoining residents.</p>
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3. Technical Analysis

The proposed design is contained in Attachment 1 to this report. Key features are:

1. A new footpath to be concrete with widths varying between 1 m to 3 m along Calga Place.
2. The travel lane is to be a minimum of 3.2 metres wide.
3. Parking spaces are to be minimum of 2.1 metres wide and 6 metres long.
Existing speed humps are to be replaced with speed humps with a gap in the middle to allow for cyclists to ride through.
4. There is no net loss of parking spaces.
5. One Bronte Surf Lifesaving Club (BSLSC) parking space near the stairs from Bronte Road to be converted to motorbike parking spaces, as approved by BSLSC.
6. Redesign and reconstruction of affected stairs into new road level at the realigned kerb and gutter at the northern end of Calga Place.
7. Realignment of 36 metres of the existing western kerb line by approximately 300mm directly south of the Bronte Road stairs.
8. Resheeting from existing new footpath (part of Bronte village upgrade project) to the top of Calga place at Macpherson Street.
9. Old parking meter bays to be removed, with the exception of bay No. 851.
10. Existing non complaint DDA parking at coastal walk entrance, south of the Cutting to be improved.
11. New kerb buildouts and pram ramps, planting on eastern buildout, at the exit of Calga Place on Macpherson Street
12. No lighting
13. No further traffic calming treatments, as confirmed by an assessment carried out by Waverley Traffic Team.
14. No pedestrian crossing at the steps from Bronte Road, as confirmed by Waverley Traffic Team.

Discussion of specific issues

Disabled parking

The existing disabled space at the southern end of the cutting (where the coastal walk leaves Calga Place) has been lengthened from 6 to 7.7 metres. A new 1.6 to 1.9 metre wide footpath is to be constructed adjacent to the parking space where there currently is no formal path. Kerb ramps are not feasible adjacent to the space.

The longitudinal gradient is steeper than the 1 in 33.3 recommended in Australian Standard AS290.5. This will restrict the use of the space by some wheelchair users. However, it will still be usable by other people with a disability. The current location provides access directly to the Coastal walk. Relocation of the space elsewhere would have to be further south and would remove this direct access.

Motor bike parking

Motor bike parking has been provided adjacent to the stairs from Bronte Road. Individual spaces are not marked. This allows for closer parking of motor bikes resulting in more spaces. It results in 5 to 6 motor bikes fitting within the bay. Marking of individual spaces would provide parking for only 4 motor bikes.

Traffic speeds

Concerns have been raised about traffic speeds along Calga Place. A weekly traffic volume / speed survey (tube count) was undertaken on Calga Place from Wednesday 10th to Tuesday 15th February.

From the survey:

1. The southbound traffic flows peaked in the morning between 6.00 and 8.00 am.
2. Peak hourly flows recorded were 114 vehicles between 6.00 and 7.00 am on Wednesday 19th February. This is less than the 200 vehicle per hour environmental goal (300 maximum) specified in the RTA Guide to Traffic Generating Developments Version 2.2 – 2002.
3. 85thile speeds were around 25 km/hr during the day and 27 to 29 km/hr in the evening. The differences are insignificant.

It is advised that:

1. The traffic flows are within acceptable levels.
2. Increases in traffic speeds in the evening when parking demands are low and are insignificant.
3. There is no need for additional traffic calming measures.

Pedestrian crossing at the steps from Bronte Road

Provision of a marked pedestrian crossing where drivers give way to pedestrians would require provision of sight lines for the drivers to see the pedestrians as they step from the kerb. This would result in the loss of parking on both sides of Calga Place (20 metres plus 3.6 metre for a formal crossing). This is a technical requirement specified in AUSTDOADS and RMS/TfNSW guidelines.

Regardless of the loss of parking, the low travel speeds make it relatively safe for pedestrians to cross Calga Place without a marked pedestrian crossing. Chevron line marked boxes have been provided at the steps to allow pedestrians to walk out from the kerb between parked cars and motor bikes and then give way to traffic if needed. This is a practical solution to minimise risk to pedestrians at this location.

In addition, the speed hump just south of the crossing point assists in minimising risk to pedestrians.

4. Financial Information for Council's Consideration

The capital works budget for 2020–2021 for the project currently is \$733,936 of which \$600,718 is grant funding from the federal Government.

5. Attachments

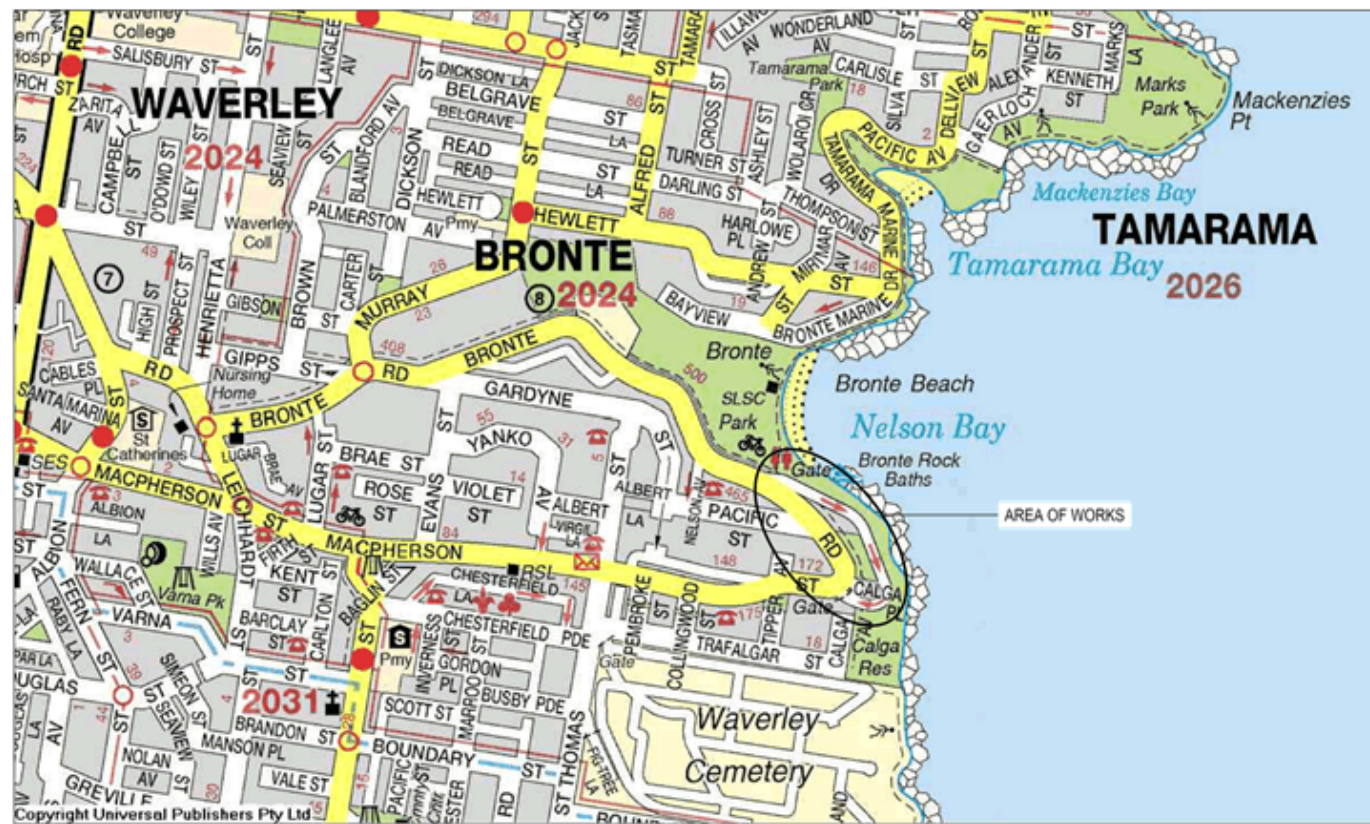
1. Bronte Cutting Safety Upgrade - 100% Detailed Design Drawings [↓](#)



WAVERLEY COUNCIL

BRONTE CUTTING SAFETY UPGRADE

12526408



LOCALITY PLAN
NTS

DRAWING LIST:

DRG No.	DRAWING TITLE	REV.
12526408-G001	COVER SHEET, DRAWING LIST AND LOCALITY PLAN	F
12526408-G002	GENERAL NOTES	B
12526408-C001	GENERAL ARRANGEMENT PLAN	E
12526408-C011	SITE PLAN - SHEET 1 OF 8	F
12526408-C012	SITE PLAN - SHEET 2 OF 8	F
12526408-C013	SITE PLAN - SHEET 3 OF 8	F
12526408-C014	SITE PLAN - SHEET 4 OF 8	F
12526408-C015	SITE PLAN - SHEET 5 OF 8	F
12526408-C016	SITE PLAN - SHEET 6 OF 8	F
12526408-C017	SITE PLAN - SHEET 7 OF 8	F
12526408-C018	SITE PLAN - SHEET 8 OF 8	F
12526408-C021	TYPICAL DETAILS SHEET 1 OF 2	F
12526408-C022	TYPICAL DETAILS SHEET 2 OF 2	B
12526408-C023	PLANTING SCHEDULE	B
12526408-C041	ALIGNMENT PLAN - SHEET 1 OF 8	B
12526408-C042	ALIGNMENT PLAN - SHEET 2 OF 8	B
12526408-C043	ALIGNMENT PLAN - SHEET 3 OF 8	B
12526408-C044	ALIGNMENT PLAN - SHEET 4 OF 8	B
12526408-C045	ALIGNMENT PLAN - SHEET 5 OF 8	B
12526408-C046	ALIGNMENT PLAN - SHEET 6 OF 8	B
12526408-C047	ALIGNMENT PLAN - SHEET 7 OF 8	B
12526408-C048	ALIGNMENT PLAN - SHEET 8 OF 8	B
12526408-C049	ALIGNMENT SETOUT TABLES	C
12526408-C051	LONGITUDINAL SECTIONS	C

PRELIMINARY

F	FINAL DETAILED DESIGN	FG	GdS	MP	16.03.21	
E	DETAILED DESIGN	FG	GdS	MP	26.02.21	
D	FINAL CONCEPT DESIGN	FG	GdS	MP	30.10.20	
C	CONCEPT DESIGN	FG	GdS	MP	16.10.20	
B	CONCEPT DESIGN	FG	GdS	MP	25.09.20	
No	Revision	Note: * indicates signatures on original issue of drawing or last revision of drawing	Drawn	Job Manager	Project Director	Date

Plot Date: 16 March 2021 - 11:12 AM Plotted by: Fraser Goulding

Cad File No: G:\21\12526408\CADD\Drawings\12526408-G001.dwg



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Drawn	F. GOULDING	Designer	G. de SWARDT
Drafting Check	G. LIDGETT	Design Check	A. RAHMAN
Approved (Project Director)	M. PRESSWELL		
Date	16.03.2021		
Scale	NTS		

This Drawing must not be used for Construction unless signed as Approved

Client	WAVERLEY COUNCIL
Project	BRONTE CUTTING SAFETY UPGRADE
Title	COVER SHEET, DRAWING LIST AND LOCALITY PLAN
Original Size	A1
Drawing No:	12526408-G001
Rev:	F

1. ALL DIMENSIONS ARE IN METRES UNLESS NOTED OTHERWISE.
2. CO-ORDINATES ARE IN MAP GRID OF AUSTRALIA (ZONE 56, GDA 2020).
3. LEVELS ARE IN AUSTRALIAN HEIGHT DATUM (AHD).
4. ALL WORKS SHALL BE CARRIED OUT IN CONJUNCTION WITH ALL DRAWINGS, SPECIFICATIONS AND THE WAVERLY COUNCIL'S STANDARD DRAWINGS AND SPECIFICATIONS.
5. ALL LEVELS SHALL BE OBTAINED FROM ESTABLISHED BENCH MARKS ONLY.
6. ALL SERVICE LOCATIONS SHALL BE VERIFIED PRIOR TO CONSTRUCTION.
7. NO WORK SHALL BE CARRIED OUT OUTSIDE THE COUNCIL'S INDICATED SITE EXTENT WITHOUT WRITTEN PERMISSION.
8. DESIGNED BASED ON SURVEY BY AXIOM SPATIAL SURVEYORS, DRAWING 2023001-00.DWG, REV.00, DATED 19/05/2020.
9. BOUNDARIES SHALL BE CLEARLY MARKED ON SITE PRIOR TO CONSTRUCTION.
10. CIVIL WORKS TO BE UNDERTAKEN IN ACCORDANCE WITH WAVERLY COUNCIL'S PUBLIC DOMAIN TECHNICAL MANUAL.

1. ALL LINE MARKINGS AND SIGNAGE IS APPLIED IN ACCORDANCE WITH THE REQUIREMENTS OF AS 2890 AND RMS STANDARDS, SPECIFICATIONS AND GUIDES.
2. LINE MARKING IN PARKING BAYS: PAINT IS WATER BASED - COLOUR WHITE. LINE SHALL BE 100mm WIDE UNBROKEN LINE FOR PARKING. LINE MARKING OTHER THAN PARKING BAYS: PAINT IS WATER BASED WITH REFLECTIVE GLASS BEADS - COLOUR WHITE.
3. LINE MARKING FOR DISABLED PARKING AREAS:
 - 3.1 PAINT IS WATER BASED WITH REFLECTIVE BEADS.
 - 3.2 COLOUR IS 'BLUE', 'YELLOW' AND 'WHITE' AS PER ASINZS 2890.6-2009.
 - 3.3 LINE MARKING AROUND THE DEDICATED DDA SPACES IS UNBROKEN 'YELLOW LINE 100mm WIDE.
 - 3.4 THE SYMBOL OF ACCESS (WHEELCHAIR) IS WHITE (800mm HIGH) ON A BLUE SQUARE (1200mm x 1200mm), SITTING 500mm BACK IN THE SPACE AND CENTRED.
 - 3.5 ALL SPACES DELINEATED IN ACCORDANCE WITH AS2890.5.
4. ALL EXISTING SIGNS AFFECTED BY PROPOSED WORKS SHALL BE REINSTATED IF IN GOOD CONDITION, OTHERWISE THEY SHALL BE REPLACED.
5. ALL SIGNS ARE TO BE ANTI-GRAFFITI COATED.

1. PLANT SPECIES TO BE SELECTED FROM POTM OR AS DIRECTED BY ENVIRONMENTAL SUSTAINABILITY

1. THE CONTRACTOR SHALL INSTALL AND MAINTAIN SOIL EROSION AND SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THE "MANAGING URBAN STORMWATER: SOIL AND CONSTRUCTION (VOLUME 1 & VOLUME 2 SERIES) PRODUCED BY LANDCOM AND THE DEPARTMENT OF ENVIRONMENT AND CLIMATE CHANGE.
2. GRADE FINISHED SURFACE TO SHED WATER EVENLY WITHOUT CHANNELING (UNTIL PIPED STORMWATER SYSTEM IS ADDED IN LATER STAGE).
3. THE PLACEMENT OF EROSION AND SEDIMENT CONTROL MEASURES MAY VARY DEPENDING UPON THE CONSTRUCTION TECHNIQUE AND PROGRAM.
4. REMOVE ALL EXCESS SPOIL FROM SITE.

1. PREPARATION FOR PAVEMENT, CLEAR SITE, STRIP TOPSOIL, CUT AND FILL AND PREPARATION OF SUB-GRADE SHALL BE AS DESCRIBED IN "EARTHWORKS".
1. SUBGRADE SHALL BE COMPACTED TO 98% STANDARD MAXIMUM DRY DENSITY OPTIMUM MOISTURE CONTENT $\pm 2\%$ IN ACCORDANCE WITH AASHTO 5.1.1.
2. BASE COURSE SHALL BE CONSTRUCTED FROM FINE CRUSHED ROCK D8820 COMPACTED TO 100% STANDARD MAXIMUM DRY DENSITY AT OPTIMUM MOISTURE CONTENT $\pm 2\%$ IN ACCORDANCE WITH AASHTO 5.1.1.
3. CONCRETE PAVEMENT/SLAB SHALL BE AS DETAILED ON THE DRAWINGS. FOR QUALITY OF CONCRETE AND CONCRETE WORK GENERALLY REFER THESE NOTES.
4. ALL WORKMANSHIP AND MATERIALS FOR CONCRETE WORK SHALL BE IN ACCORDANCE WITH AASHTO AND AASHTO CURRENT EDITIONS WITH AMENDMENTS, EXCEPT WHERE VARYED BY THE CONTRACT DOCUMENTS.
5. CONCRETE QUALITY, ALL CEMENT SHALL BE ACSE SPECIFICATION TYPE A.

STRUCTURAL ELEMENT	BUILDING	ELEMENT 1	ELEMENT 2	ELEMENT 3
EXPOSURE CLASSIFICATION	B1	A1,A2,B1,B2 OR U		
STRENGTH GRADE (MPa)	N7	N20, N25, S32, S40, S50, D45 OR D50	S32	S40
SLUMP (mm)	-	-		
MAX. AGGREGATE SIZE (mm)	-	10, 14 OR 20		
MINIMUM CEMENTITIOUS CONTENT (kg/m ³)	100		300	350

PROJECT CONTROL TESTING SHALL BE CARRIED OUT IN ACCORDANCE WITH A33003.

6. NO ADMIXTURES SHALL BE USED IN CONCRETE UNLESS APPROVED IN WRITING.
7. CLEAR CONCRETE COVER TO ALL REINFORCEMENT FOR DURABILITY SHALL BE 50mm.
8. CONSTRUCTION JOINTS WHERE NOT SHOWN SHALL BE LOCATED TO THE APPROVAL OF THE ENGINEER.
9. THE FINISHED CONCRETE SHALL BE MECHANICALLY VIBRATED TO ACHIEVE A DENSE, UNIFORM, HOMOGANEOUS MASS, COMPLETELY FILLING THE FORMWORK THOROUGHLY EMBEDDING THE REINFORCEMENT AND FREE OF STONE POCKETS. CONCRETE SHALL BE COMPACTED WITH MECHANICAL VIBRATORS.
10. CURING OF ALL CONCRETE SHALL BE ACHIEVED BY KEEPING SURFACES CONTINUOUSLY WET FOR A PERIOD OF THREE DAYS, AND THE PREVENTION OF LOSS OF MOISTURE FOR A TOTAL OF 7 DAYS FOLLOWED BY A GRADUAL DRYING OUT.
11. REPAIRS TO CONCRETE SHALL NOT BE ATTEMPTED WITHOUT THE PERMISSION OF THE ENGINEER.

- UNLESS NOTED OTHERWISE, THE IN-GROUND STORMWATER PIPES SHALL BE REINFORCED CONCRETE CLASS 3 APPROVED SPIGOT AND SOCKET WITH RUBBER RING JOINTS
- STORMWATER PIPES ARE TO BE INSTALLED IN ACCORDANCE WITH AS 3725, ALL BEDDING TO BE TYPE H2 UNLESS NOTED OTHERWISE
- CARE IS TO BE TAKEN WITH LEVELS OF STORMWATER LINES. GRADE SHOWN ARE NOT TO BE REDUCED WITHOUT APPROVAL
- GRATES AND COVERS SHALL CONFORM WITH AS 3996-1992
- ALL WORKS SHALL BE CARRIED OUT IN ACCORDANCE WITH CURRENT STANDARDS AND CONSTRUCTION SPECIFICATION
- THE CONTRACTOR SHALL VERIFY LOCATIONS OF EXISTING SERVICES WITH ALL RELEVANT AUTHORITIES BEFORE COMMENCING CONSTRUCTION
- ALL STRUCTURES ARE TO BE CONSTRUCTED TO NEW FINISHED SURFACE LEVELS
- ORIGIN OF LEVELS ARE BASED ON AHD, THE POSITION, SIZE, SURFACE LEVELS AND INVERT LEVELS OF EXISTING STORMWATER MANHOLES AND PITS ARE PROVIDED. THESE NEED TO BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION
- ALL STORMWATER PITS SHALL BE CONSTRUCTED USING CAST IN-SITU CONCRETE OR HEAVY DUTY PRECAST PITS
- CONNECTION TO EXISTING STORMWATER PITS TO BE CARRIED OUT BY THE CONTRACTOR IN ACCORDANCE WITH THE SPECIFICATION. MAKE REQUIRED OPENING INSIDE OF THE EXISTING STORMWATER PIT WALL AT THE CONNECTION POINT OF NEW LINE. REPAIR AND MAKE GOOD THE PIT WALL INCLUDING NECESSARY ADJUSTMENT OF THE PIT BENCHING TO MATCH NEW PIPE CONNECTIONS
- FOR DRAINAGE DETAILS REFER TO DGR. 12526408-0221
- GRATES, FRAMES AND COVERS IN ROADWAYS TO BE DURHAM OR RIMS APPROVED HEAVY DUTY CALSIO
- GRATES AND FRAMES NOT IN ROADWAYS TO BE DURHAM OR EQUIVALENT MEDIUM DUTY CLASS
- ALL GRATES TO BE HINGED AND LOCKABLE. PROVIDE HEEF SEAL AS PER AS 1428-1.7.3 IN ROADWAYS AND PARKING AREA (PEDESTRIAN TRAVERSING AREAS)
- STORMWATER DRAINAGE SHALL BE GENERALLY IN ACCORDANCE WITH AS3500.3
- BACKFILL TRENCHES WITH APPROVED FILL, SUCH AS SANDY LOAM, COMPACTED IN 200mm LAYERS TO 98% OF STANDARD DENSITY TO AS12895.1.1
- BUILD INTO UPSTREAM FACE OF ALL PITS A 3.0m SUBSOIL LINE FALLING TO PITS TO MATCH PIT INVERT
- SUBSOIL LINE - PIPES AND FITTINGS SHALL BE PERFORATED PLASTIC TO AS2439 PART 1 LAY PIPS ON FLOOR OF TRENCH GRADED 1% AND OVERLAY WITH FILTER MATERIAL FILTERED TO WITHIN 200µ OF SURFACE. PROVIDE FILTER FABRIC OF PERMEABLE POLYPROPYLENE BETWEEN FILTER MATERIAL AND TOPSOIL
- DRAINAGE PITS MAY ONLY BE SUBSTITUTED WITH ALTERNATIVE PRE CAST PITS WITH THE PRIOR APPROVAL OF THE ENGINEER

B	FINAL DETAILED DESIGN	FG	GdS	MP	16.03.21	
A	DETAILED DESIGN	NA	GdS	MP	28.02.21	
No.	Revision	Note: * indicates signatures on original issue of drawing or last revision of drawing	Drawn Job Manager	Project Director	Date	



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Drafting Check G. LIOGETT

Design
Check A. RAHMAN

WELL

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Project **BRONT**

Title **GENER**

Original Size
A1 Drawing

Client	WAVERLEY COUNCIL
Project	BRONTE CUTTING SAFETY UPGRADE
Title	GENERAL NOTES

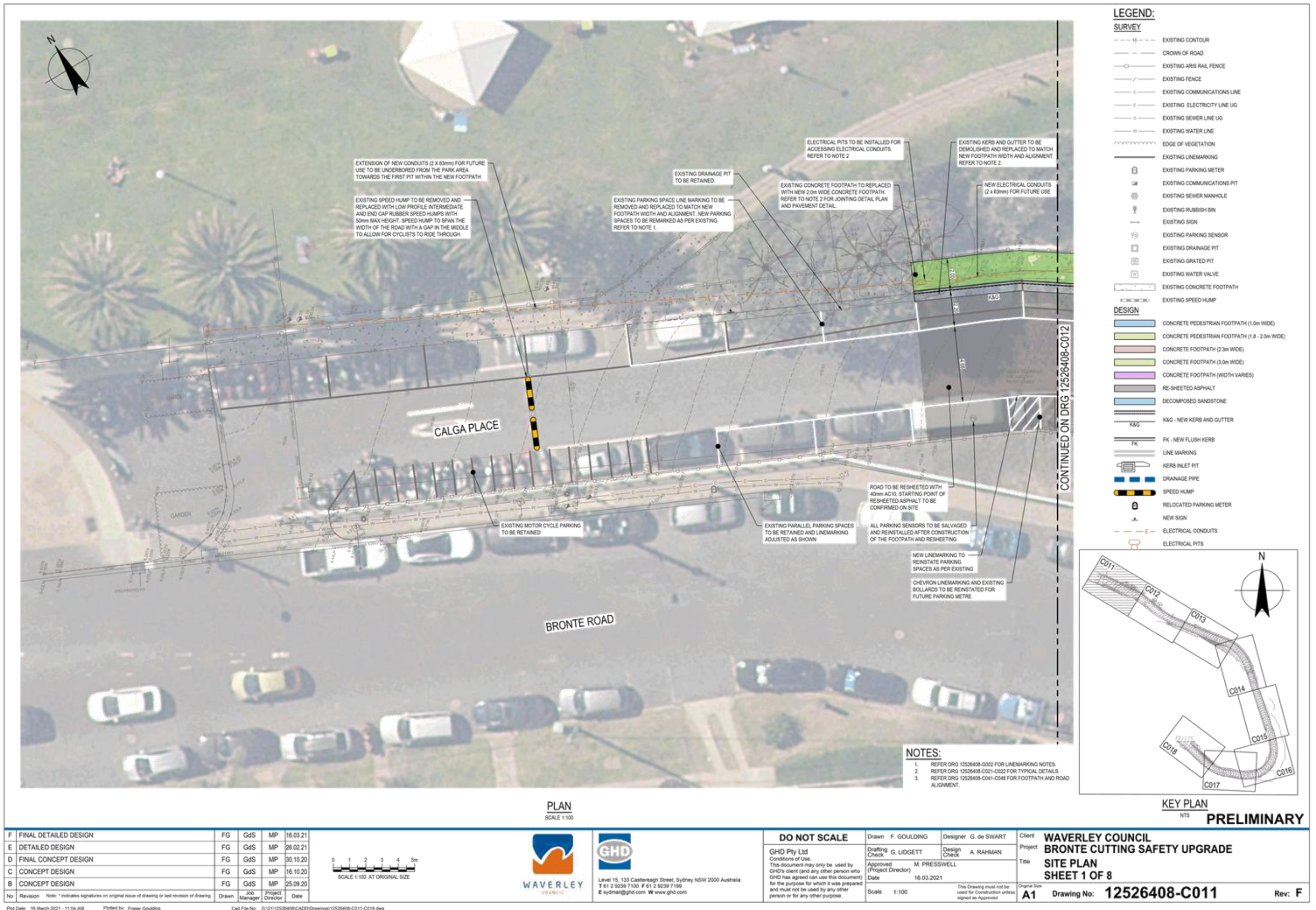
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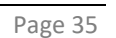
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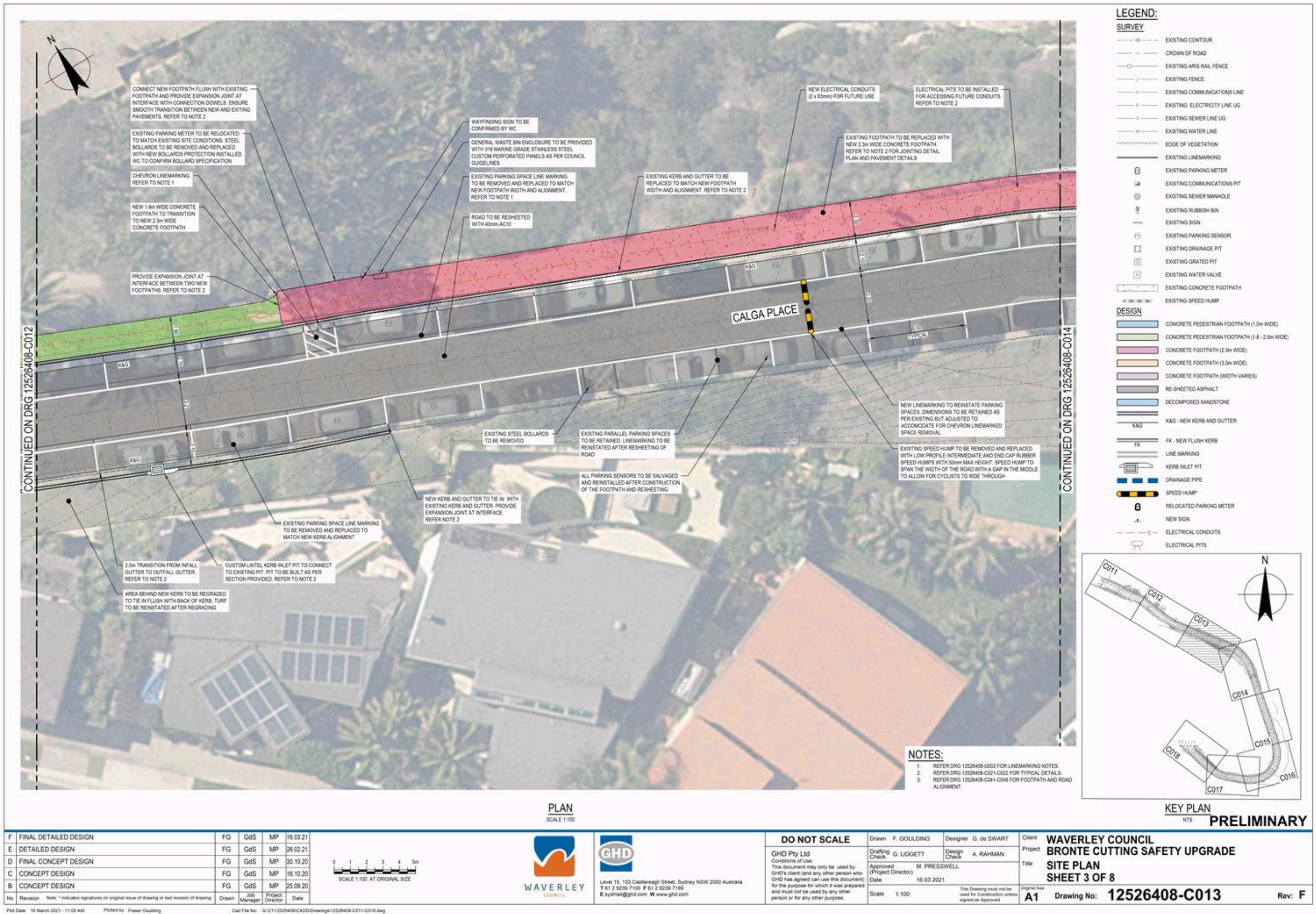


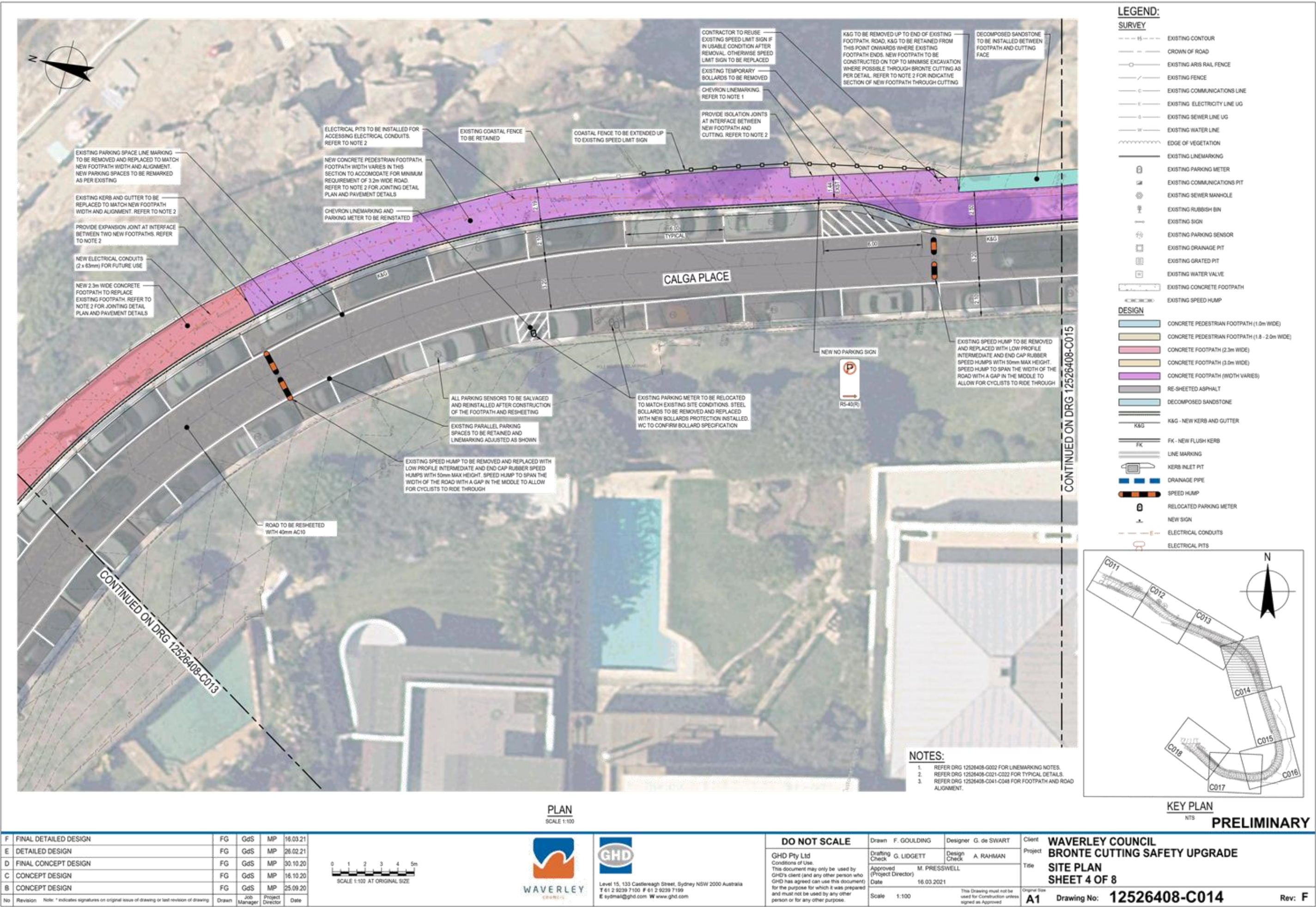
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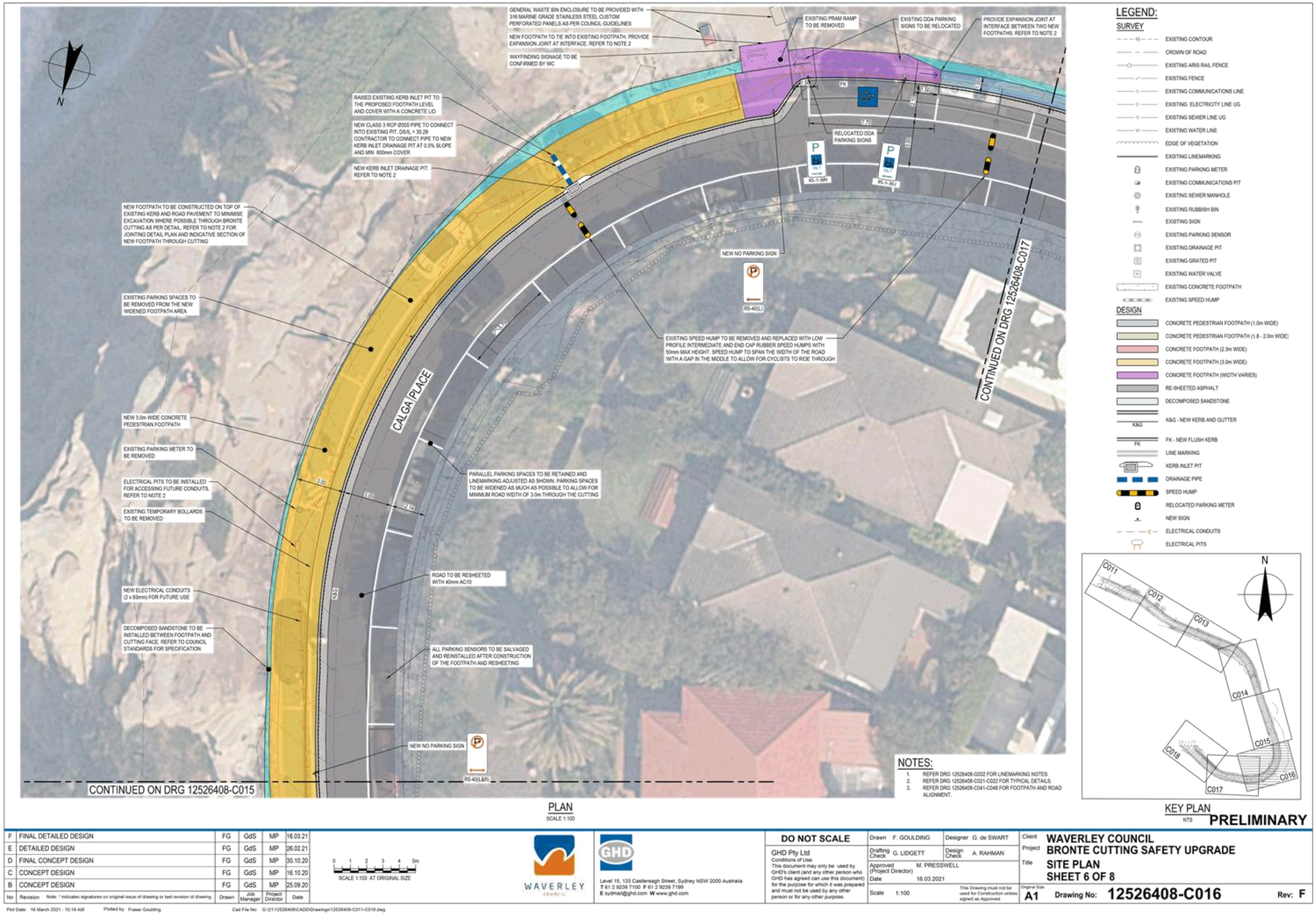


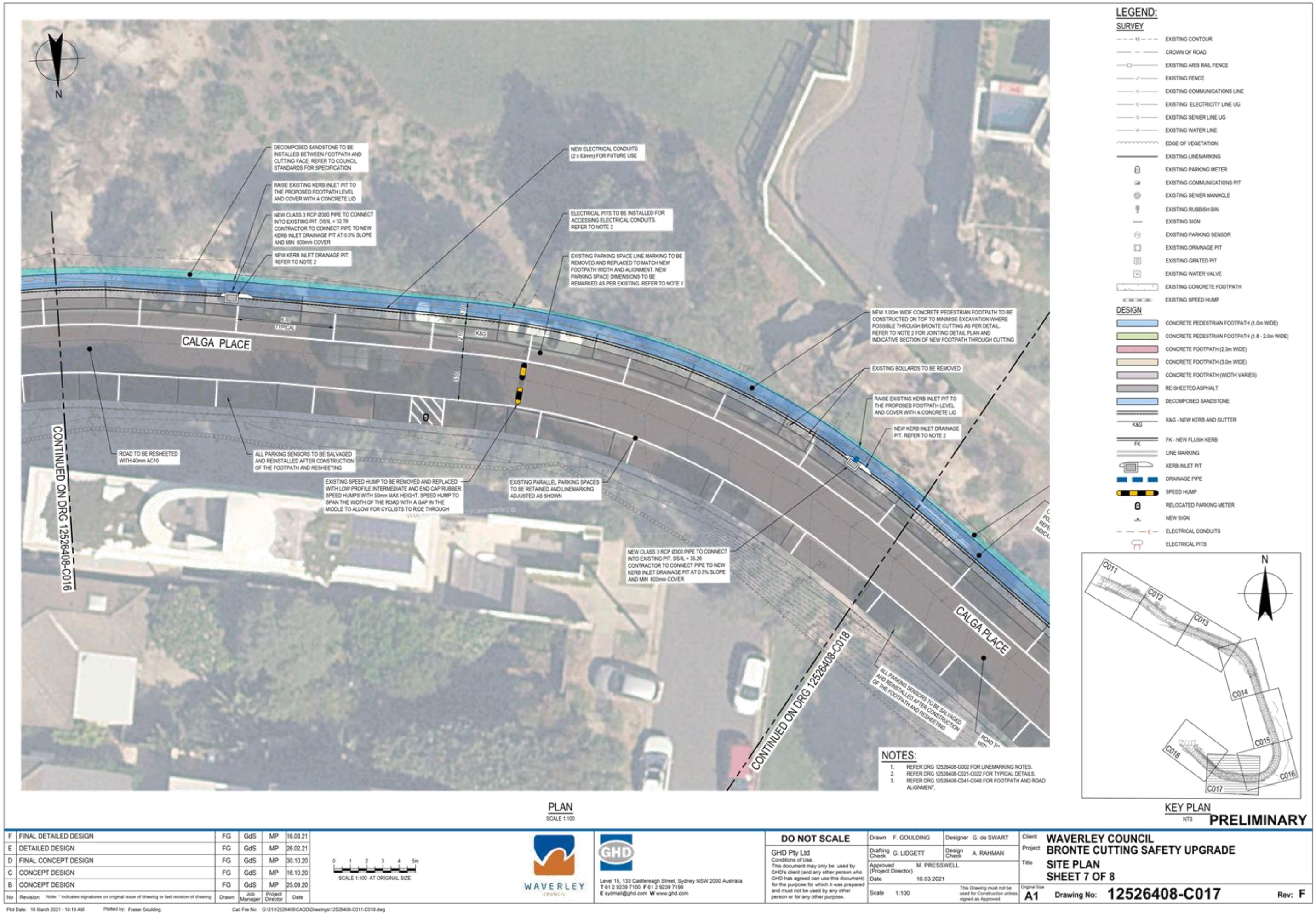


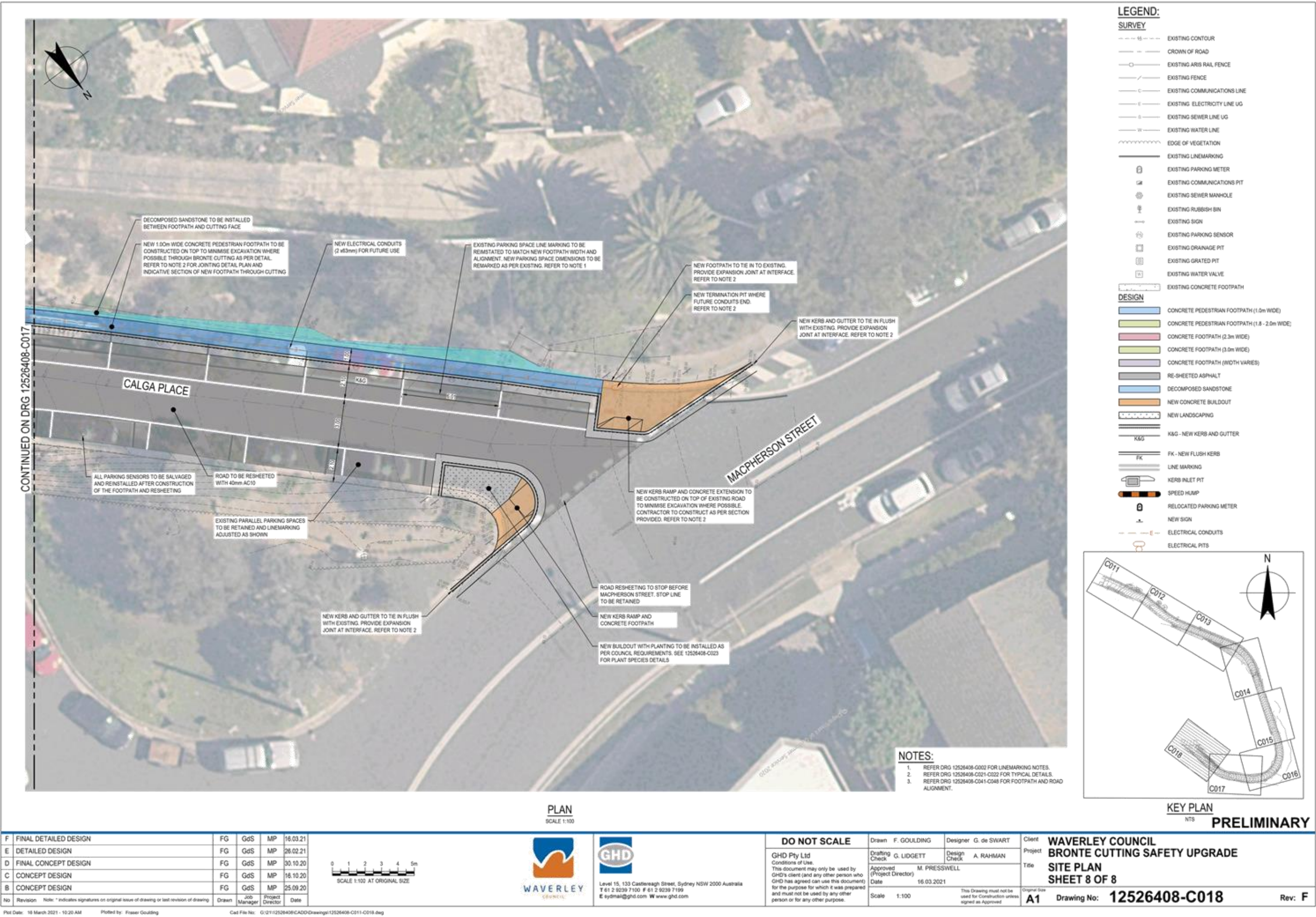


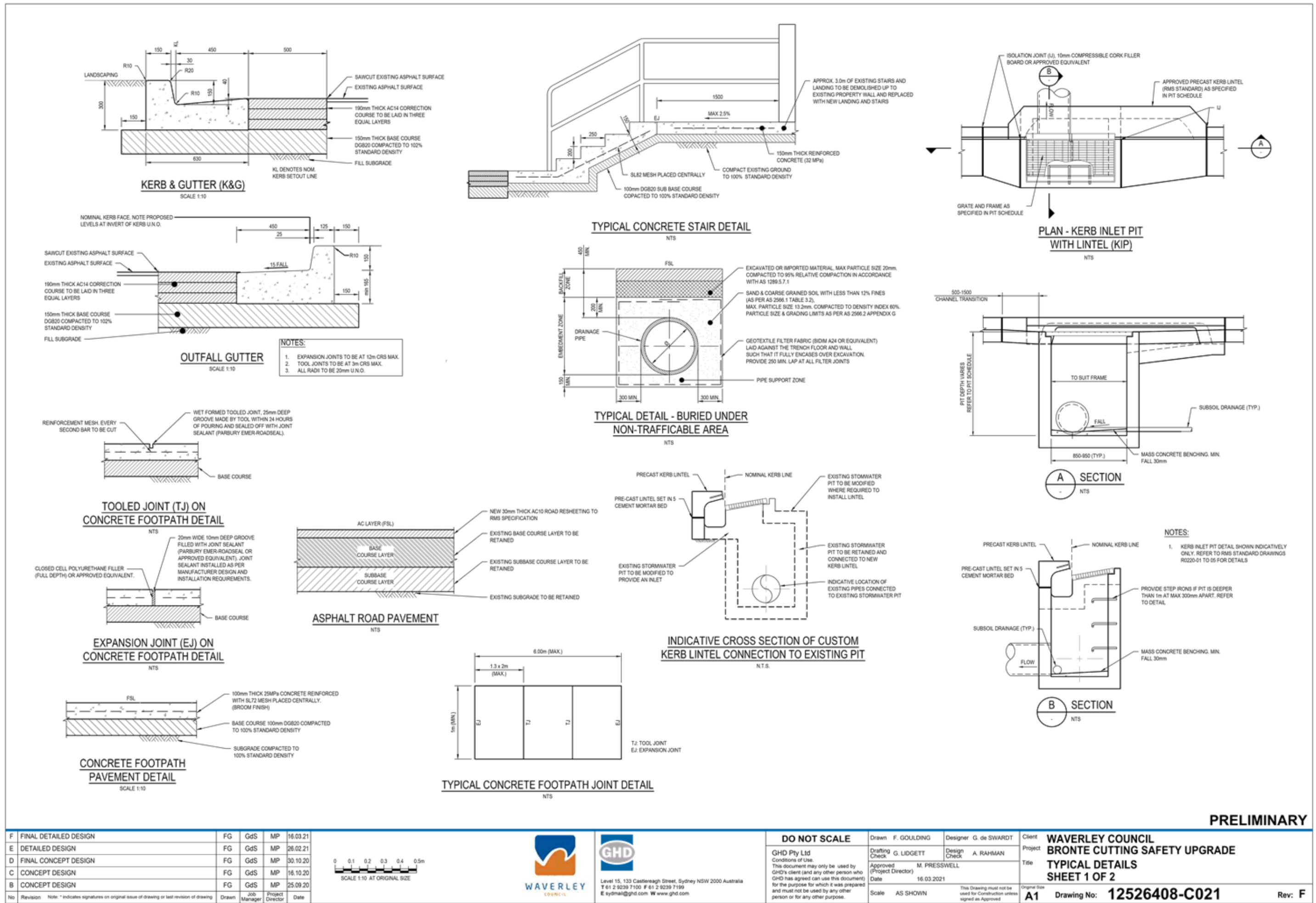


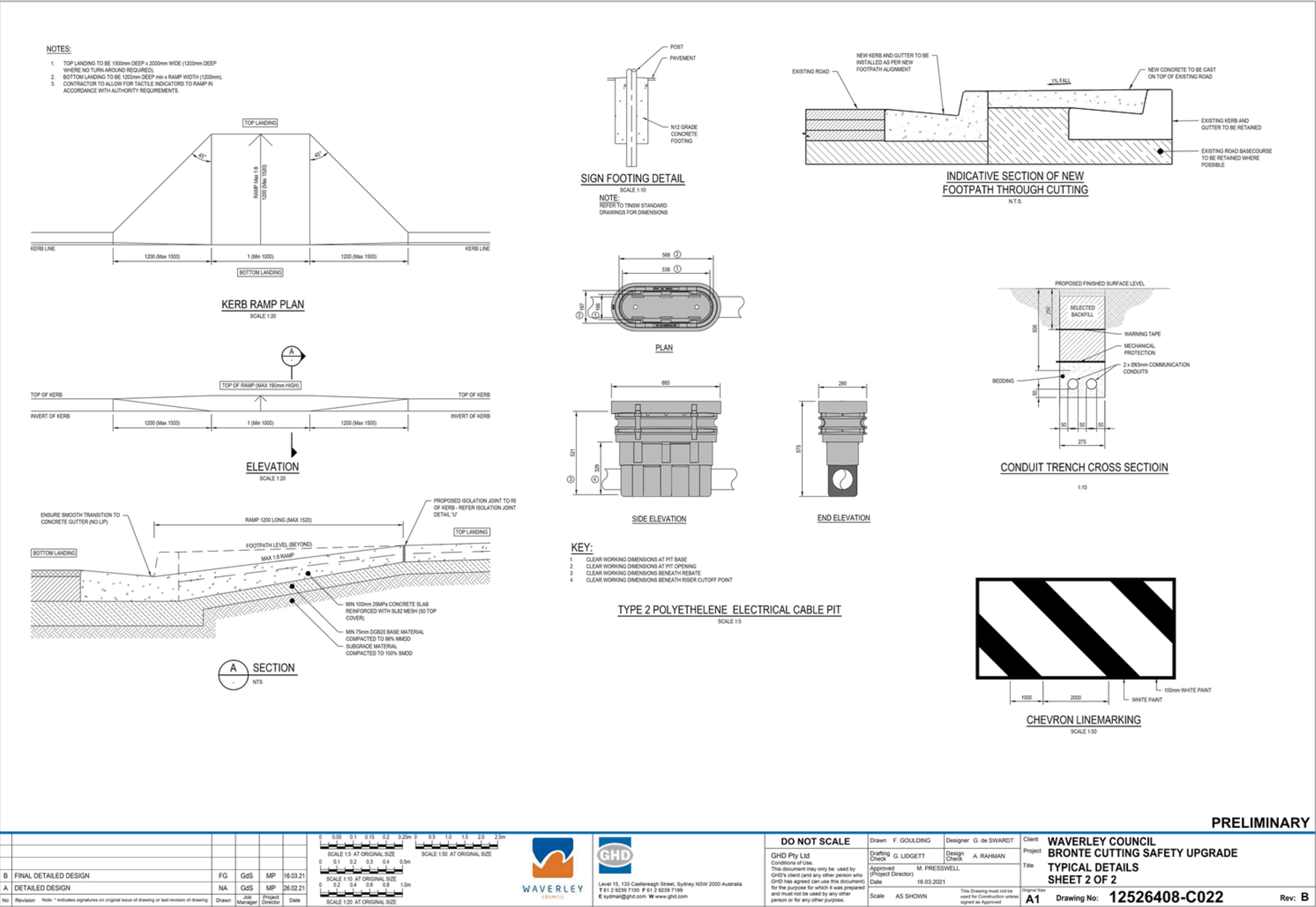












PLANTING SCHEDULE:

CODE	BOTANICAL NAME	COMMON NAME	PLANTING SCHEDULE		WIDTH	DENSITY /m2	PERCENTAGE	QUANTITY
			POT SIZE	HEIGHT				
WES	<i>Westringia fruticosa</i> 'Aussie Box'	Aussie Box	140mm	700-1000mm	700-1000mm	4	45.00%	23
GRE	<i>Grevillea poorinda</i> 'Royal Mantle'	Grevillea 'Royal Mantle'	140mm	200mm	6000mm	2	25.00%	6
LOM	<i>Lomandra longifolia</i>	Spiny Mat Rush	140mm	500-700mm	600-1000mm	3	30.00%	11

LANDSCAPE SPECIFICATION NOTES:

1. ERADICATE WEEDS
ERADICATE WEEDS: USE ENVIRONMENTALLY ACCEPTABLE METHODS, SUCH AS NON-RESIDUAL GLYPHOSATE HERBICIDE IN ANY OF ITS REGISTERED FORMULAE, AT THE RECOMMENDED MAXIMUM RATE. REGULARLY REMOVE, BY HAND, RUBBISH AND WEED GROWTH THROUGHOUT GARDEN AND COMMUTER CARPARK AREA. CONTINUE ERADICATION THROUGHOUT THE COURSE OF THE WORKS AND DURING THE PLANTING ESTABLISHMENT PERIOD.

PROVIDE COMPOSTS, SOIL CONDITIONERS AND MULCHES TO AS445
2. SOIL PREPARATION AND INSTALLATION
CARRY OUT SOIL TESTS ON EXISTING SITE TOPSOIL TO DETERMINE VIABILITY OF SITE SOILS TO SUSTAIN HEALTHY AND PROLONGED GROWTH OF SPECIFIED PLANT MATERIAL. TESTS TO BE IN ACCORDANCE WITH AS4419 AT AN ACCREDITED SOILS LABORATORY. IF TOPSOIL IS DEEMED TO BE UNSUITABLE, REMOVE FROM SITE AND IMPORT TOPSOIL MIX AS SPECIFIED. IF TOPSOIL IS SUITABLE OR CAN BE AMELIORATED TO ACHIEVE SUITABLE STANDARD, STRIP SITE TOPSOIL AND STOCKPILE FOR REUSE. MAXIMUM STOCKPILE HEIGHT TO BE 2 METRES. MAINTAIN A WEED FREE STOCKPILE FOR ITS DURATION. PROVIDE DOCUMENTATION OF SOIL TEST RESULTS AND SUITABILITY FOR REUSE IN THE PROJECT. IF THE SITE TOPSOIL IS NOT SUITABLE FOR RE-USE IN PLANTING AREAS, SUPPLY PREMIUM GRADE ORGANIC SOIL MIX SUITABLE FOR NATIVE PLANT SPECIES EQUAL TO ANL NATIVE PLANT MIX LOW 'P'. PROVIDE PRODUCT DETAILS, SUPPLIER AND SAMPLE FOR APPROVAL.

SOIL TYPES USED IN PLANTED AREAS SHALL SUIT THE NEEDS AND CONDITIONS OF THE PLANT SPECIES BEING PLANTED.

PROVIDE COMPOSTS, SOIL CONDITIONERS AND MULCHES TO AS 4454
3. PREPARATION OF PLANTING BEDS
PLANTING BEDS: EXCAVATE TO 575mm BELOW FINISHED LEVELS OF PLANTING BEDS. CULTIVATE SUBGRADE TO MINIMUM 300mm AT THE BASE OF THE PLANTING BEDS. FILL WITH AMELIORATED SITE SOIL OR IMPORTED SOIL MIX, AS SPECIFIED. OVERFILL AND CONSOLIDATE PLANTING BEDS WITH SOIL AND COMPACT TO 85% MMD. ALLOW FOR FUTURE SETTLEMENT.

NO WORK TO BE CARRIED OUT ON PLANTED AREAS WHILEST SOIL IS WET. TO AVOID COMPACTION OF THESE AREAS. RAKE UP ALL LARGE STONES, PIECES OF TIMBER AND OTHER DEBRIS FROM PROPOSED PLANTED AREAS. REMOVE DEBRIS FROM SITE. ALLOW PREPARED SOIL TIME TO CURE PRIOR TO INSTALLING PLANTS. ENSURE THAT ALL PLANTED AREAS DRAIN SATISFACTORILY. IF DRAINAGE PROBLEMS EXIST WITH REGARD TO EXISTING SUBGRADE CONDITIONS, ADVISE SUPERINTENDENT AND AWAIT FURTHER INSTRUCTIONS.

CULTIVATE PLANTING AREAS TO A MINIMUM DEPTH OF 100MM.
DO NOT DISTURB SERVICES OR TREE ROOTS, IF REQUIRED CULTIVATE THESE AREAS BY HAND.
MIX IN MATERIALS REQUIRED TO INCORPORATE INTO THE SUBSOIL. CULTIVATE MANUALLY WITHIN 300MM OF PATHS OR STRUCTURES.

COMPLETE SOIL PREPARATIONS TO THE APPROVAL OF THE LANDSCAPE ARCHITECT. PROVIDE CUBIC METRE RATES FOR SUPPLY AND INSTALLATION (INCLUDING CULTIVATION) OF SOIL CONDITIONER AND ORGANIC SOIL MIX SHOULD ADDITIONAL QUANTITIES OF THESE MATERIALS BE REQUIRED.

TRIM SURFACE TO FINAL DESIGN LEVELS ALLOWING FOR FINISHED SURFACE TREATMENTS WHERE CONCRETE EDGING IS INSTALLED SUBGRADE SHOULD BE COMPACTED TO MINIMUM DEPTH OF 150MM BELOW.
4. PLANTING, FERTILISING AND STAKING
PROVIDE PLANT MATERIAL AS DETAILED ON THE PLANTING PLANS AND IN THE PLANT SCHEDULE. ALL PLANT MATERIAL SHALL MEET OR EXCEED NATSPEC GUIDELINES FOR NURSERY GROWN PLANT MATERIAL.
ALL PLANTS TO BE TRUE TO SPECIES OR CULTIVARS AS NAMED.
THE CONTRACTOR SHALL SEEK APPROVAL FROM THE LANDSCAPE ARCHITECT BEFORE MAKING ANY SUBSTITUTIONS OR CHANGES.

PLANTS MUST BE WELL GROWN, HEALTHY, OF GOOD FORM AND NOT FORCED GROWN. SIZE MUST BE PROPORTIONATE TO CONTAINER SIZE WITH WELL ESTABLISHED ROOTBALL, BUT NOT ROOT BOUND. PLANTS MUST BE FREE FROM DAMAGE, PEST AND DISEASES.

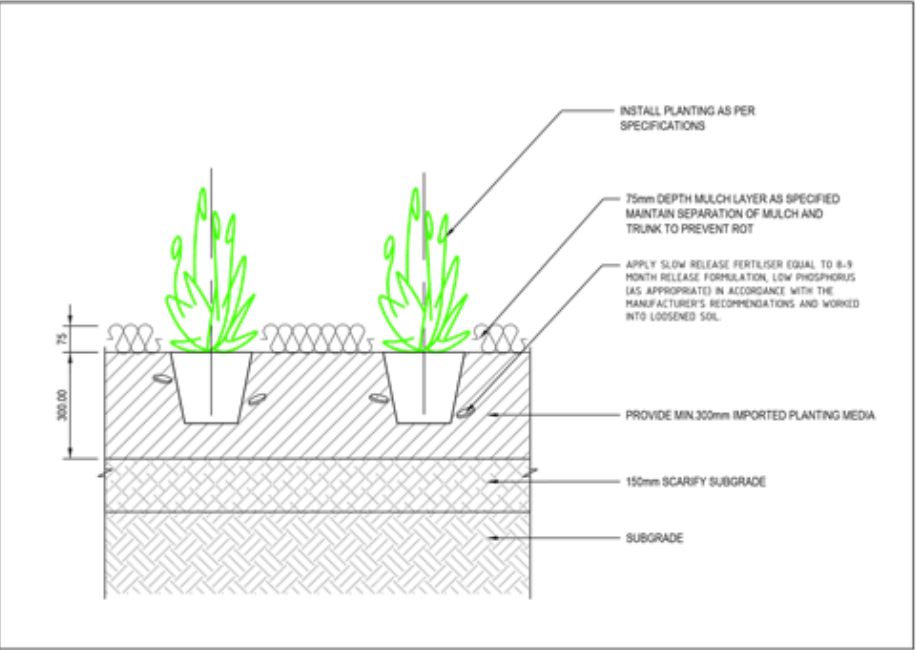
WATER PLANTS THOROUGHLY BEFORE PLANTING. SPECIES WITHIN EACH PLANTING MIX ARE TO BE PLANTED IN MODULAR GROUPINGS OF THE SAME SPECIES. MODULAR GROUPINGS SHOULD BE OFFSET IN DIAGONAL ARRANGEMENTS TO AVOID LONG LINES OF PLANTING. PLANT SAME SPECIES IN GROUPS OF ODD NUMBERS BETWEEN 3-5 PLANTS PER GROUP.

APPLY SLOW RELEASE FERTILISER EQUAL TO 8-9 MONTH RELEASE FORMULATION, LOW PHOSPHORUS (AS APPROPRIATE) IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND WORKED INTO LOOSENEED SOIL.

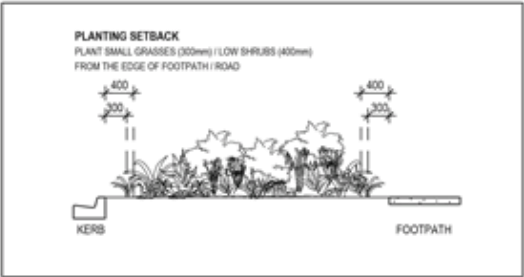
INSTALL PLANTS PLUMB AND SO THAT TOP OF ROOTBALL IS FLUSH WITH FINISHED SURFACE LEVEL OF PLANTING BED. BACKFILL WITH FRABLE SOIL. FORM A DISH AT THE BASE OF EACH PLANT TO AID WATER COLLECTION AND ABSORPTION.
WATER PLANTS IMMEDIATELY AND THOROUGHLY AFTER INSTALLATION. ENSURE PLANTS ARE KEPT WELL WATERED UNTIL THE END OF THE MAINTENANCE PERIOD.

ONLY STAKE PLANTS IF NECESSARY, TO RESIST WINDS OR PREVENT DAMAGE. STAPLE HESSIAN TIES TO THE STAKE FOR SUPPORT.
5. MULCHING
MULCH ALL PLANTING BEDS TO A DEPTH OF 75mm WITH APPROVED ORGANIC 'PINE BARK' MULCH AS SUPPLIED BY AUSTRALIAN NATIVE LANDSCAPES OR APPROVED EQUAL.
MAINTAIN SEPERATION OF MULCH AND TRUCK TO PREVENT ROT. KEEP MULCH 50mm FROM PLANT STEM.
ON SLOPES THAT ARE 1:3 OR STEEPER, INSTALL JUTE EROSION CONTROL MESH BLANKET IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. MULCH INSTALLED ADJACENT CONCRETE EDGING MUST SIT 25MM BELOW THE TOP SURFACE OF SAID EDGE.

LANDSCAPE PLANTING DETAILS:



TYPICAL INDIVIDUAL PLANTING
SCALE 1:10



PLANTING SETBACK
SCALE 1:50

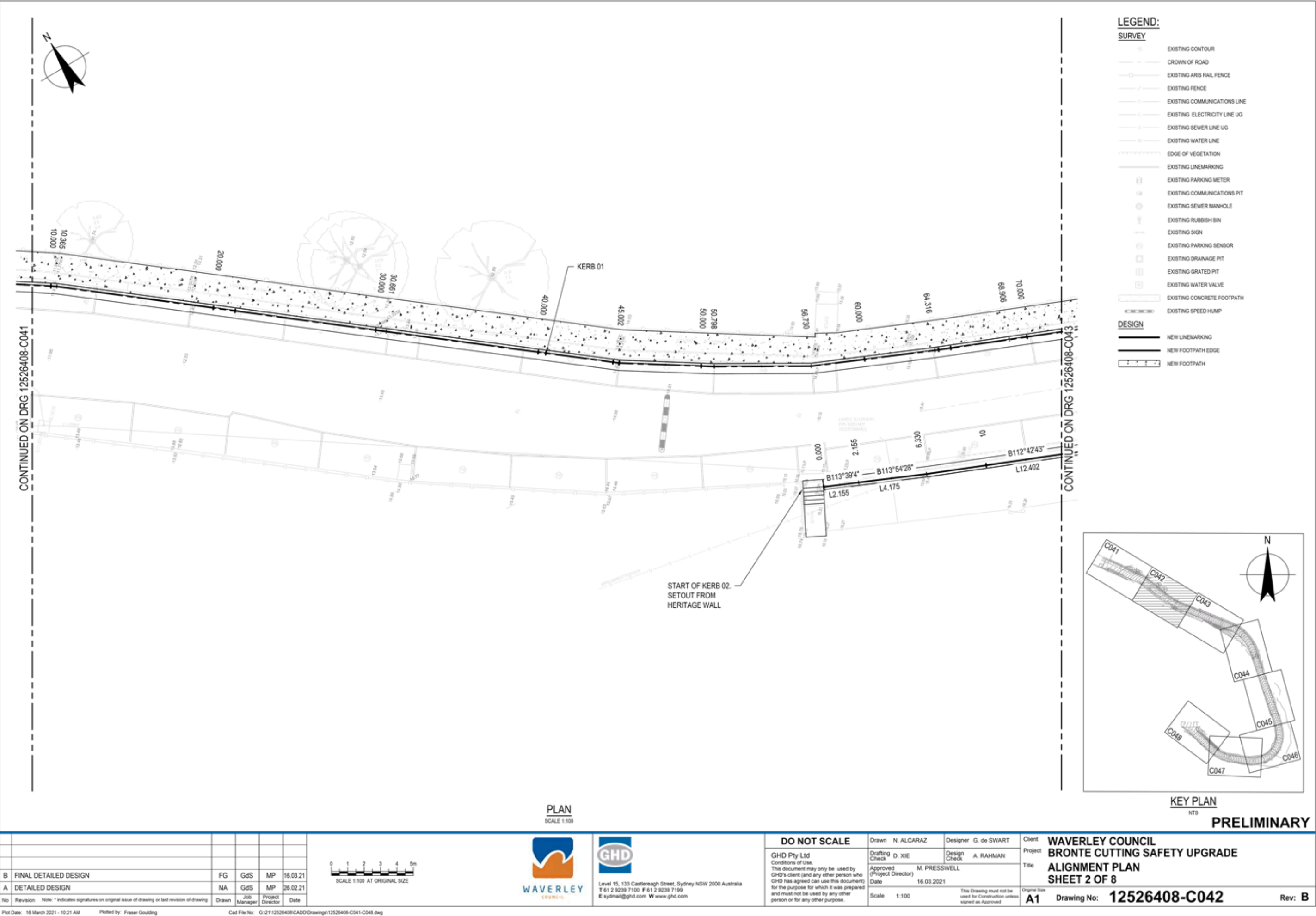
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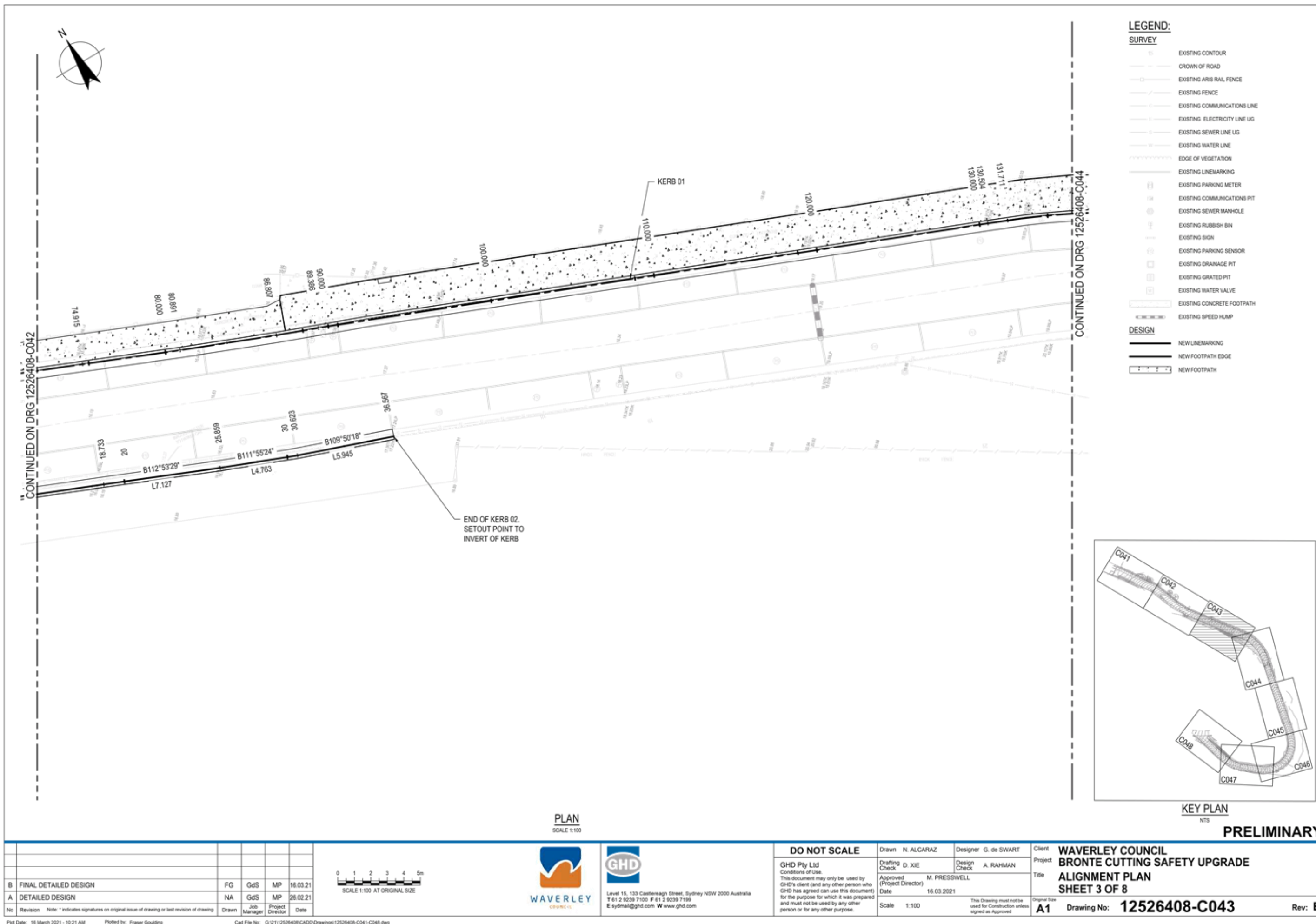
											DO NOT SCALE GHD Pty Ltd Conditions of Use: This document may only be used by GHD's client (and any other person who GHD has agreed can use this document) for the purpose for which it was prepared and must not be used by any other person or for any other purpose.		Drawn F. GOULDING Drafting Check G. LIDGETT Approved (Project Director) M. PRESSWELL Date 16.03.2021 Scale 1:1000	Designer G. de SWARDT Design Check A. RAHMAN	Client WAVERLEY COUNCIL Project BRONTE CUTTING SAFETY UPGRADE Title PLANTING SCHEDULE Original Size A1 Drawing No: 12526408-C023 Rev: B
B	FINAL DETAILED DESIGN		FG	GdS	MP	16.03.21									
A	DETAILED DESIGN		FG	GdS	MP	12.03.21									
No	Revision	Note: * indicates signatures on original issue of drawing or last revision of drawing	Drawn	Job Manager	Project Director	Date									

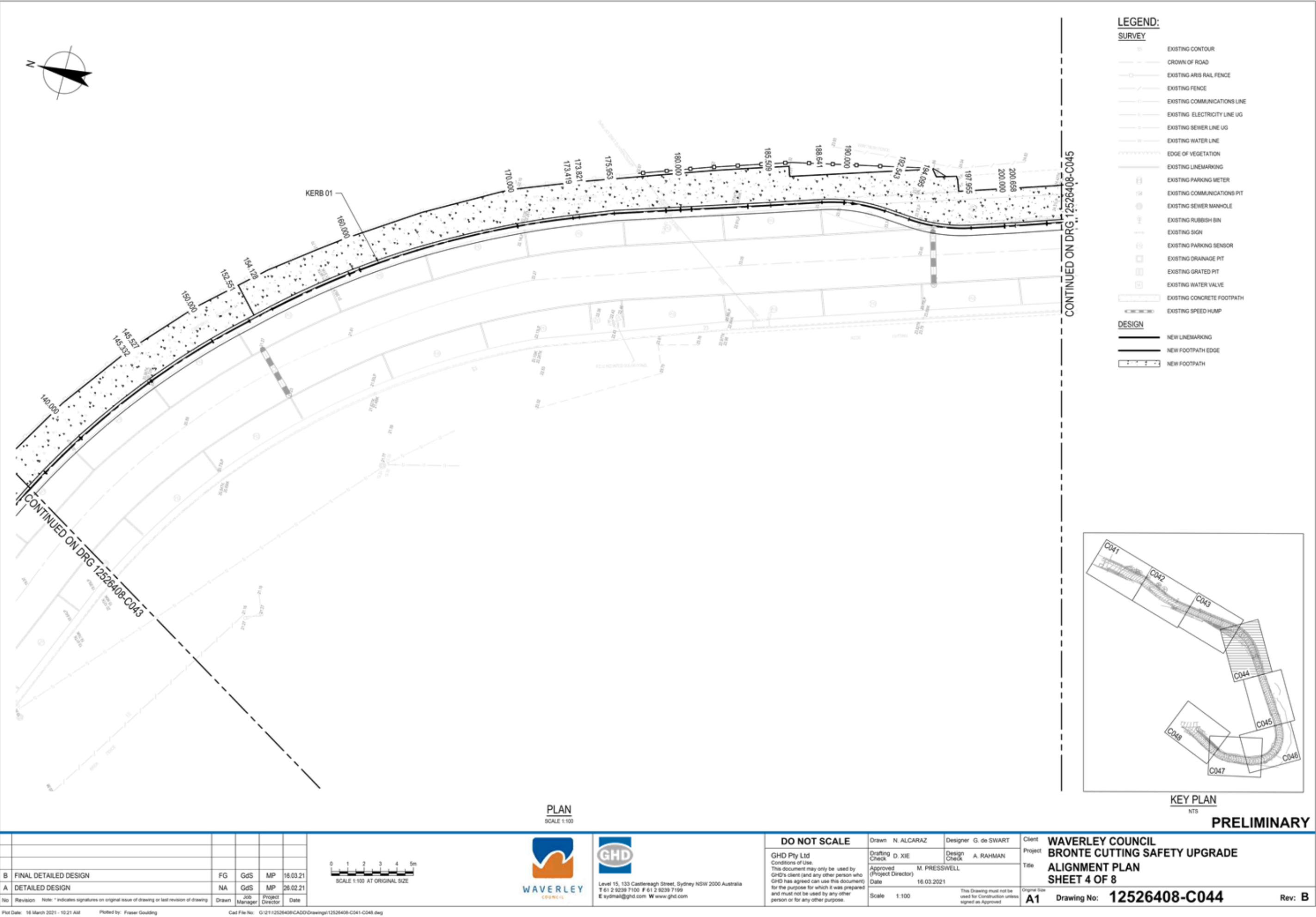
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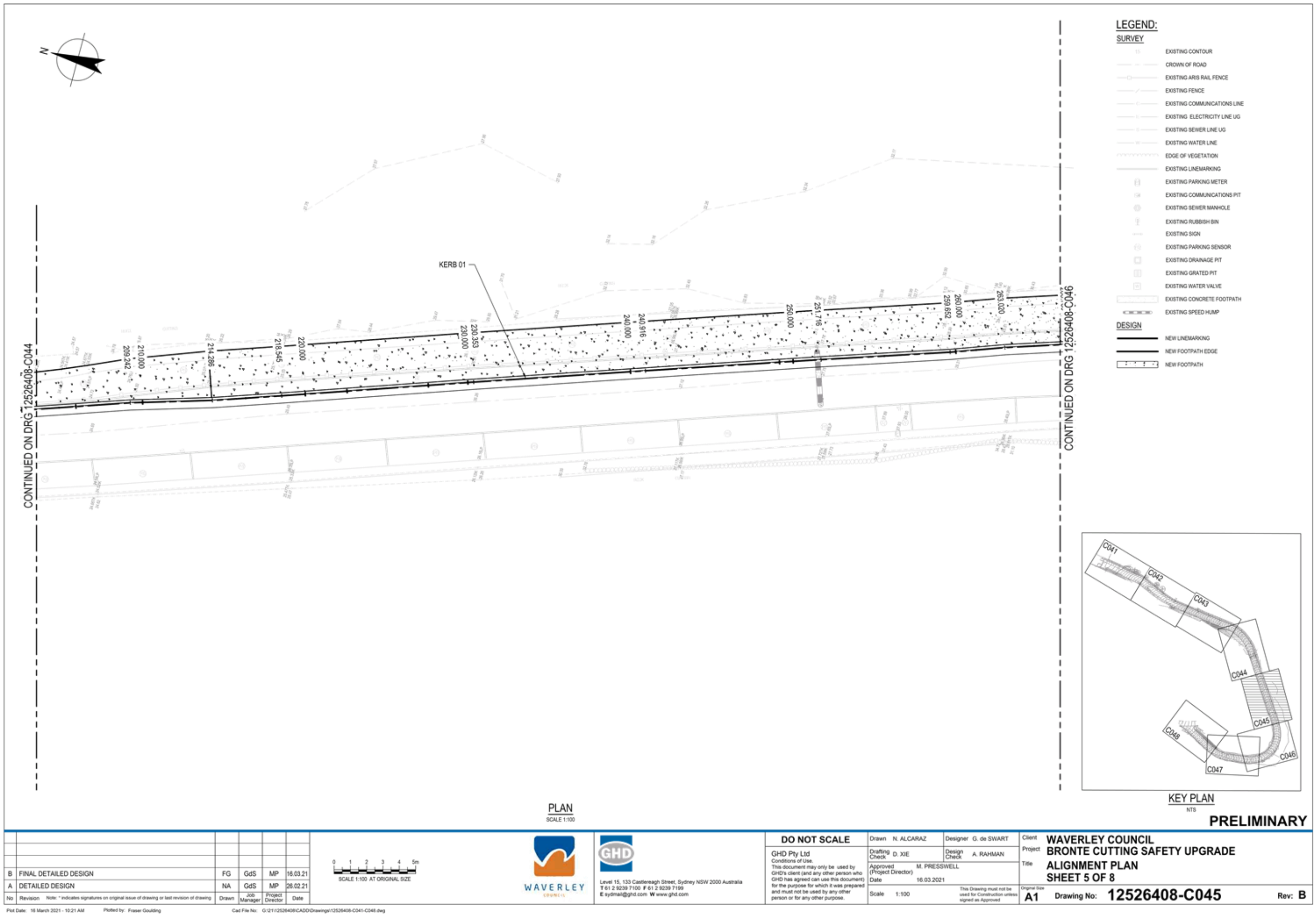
Plotted by: Fraser Goulding

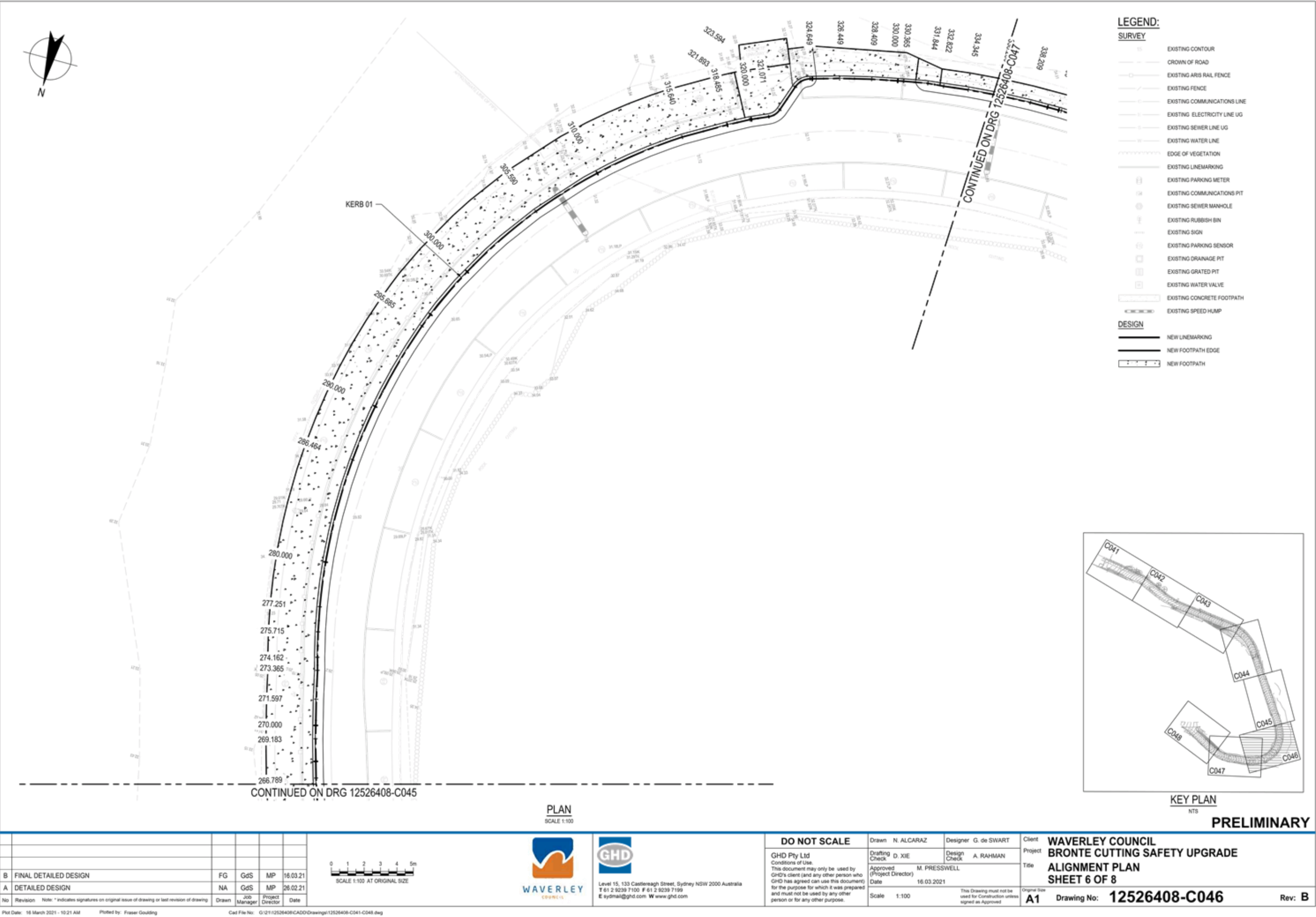
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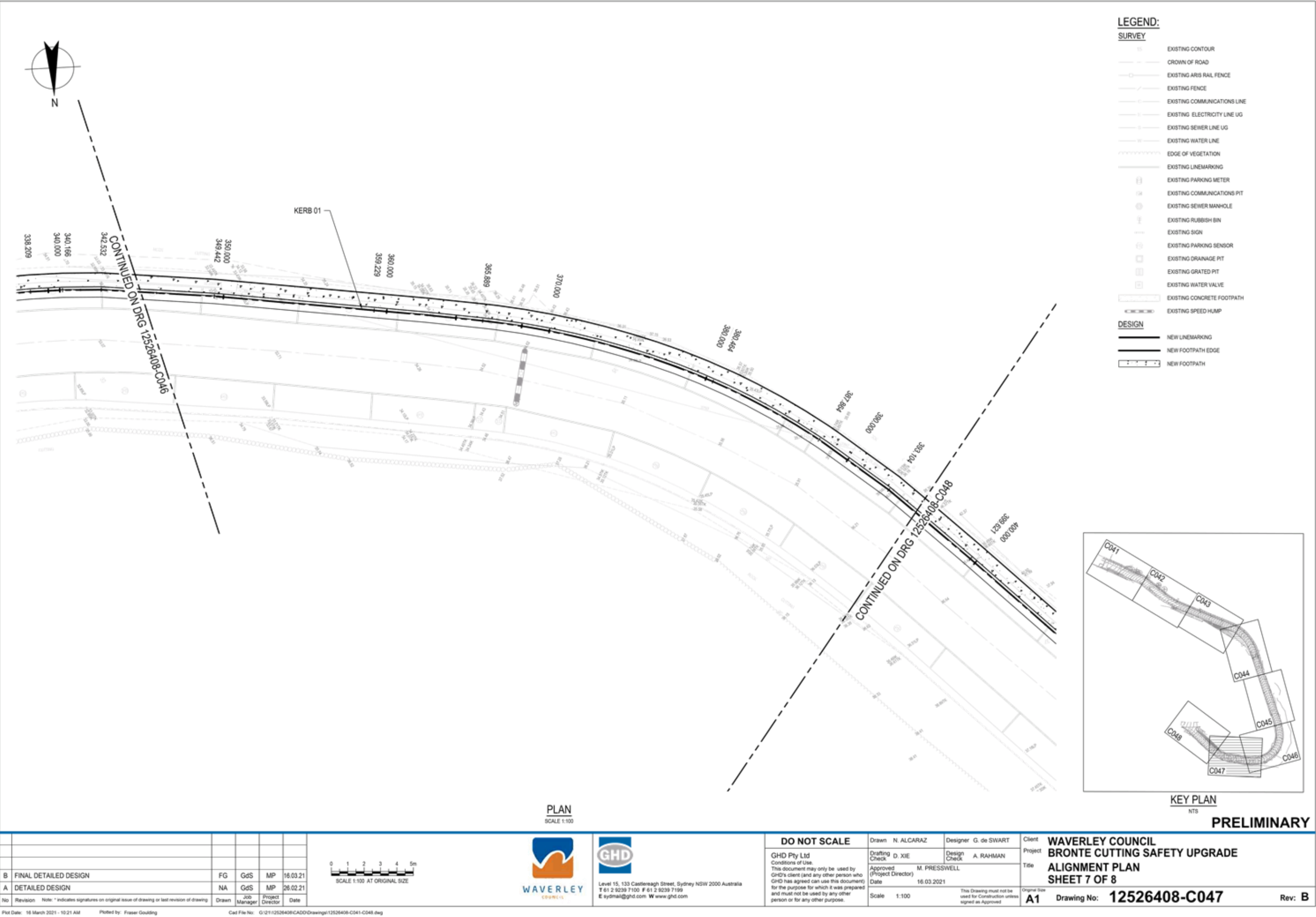














NEW KERB 1->NEW ALIGNMENT HORIZONTAL POINTS								
PT	CHAINAGE	EASTING	NORTHING	HEIGHT	BEARING	RAD/SPIRAL	A.LENGTH	DEFL.ANGLE
IP 1	0.000	339867.516	6247037.105		113°58'57.05"			
IP 2	4.355	339871.495	6247035.335	10.478				
TC	6.454	339873.388	6247034.427	10.595	115°38'05.67"			
IP 3	6.529	339873.456	6247034.395	10.599		R = 1.000	0.150	8°37'03.45"
CT	6.604	339873.518	6247034.352	10.603	124°15'08.12"			
IP 4	10.365	339876.626	6247032.235	10.816				
IP 5	30.661	339892.446	6247019.521	12.053				
IP 6	45.002	339903.639	6247010.555	13.011				
IP 7	50.798	339908.493	6247007.387	13.380				
IP 8	56.730	339913.590	6247004.353	13.758				
IP 9	64.316	339920.538	6247001.307	14.242				
IP 10	68.906	339924.772	6246999.535	14.561				
IP 11	74.915	339930.315	6246997.215	14.980				
IP 12	80.891	339935.821	6246994.891	15.397				
IP 13	86.807	339941.300	6246992.662	15.811				
IP 14	89.386	339943.707	6246991.735	15.968				
IP 15	130.504	339961.796	6246976.244	18.856				
TC	131.711	339962.909	6246975.778	18.944	112°43'11.80"			
IP 16	138.522	339969.234	6246973.130	19.394		R = 47.979	13.621	16°15'58.56"
CT	145.332	339994.563	6246968.816	19.819	128°59'10.36"			
IP 17	145.527	339994.714	6246968.694	19.831				
TC	145.579	339994.755	6246968.651	19.834	129°18'03.63"			
IP 18	149.065	339997.457	6246966.449	20.052		R = 47.248	6.972	8°27'18.28"
CT	152.551	339999.805	6246963.863	20.269	137°45'21.91"			
IP 19	152.592	339999.833	6246963.833	20.272				
TC	154.128	340000.867	6246962.697	20.367	137°41'57.75"			
IP 20	163.774	340007.431	6246955.483	20.969		R = 53.000	19.292	20°51'18.84"
CT	173.419	340010.997	6246946.405	21.570	158°33'16.59"			
IP 21	173.821	340011.144	6246946.031	21.595				
IP 22	175.953	340011.923	6246944.047	21.728				
IP 23	185.509	340014.866	6246934.954	22.261				
TC	188.641	340015.851	6246931.981	22.416	161°40'00.67"			
IP 24	190.592	340016.473	6246930.105	22.512		R = 10.000	3.902	22°21'26.12"
CT	192.543	340016.334	6246928.134	22.609	184°01'26.79"			
TC	194.095	340016.225	6246926.586	22.685	184°01'26.79"			
IP 25	196.025	340016.088	6246924.637	22.781		R = -10.000	3.859	22°06'47.47"
CT	197.955	340016.695	6246922.780	22.876	161°54'39.32"			
IP 26	200.658	340017.534	6246920.210	23.009				
IP 27	209.242	340020.321	6246912.090	23.505				
IP 28	214.288	340021.768	6246907.258	23.809				
IP 29	218.545	340023.139	6246903.227	24.065				
IP 30	230.353	340027.000	6246892.068	24.786				
IP 31	240.916	340030.401	6246882.067	25.442				
IP 32	251.716	340033.850	6246871.832	26.123				
IP 33	259.652	340036.339	6246864.297	26.652				
IP 34	263.020	340037.485	6246861.130	26.876				
IP 35	266.789	340038.648	6246857.545	27.127				
IP 36	269.183	340039.297	6246855.240	27.286				
IP 37	271.597	340039.880	6246852.898	27.447				
IP 38	273.365	340040.262	6246851.172	27.565				
IP 39	274.162	340040.473	6246850.403	27.618				
IP 40	275.715	340040.837	6246848.893	27.721				
IP 41	277.251	340041.132	6246847.366	27.823				
IP 42	281.858	340041.908	6246842.820	28.130		R = 36.526	9.214	14°27'09.81"
IP 43	286.464	340041.520	6246836.205	28.437				
IP 44	291.075	340041.101	6246833.589	28.744		R = 36.458	9.221	14°29'25.45"
IP 45	295.685	340039.541	6246829.225	29.051				
IP 46	300.837	340037.767	6246824.557	29.380		R = 31.556	9.905	17°59'03.89"
IP 47	305.590	340034.639	6246820.664	29.710				
IP 48	310.615	340031.439	6246816.738	30.044		R = 32.484	10.050	17°43'15.08"
IP 49	315.640	340027.195	6246813.972	30.379				
IP 50	318.485	340024.734	6246812.545	30.569				
IP 51	319.907	340023.481	6246811.874	30.633				
TC	321.071	340022.414	6246811.407	30.685	246°22'47.24"			
IP 52	321.482	340022.015	6246811.232	30.703		R = -1.000	0.823	47°08'35.80"
CT	321.893	340021.871	6246810.821	30.722	199°14'11.44"			
TC	323.594	340021.310	6246809.215	30.798	199°14'11.44"			
IP 53	324.122	340021.122	6246808.675	30.822		R = 1.100	1.055	54°55'35.27"
CT	324.649	340020.572	6246808.519	30.846	254°09'46.70"			
IP 54	326.449	340018.841	6246806.028	30.927				
IP 55	328.409	340016.935	6246807.567	31.015				

NEW KERB 1->NEW ALIGNMENT HORIZONTAL POINTS								
PT	CHAINAGE	EASTING	NORTHING	HEIGHT	BEARING	RAD/SPIRAL	A.LENGTH	DEFL.ANGLE
IP 1	0.000	339867.516	6247037.105		113°58'57.05"			
IP 56	330.365	340015.017	6248807.183	31.103				
IP 57	331.844	340013.581	6248806.922	31.169				
IP 58	332.822	340012.587	6248806.846	31.213				
IP 59	334.345	340011.074	6248806.668	31.282				
IP 60	336.250	340009.175	6248806.519	31.367				
IP 61	338.209	340007.217	6248806.442	31.455				
IP 62	340.166	340005.260	6248806.444	31.565				
IP 63	342.532	340002.897	6248806.549	31.732				
IP 64	348.442	339996.028	6248807.305	32.219				
IP 65	356.229	339986.338	6248808.676	32.910				
IP 66	362.559	339983.038	6248809.129	33.144		R = 286.387	6.661	1°19'57.30"
IP 67	365.889	339979.750	6248809.856	33.379				
IP 68	373.177	339972.508	6248810.854	33.857		R = 49.606	14.575	16°50'04.04"
IP 69	380.464	339965.922	6248814.097	34.318				
IP 70	384.164	339962.609	6248815.764	34.552		R = 42.518	7.399	9°56'15.99"
IP 71	387.864	339959.635	6248817.980	34.786				
IP 72	390.484	339957.584	6248819.611	34.953		R = 533.722	5.241	0°33'45.30"
IP 73	393.104	339955.549	6248821.262	35.124				
IP 74	399.621	339950.801	6248825.726	35.560				
IP 75	411.970	339941.954	6248834.341	36.429				
IP 76	424.062	339933.253	6248842.736	37.292				
IP 77	426.652	339931.420	6248844.567	37.477				
IP 78	429.962	339929.050	6248846.879	37.699				
IP 79	432.062	339930.524	6248848.376	37.807				
TC	435.000	339928.342	6248850.343	37.958	312°02'05.10"			
IP 80	435.337	339928.081	6248850.578	37.976		R = -1.000	0.675	38°40'30.65"
CT	435.675	339927.731	6248850.596	37.993	273°21'34.45"			
IP 81	443.079	339925.340	6248851.032		273°21'34.45"			

CTRL KERB->KERB 02 HORIZONTAL POINTS					
PT	CHAINAGE	EASTING	NORTHING	HEIGHT	BEARING
IP 1	0.000	339910.430	6246997.649	15.090	113°39'03.88"
IP 2	2.155	339912.404	6246996.784	15.241	
IP 3	6.330	339916.221	6246995.092	15.535	
IP 4	18.733	339927.661	6246990.304	16.104	
IP 5	25.859	339934.227	6246987.531	16.508	
IP 6	30.823	339938.646	6246985.753	16.790	
IP 7	36.567	339944.238	6246983.736	17.143	109°50'17.67"

CTRL KERB->KERB 03 HORIZONTAL POINTS								
PT	CHAINAGE	EASTING	NORTHING	HEIGHT	BEARING	RAD/SPIRAL	A.LENGTH	DEFL.ANGLE
IP 1	0.000	339940.504	6246845.853		224°16'42.64"			
IP 2	2.106	339939.032	6246844.344	37.968				
TC	6.231	339936.075	6246847.217	38.133	314°10'25.70"			
IP 3	7.382	339935.144	6246846.121	38.180		R = 2.000	2.302	65°57'21.57"
CT	8.533	339935.591	6246849.339	38.226	20°57'47.27"			
IP 4	9.685	339936.037	6246850.558	38.242		R = 2.000	2.302	65°57'21.57"
CT	10.836	339937.332	6246850.646	38.199	86°05'08.84"			
IP 5	16.843	339943.326	6246851.057	38.000	86°05'08.84"			

PRELIMINARY

					 WAVERLEY COUNCIL	 Level 15, 133 Castlereagh Street, Sydney NSW 2000 Australia T 61 2 9239 7100 F 61 2 9239 7199 E sydney@ghd.com W www.ghd.com	DO NOT SCALE		Drawn F. GOULDING	Designer G. de SWARDT	Client WAVERLEY COUNCIL Project BRONTE CUTTING SAFETY UPGRADE Title ALIGNMENT SETOUT TABLES	
C	FINAL DETAILED DESIGN	FG	GdS	MP			GHD Pty Ltd Conditions of Use: This document may only be used by GHD's client (and any other person who GHD has agreed can use this document) for the purpose for which it was prepared and must not be used by any other person or for any other purpose.		Drafting Check G. LIDGETT	Design Check A. RAHMAN		
B	DETAILED DESIGN	NA	GdS	MP			Approved (Project Director) M. PRESSWELL		Date 16.03.2021			
A	FINAL CONCEPT DESIGN	NA	GdS	MP			Scale N.T.S.		This Drawing must not be used for Construction unless signed as Approved			
No	Revision	Note: * indicates signatures on original issue of drawing or last revision of drawing			Drawn	Job Manager	Project Director	Date	Original Size A1		Drawing No: 12526408-C049	Rev: C

Plot Date: 16 March 2021 - 10:22 AM

Plotted by: Fraser Goulding

Cad File No: G:\21\12526408\CADD\Drawings\12526408-C049.dwg

Rev: C

REPORT
TC/C.02/21.03

Subject: York Road and Baronga Avenue - Slip Lane

TRIM No: SF21/1014

Author: Hamoon Bahari, Professional Engineer, Traffic and Transport
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council approves the construction of a left-turn slip lane on York Road at intersection with Baronga Avenue and associated lines and signs as shown in Attachment 1.

1. Executive Summary

Council has developed design plans for an upgrade to the intersection of York Road and Baronga Avenue Queens Park by introducing a left-turn slip lane into Baronga Avenue.

The intersection and subject area in Queens Park shown in Figure 2.

The proposed design is shown in Figure 1. Detailed plans are contained in Attachment 1.

The left-turn slip lane will provide more opportunities for westbound drivers on York Road to enter Baronga Avenue as they will no longer have to give way to eastbound drivers turning left into Baronga Avenue. Instead eastbound drivers turning left into Baronga Avenue will have to give way to westbound drivers turning right into Baronga Avenue

It is recommended that the attached design plans are supported.



Figure 1. Intersection of York Road and Baronga Avenue.

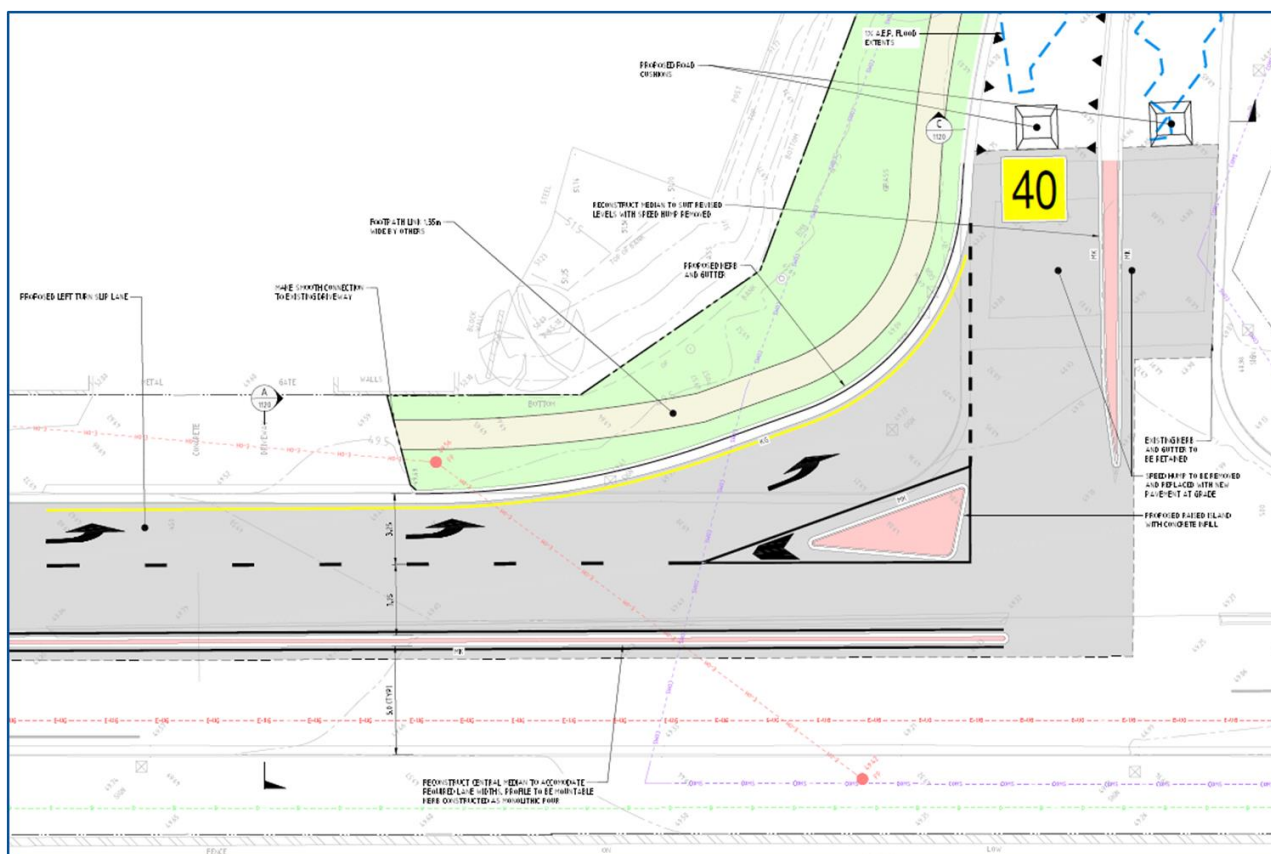


Figure 2. York Road Slip Lane.

2. Introduction/Background

Council is undertaking renewal works for York Road between Darley Road and Queens Park Road. The works will include extensive pavement reconstruction along this stretch of road.

Officers are also looking at how to improve congestion and safety at this intersection of York Road and Baronga Avenue. The increased growth of the Moriah College on the corner of this intersection has meant that a change to the existing configuration may be needed to accommodate additional traffic flows.

3. Technical Analysis

The York Road / Baronga Avenue intersection will continue to operate as per existing arrangement in addition to a left-turn slip lane on York Road proposed for vehicles heading northbound into Baronga Avenue.

Existing intersection arrangement

The current layout is shown below in Figure 2. The existing lane widths are:

- A 5.5 metre wide left/through lane eastbound on York Road.
- A 2.5 metre wide through lane and a 3 metre wide right turn lane westbound on York Road.
- A 6.25 metre wide northbound travel lane on Baronga Avenue.
- A 4.7 metre wide southbound travel lane on Baronga Avenue.

A raised speed hump with a concrete island separating the two directions of travel is currently installed. This raised speed hump is proposed to be removed with two speed cushions installed just north of the speed hump. This is required because a speed hump over the full width of the road would need to be located further north of the current position due to conflicts with the left turn into Baronga Road. This, in turn results in the chance of water flooding over the roadway on the northern side of the hump. Speed cushions do not result in flooding issues.

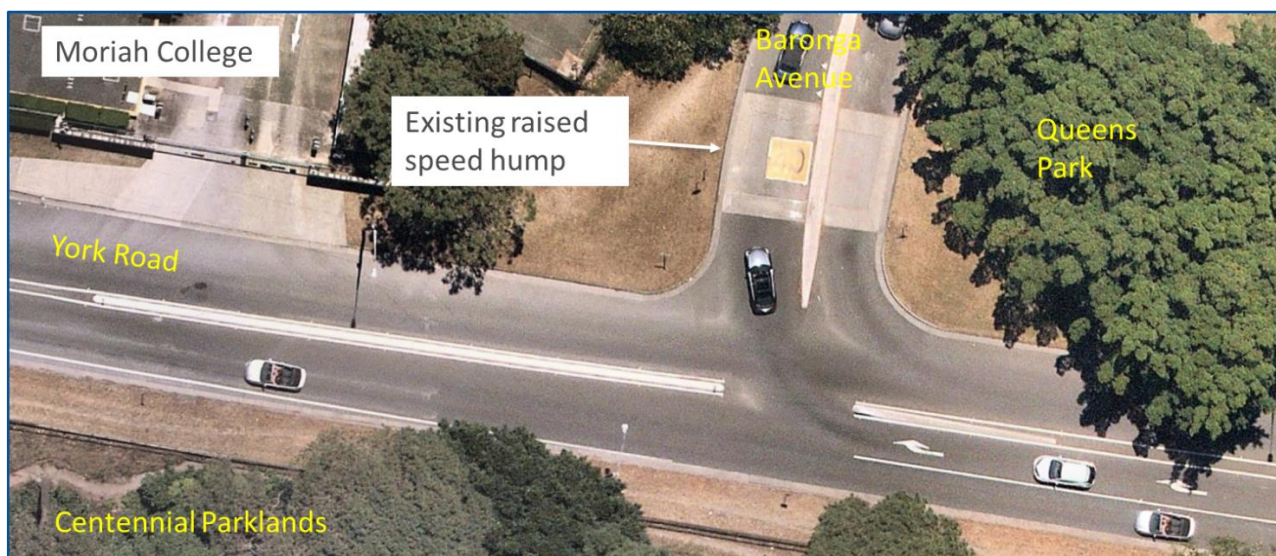


Figure 2. York Road and Baronga Avenue existing raised speed hump.

Proposed changes and improvements

The proposed changes will bring about a new slip lane built into the existing council verge at the north-western corner of the intersection.

At the entrance, the proposed lane widths will become:

- A 3.25 metre wide eastbound left turn slip lane on York Road.
- A 3.25 metre wide eastbound through travel lane on York Road, narrowing to 3.0 metres as it passes Baronga Avenue.
- A 2.5 metre wide through lane and a 3 metre wide right turn lane westbound on York Road.
- A 6.25 metre wide northbound travel lane on Baronga Avenue.
- A 4.7 metre wide southbound travel lane on Baronga Avenue.

Shown below is the visualisation of the slip lane heading northbound from York Road into Baronga Avenue.

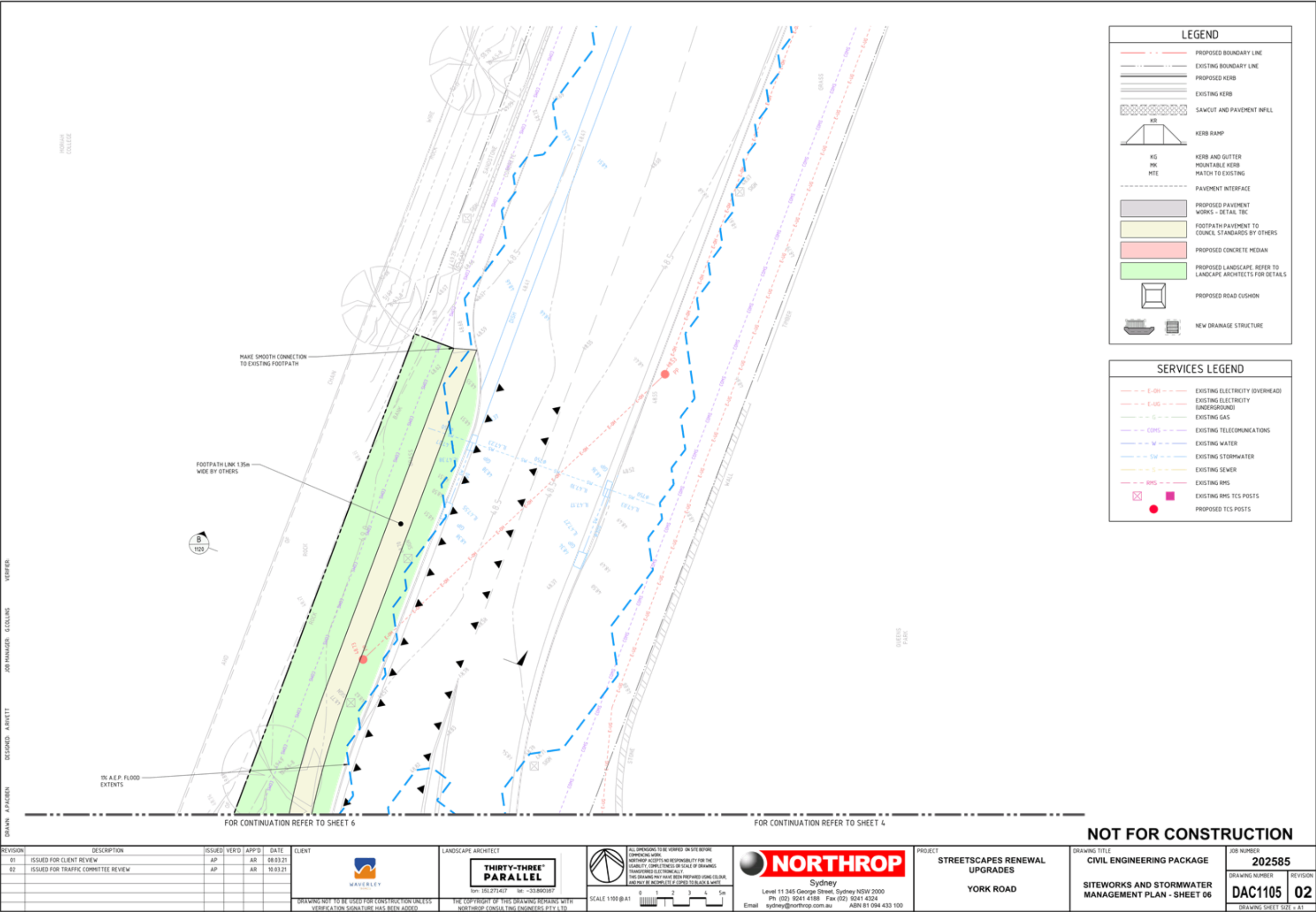
Detailed plans, dimensions, placement of associated signage and the flat top speed cushions are shown in Attachment 1.

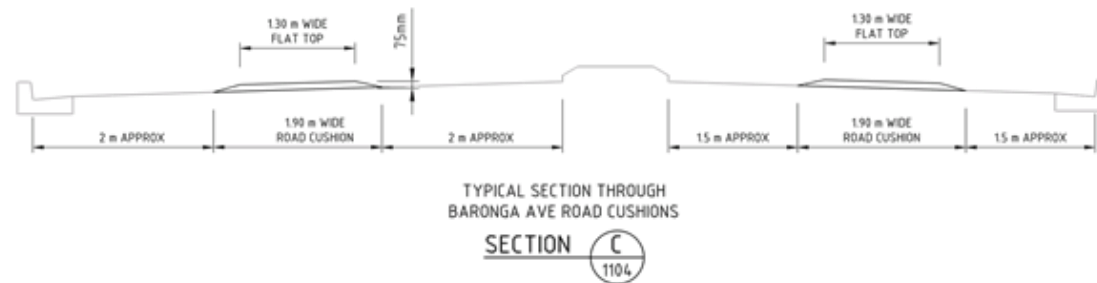
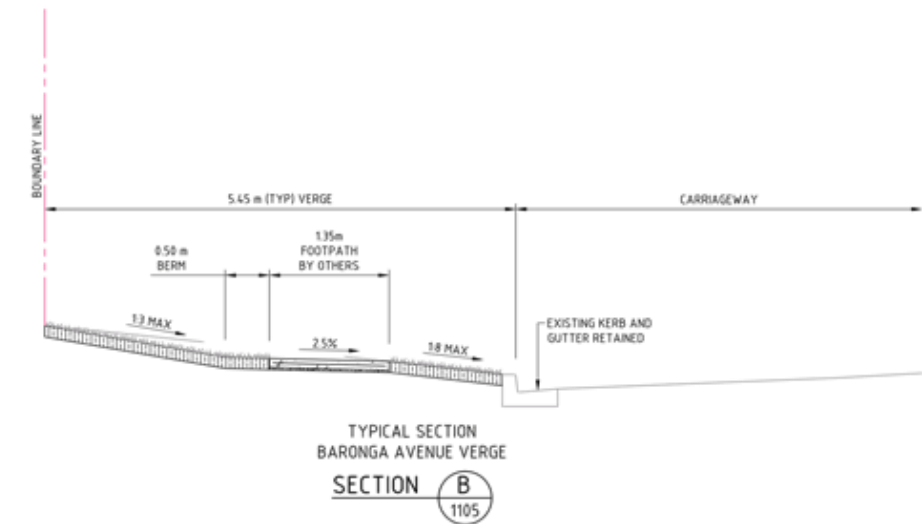
4. Financial Information for Council's Consideration

The project is co-funded by Waverley Council and TfNSW under the Regional Roads Program.

5. Attachments

1. York Road Slip Lane Design Final [↓](#)

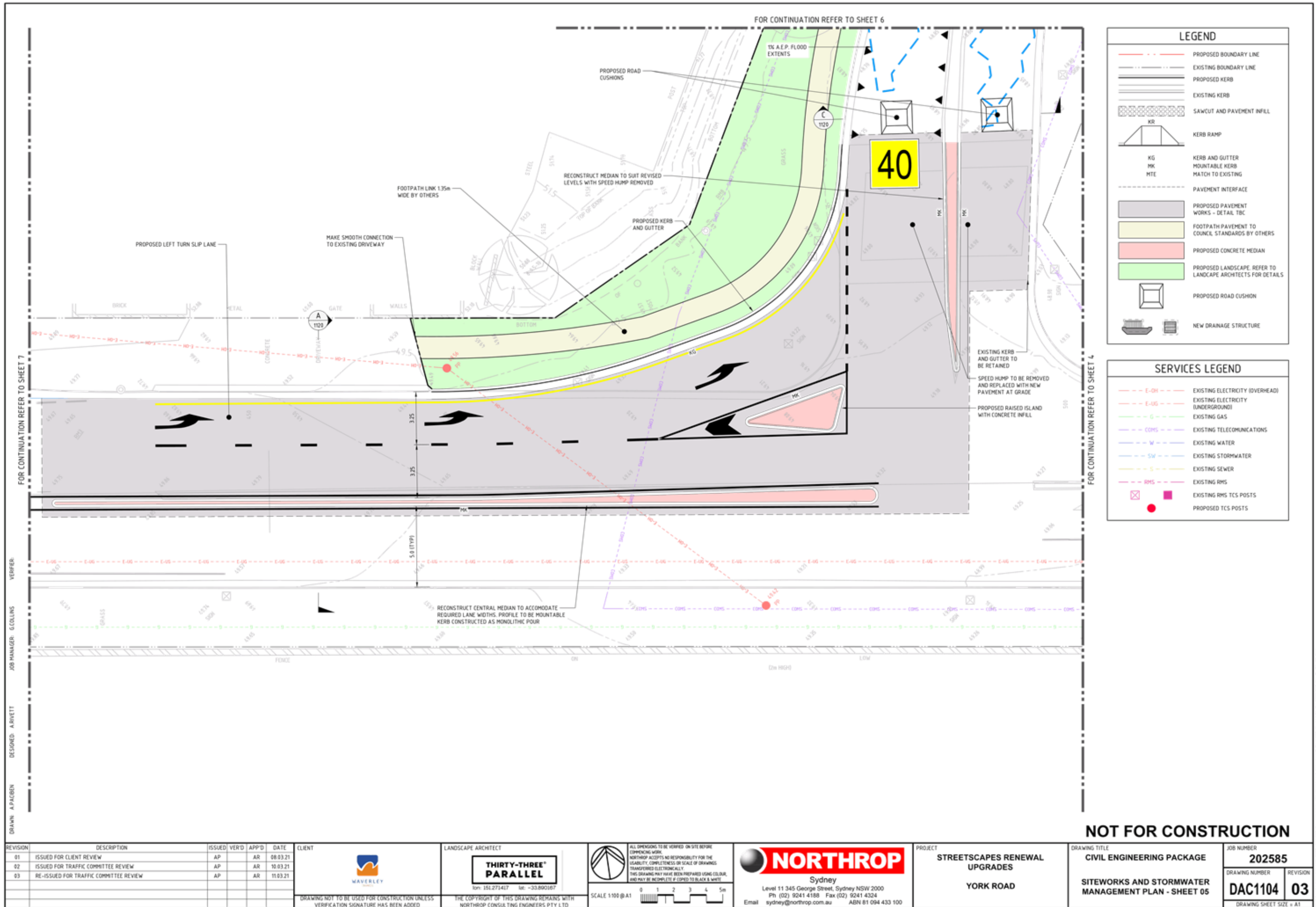




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02	ISSUED FOR TRAFFIC COMMITTEE REVIEW			AP		AR	10.03.21							YORK ROAD	TYPICAL ROAD CROSS SECTIONS - SHEET 01	DRAWING NUMBER	REVISION	
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REPORT

TC/C.03/21.03



Subject: 40 km/h Speed Limit Changes - Traffic Control Devices - Park Parade and Dickson Street at Birrell Street, Bondi

TRIM No: A18/0579

Author: Malik Almuhanha, Senior Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the installation of centre line marking with kerb extensions at the intersection of Park Parade and Birrell Street as shown in Figure 2.
2. Approves the installation of a pedestrian refuge island with kerb extensions at the intersection of Dickson Street and Birrell Street as shown in Figure 3.
3. Approves the installation of a 'No Right Turn, 8am-9:30am and 2:30pm-4pm, Monday to Friday' restriction on the southbound approach to Birrell Street at the intersection of Park Parade and Birrell Street.
4. Approves the installation of a 'No Right Turn, 8am-9:30am and 2:30pm-4pm, Monday to Friday' restriction on the northbound approach to Birrell Street at the intersection of Dickson Street and Birrell Street.
5. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

1. Executive Summary

This report seeks endorsement of the designs for the intersections of Park Parade and Dickson Street with Birrell Street (see Figure 1 for locations).

The matter was previously considered by the Traffic Committee in November 2020. At its meeting on 8 December 2020, Council resolved to consult with the community on the following options for both intersections:

- *Option a: Centre line marking with kerb extensions.*
- *Option b: Centre line marking with kerb extensions and a 'No Right Turn' restriction.*
- *Option c: Pedestrian refuge island with kerb extensions.*
- *Option d: Retain the existing conditions (leave as is)*

1,420 households consulted via letterbox drop. 151 responses were received, some of which were from the same household. These multiple responses were treated as a single submission per household.

Park Parade/Birrell Street

The community preferred the centre line marking and kerb extensions option which received 45 votes with additional 21 votes favouring adding a 'No Right Restriction' to option 1a. Centre line markings and kerbs extensions plus a no right turn in the morning and afternoon peaks are recommended (see Figure 2).

Park Parade/Dickson Street

The majority of the community preferred the pedestrian refuge island option with 61 votes. The pedestrian refuge island plus a no right turn in the morning and afternoon peaks is recommended.

The option for both Park Parade and Dickson Street included a full time right turn ban. The recommended part time restrictions address congestion issues which do not occur during off peak times (see Figure 3).



Figure 1. Park Parade and Dickson Street intersections with Birrell Street.

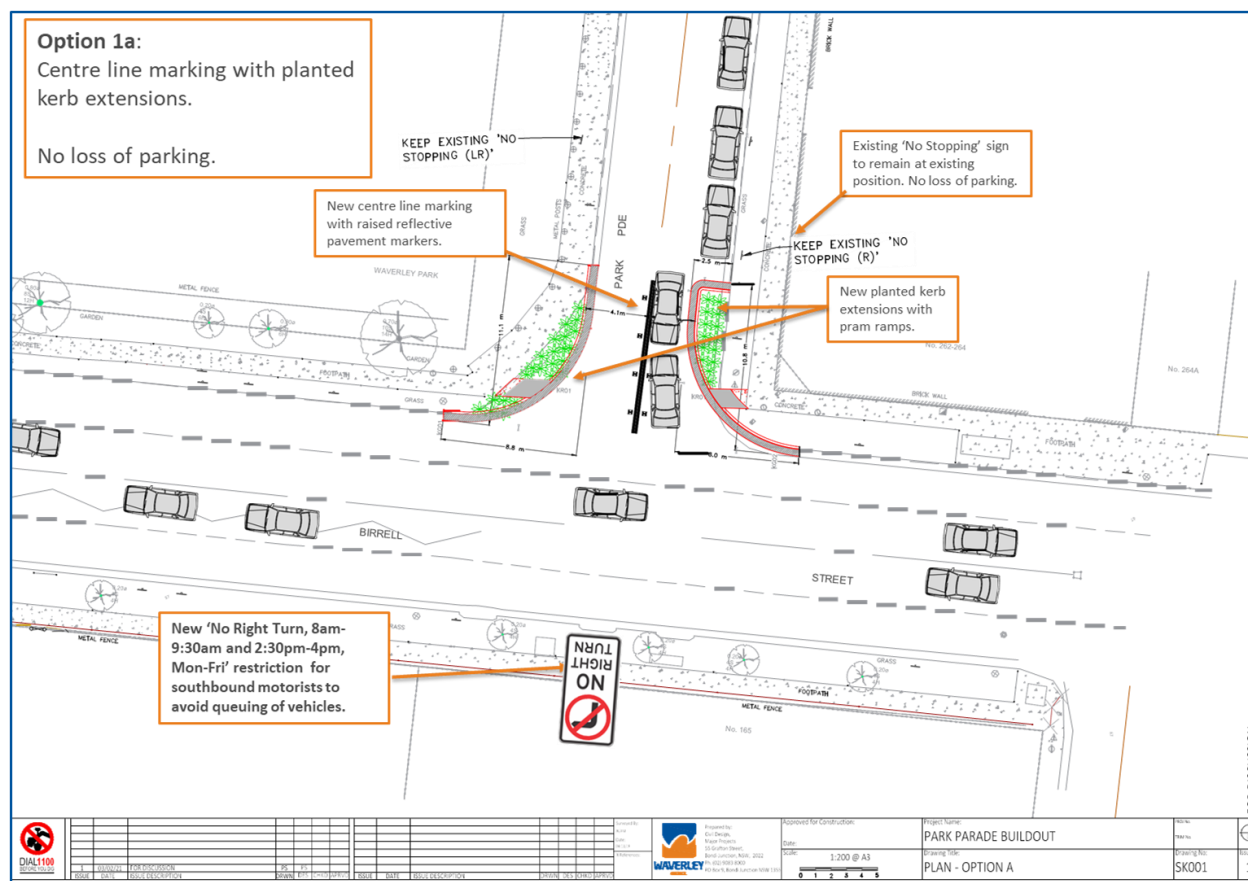


Figure 2. Park Parade intersection with Birrell Street – Recommended treatment design.

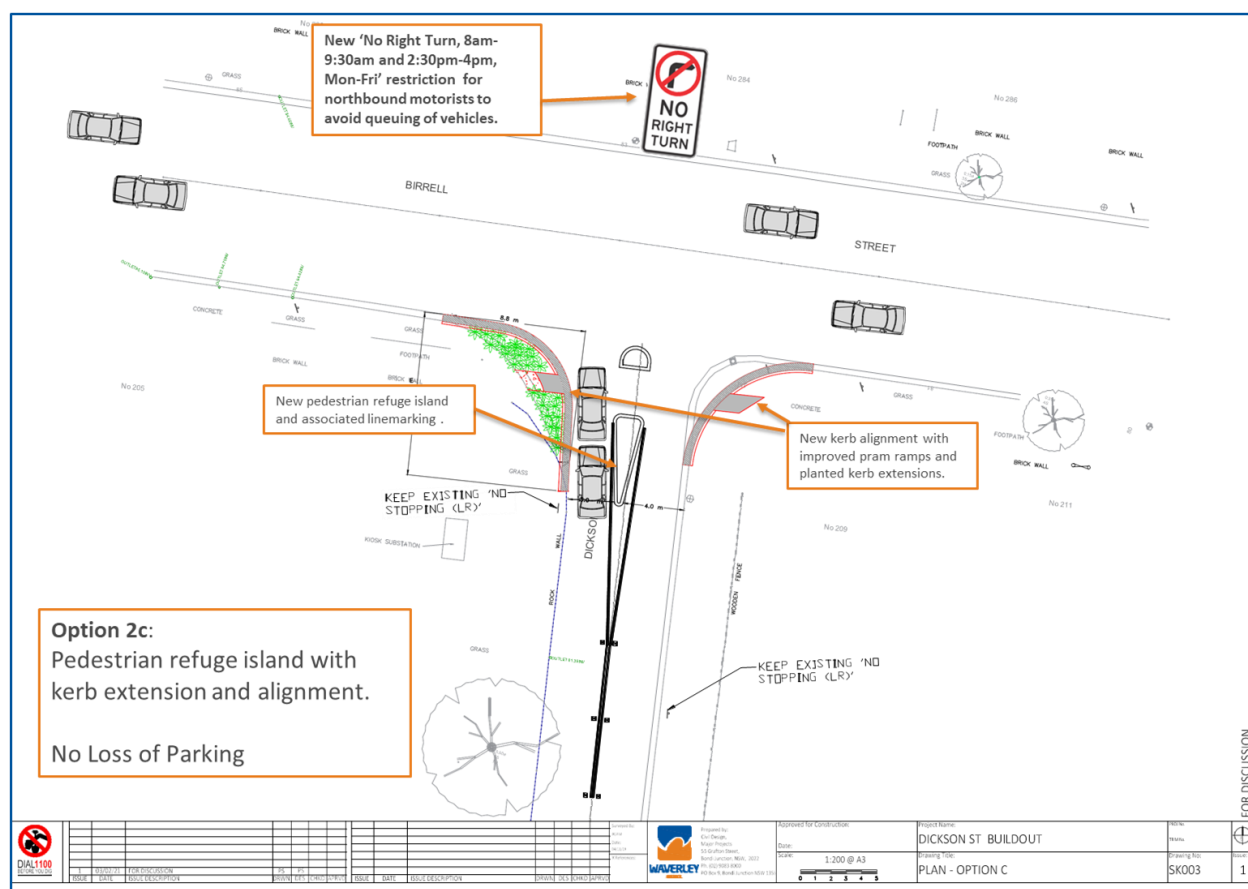
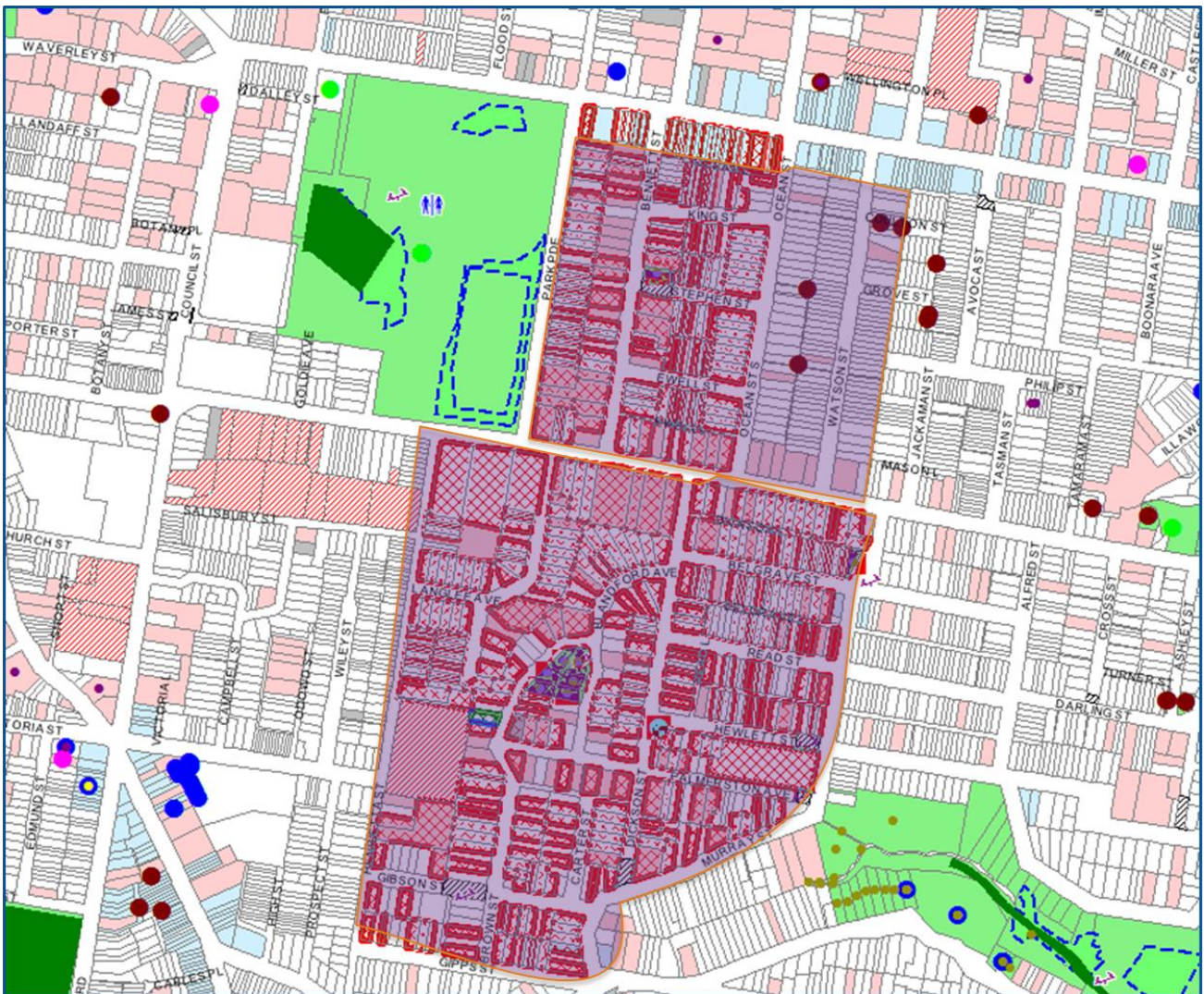


Figure 3. Dickson Street intersection with Birrell Street- Recommended treatment design.

This report seeks endorsement of recommended designs for the intersections of Park Parade and Dickson Street with Birrell Street.

The designs presented introduce kerb extensions in order to reduce pedestrian crossing distances across Park Parade and Dickson Street. This results in significant safety improvements for pedestrians at these two locations.

The designs are in response to community consultation via letterbox drop to occupants of 1,420 dwellings in the vicinity of the works. Figure 4 shows the consultation area.



Intersection options

The options for intersection treatments that were delivered to residents are presented in Figures 5 and 6.

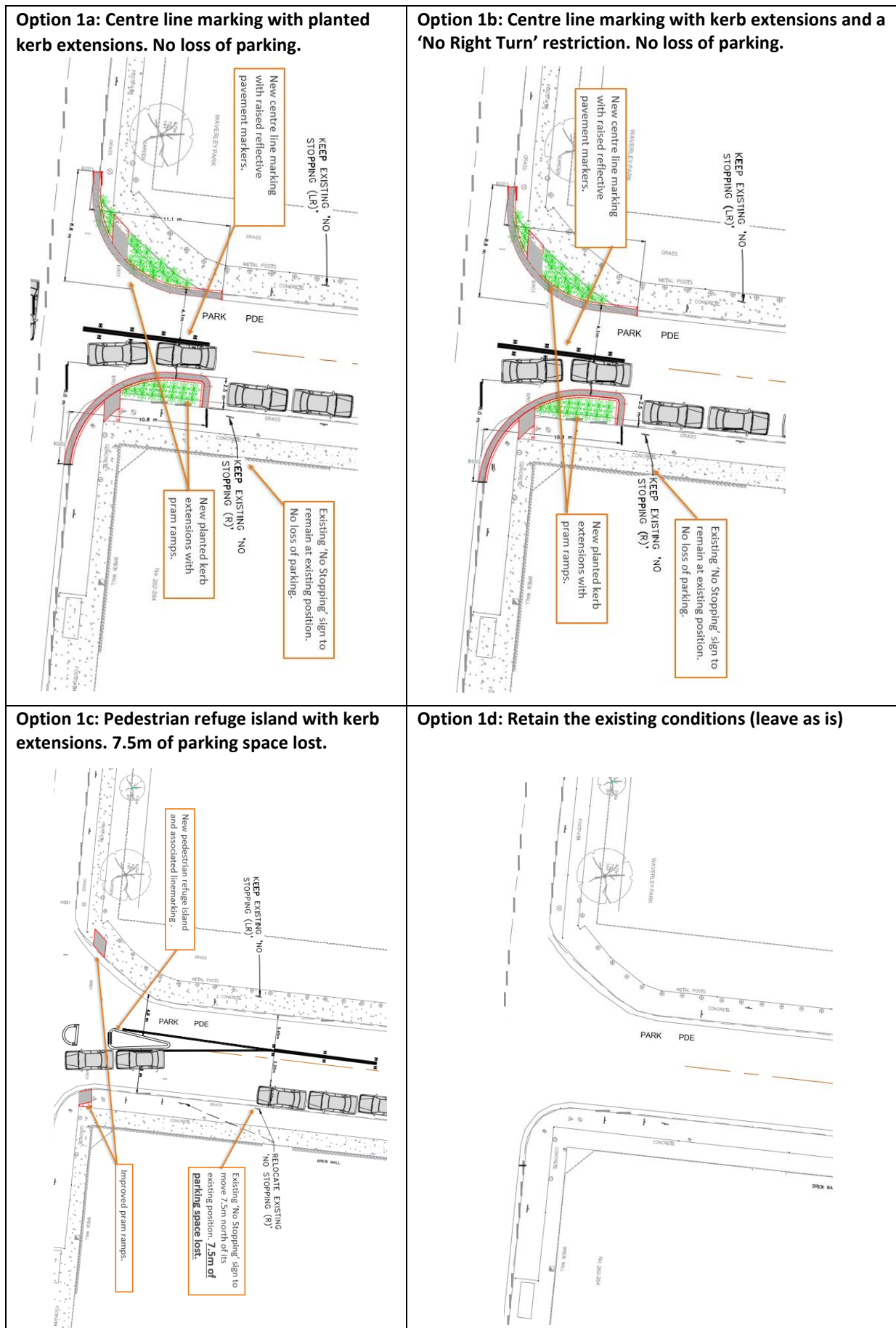


Figure 5. Park Parade / Birrell Street options presented to residents.



Figure 6. Dickson Street / Birrell Street options presented to residents.

3. Technical Analysis

Proposed designs

The proposed intersection treatments are expected to provide a safer crossing experience for pedestrians whilst reducing speeds. The kerb extensions will minimise crossing distances for pedestrians in both Park Parade and Dickson Street intersections with Birrell Street.

The traffic devices will also act as entry threshold treatments that will provide traffic calming. This is especially suitable when transitioning from a local to a non-local street, which is the case here.

The proposed designs for Park Parade intersection with Birrell Street and Dickson Street intersection with Birrell Street are not expected to have an impact on existing kerbside parking. The intersection treatments will have little to no traffic noise generation, which is desirable in local areas.

Community Feedback Analysis

1,420 households were consulted via letterbox drop, and 151 responses were received (Figure 4 shows the consultation area).

Park Parade intersection

Figure 7 presents a summary of the feedback on the Park Parade options.

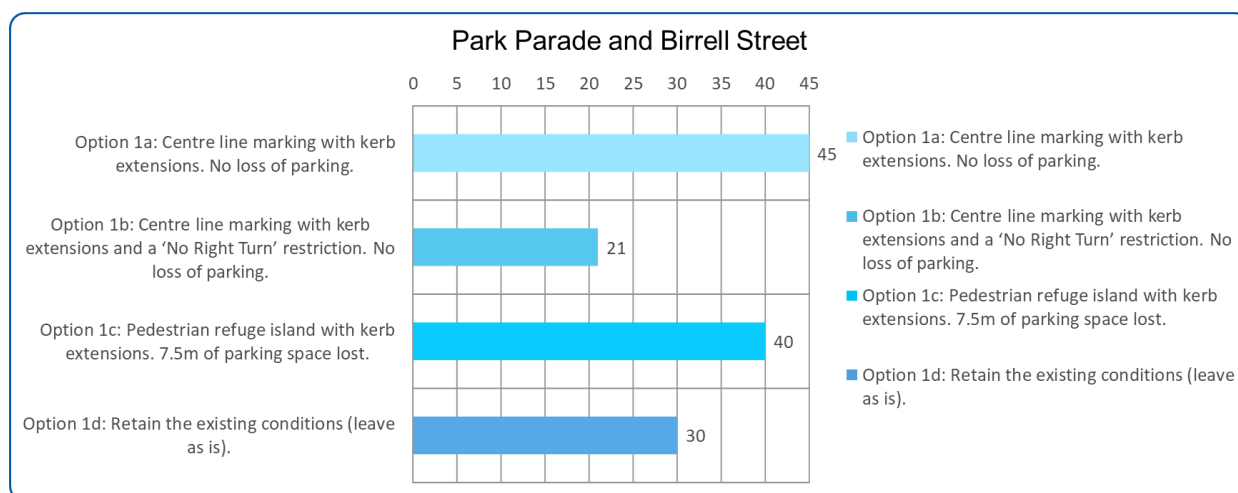


Figure 7. Community feedback on Park Parade intersection with Birrell Street treatment design.

45 votes were in favour of option 1a, with option 1c being a close second with 40 votes.

Option 1c is expected to be the most effective in providing a safer crossing for pedestrians and reducing speeds around the intersection. However, the 7.5m loss of kerbside parking was not supported. It is recommended that option 1a as shown in Figure 2 is adopted.

It is also recommended that an addition of a 'No Right Turn, 8am-9:30am and 2:30pm-4pm, Mon-Fri' restriction for southbound motorists is approved to avoid queuing of vehicles at peak hours. This is derived from community concerns raised during the consultation period.

Dickson Street intersection

Figure 8 presents a summary of the feedback on the Dickson Street options.

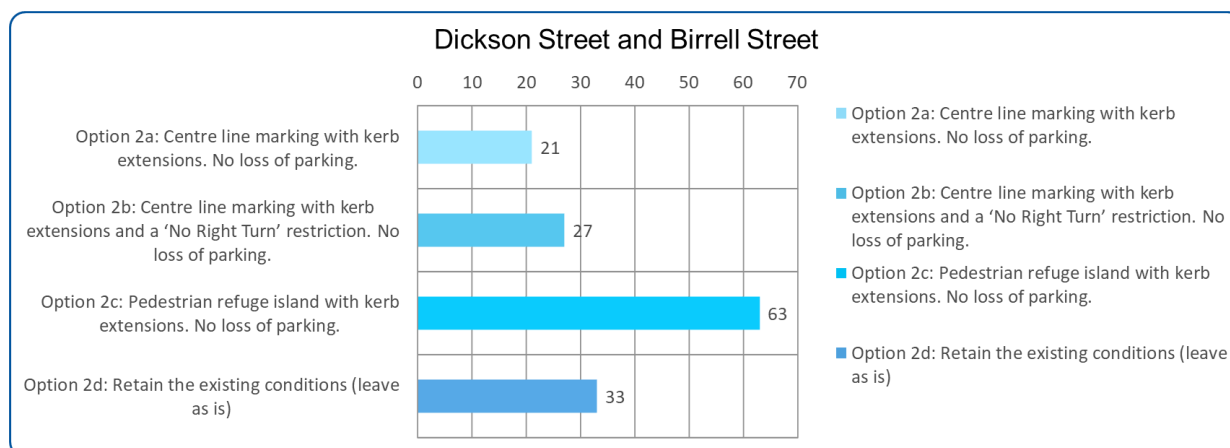


Figure 8. Community feedback on Dickson Street intersection with Birrell Street treatment design.

Option 2c, which received 63 votes.

Option 2c is expected to be the most effective in providing a safer crossing for pedestrians and reducing speeds around the intersection. It is recommended that option 2c as shown in Figure 3 is adopted.

It is also recommended that an addition of a 'No Right Turn, 8am-9:30am and 2:30pm-4pm, Mon-Fri' restriction for northbound motorists is approved to avoid queuing of vehicles at peak hours. This is derived from community concern during the consultation period.

4. Financial Information for Council's Consideration

Council has received a commitment of \$1 million for construction from the TfNSW Safe Speeds in High Pedestrian Activity and Local Area program. The \$1 million is required to be spent or committed by 30 June 2021.

5. Attachments

Nil

REPORT
TC/C.04/21.03

Subject: 49 Blair Street, Bondi Beach - 'P Disability only' and
"Motor Bikes Only' Zones

TRIM No: A02/0273

Author: Emraul Kayes, Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council installs:

1. A 5.4 metre 'P Disability Only' parking zone outside 49 Blair Street, Bondi Beach.
2. A 3.6 metre 'P Motor Bikes Only' zone between the driveway of 47 Mitchell Street, Bondi Beach and the new 'P Disability Only' zone.

1. Executive Summary

Council has received a request for a disability parking space at 49 Blair Street, Bondi Beach.

An assessment of the request was undertaken for the disability parking space in March 2021. It satisfies Council's requirements. The location of the site is shown in Figure 1.



Figure 1: Site location

There is 9 metres of kerb side parking between a driveway at number 47 and a No Stopping zone on the approach to a pedestrian crossing fronting property number 49 (shown in the Figure 2). This can accommodate one disabled space leaving 3.6 metres of alienated parking (too short to accommodate most cars). It is proposed that this alienated 3.6 metres is signposted for motor bike parking.

A 'No Stopping' sign previously installed by Council is missing. This will be replaced.

The proposed disability parking and motor bike parking zones are shown in Figure 2.

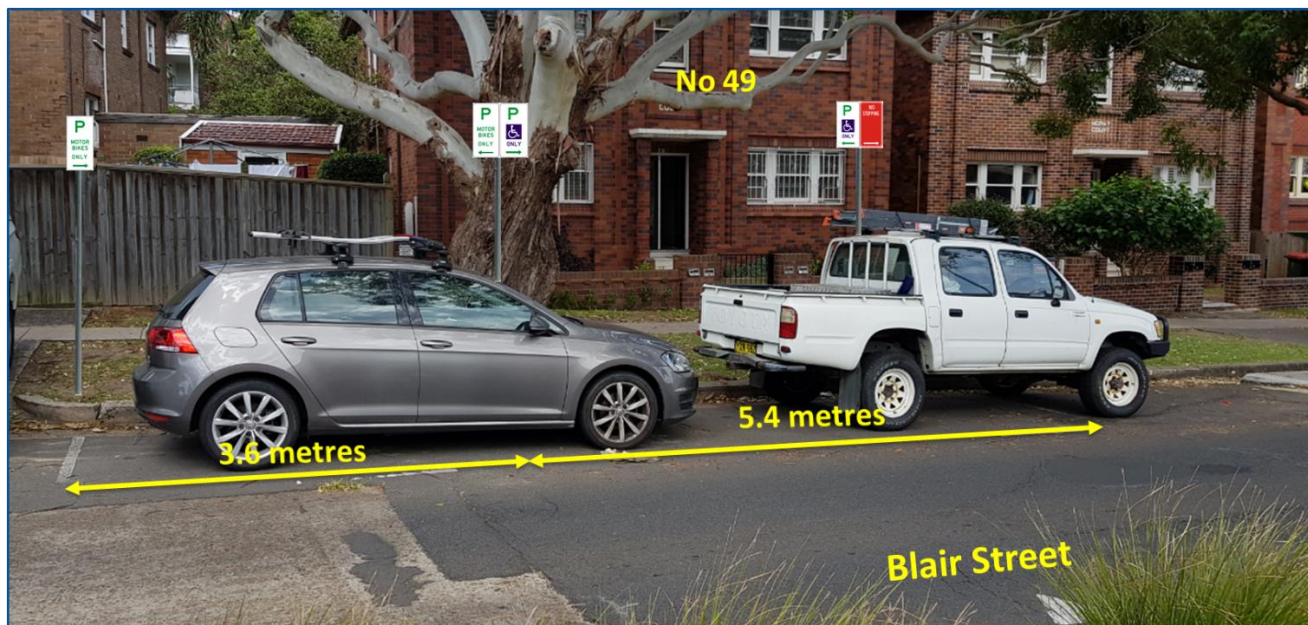


Figure 2: Proposed 'P Disability Only' and 'P Motor Bikes Only' parking zones location

Note: Figure 2 shows 2 cars parked within the subject area. One of the cars intrudes into the existing No Stopping zone.

2. Introduction/Background

Council requirements for disability parking spaces include:

1. Applicant must have a vehicle registered to the address fronting the new disability parking space.
2. Applicant must have an approved RMS Mobility Parking Permit.
3. Applicant must not have access to off-street parking within their property.
4. The level of parking within 50 metres either side and opposite of the property to be surveyed on 2 occasions at random by Council officers. The parking demand is to be above 85% capacity on both occasions.
5. Letters to be sent to all disability parking space holders annually to determine if still required.

3. Technical Analysis

Compliance of the proposal with Council standard requirements is presented below.

Table 1. Compliance with Council requirements.

Address	49 Blair Street, Bondi Beach
Mobility Parking Permit	Yes
Circumstances leading to requirement	Applicant's husband has had a serious stroke in 2019 leaving him with a heart condition and neurological consequences leaving him often unable to walk home without assistance.
Off-street parking available	No
Length of property frontage	12 metres
Length of zone	5.4 metres
On-street parking occupancy within 50 metres either side of the property	89%-100% occupied during the day.
Other on-street disability spaces	Nil within 50 metres. One in 63 Blair Street (distance=170 meters), another in 79 Blair Street (distance=200 meters).

The proposed length is based on Australian Standard AS2890.5-2020 – On-street parking. This is shown in Figure 3 below.

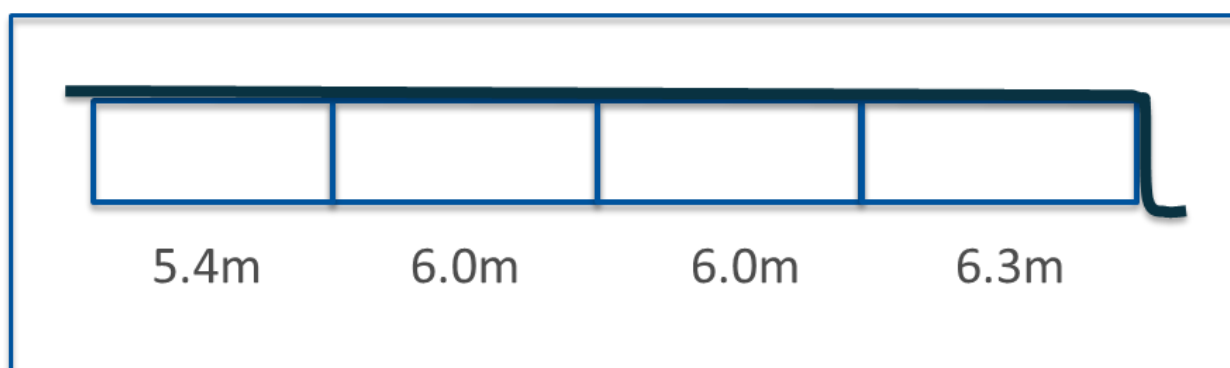


Figure 3. Dimensions for on-street disability parking spaces

'P Motor Bikes Only'

Austroroads guidelines and Waverley Council resolutions support the use of motor bike parking spaces in certain circumstances.

The Austroroads' Guide to Traffic Management - Part 11: Parking (Section 7.8.3), states that motor bike parking provision can be achieved by utilising under-sized parking spaces. The Traffic Committee and Council have supported this principle and have previously been in favour of installing 'P Motor Bikes Only' zones in short sections of kerb where other vehicles would obstruct vehicular access.

Warrants

Criteria for consideration when assessing the justification for installing a 'P Motor Bike Only' zone are presented below.

1. Distance check – Areas where there is less than 4.2 metres available for parking are eligible for consideration of motor bike parking instead. 4.2 metres can cater for the average small car. Lengths less than 4.2 metres can cater for smaller "city" cars (around 3.5 metres long). The ownership of these smaller vehicles is a small proportion of the total car fleet on the road.

2. Motor bike parking demand check – A site visit was conducted on 11 March 2021. More than 95% of the kerb-side parking on Blair Street was occupied. The high demands will result in cars being parked in short sections of kerb time to time. These conditions warrant consideration of installing motor bike parking.

The proposed motor bike parking zone will have no impact on on-street parking for cars. A motor bike parking zone as shown in Figure 2 is recommended.

Signage

The proposed signage is shown below.



4. Financial Information for Council's Consideration

Council will supply and install the signs and remove existing signs with funds from existing budgets.

5. Attachments

Nil

REPORT
TC/C.05/21.03

Subject: 40 km/h Speed Limit Changes - Traffic Control Devices -
Tamarama Marine Drive

TRIM No: A18/0579

Author: Malik Almuhanha, Senior Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the detailed design of the pedestrian crossing and relocation of bus stops on Tamarama Marine Drive, Tamarama as shown in Attachment 1.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

1. Executive Summary

Council determined the need for the installation of a raised pedestrian crossing on Tamarama Marine Drive, Tamarama, in the vicinity of the existing pedestrian refuge island (see Figure 1).

The matter was last presented to the November 2020 Waverley Traffic Committee meeting, Council resolved the following at its December 2020 meeting:

That Council:

1. Approves the detailed design of the pedestrian crossing on Tamarama Marine Drive, Tamarama, attached to the report.
2. Officers, during the design for construction, further investigate:
 - (a) The widening of the kerbside lane on the eastern side to allow for bus draw in.
 - (b) A traffic island to assist in the safe direction of traffic.
 - (c) The length of zigzags on the approach to the crossing.
3. Notes that Transport for NSW has issues with the recommendation pending a review of the design for construction.

The crossing design has now been expanded to include:

1. Relocation of the existing bus stops 10 to 16 metres to the south. This allows for the pedestrian crossing itself to now align with the path through Tamarama Park.

2. Widening of the footpath on the eastern side of Tamarama Marine Drive. This improves sight lines between southbound drivers and pedestrians.
3. Realignment of the travel lanes between Pacific Avenue and the crossing so that southbound car drivers have a better direct view of the crossing itself.

This report seeks Council's approval of the proposed location and design for the pedestrian crossing as shown in Figure 2.

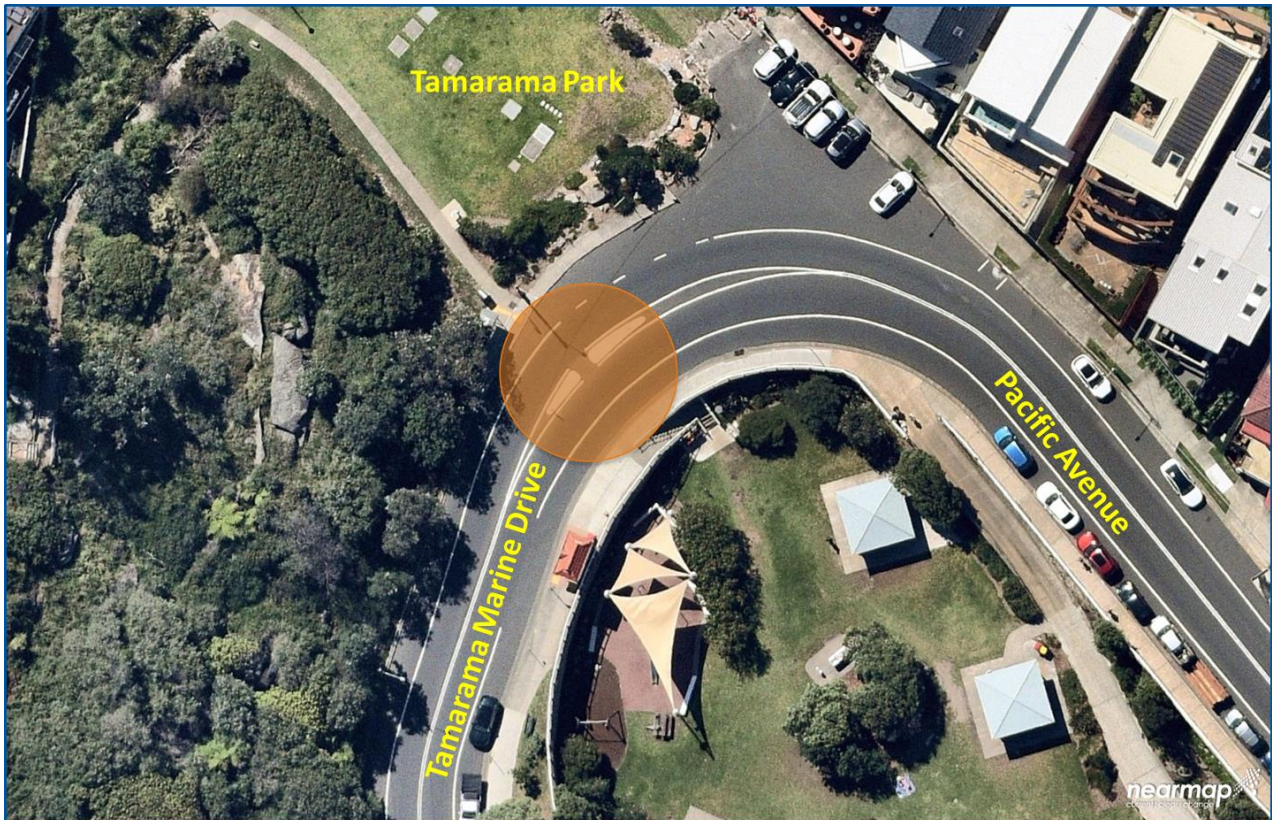


Figure 1. Aerial view of the existing refuge island at Tamarama Marine Drive.

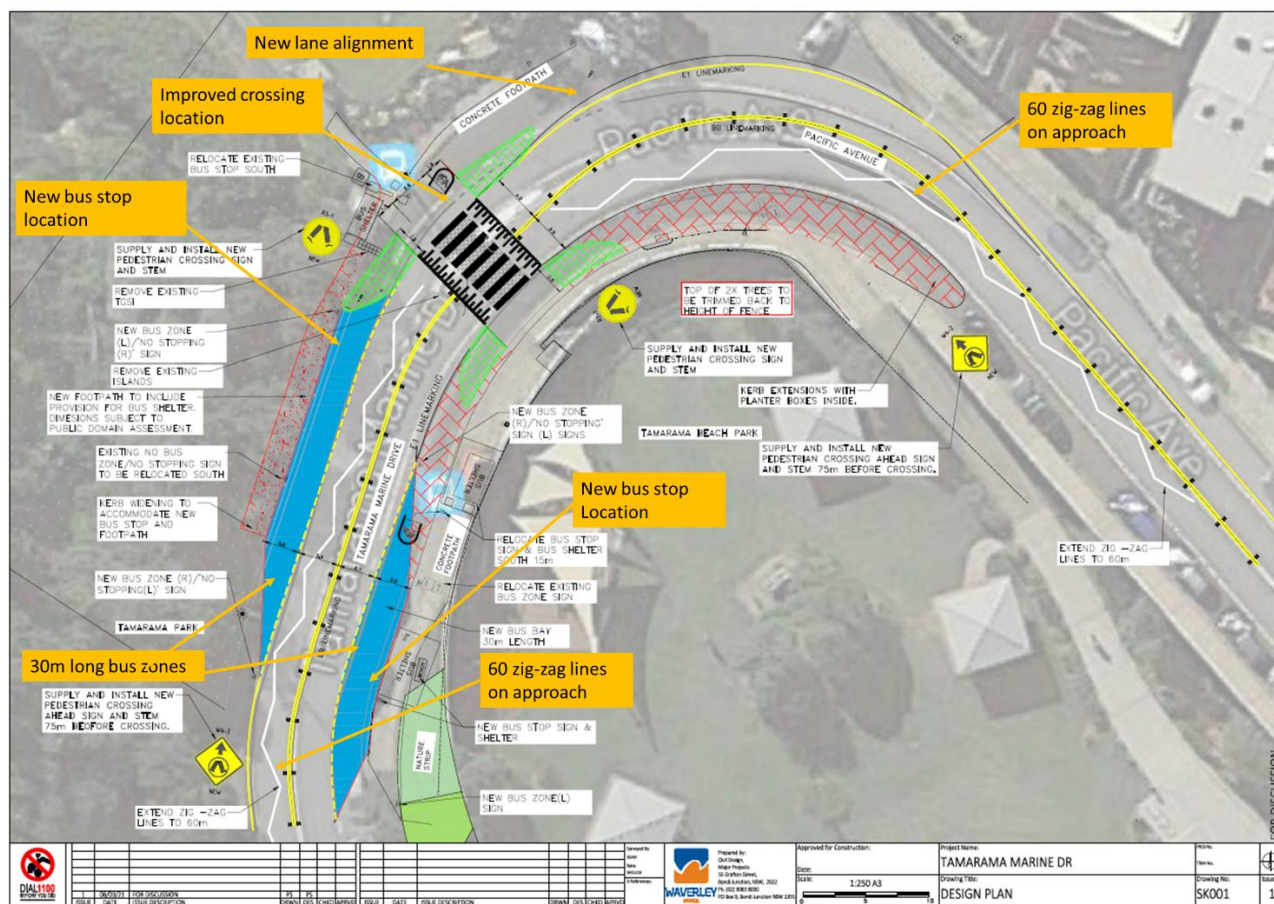


Figure 2. Concept of proposed pedestrian crossing design.

2. Introduction/Background

Council determined the need to install a pedestrian crossing on Tamarama Marine Drive outside Tamarama Park to replace the existing refuge island. The proposed pedestrian crossing minimises conflict between pedestrians crossing the road and vehicles travelling along the Tamarama Marine Drive by providing a time separating facility where priority is assigned to pedestrians under the NSW Road Rules 2014.

3. Technical Analysis

Bus stops and sight distances

The bus stop on the western side will be moved around 10 metres to the south. The new eastern bus zone will be 30 metres long.

The bus stop on the eastern side will be moved around 16 metres to the south. The new eastern bus zone will be 30 metres long.

Additional kerb buildouts have been provided on the sides of the crossing to enhance sight distances for traffic northbound and southbound.

60 metre long zig-zag lines are provided for both approaches.

Requirement for bus stop and accessible access

Retaining the western bus stop has been assessed. The need to keep the bus stop is demonstrated by the utilisation as shown in Figure 3. Significant boardings occur at this bus stop (22 people recorded on 11 January 2020).

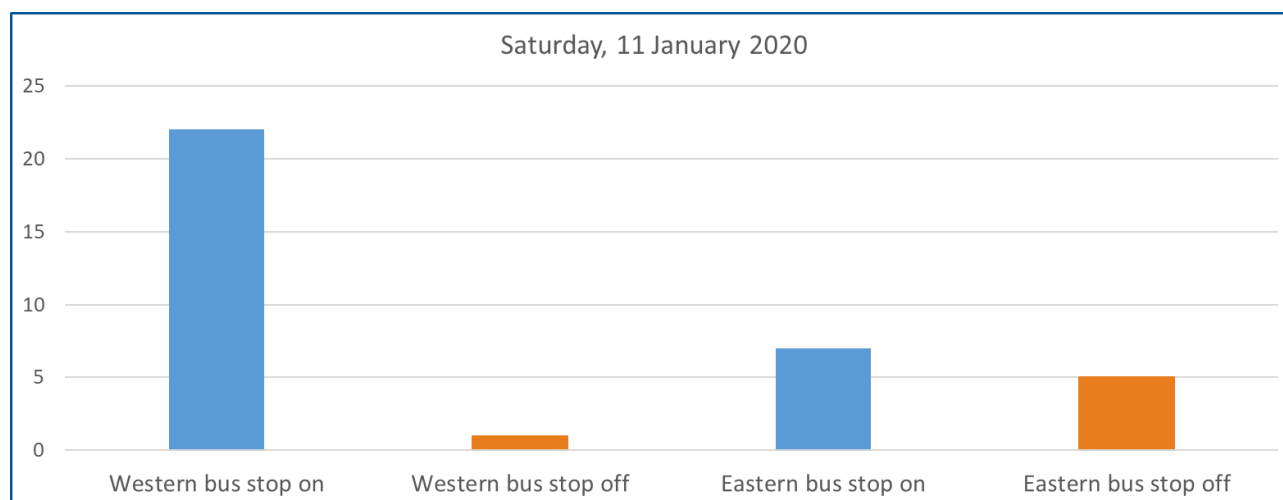


Figure 3. Boardings and alightings at bus stops adjacent to Tamarama Park.

Pedestrian crossing warrant

The proposed design utilises the existing pedestrian desire line between Tamarama Beach and Tamarama park. The crossing warrants have been met (see Table 1) as three separate pedestrian and vehicle volume counts were undertaken in Tamarama Marine Drive, on Friday, 19 October 2018.

Table 1. Results of pedestrian and vehicle volume counts.

Period			P	V	P × V
11.50am	to	12.50pm	209	501	104,709
1.20pm	to	2.20pm	154	517	79,618
2.30pm	to	3.30pm	207	627	129,789

Australian Standard AS 1742.10:2009 *Manual of Uniform Traffic Control Devices – Part 10: Pedestrian Facilities* no longer recommends minimum warrants for the installation of pedestrian facilities. These are considered policy matters to be determined by each road authority. Since Tamarama Marine Drive is a non-classified (local) road, Council is the Roads Authority under the *Roads Act 1993*.

Transport for NSW (TfNSW) provides a set of warrants for pedestrian crossings in its *Austroads Guide Supplement Austroads Guide to Traffic Management Part 6 – Intersections, Interchanges and Crossings*. This supplement provides guidance for local councils to observe, which recommends that a pedestrian crossing is warranted in the following circumstances:

If in each of three separate 1-hour periods in a typical day:

- $P \geq 30$ ped/h, and
- $V \geq 500$ veh/h, and
- $P \times V \geq 60,000$.

The pedestrian and vehicle volumes satisfy the warrant for installation of the marked pedestrian crossing.

Australian Standard AS 1742.10:2009 *Manual of Uniform Traffic Control Devices – Part 10: Pedestrian Facilities* stipulates that the following site conditions are to be taken into consideration at pedestrian crossings:

- No more than one lane of moving traffic in any one direction to be encountered by a pedestrian using a crossing.
- Adequate sight distance to be provided, primarily achieved by means of parking restrictions and possibly kerb extensions.
- Approach speed limit ≥ 50 km/h and 85th percentile speed ≥ 60 km/h.
- Crossings are not be used on arterial roads.

The proposed design meets the appropriate site distance criteria, as specified by the TfNSW's Technical Direction TDT 2002/12c *Stopping and Parking Restrictions at Intersections and Crossings*.

4. Financial Information for Council's Consideration

Council has received a commitment of \$1 million for construction from the RMS NSW Safe Speeds in High Pedestrian Activity and Local Area program. The \$1 million is required to be spent or committed by 30 June 2021.

5. Attachments

1. Tamarama Marine Dr design plan - Final Design [↓](#)



REPORT
TC/V.01/21.03

Subject: Old South Head Road and Diamond Bay Road, Vaucluse -
Installation of Double Centre (BB) Line Marking

TRIM No: A14/0145

Author: Malik Almuhanha, Senior Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council installs double centre (BB) line marking on Diamond Bay Road at the intersection with Old South Head Road, Vaucluse, as shown in Attachment 1 of the report.

1. Executive Summary

At its September 2020 meeting Council approved an upgrade to intersection of Old South Head Road and Diamond Bay Road, Vaucluse. The approved design included the installation of a concrete kerb side traffic island and changes to line-marking at the intersection.

Figure 1 shows the location. Figure 2 shows the existing intersection. Figure 3 shows the approved design.

The addition of a double barrier (BB) line-marking is recommended to enhance safety at the intersection and better guide motorists entering Diamond Bay Road from Old South Head Road. Right turning motorists inbound to Diamond Bay Road currently tend to encroach into what should be the outbound lane.

Figure 4 shows the proposed design. No parking spaces will be lost as a result of this addition.

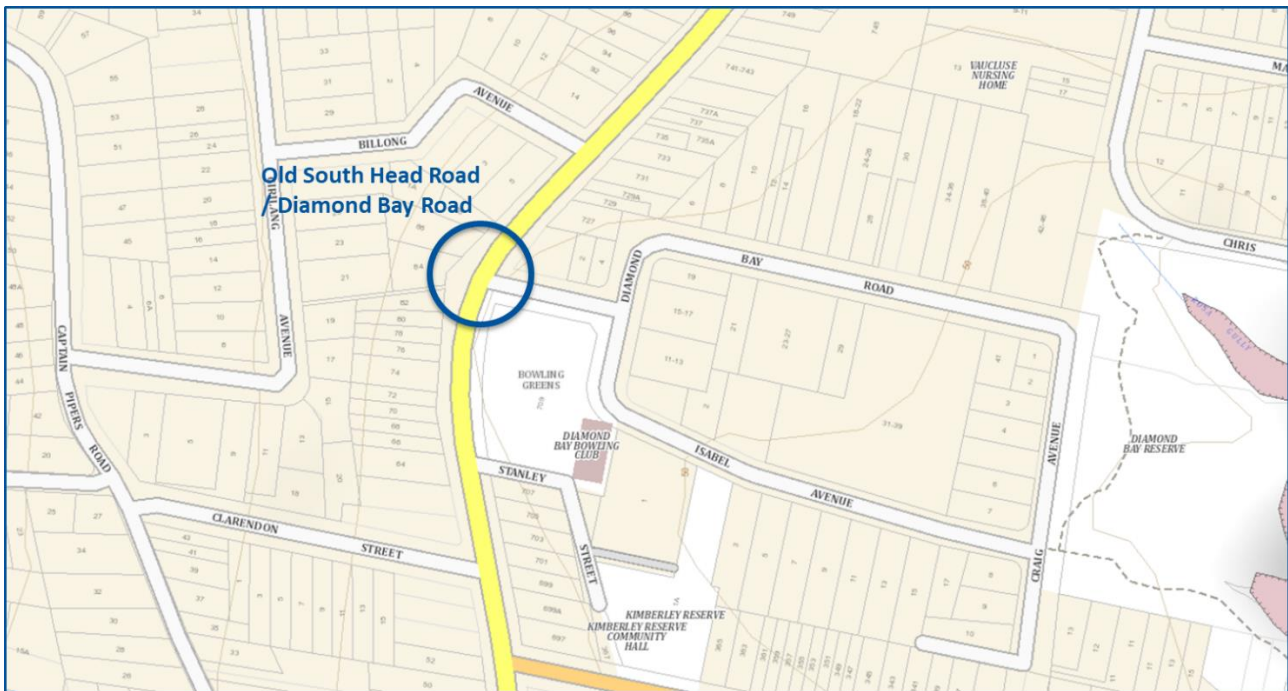


Figure 1: Site location

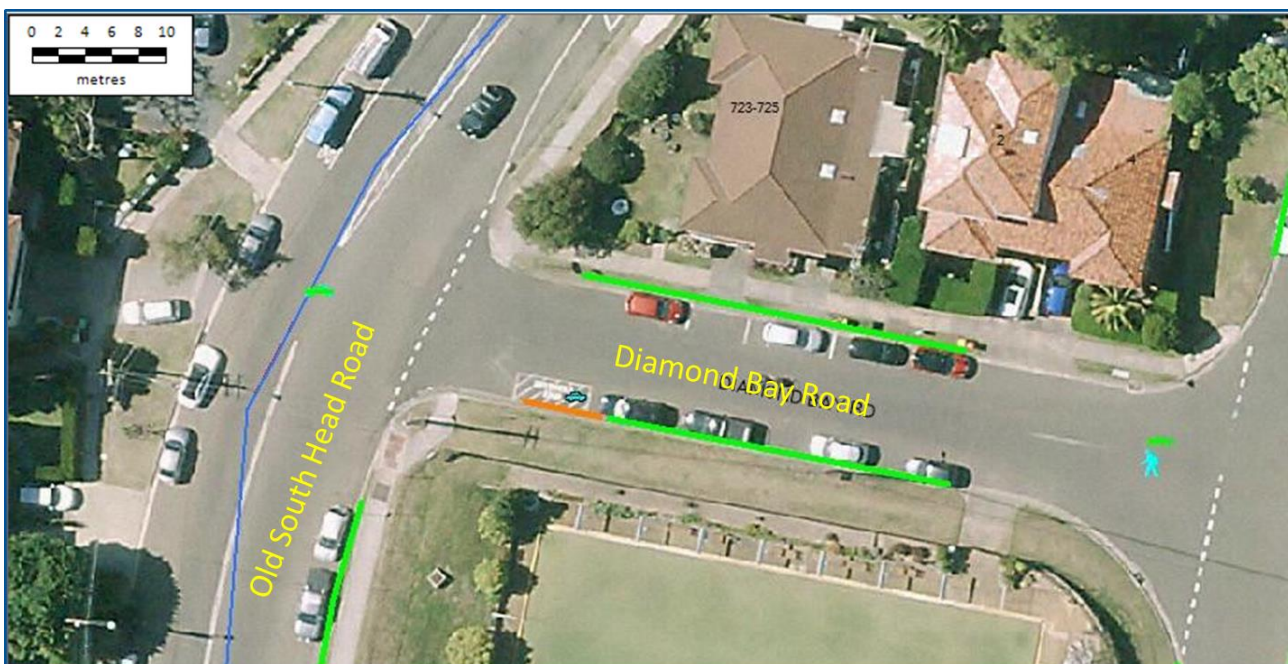
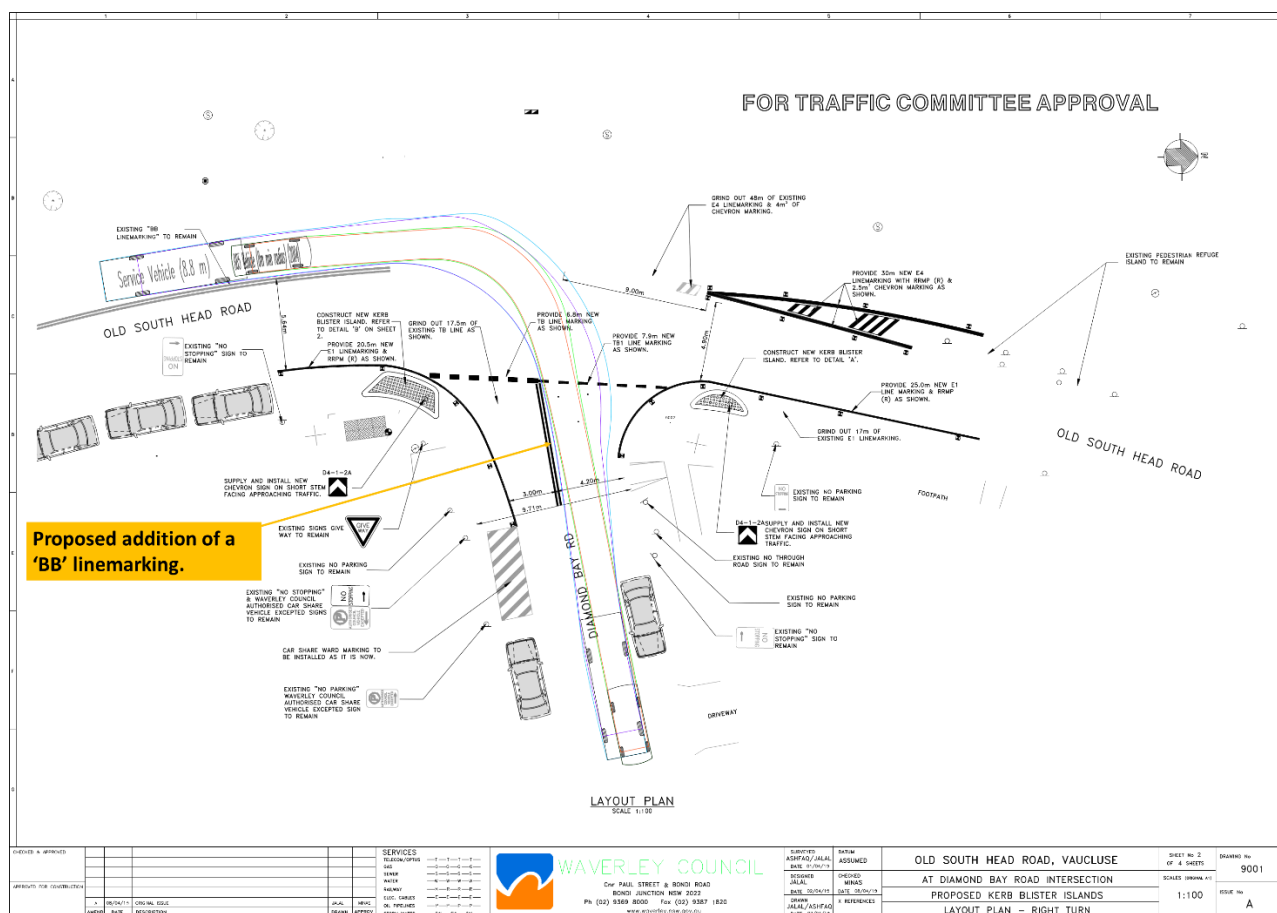
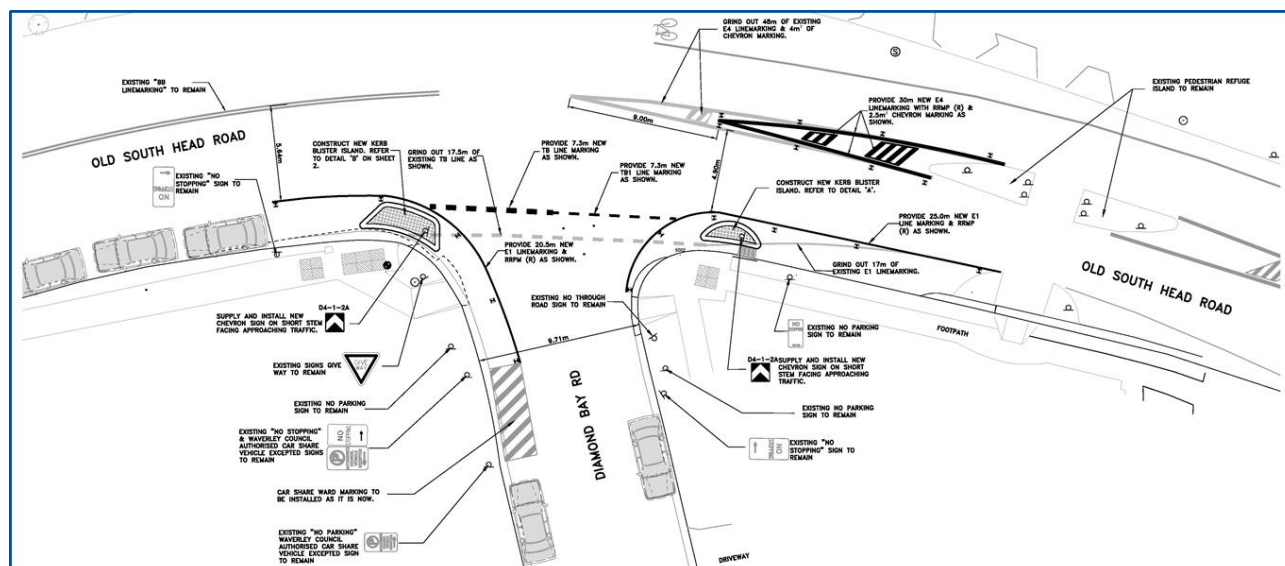


Figure 2: Existing arrangements at Old South Head Road and Diamond Bay Road



2. Introduction/Background

Diamond Bay Road has unrestricted parking on both sides of the road, apart from a single car share space is located on the southern side of Diamond Bay Road.

Council has received feedback requesting a review of the existing traffic arrangement at the intersection of Diamond Bay Road and Old South Head Road, Vaucluse, where it is reported that motorists turning right from Old South Head Road cross over into the outbound lane on Diamond Bay Road.

Installation of a double barrier (BB) line-marking will address this issue and improve turning paths for vehicles entering/exiting Diamond Bay Road, ensuring vehicles are better guided to turn to the right side of the road.

3. Technical Analysis

Council at its September 2020 meeting approved the design outlined in Figure 3, which included the following:

- Changes to line-marking in Old South Head Road at the exiting pedestrian refuge island.
- Changes to line-marking in Diamond Bay Road on the eastern and western side.
- Installation of two kerb blister island at the eastern and western end of intersection.
- Installation of directional guidance chevrons on both kerb blister islands.

The approved plans from Council's September 2020 meeting are expected commence construction in the first half of 2021.

An addition of a double barrier (BB) linemarking (see Figure 4) is recommended to enhance safety at the intersection and better guide motorists to turn to the right side of the road. No parking spaces will be lost as a result of this addition.

4. Financial Information for Council's Consideration

Council will fund the installation of the intersection treatment.

5. Attachments

1. Diamond Bay Road at Old South Head Road - Design [↓](#)





REPORT
TC/V.02/21.03

Subject: 32 Tower Street, Vaucluse - Removal of 'P Disability Only' Zone

TRIM No.: A20/0534

Author: Emraul Kayes, Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council removes the existing 5.5 metre long 'P Disability Only' zone outside 32 Tower Street, Vaucluse.

1. Executive Summary

The son of the owner of 32 Tower St, Vaucluse has requested the removal of the existing 'P Disability Only' zone fronting the property. The owner of 32 Tower St, Vaucluse no longer drives a car and no longer uses the allocated space.

Council will need to exercise its delegated functions to implement the proposals.

2. Introduction/Background

The Traffic Committee meeting held on 23 February 2017 recommended installation of a 'P Disability Only' parking space fronting 32 Tower Street. The Waverley Traffic Committee recommendation was adopted by the Council at 21 March 2017 meeting. The installation of signage was completed on 23 March 2017.

3. Technical Analysis

Council has been advised by the son of the applicant that his father no longer drives a car. The allocated space currently is not used by the owner.

The existing 'P Disability Only' parking zone outside of 32 Tower St is no longer required.

Therefore, it proposed that the existing 5.5 metres long 'P Disability Only' zone in 32 Tower St be removed.



Figure 1: Existing location of 'P Disability Only' zone in Tower Street.

4. Financial Information for Council's Consideration

If changes to signs and/or line marking are approved, Council will supply and install the signs and/or line-marking and remove/adjust existing signs with funds from existing budgets.

5. Attachments:

Nil

REPORT

TC/V.03/21.03



Subject: 4 MacDonald Street, Vaucluse - Construction Zone

TRIM No: A03/2514-04

Author: Emraul Kayes, Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 10.9 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 4 MacDonald Street, Vaucluse.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

1. Executive Summary

Council has received an application from the builder/developer at 4 MacDonald Street, Vaucluse, for the installation of a construction zone along the frontage of the property (see Figure 1).

Council officers propose the installation of a 10.9 metre construction zone as shown in Figure 2.

Council will need to exercise its delegated functions to implement the proposal.



Figure 1. Site location.



Figure 2. Location to install construction zone signs.

2. Introduction/Background

In accordance with standard practice at Council, it is proposed that the construction zone is signposted 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' for the approved construction hours under the development consent. Council will then supply the applicant with transferable permits to be used on the applicant's construction vehicles. The Traffic Committee and Council's requirements for permit approvals are a minimum length of 9 metres along the site frontage with a minimum period of 13 weeks.

3. Technical Analysis

The subject site has a frontage of 17 metres on MacDonald Street including a 4.2-metre-wide driveway accessing No. 4.

The applicant has requested a 10.9 metre construction zone along the frontage on MacDonald Street.

Council officers propose to install a 10.9 metre construction zone. The 10.9 metre construction zone will include the existing driveway accessing the property. The existing and recommended parking allocation is shown in Figures 3 and 4 below.

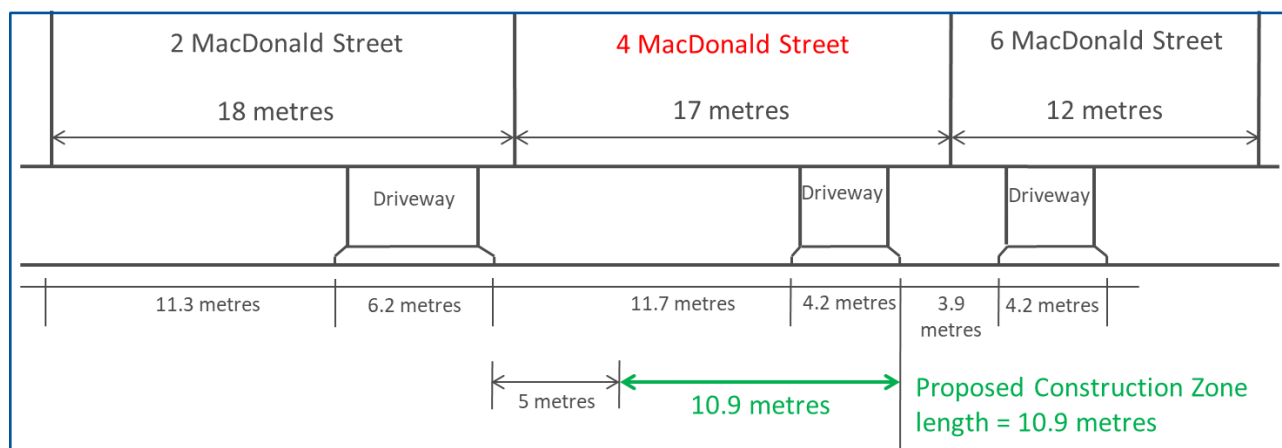


Figure 3. On-street parking for site and adjacent properties.

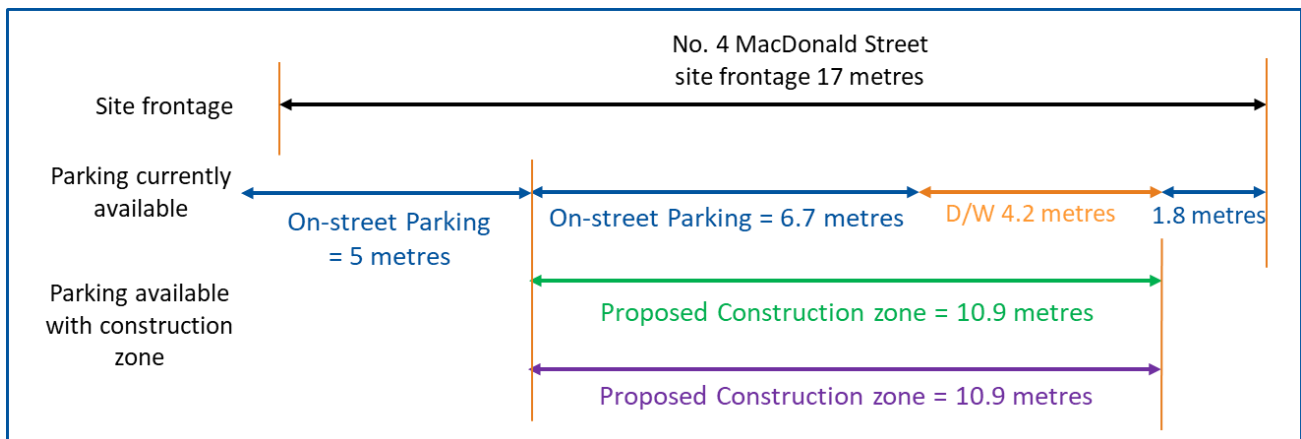


Figure 4. On-street parking for site frontage only.

Table 1. Application details

Applicant	Rafa Haidary
Development application	DA-300/2019/A
Works	Demolition of dwelling and construction of a two-storey dual occupancy with integrated garage, swimming pool to the rear and strata subdivision
Approved hours of construction	7 am–5 pm Monday–Friday; 8 am–3 pm Saturday
Frontage length	17 metres
Road	MacDonald Street
Existing parking	No parking restrictions (unrestricted)
Length requested by applicant	10.9 metres
Length to be signposted	10.9 metres
Effective construction zone - Total length available for construction	10.9 metres
Duration	12 weeks
Fee area	Residential area with no parking restrictions
Estimated fees	\$752.10 per week (10.9 metres x \$69.00 per metre)

Signage

The proposed signage is shown below.

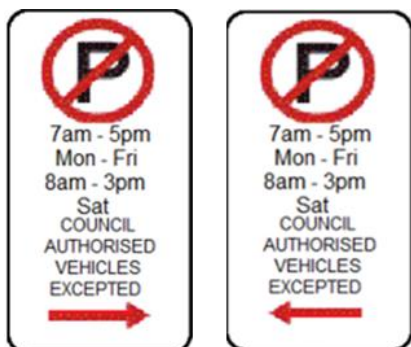


Figure 4. Proposed signage.

4. Financial Information for Council's Consideration

The applicant will be required to pay the approved application fee, which covers the cost of administration and signage.

The approved usage charges for the 2020–2021 financial year from 1 January 2021 are:

- \$69.00 per metre per week in a residential area.
- \$95.00 per metre per week in a commercial or mixed-use area.
- \$377.00 per metered (ticket) car parking space per week in addition to above fees (where applicable).

The cost to the applicant for the 10.9 metres made available for construction vehicles will be \$752.10 per week (10.9 metres x \$69.00 per metre).

5. Attachments

Nil

REPORT

TC/V.04/21.03



Subject: Old South Head Road, Bondi Junction- Bondi Road to Bon Accord Avenue - Shared Path Upgrade

TRIM No: SF19/3435

Author: Hamoon Bahari, Professional Engineer, Traffic and Transport
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs shared pathing including continuous footpath treatments at the intersections of Old South Head Road with Paul Street and Old South Head Road with Bon Accord Avenue as shown in Attachment 1.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

1. Executive Summary

The Old South Head Road shared path between Bondi Road and Bon Accord Avenue (see Figure 1) is to be renewed as it is in poor condition and is deteriorated. Upgrades include pavement markings and continuous footpath treatments at Paul Street and Bon Accord Avenue.



Figure 1. Section of shared path from Bondi Road to Bon Accord Avenue.

2. Introduction/Background

A concept design for a proposed new layout and upgrade to the intersections of Paul Street and Bon Accord Avenue was developed and placed on community consultation in February 2017.

The concept included an extension of the shared path from Bon Accord Avenue to Victoria Street. The extension was assessed at the February 2018 Traffic Committee meeting and completed that year.

At the September 2019 meeting Council approved an upgrade to pavement markings and plantings along the shared path between Bondi Road and Bon Accord Avenue. The Council approval also noted the intention to:

- a) Remove the concrete islands at the Paul Street intersection to make way for the new continuous footpath treatment subject to RMS and future Traffic Committee approval.
- b) Install a continuous footpath or alternative traffic calming at the intersection of Bon Accord Avenue and Old South Head Road subject to RMS and future Traffic Committee approval.
- c) Change the red light and speed sign from two poles to one poles subject to RMS approval.

The proposed plans address these issues.

3. Technical Analysis

The attached design for the shared path will add on to previous approved works and has the inclusions shown below.

Table 1. components of shared path upgrades.

Location of Work	Comments
Path surface upgrade	Path will be rebuilt from the red light cameras at Bondi Road to Bon Accord Avenue.
(Red Speed Light Camera Ahead & 50kph speed limit) double sign	Remount double sign to single pole at similar distance to kerb as all other poles in this section subject to RMS approval.
Garden Beds and Tree Plantings	In conjunction with existing kerb side poles subject to foliage and branches not protruding into the operating space of the path uses.
Paul Street and Old South Head Road Intersection	Continuous footpath is out of scope and requires further detailed design and approvals from RMS. Detailed design will include a finish more in line with other cycleway projects.
Paul Street and Old South Head Road Intersection	Non line-marked motor bike parking on the eastern kerb-line of Paul Street at the new traffic island
Bon Accord Avenue and Old South Head Road Intersection	Raised continuous footpath treatment at the same level of existing footpath along Old South Head Road for left in and left out movement only

Figure 2 shows the design principles applied at Paul Street and Bon Accord Avenue. They are taken from TfNSW Guidelines TDT/2013/05 Continuous footpath treatments.

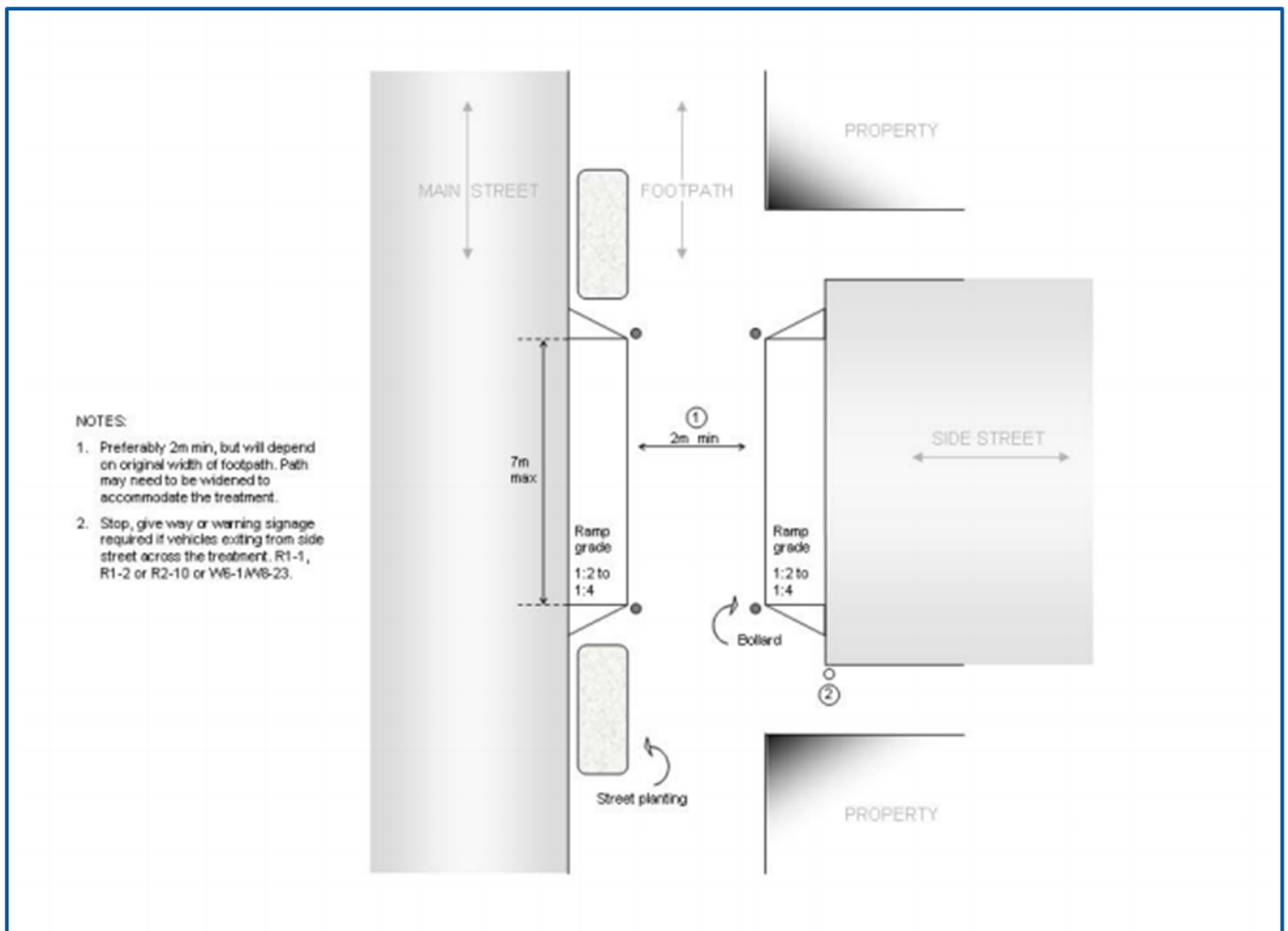


Figure 2. Example of a continuous footpath treatment layout.

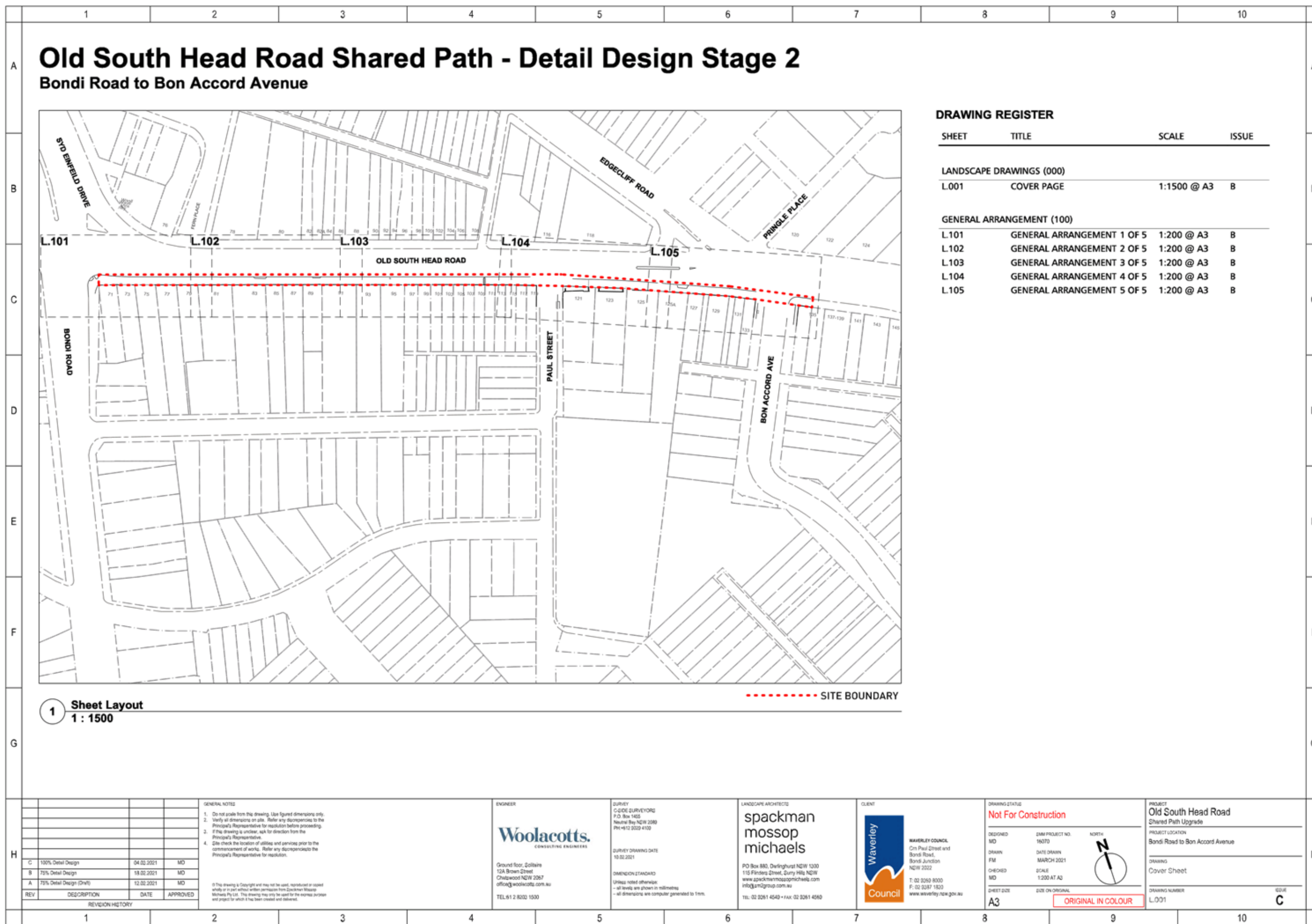
Attachment 1 contains the proposed upgrades.

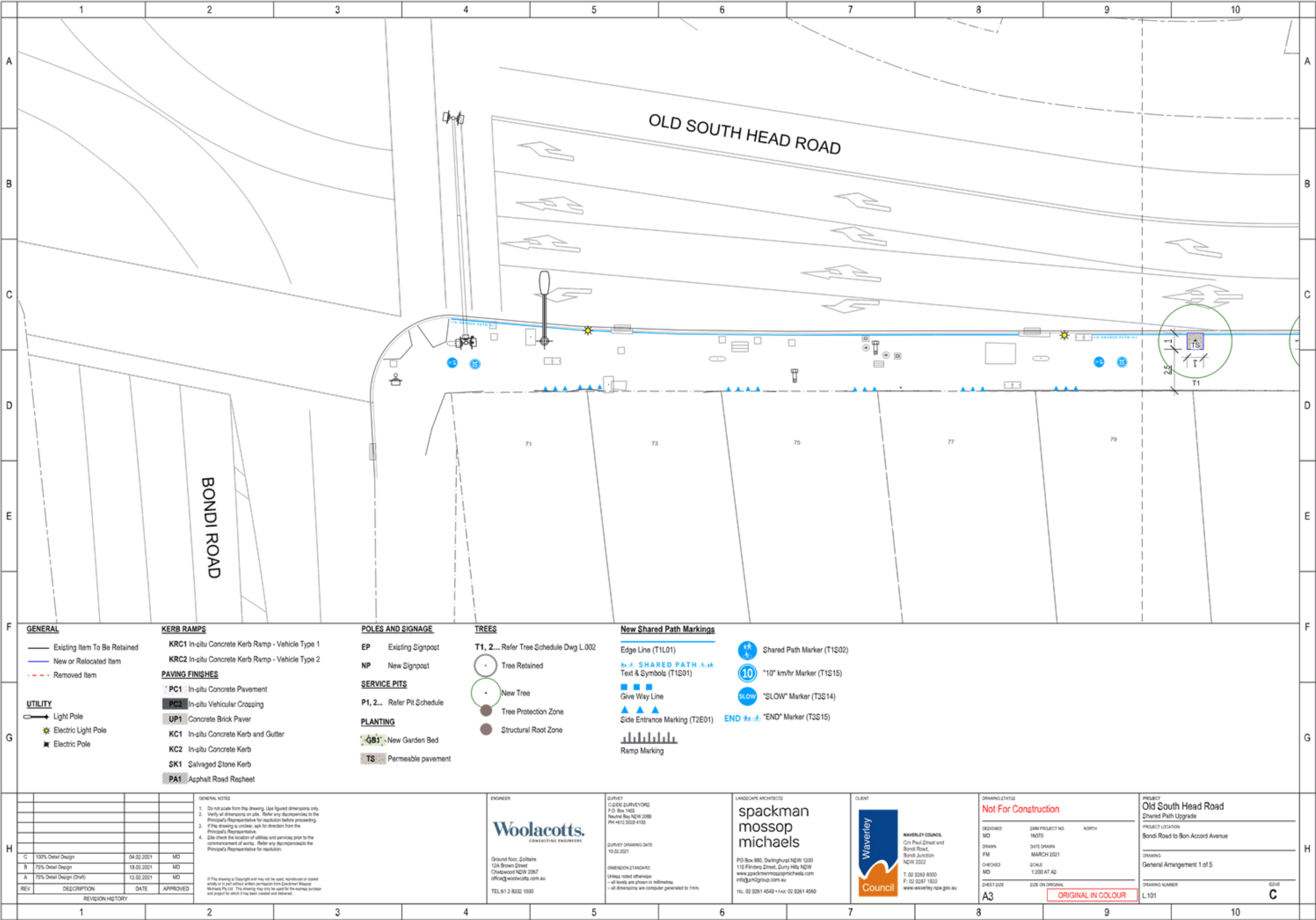
4. Financial Information for Council's Consideration

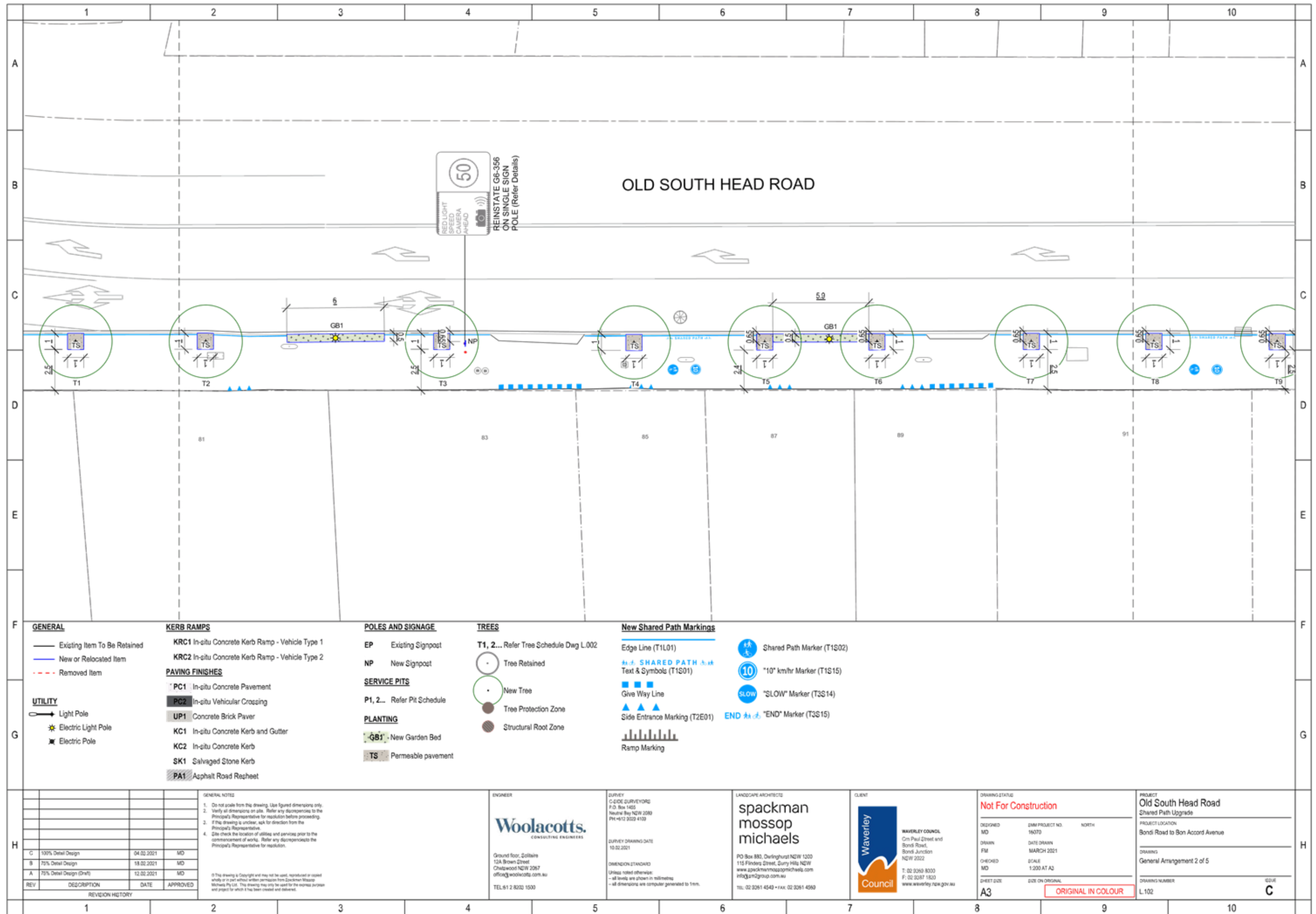
The shared path upgrade works will be funded under existing Council budget C0942.

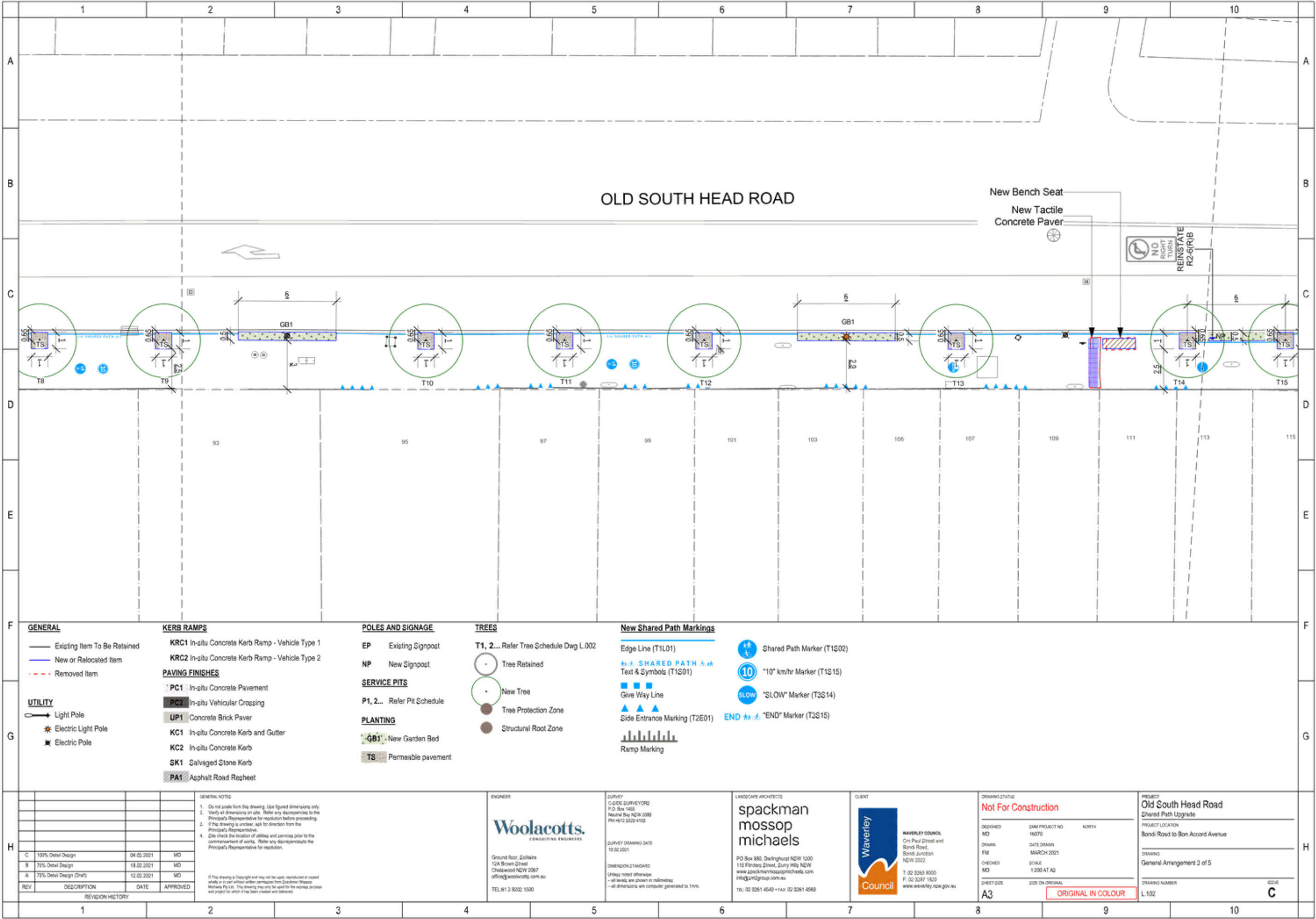
5. Attachments

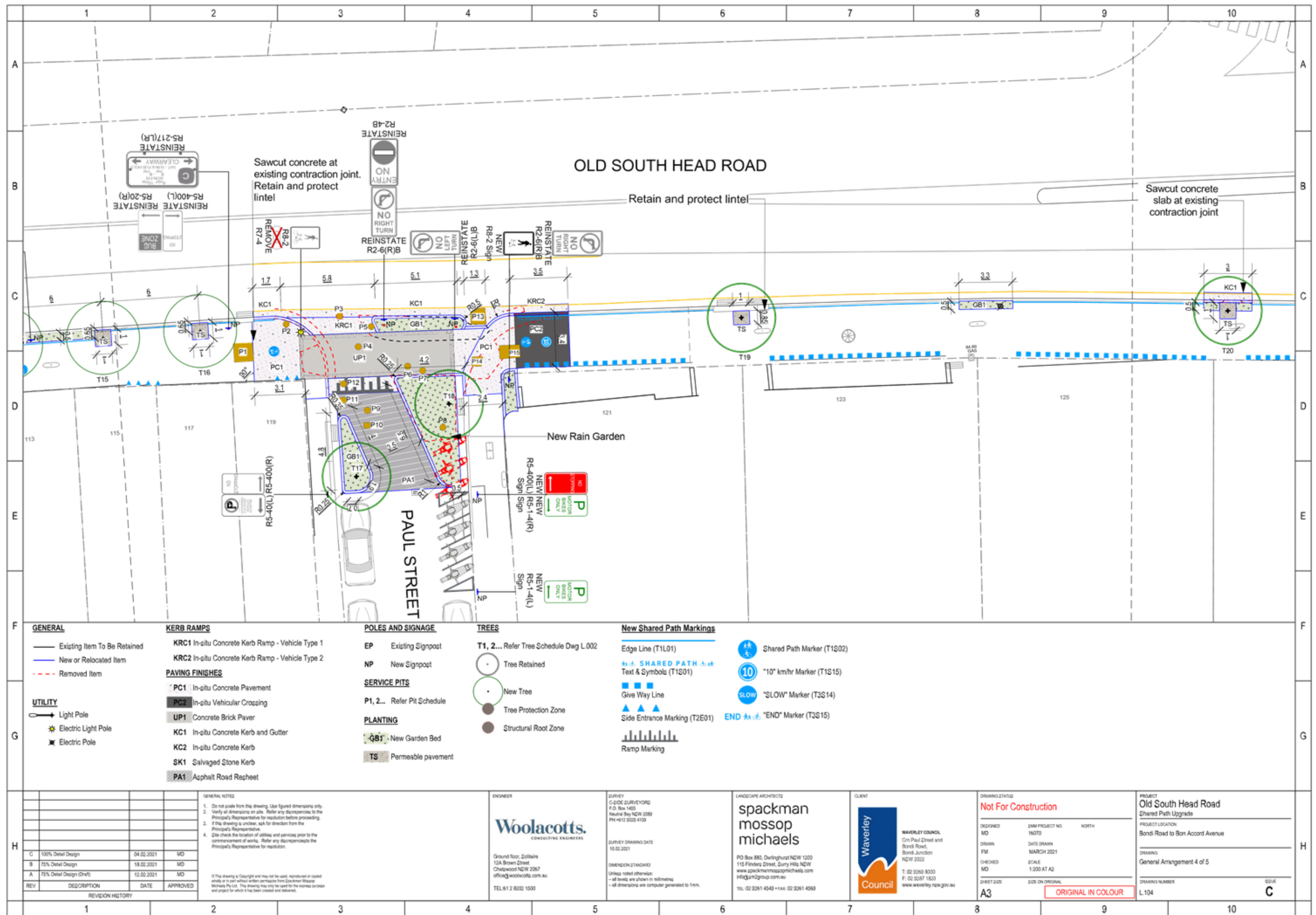
1. Attachment 1 - Design Plans - Old South Head Road shared path upgrade from Bondi Road to Bon Accord Avenue [↓](#)

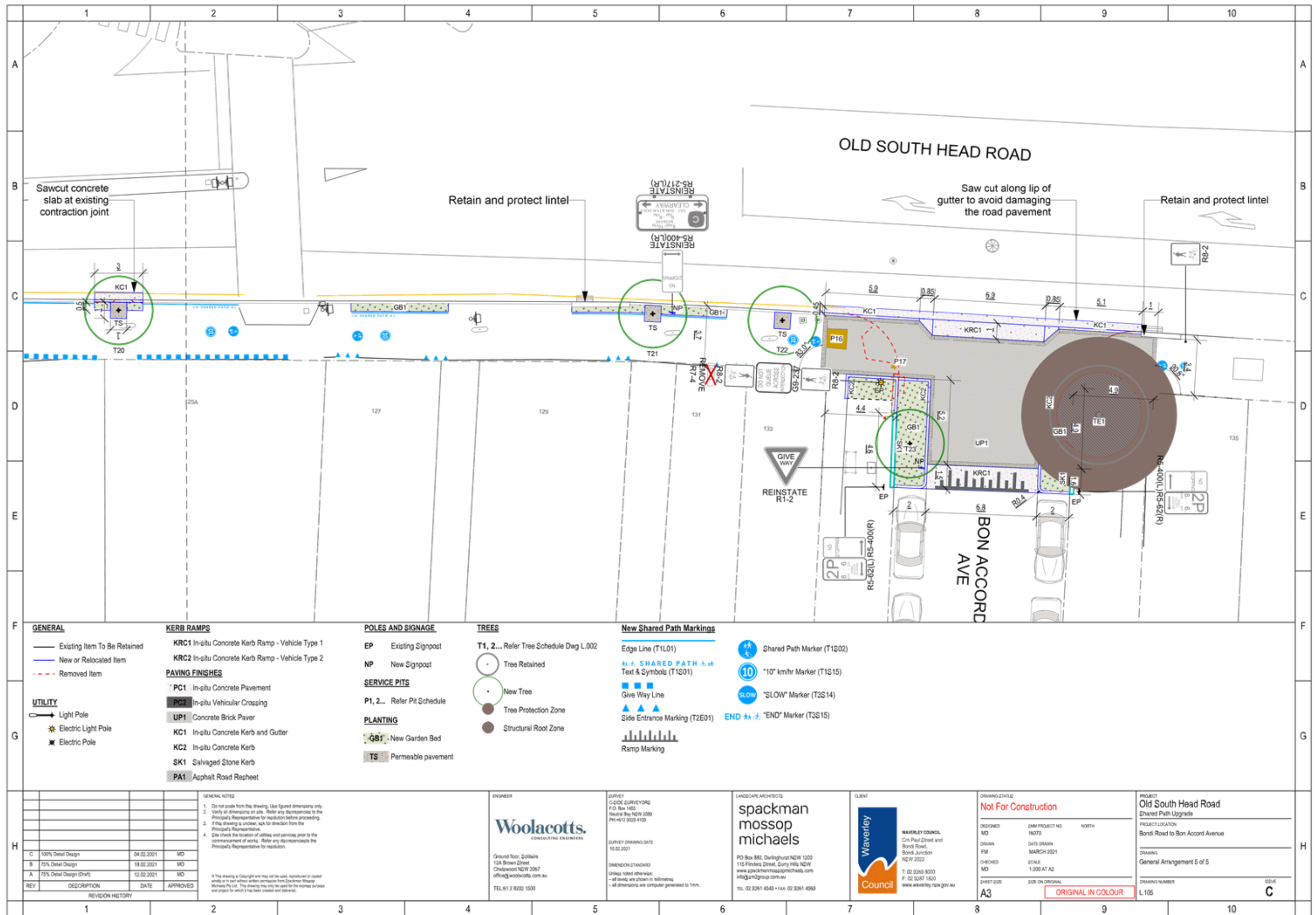












REPORT
TC/V.05/21.03

Subject: Barracluff Avenue, Bondi Beach - Traffic Calming Scheme

TRIM No: A03/0042-04

Author: Malik Almuhanha, Senior Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the traffic calming scheme for Barracluff Avenue as shown in Attachment 1.
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

1. Executive Summary

At its meeting on 8 December 2020, Council resolved to undertake community consultation with the residents of Barracluff Avenue on a traffic calming scheme for Barracluff Avenue.

The proposed traffic calming scheme consists of pedestrian refuge islands at the intersections of Barracluff Avenue and Francis Street, and Barracluff Avenue and Lamrock Avenue. Angle Parking on the southern side of Barracluff Avenue with planted kerb extensions are also part of the scheme (see Figure 1).

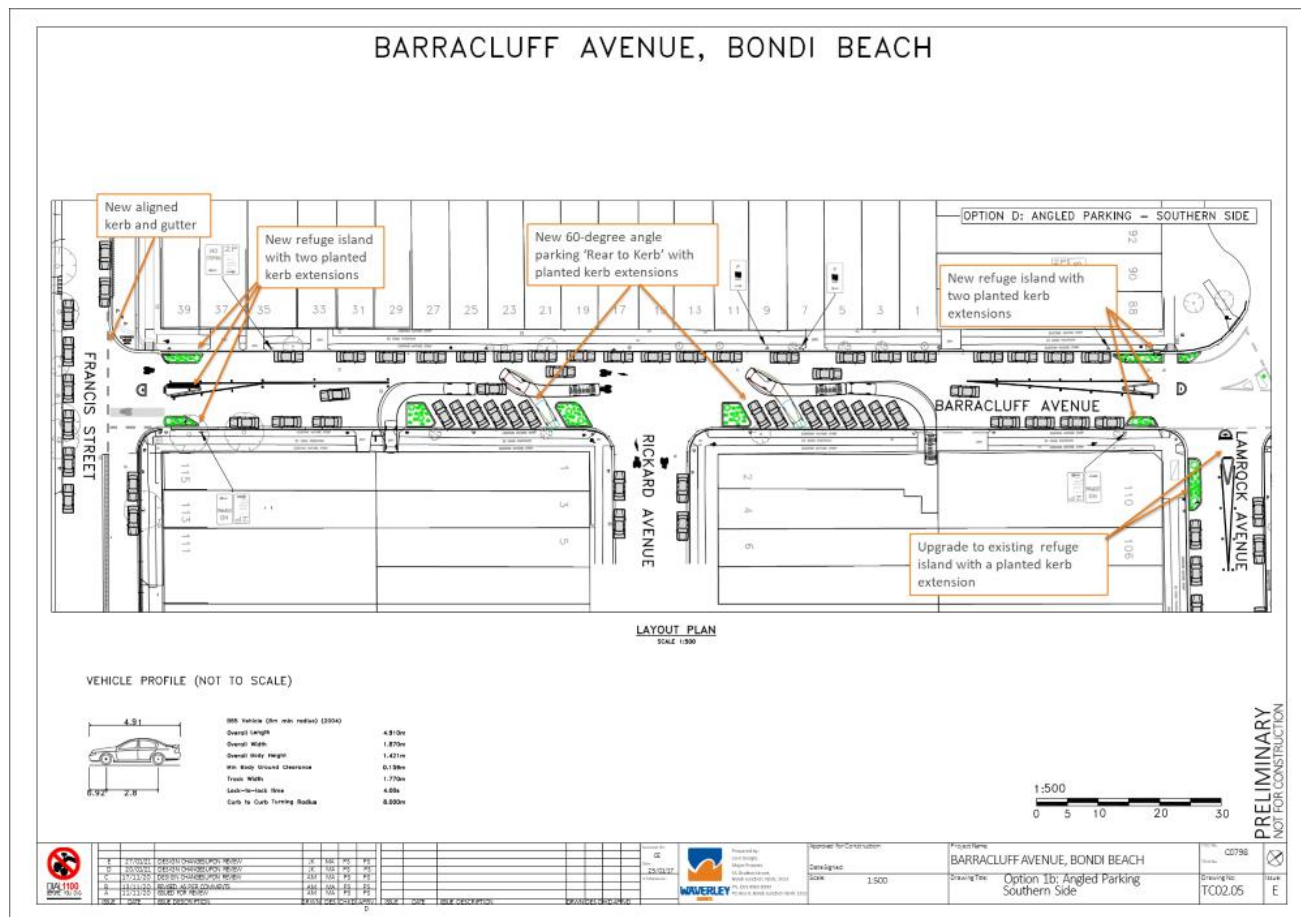
Community consultation was undertaken with options for traffic calming at Barracluff Avenue. The survey presented two options:

- Option 1 – Pedestrian refuge islands with angle parking and planted kerb extensions.
- Option 2 – Retain existing conditions.

30 residences in or adjacent to Barracluff Avenue were notified by letter box drop (see Figure 2). There was overwhelming support with 25 responses of which 23 were in favour of the proposed traffic calming scheme.

20 residents from surrounding streets outside the consultation area have also submitted responses despite the scheme having little to no impact outside Barracluff Avenue. 8 of these submissions were in favour of the proposal.

It is recommended that Council adopts the traffic calming scheme shown in Figure 1 and Attachment 1.



Council determined the need for consultation with the residents of Barracluff Avenue on a traffic calming scheme for Barracluff Avenue.

The feedback from residents indicated a majority support for the proposed traffic calming scheme..

Relevant Council resolutions are set out in the table below.

Table 1. Relevant Council resolutions.

Meeting and date	Item No.	Decision/Recommendation
Council 8 Dec 2020	CM/5.3.1/20.12	<p>That Council adopts option D with investigation of the following design modifications for community consultation:</p> <ol style="list-style-type: none"> 1. Replace current blisters and incorporate footpath extensions on all corners. 2. Move the proposed intersection treatment on the corner of Barracluff Avenue and Francis Street towards the south in order to create a safer pedestrian outcome. 3. Consider the reintroduction of parking for cars or dedicated motorcycle parking on the western side of Barracluff Avenue at the intersection of Francis Street.
Waverley Traffic Committee 26 Nov 2020 (recommendation)	TC/V.01/20.11	<p>That Council undertakes community consultation with the residents of Barracluff Avenue on the following traffic and parking options for Barracluff Avenue:</p> <ol style="list-style-type: none"> 1. Option A – No change. Leave Barracluff Avenue as it is, as shown in Attachment 1 of the report. 2. Option B – Parallel parking on both sides of Barracluff Avenue with kerb blisters, as shown in Attachment 2 of the report. 3. Option C – Angle parking on the northern side of Barracluff Avenue, as shown in Attachment 3 of the report. 4. Option D – Angle parking on the southern side of Barracluff Avenue, as shown in Attachment 4 of the report.
Operations and Community Services Committee 3 November 2020	OC/5.1/20.11	<p>That Council:</p> <ol style="list-style-type: none"> 1. Receives and notes the petition against the closure of Barracluff Avenue, Bondi Beach. 2. Notes that over 500 submissions have been received from residents of Barracluff Avenue and surrounding streets by Council on the community consultation for traffic management in Barracluff Avenue

		<ol style="list-style-type: none"> 3. Further notes that over 95% of the submissions do not support the option of a road closure in Barracluff Avenue. 4. Does not support the closure of Barracluff Avenue. 5. Officers inform the key petitioner, residents of Barracluff Avenue and Rickard Avenue, and the Bondi Precinct Committee of this resolution.
Council 16 June 2020	CM/5.2.2/20.06(2)	<p>That Council further develops design options for Barracluff Avenue and undertakes community consultation with the residents of Barracluff Avenue and adjoining streets on the following two scenarios:</p> <ol style="list-style-type: none"> 1. Construction of pedestrian/traffic islands with associated line marking and signage at the intersections of Barracluff Avenue and Francis Street, and Barracluff Avenue and Lamrock Avenue, Bondi Beach, as shown in Drawing 9005 Issue A attached to this report. <p>Including the installation of kerb extensions in Barracluff Avenue on both sides of the road opposite:</p> <ol style="list-style-type: none"> (a) 23 Barracluff Avenue and 25 Barracluff Avenue. (b) 5 Barracluff Avenue and 7 Barracluff Avenue. <ol style="list-style-type: none"> 2. Construction of a road closure of Barracluff Avenue in the vicinity of 21 Barracluff Avenue.

3. Technical Analysis

The proposed traffic calming scheme consists of pedestrian refuge islands at the intersections of Barracluff Avenue and Francis Street, and Barracluff Avenue and Lamrock Avenue. Additional 60-degree angle parking along the southern side of Barracluff Avenue with planted kerb extensions are also part of the scheme.

There are currently 40 parking spaces along Barracluff Avenue and the proposed design is expected to provide 2 more parking spaces.

The section of the Bondi Junction–Bondi Beach bike route along Barracluff Avenue between Francis Street and Rickard Avenue will be retained.

Community feedback on the proposed design at Barracluff Avenue

Community consultation was undertaken in February 2020 with a traffic calming scheme option at Barracluff Avenue. The survey presented two options:

- Option 1 – Pedestrian refuge islands with angle parking and planted kerb extensions.
- Option 2 – Retain existing conditions.

30 residences were letter dropped, and 25 responses were received from residents within the specified consultation area (see Figure 2). 23 out of the 25 votes received were in favour of the proposed traffic calming scheme.

20 residents from surrounding streets outside the consultation area have also submitted responses despite the scheme having little to no impact outside Barracluff Avenue. 8 were in favour of the proposal. Table 2 and 3 summarise the responses.

Table 2. Summary of responses

Residents surveyed	Responses from specified consultation area	Responses from other streets outside the consultation area
Responses	25	20
Traffic Calming Scheme	23	8
Retain existing conditions	2	12

Table 3. All responses

	For	Against
Total	31	14

From Table 2 and 3 it can be seen that there is strong support from residents for the scheme.

It is recommended that Council adopts the traffic calming scheme shown in Attachment 1.

4. Financial Information for Council's Consideration

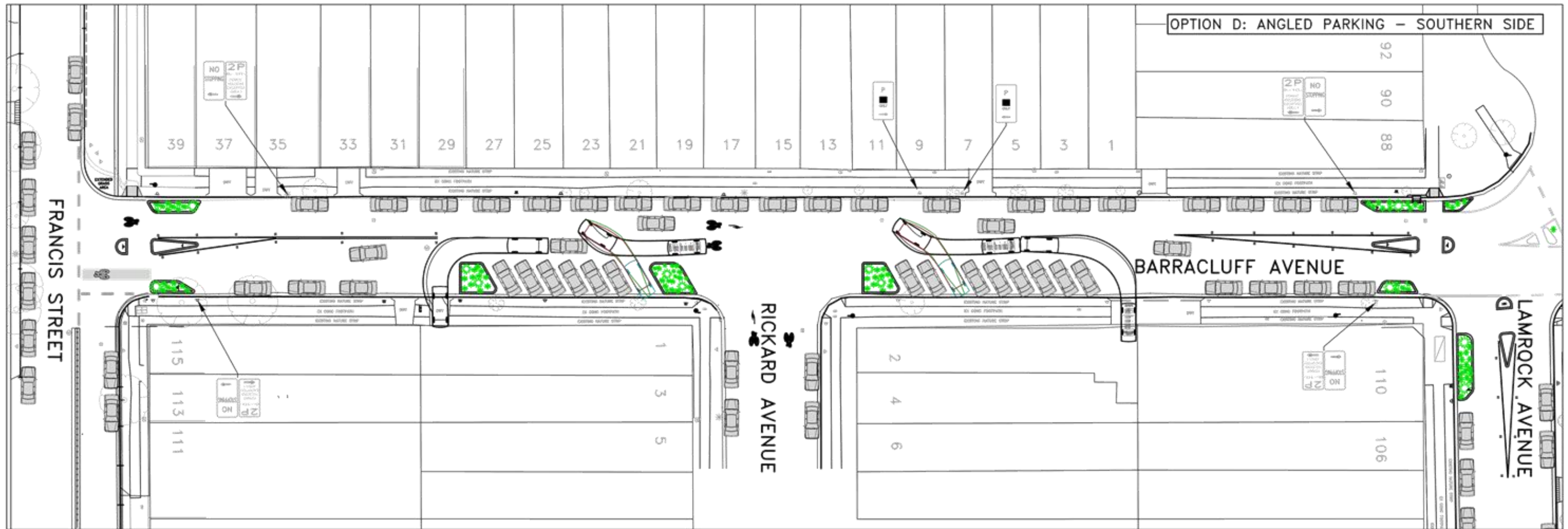
Council currently has not allocated funds to the cost of installing a traffic calming scheme as one has not been determined.

5. Attachments

1. Barracluff Avenue Design D

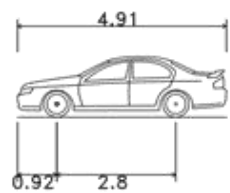
BARRACLUFF AVENUE, BONDI BEACH

OPTION D: ANGLED PARKING – SOUTHERN SIDE



LAYOUT PLAN
SCALE 1:500

VEHICLE PROFILE (NOT TO SCALE)



B85 Vehicle (8m min radius) (2004)

Overall Length	4.910m
Overall Width	1.870m
Overall Body Height	1.421m
Min Body Ground Clearance	0.159m
Track Width	1.770m
Lock-to-lock time	4.00s
Curb to Curb Turning Radius	8.000m



PRELIMINARY
NOT FOR CONSTRUCTION



ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD	ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD
E	27/01/21	DESIGN CHANGES UPON REVIEW	JK	MA	PS	PS							
D	20/01/21	DESIGN CHANGES UPON REVIEW	JK	MA	PS	PS							
C	17/11/20	DESIGN CHANGES UPON REVIEW	AM	MA	PS	PS							
B	13/11/20	REVISED AS PER COMMENTS	AM	MA	PS	PS							
A	11/11/20	ISSUED FOR REVIEW	AM	MA	PS	PS							

Surveyed By:
CE
Date:
25/01/17
X-References:

Prepared by:
Civil Design,
Major Projects
55 Grafton Street,
Bondi Junction, NSW, 2022
Ph: (02) 9083 8000
PO Box 9, Bondi Junction NSW 1555

Approved for Construction:
Date Signed:
Scale: 1:500

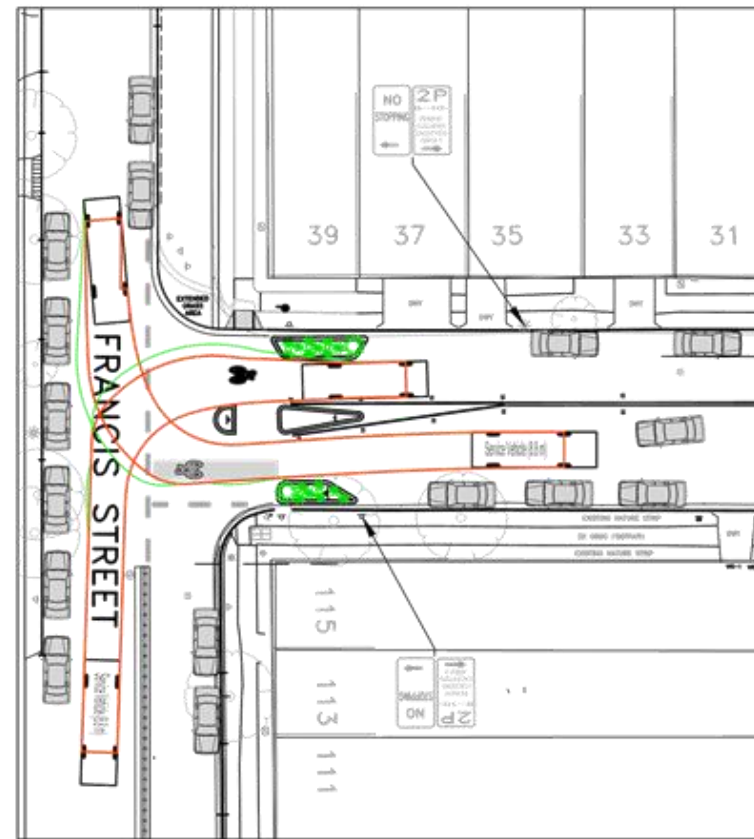
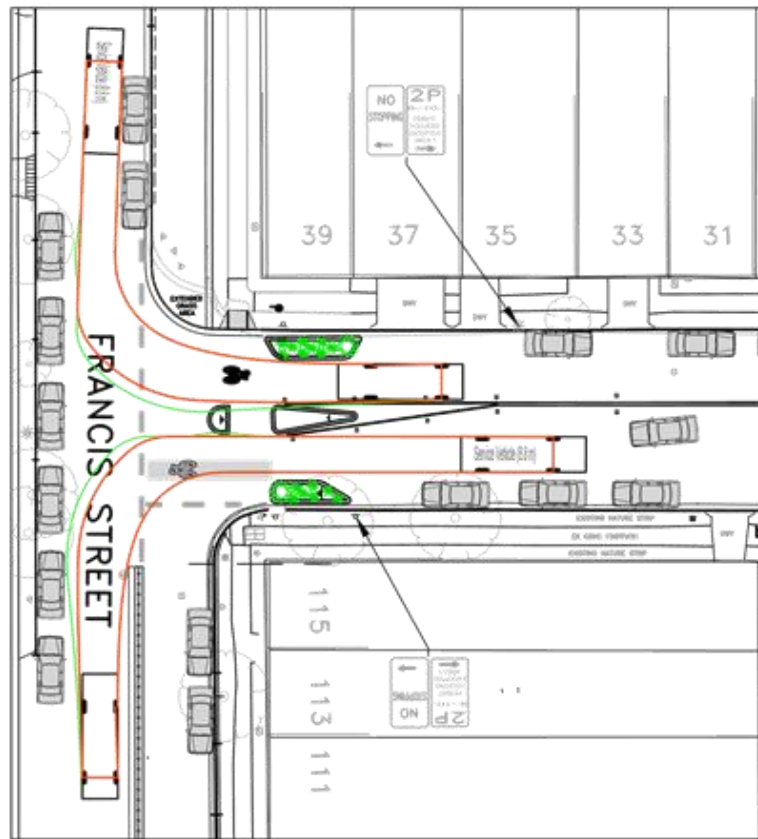
Project Name:
BARRACLUFF AVENUE, BONDI BEACH
Drawing Title:
Option 1b: Angled Parking
Southern Side

PROJ No:
C0798
Drawing No:
TC02.05

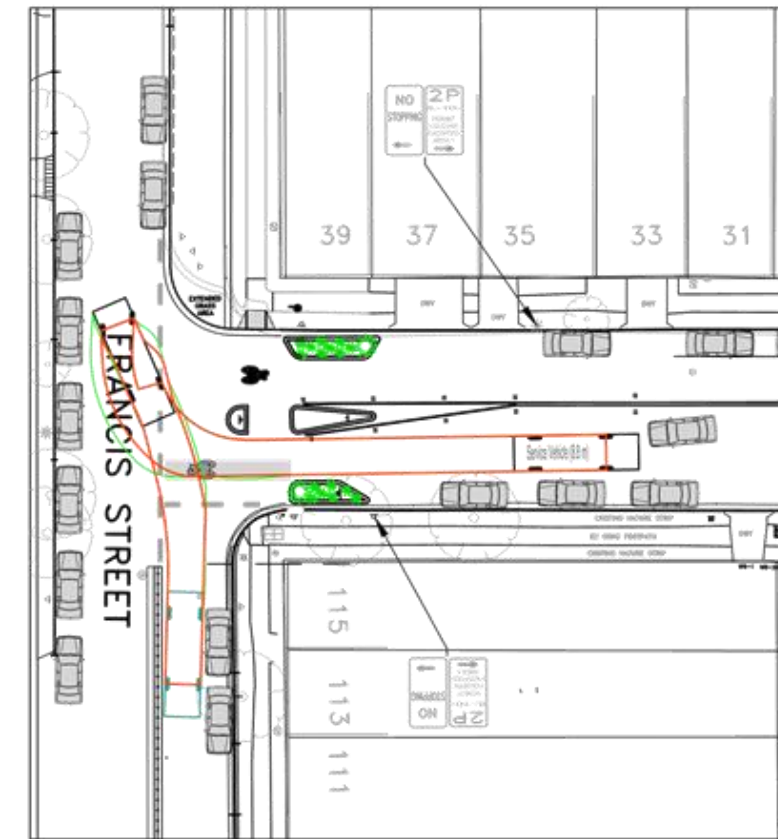
Issue:
E

BARRACLUFF AVENUE, BONDI BEACH

OPTION D: ANGLED PARKING – SOUTHERN SIDE

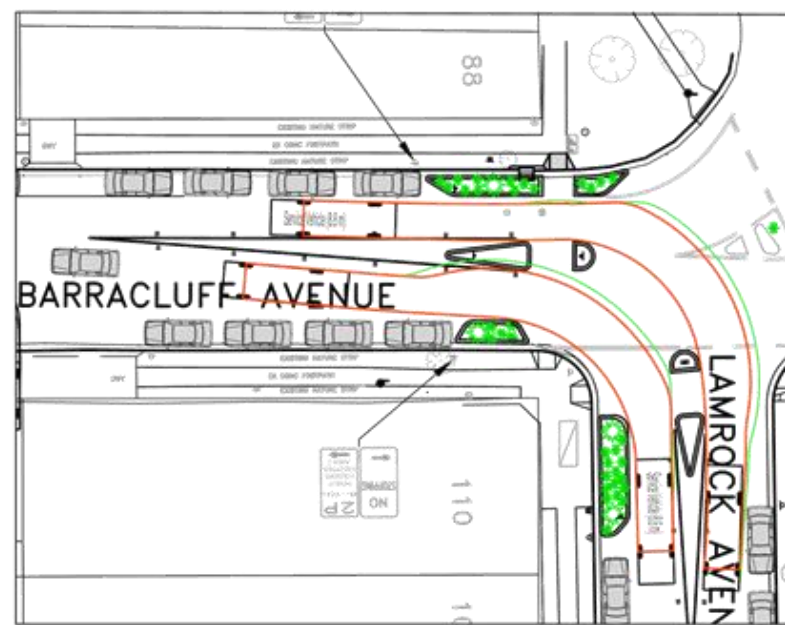


VEHICLE TURNING PATHS AT FRANCIS ST INTERSECTION
SCALE 1:500



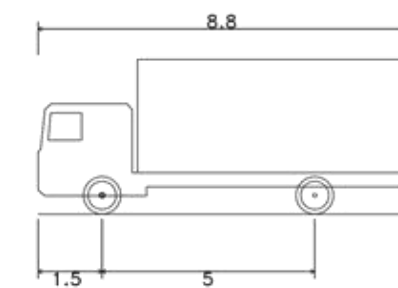
LEGEND

- WHEEL PATH
- VEHICLE OVERHANG PATH



VEHICLE TURNING PATHS AT LAMROCK AVE INTERSECTION
SCALE 1:500

VEHICLE PROFILE FOR TURNING TEST



MRV – Medium Rigid Vehicle	
Overall Length	8.800m
Overall Width	2.500m
Overall Body Height	3.633m
Min Body Ground Clearance	0.428m
Track Width	2.500m
Lock-to-lock time	4.00s
Curb to Curb Turning Radius	10.000m

1:500



PRELIMINARY
NOT FOR CONSTRUCTION



ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD	ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD
E	27/01/21	DESIGN CHANGES UPON REVIEW	JK	MA	PS	PS							
D	20/01/21	DESIGN CHANGES UPON REVIEW	JK	MA	PS	PS							
C	17/11/20	DESIGN CHANGES UPON REVIEW	AM	MA	PS	PS							
B	13/11/20	REVISED AS PER COMMENTS	AM	MA	PS	PS							
A	11/11/20	ISSUED FOR REVIEW	AM	MA	PS	PS							

Surveyed by:
CE
Date:
25/01/17



Prepared by:
Civil Design,
Major Projects
55 Grafton Street,
Bondi Junction, NSW, 2022
Ph: (02) 9083 8000
PO Box 9, Bondi Junction NSW 1555

Approved for Construction:

Date Signed:

Scale: 1:500

Project Name:

BARRACLUFF AVENUE, BONDI BEACH

Drawing Title:

Option 1b: Angled Parking
Southern Side - Vehicle Paths

Project No:

C0798

Drawing No:

TC03.00

Issue:
E

REPORT

TC/V.06/21.03



Subject: 2-4 Jaques Avenue, Bondi Beach - Construction Zone

TRIM No: A03/2514-04

Author: Emraul Kayes, Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 9 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 2-4 Jaques Avenue, Bondi Beach.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

1. Executive Summary

Council has received an application from the builder/developer at 2-4 Jaques Avenue, Bondi Beach, for the installation of a construction zone along the frontage of the property (see Figure 1).

Council officers propose the installation of a 9 metre construction zone as shown in Figure 2.

Council will need to exercise its delegated functions to implement the proposal.

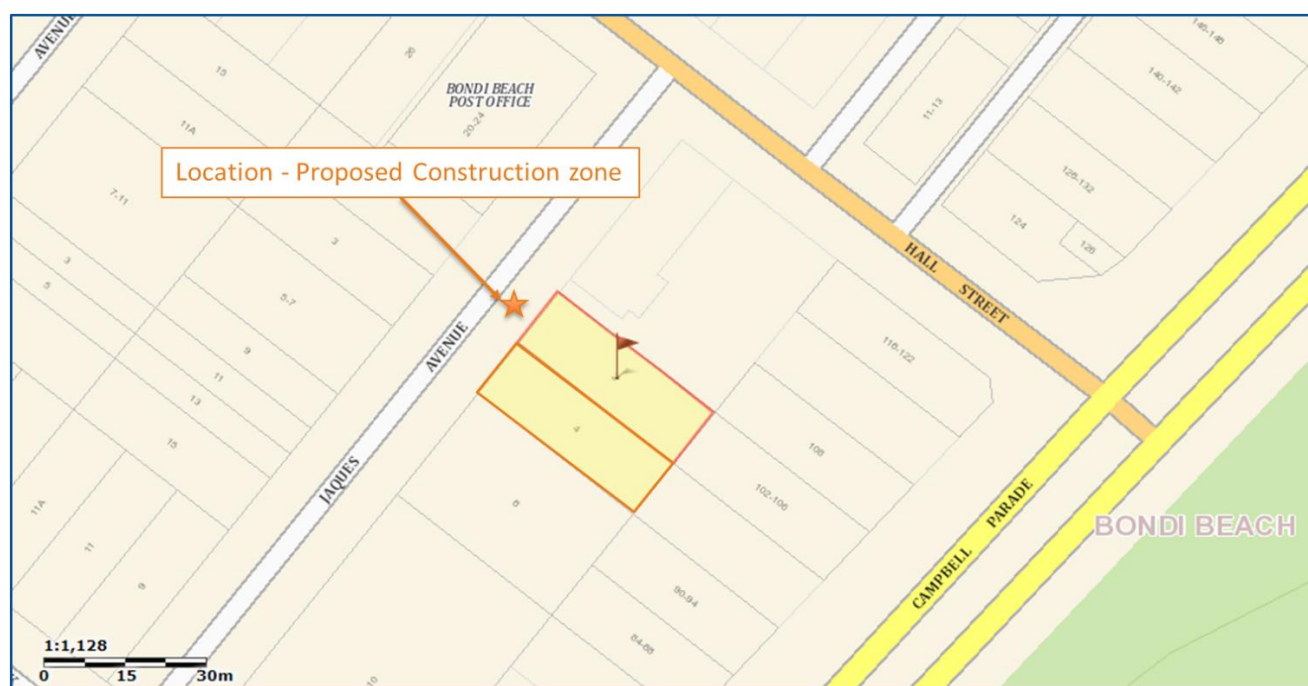


Figure 1. Site location.



Figure 2. Location to install construction zone signs.

2. Introduction/Background

In accordance with standard practice at Council, it is proposed that the construction zone is signposted 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' for the approved construction hours under the development consent. Council will then supply the applicant with transferable permits to be used on the applicant's construction vehicles. The Traffic Committee and Council's requirements for permit approvals are a minimum length of 9 metres along the site frontage with a minimum period of 13 weeks.

3. Technical Analysis

The subject site has a frontage of 24 metres on Jaques Avenue. The applicant has requested a 9 metre construction zone along the frontage on Jaques Avenue. This will displace 4 x angle parking spaces.

On-street parking demands on Jaques Avenue are high. Restricting the construction zone to 9 metres minimises the loss of parking during the construction of the building.

Council officers propose to install a 9 metre construction zone. The existing and recommended parking allocation is shown in Figures 3 and 4 below.

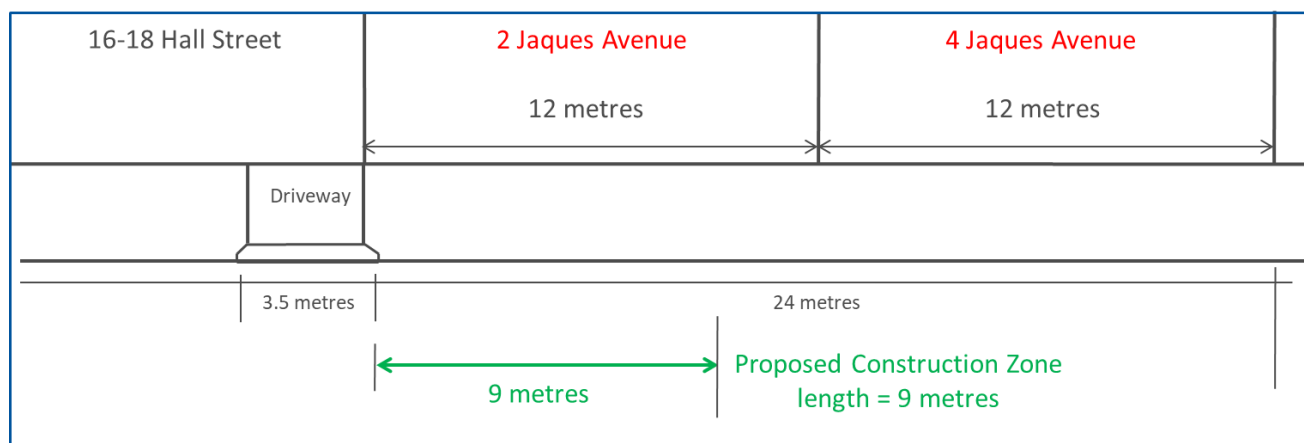


Figure 3. On-street parking for site and adjacent properties.

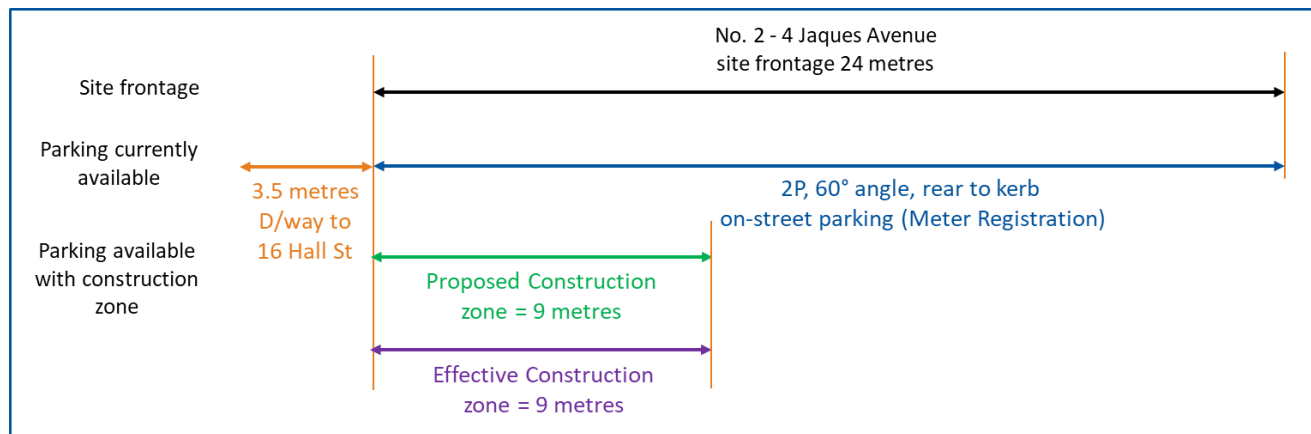


Figure 4. On-street parking for site frontage only.

Table 1. Application details

Applicant	TC Build, 25/151 Foveaux Street Surry Hills
Development application	DA-296/2015
Works	Demolition of existing building and construction of a five-storey shop-top housing development (retail at ground level and residential apartments above) with two levels of basement parking
Approved hours of construction	7 am–5 pm Monday–Friday; 8 am–3 pm Saturday
Frontage length	24 metres
Road	Jaques Avenue
Existing parking	2P 8am-10pm Permit Holders Excepted Area 8, 60° angle, rear to the kerb, on-street parking (Meter Registration)
Length requested by applicant	9 metres
Length to be signposted	9 metres
Effective construction zone - Total length available for construction	9 metres
Duration	1 April 2021 to 1 October 2022
Fee area	Areas zoned neighbourhood centre, commercial core, or mixed use
Estimated fees	\$3187.49 per week comprising: - 9 metres x \$186.61 per metre - 4 metered angle parking spaces x \$377.00

Signage

The proposed signage is shown below.

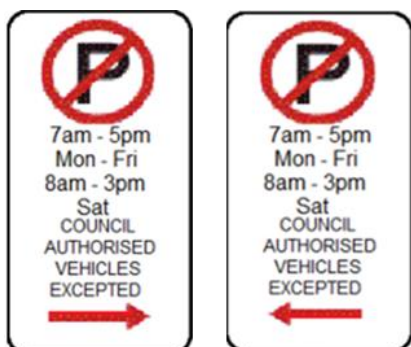


Figure 4. Proposed signage.

4. Financial Information for Council's Consideration

The applicant will be required to pay the approved application fee, which covers the cost of administration and signage.

The approved usage charges for the 2020–2021 financial year from 1 January 2021 are:

- \$186.61 per metre per week for angle parking spaces in a commercial or mixed-use area.
- \$377.00 per metered (ticket) car parking space per week in addition to above fees (where applicable).

The cost to the applicant for the 9 metres made available for construction vehicles will be \$3187.49 per week (9 metres x \$186.61 per metre+ \$377.00 x 4 spaces).

5. Attachments

Nil

REPORT
TC/V.07/21.03

Subject: 43-45 Hall Street, Bondi Beach - Construction Zone

TRIM No: A03/2514-04

Author: Emraul Kayes, Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs a 18.5 metre 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' construction zone along the frontage of 43-45 Hall Street, Bondi Beach.
2. Delegates authority to the Executive Manager, Infrastructure Services, to adjust the length and duration of, or remove, the construction zone, as necessary.

1. Executive Summary

Council has received an application from the builder/developer at 43-45 Hall Street, Bondi Beach, for the installation of a construction zone along the frontage of the property (see Figure 1).

Council officers propose the installation of a 18.5 metre construction zone as shown in Figure 2.

The construction zone will result in the displacement of 2 on-street car parking spaces.

Council will need to exercise its delegated functions to implement the proposal.

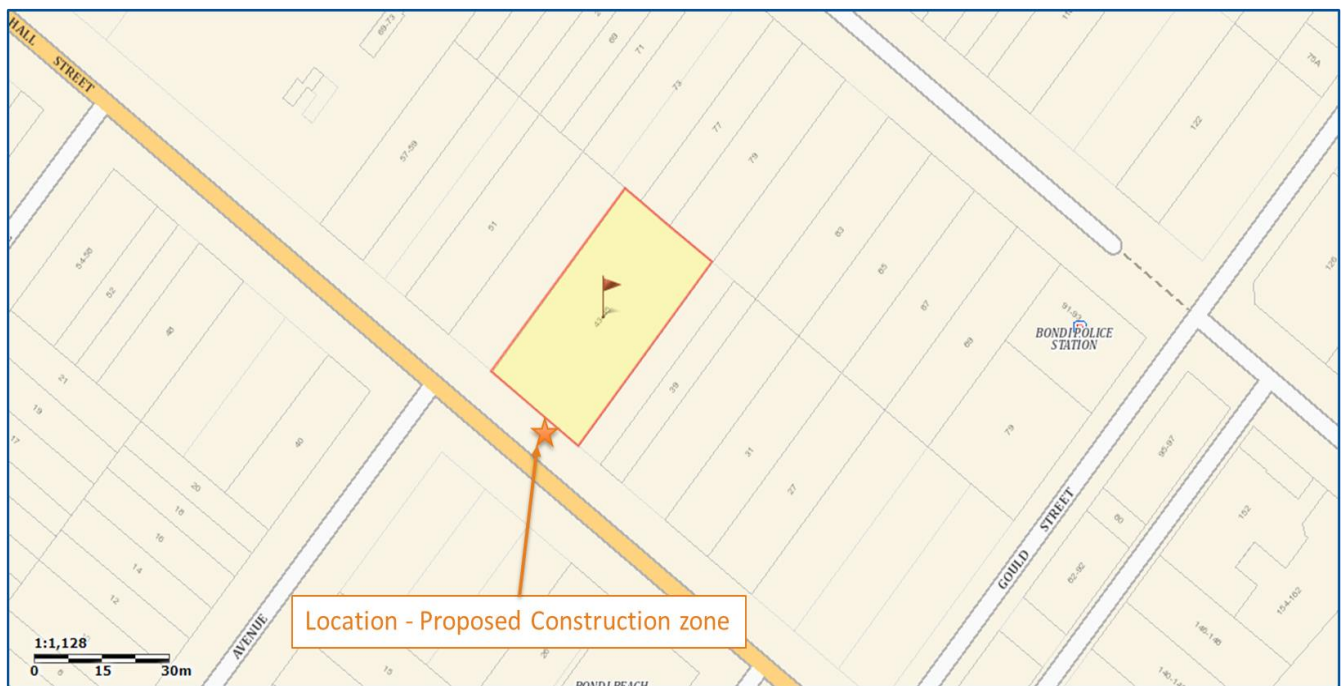


Figure 1. Site location.



Figure 2. Location to install construction zone signs.

2. Introduction/Background

In accordance with standard practice at Council, it is proposed that the construction zone is signposted 'No Parking 7 am–5 pm Mon–Fri, 8 am–3 pm Saturday Council Authorised Vehicles Excepted' for the approved construction hours under the development consent. Council will then supply the applicant with transferable permits to be used on the applicant's construction vehicles. The Traffic Committee and Council's requirements for permit approvals are a minimum length of 9 metres along the site frontage with a minimum period of 13 weeks.

3. Technical Analysis

The subject site has a frontage of 24 metres on Hall Street including a 3-metre-wide driveway accessing No. 43.

The applicant has requested a 18.5 metre construction zone along the frontage on Hall Street.

Council officers propose to install a 18.5 metre construction zone. The 18.5 metre construction zone will include the existing driveway accessing the property. The existing and recommended parking allocation is shown in Figures 3 and 4 below.

The construction zone will result in the displacement of 2 on-street car parking spaces.

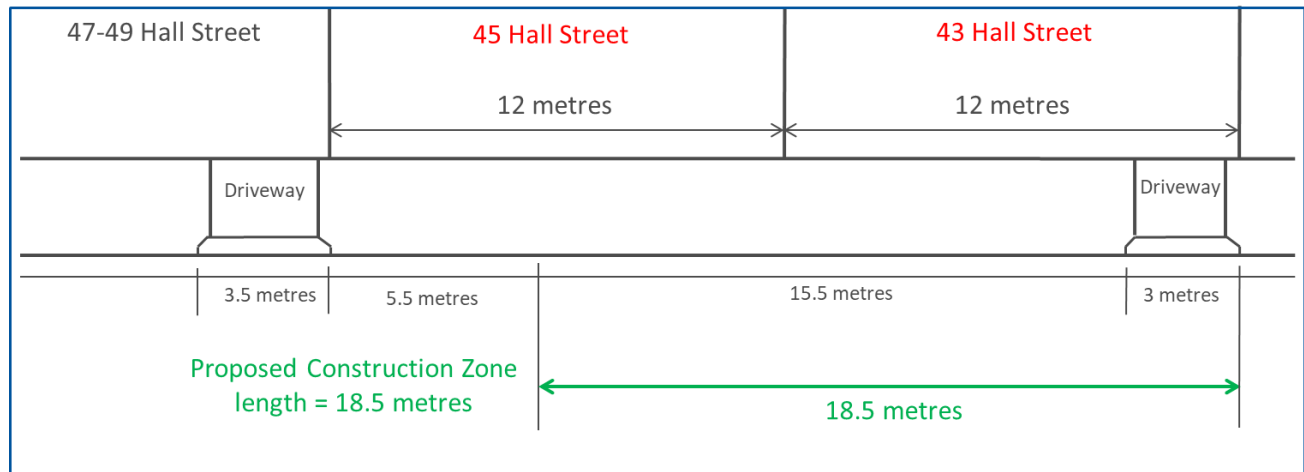


Figure 3. On-street parking for site and adjacent properties.

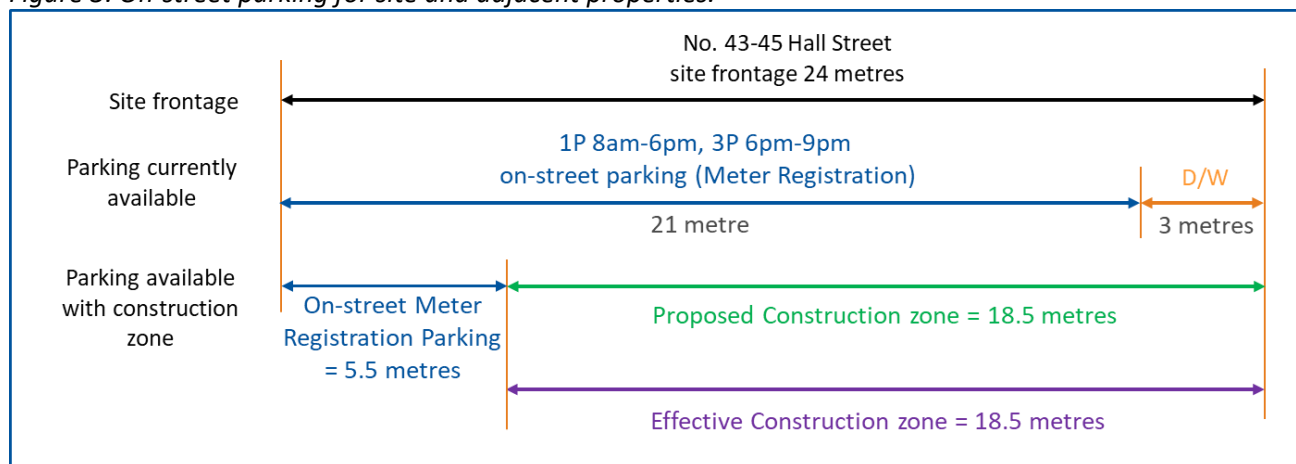


Figure 4. On-street parking for site frontage only.

Table 1. Application details

Applicant	James Maroun
Development application	DA-391/2018
Works	Demolition of mixed use building and granny flat and construction of a five storey shop top housing development with integrated basement carparking
Approved hours of construction	7 am–5 pm Monday–Friday; 8 am–3 pm Saturday
Frontage length	24 metres
Road	Hall Street
Existing parking	1P 8am-6pm, 3P 6pm-9pm on-street parking (Meter Registration)
Length requested by applicant	18.5 metres
Length to be signposted	18.5 metres
Effective construction zone - Total length available for construction	18.5 metres
Duration	1 August 2021 to 29 February 2022
Fee area	Areas zoned neighbourhood centre, commercial core, or mixed use
Estimated fees	\$2,511.50 per week (18.5 metres x \$95.00 per metre + 2 metered parking spaces x \$377)

Signage

The proposed signage is shown below.

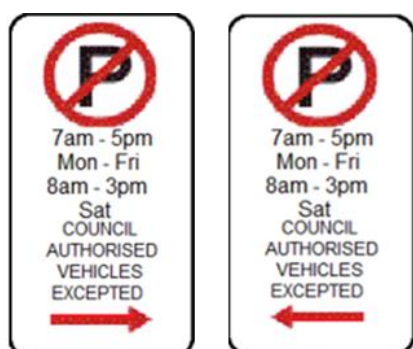


Figure 4. Proposed signage.

4. Financial Information for Council's Consideration

The applicant will be required to pay the approved application fee, which covers the cost of administration and signage.

The approved usage charges for the 2020–2021 financial year from 1 January 2021 are:

- \$95.00 per metre per week in a commercial or mixed-use area.
- \$377.00 per metered car parking space per week in addition to above fees (where applicable).

The cost to the applicant for the 18.5 metres made available for construction vehicles will be \$2511.50 per week (18.5 metres x \$95.00 per metre + 2 metered spaces x \$377.00).

5. Attachments

Nil

REPORT
TC/V.08/21.03

Subject: Francis Street south of Wellington Street, Bondi Beach -
Proposed Double Centre (BB) Line Marking

TRIM No: A03/0042-04

Author: Malik Almuhanha, Senior Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Installs 10 metres of B-B double centre line marking on Francis Street south of Wellington Street, Bondi Beach, as shown in Attachment 1 of the report.
2. Extends the existing statutory 'No Stopping' restrictions on Francis Street south of Wellington Street, Bondi Beach, to meet the minimum 10 metre distance requirement as shown in Attachment 1 of the report.

1. Executive Summary

Council has received requests to review the intersection of Francis Street and Wellington Street, Bondi Beach. This is due to motorists turning into Francis Street risking a collision with exiting traffic because of the narrow width of Francis Street.

No Stopping restrictions at Francis Street south of Wellington Street are currently located 7 metres on the northern side and 3 metres on the southern side in from Wellington Street. It is proposed to increase this to 10 metres on both sides of Francis Street in accordance with statutory No Stopping distances.

The addition of a 10 metre double barrier (BB) line-marking along the centre of Francis Street is also recommended to enhance safety at the intersection and better guide motorists entering Francis Street from Wellington Street. Right turning motorists inbound to Francis Street currently tend to encroach into what should be the outbound lane.

The on-street parking available on Francis Street will be reduced by 7 metres on the southern side and 3 metres on the northern side.

Figure 1 shows the proposed restrictions.

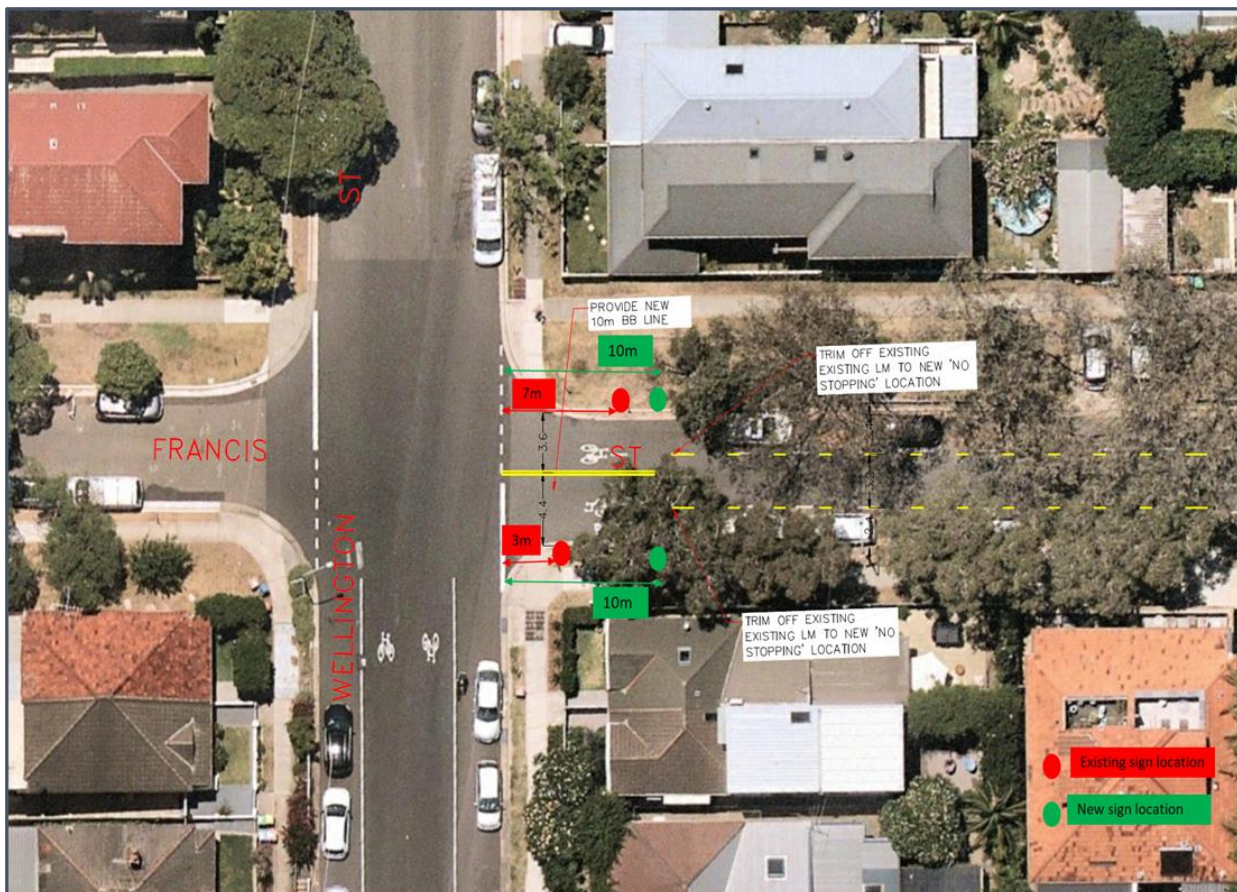


Figure 1: Proposed BB line and extensions of statutory 'No Stopping' restrictions

2. Introduction/Background

Council has received feedback requesting a review of the existing traffic arrangement at the intersection of Francis Street and Wellington Street, where it is reported that motorists turning right from Francis Street cross over into the outbound lane on Wellington Road.

This report recommends the installation of 10m BB lines as well as adjusting the existing statutory 'No Stopping' restrictions to 10m to meet standards.

3. Technical Analysis

Francis Street is an 8m wide street south of its intersection with Wellington Street. It services a two-way traffic movement with parking on both sides. The current signposted 'No Stopping' restrictions at Francis Street are 7m and 3m long along the northern and southern side respectively, which is less than the minimum 10m statutory (see Figure 1).

Motorist have been reported to be turning to the middle of Francis Street risking a collision with exiting traffic. Installation of a double barrier (BB) line-marking will address this issue and improve turning paths for vehicles entering/exiting Francis Street, ensuring vehicles are better guided to turn to the right side of the road.

Two parking spaces will be removed as a result of improving statutory 'No Stopping' distance. Affected residents will be notified of Council's outcome.

4. Financial Information for Council's Consideration

Council will fund the installation of the intersection treatment.

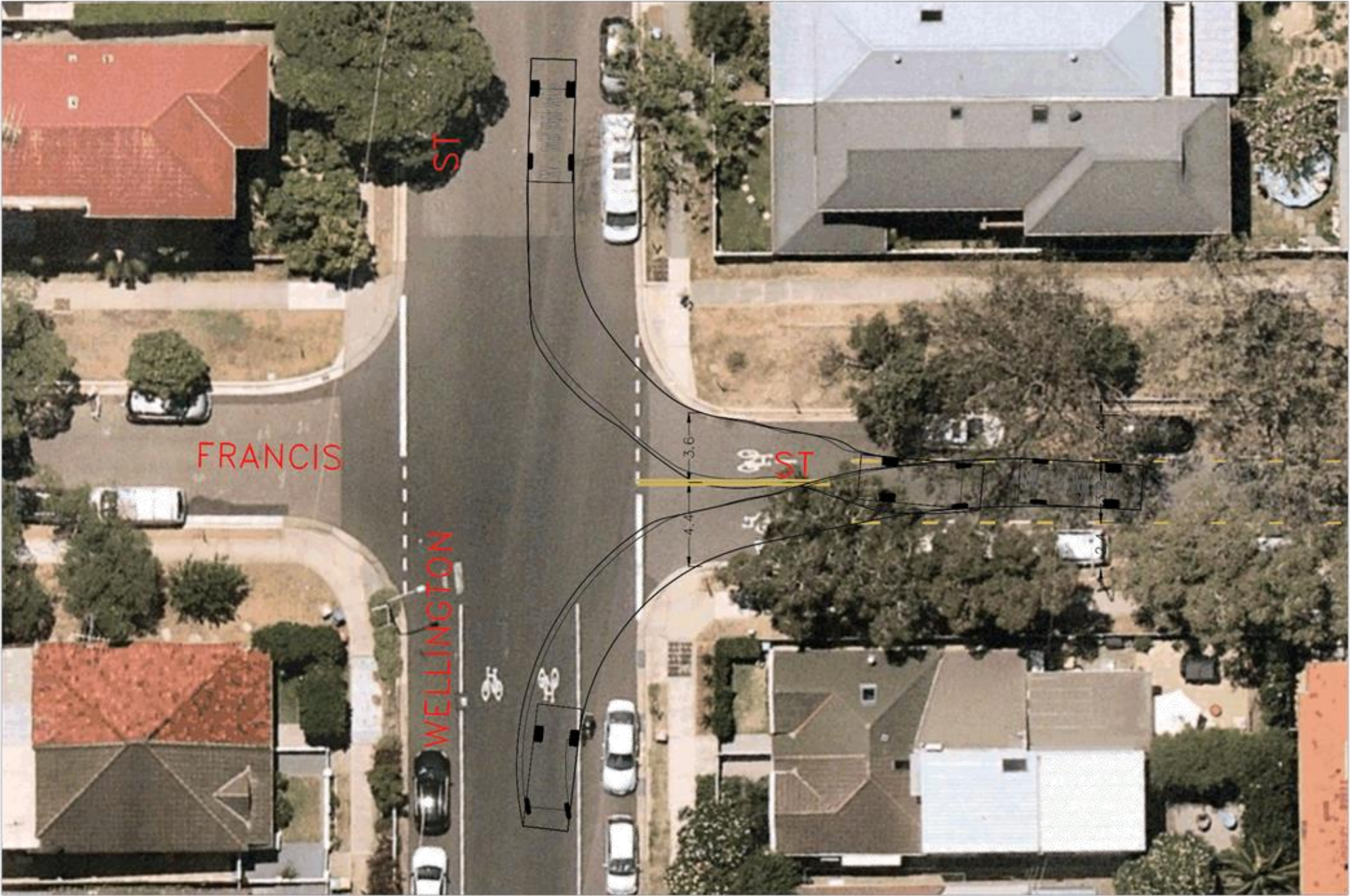
5. Attachments

1. Francis Street Design [↓](#)



FOR DISCUSSION

[illegible]



SRV – Small Rigid Vehicle

Overall Length6.400m

Overall Width2.330m

Overall Body Height3.500m

Min Body Ground Clearance0.398m

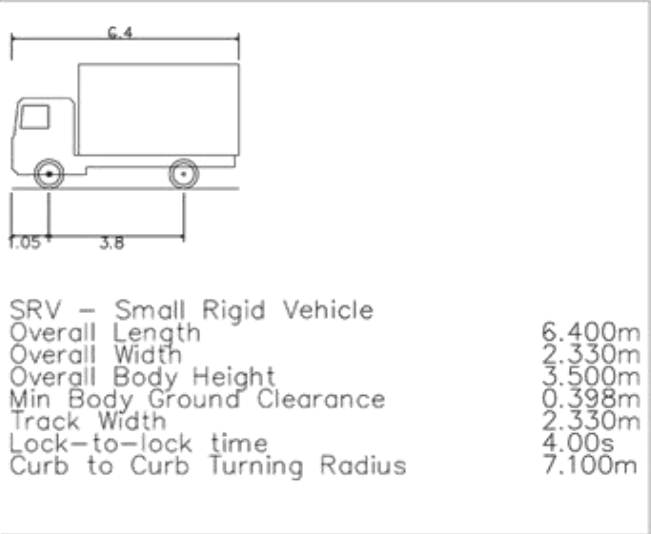
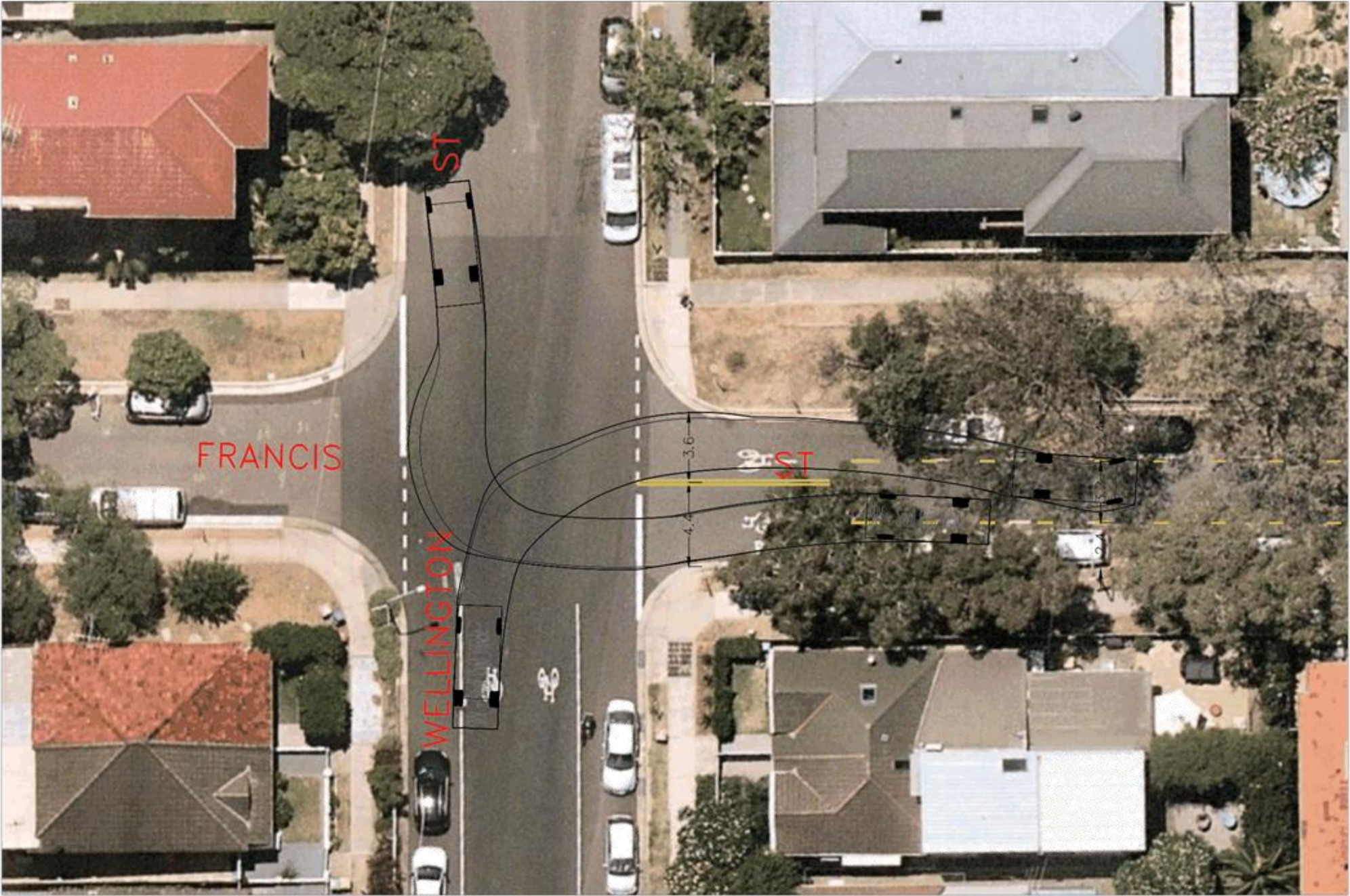
Track Width2.330m

Lock-to-lock time4.00s

Curb to Curb Turning Radius7.100m

FOR DISCUSSION

										Prepared by: Civil Design Major Projects 30 Clifton Street, Bondi Junction NSW, 2022 Ph: (01) 9533 1000 PO Box 9, Bondi Junction NSW 1555	Approved for Construction: Date: Scale: 1:250 A3	Project Name: FRANCIS ST BB LINES Drawing Title: SRV - LEFT TURNS	Drawing No: SK002	Issue: 1
	1	25/02/21	FOR TRAFFIC COMMITTEE	PS	PS	CHKD	APRVD	ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD
	ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD	ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD



									Sealed By: _____ Date: _____ Witnessed: _____	 Prepared by: Civil Design, Major Projects 30 Clifton Street, Bondi Junction, NSW, 2022 Ph: (02) 9533 8000 PO Box 9, Bondi Junction NSW 1535	Approved for Construction: Date: Scale: 1:250 A3	Project Name: FRANCIS ST BB LINES Drawing Title: SRV - RIGHT TURNS	FIG No: TRM No: Drawing No: SK003	Issue: 1
	1	25/02/21	FOR TRAFFIC COMMITTEE	PS	PS	CHKD	APRVD	ISSUE	DATE	ISSUE DESCRIPTION	DRWN	DES	CHKD	APRVD

FOR DISCUSSION

REPORT
TC/CV.01/21.03

Subject: Bondi to Bronte Coastal Walk - One Way System

TRIM No: A20/0500

Author: Paul Cai, Traffic Engineer
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Does not implement a permanent one-way road system for Gaerloch Avenue and Dellview Street.
2. Undertakes a detailed assessment of appropriate crossing treatments at:
 - (a) Wilga Street on the eastern side of Sandridge Street - a continuous footpath treatment.
 - (b) Fletcher Street on the eastern side of Sandridge Street - a marked pedestrian crossing.
 - (c) Kenneth Street on the eastern side of Alexander Street - a marked pedestrian crossing with kerb extensions.
 - (d) Southeast of Gaerloch Avenue at 22 Gaerloch Avenue (the 90-degree bend in the road) - a continuous footpath treatment.
3. Delegates authority to the Executive Manager, Infrastructure Services, to investigate alternative designs as part of the detailed design assessment.

1. Executive Summary

At its November 2020 Council meeting Council resolved to undertake community consultation for converting Dellview Street and Gaerloch Avenue to a one way pair and pedestrian crossing treatments between Bondi and Tamarama Beaches along Sandridge Street, Alexander Street, and Gaerloch Avenue.

The community consultation was to include interested Precinct Committees, and the area bounded by and including:

- a) Bondi Road and Hunter Park to the north.
- b) Boonara Avenue, Tamarama Street, Alfred Street to the west.
- c) Hewlett Street to the south.

Precinct Committee discussions

The proposal was discussed at South Bondi / Tamarama (joint) and Rose Bay precinct meetings. Objections were raised to the one-way road proposal. Concerns were raised over the pedestrian crossing treatments

being an alternative to widening the coastal walk instead. Some attendees did not see the need for the pedestrian treatments.

Community survey feedback

The community consultation was undertaken by letter box drop. It presented three options:

1. Implement the proposed one-way road system and alternative pedestrian route comprising:
 - An alternative pedestrian route between Bondi and Tamarama Beach via Gaerloch Avenue, Alexander Street, and Sandridge Street.
 - A permanent one-way road system for Gaerloch Avenue and Dellview Street (the same system that is temporarily put in place during Sculpture by the Sea).
2. Do nothing/ leave as it is.
3. Do something else.

About 1800 letters were distributed in the survey areas shown in Figure 1. 241 individual responses were received. Of the individual responses:

- 29% were in favour of implementing the proposed one-way road system and alternative pedestrian route.
- 51% were in favour of doing nothing.
- 18% suggested doing something else.
- 2% did not select an option, instead choosing to raise issues. 2 responses were blank.

The responses to the pedestrian crossing treatments were overshadowed by the strong opposition to the one-way system. A review of the written responses indicated there was support for improving conditions for pedestrians but not as an alternative to the Coastal Walk.

It is recommended that permanent one-way road system for Gaerloch Avenue and Dellview Street is not adopted.

It is recommended that pedestrian treatments are further investigated to improve safety for both local residents and other people accessing the beaches and coastal walk. This may result in the crossing as propose being implemented or alternative crossing treatments being considered.



Figure 1. Area map of community consultation.

2. Introduction/Background

Council resolved a number of actions in response to the management of COVID-19 at its meeting on 15 September 2020 (CM/6.1/20.09). One action was that Council:

'Investigates a possible solution to overcrowding on the Bondi to Bronte Coastal Walk to see if a one-way system could be safer; for example, one-way south on the actual walkway and a safe northern passage along our streets'.

A report submitted to Waverley Traffic Committee on 22 October 2020 identified the traffic related infrastructures required to manage overcrowding on the Bondi to Bronte Coastal Walk and a street walk route. The infrastructures involve converting Gaerloch Avenue and Dellview Street from two way to one way:

- Gaerloch Avenue (between Kenneth Street and Pacific Avenue): southbound only from Kenneth Street to Pacific Avenue / Dellview Street.
- Dellview Street (between Gaerloch Avenue and Fletcher Street): northbound only from Gaerloch Avenue to Fletcher Street.

This is intended to minimise potential conflict with pedestrians on the street walk route.

Council determined the need for consultation with the residents of Gaerloch Avenue, Dellview Street and surrounding streets on the permanent one-way road system for Gaerloch Avenue and Dellview Street, and footpath treatments for an alternative pedestrian route between Bondi and Tamarama Beach.

Council also determined the need to reassess the continuous footpath treatments at the southern end of Gaerloch Avenue with consideration being given to the street walk on the southern and eastern sides of Gaerloch Avenue rather than the northern and western sides, as proposed.

The table below sets out the previous Council resolutions:

Table 1. Previous Council resolutions.

Meeting and date	Minute No.	Decision
Council 17 November 2020	CM/5.2.2/20.11	<p>That Council defers this item to a Councillor workshop to give consideration to the following:</p> <ol style="list-style-type: none"> 1. Endorses the following traffic management concepts to be constructed, if and when deemed necessary, between Bondi Beach and Tamarama Beach for community consultation: <ol style="list-style-type: none"> (a) Gaerloch Avenue on the eastern side of Dellview Street – 2 x ‘No Entry’ signs preventing access to Gaerloch Avenue eastbound. (b) Gaerloch Avenue on the light pole opposite 31 Gaerloch Avenue (the 90-degree bend in the road) – A ‘No Right Turn’ sign preventing access to Gaerloch Avenue northbound. (c) Gaerloch Avenue eastern side – 2 x ‘One Way (Arrow Right)’ signs indicating one-way direction on Gaerloch Avenue. (d) Gaerloch Avenue western side facing Kenneth Street – a ‘One Way (Arrow Left)’ sign indicating one-way direction on Gaerloch Avenue. (e) Dellview Street on the southern side of Fletcher Street – 2 x ‘No Entry’ signs preventing access to Dellview Street southbound. (f) Dellview Street eastern side between Carlisle Street and Dellview Lane – 2 x ‘One Way (Arrow Left)’ signs indicating one-way direction on Dellview Street. (g) Dellview Lane on western side of Dellview Street – A ‘No Right Turn’ sign preventing access to Dellview Street southbound. (h) Carlisle Street on the northern side of Dellview Street – a ‘No Right Turn’ sign preventing access to Dellview Street southbound. (i) Dellview Street on the southern side of Carlisle Street – 2 x ‘No Entry’ signs preventing access to Dellview Street southbound.

Meeting and date	Minute No.	Decision
		<p>(j) Western side of Dellview Street facing Gaerloch Avenue – A 'One Way (Arrow Right)' indicating one-way direction on Dellview Street.</p> <p>2. Endorses the following traffic control devices concepts to be constructed between Bondi Beach and Tamarama Beach for community consultation:</p> <p>(f) A continuous footpath treatment – Wilga Street on the eastern side of Sandridge Street.</p> <p>(g) A marked pedestrian crossing – Fletcher Street on the eastern side of Sandridge Street.</p> <p>(h) A marked pedestrian crossing with kerb extensions – Kenneth Street on the eastern side of Alexander Street.</p> <p>(i) A continuous footpath treatment – 22 Gaerloch Avenue (the 90-degree bend in the road), subject to the reassessment in clause 3 below.</p> <p>(j) A continuous footpath treatment – Gaerloch Avenue on the eastern side of Dellview Street, subject to the reassessment in clause 3 below.</p> <p>3. Reassesses the need for continuous footpath treatments at the southern end of Gaerloch Avenue with consideration being given to the Street Walk on the southern and eastern sides of Gaerloch Avenue rather than the northern and western sides, as proposed, with outcomes to be included in the detailed design.</p> <p>4. Undertakes community consultation with interested Precinct Committees, and for the area bounded by and including:</p> <p>(a) Bondi Road and Hunter Park to the north.</p> <p>(b) Boonara Avenue, Tamarama Street, Alfred Street to the west.</p> <p>(c) Hewlett Street to the south.</p> <p>5. Prepares a report on the outcomes of the community consultation to Council's Traffic Committee.</p> <p>6. Prepares detailed plans and warrants for crossings and treatments identified in clause 2 above, to be submitted to Council's Traffic Committee for review and approval by Council, subject to the outcomes of</p>

Meeting and date	Minute No.	Decision
		<p>the community consultation.</p> <p>7. Investigates footpath widening to support the Street Walk route.</p> <p>8. Delegates authority to the Executive Manager, Infrastructure Services, to investigate alternative designs as part of the detailed design assessment.</p>
Council 15 September 2020	CM/6.1/20.09	<p>That Council:</p> <p>1. Notes that:</p> <p>(a) COVID-19 Beach Management plans for this spring and summer have been developed and will be published in the near future.</p> <p>(b) The plan includes significant communication on beach capacity, and infrastructure, increased staffing and an escalation protocols to manage and restrict numbers and access, should numbers at our beaches and public open spaces be too great. The closure of our beaches, open spaces and coastal walk is a last resort and safe access to the water will be maintained.</p> <p>(c) The plan also involves working with closely with other agencies, including NSW Police, NSW Health, Transport for NSW, Surf Life Saving NSW, other councils and LGNSW to take a whole of system approach to managing beach numbers, including promoting social distancing and public health order compliance and trying to prevent too larger numbers of people attending the beach.</p> <p>(d) The on-beach components of this planning work form the basis of current operations, with additional rangers and lifeguards continuing to monitor numbers and ensure the current PHOs are being implemented.</p> <p>(e) In developing the plans, extensive consultation and collaboration have been undertaken with NSW Police, NSW Health, Transport for NSW, and between beach-side Councils in Sydney.</p> <p>(f) The PHOs no longer specify maximum numbers in public open spaces, and instead specify that people can gather in groups of up to 20, and the one person per four square metres rule applies. It is further noted that social distancing of 1.5 metres is health advice only and is not a PHO requirement, but is being encouraged in our</p>

Meeting and date	Minute No.	Decision
		<p>beach management approach.</p> <p>(g) LGNSW has convened a meeting of beach-side Mayors, the Police and Surf Life Saving NSW that has agreed that LGNSW write to the Premier on our behalf seeking funding for additional staff to act as COVID ambassadors/marshals and the development and funding for an education and advertising campaign addressing beach crowding and behaviour.</p> <p>(h) This year's spring and summer temperatures are expected to be above average.</p> <p>2. Enters into discussions with the Bondi and District Chamber of Commerce with a view to establishing a voluntary register of businesses and venues that have COVID-safe plans as a means to encourage and facilitate the adoption of best practice COVID-safe plans by businesses and venues, and that potential customers can refer to.</p> <p>3. Develops a COVID counter on Council's home page that shows the number of COVID-19 cases in Waverley at a specified date and refers people to the NSW Health website for further information.</p> <p>4. Writes to the Minister for Health and Medical Research, the Minister for Planning and Open Spaces, and the Minister for Transport seeking a meeting with a delegation of mayors from Waverley, Woollahra, Randwick and Bayside Councils, to discuss the PHOs, our COVID-19 plans, specific challenges, given the ease of access to our iconic city beaches that will need to be met this coming summer, and initiatives for how these may be met.</p> <p>5. Immediately steps up our own community education efforts aimed at encouraging people to help stop the spread, including wearing masks and continuing to use hand sanitiser, as well as a broad social media campaign, given the number of active cases of COVID-19 has been slowly increasing in the Waverley local government area.</p> <p>6. Acknowledges the difficult job our rangers, lifeguards, ambassadors and other staff have in trying to keep our residents, community and visitors safe and appreciates their professionalism and dedication in the execution of their duties in this COVID pandemic environment.</p> <p>7. Investigates a possible solution to overcrowding on the Bondi to Bronte Coastal Walk to see if a one-way</p>

Meeting and date	Minute No.	Decision
		system could be safer; for example, one-way south on the actual walkway and a safe northern passage along our streets.

3. Technical Analysis

Reassessment of continuous footpath on Gaerloch Avenue

At its meeting on 17 November 2020, Council decided to further assess the need for continuous footpath treatments at the southern end of Gaerloch Avenue “with consideration being given to the Street Walk on the southern and eastern sides of Gaerloch Avenue rather than the northern and western sides, as proposed, with outcomes to be included in the detailed design.”

The assessment concluded that a single crossing is preferable on the south-eastern side of Gaerloch Avenue.

General community feedback

Figure 3 presents the community feedback on the proposed one-way road system and alternative pedestrian route. Over 50% of the responses objected to the proposal. 29% of the responses were in favour of the proposal.

It is clear that there is not strong community support for the one way road system. The support for improving pedestrian crossing points is not clear from the raw data as it was linked to the one way road system. A review of the written responses indicated there was support for improving conditions for pedestrians but not as an alternative to the Coastal Walk.

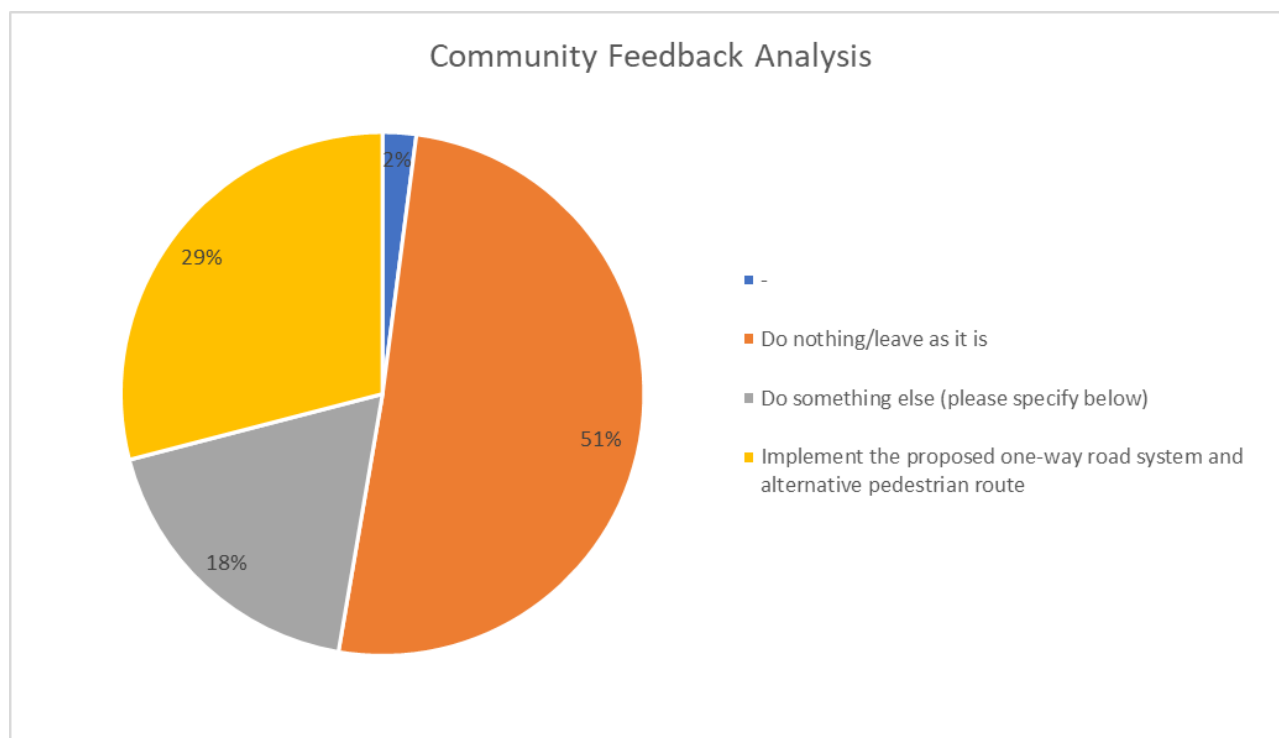


Figure 3. Community feedback on the proposed one-way road system and alternative pedestrian route.

Feedback from residents of Gaerloch Avenue and Dellview Street

Table 1 presents the responses received by street from residents most directly impacted by the proposal.

55 responses (about 23% of total responses) were received from residents on Dellview Street. 72% of the responses from Dellview Street objected to the proposal. 11% were in favour of the proposed one-way road system and alternative pedestrian route treatments.

15 responses (about 6% of total responses) were received from residents on Gaerloch Avenue. 40% of the responses were in favour of the proposed one-way road system and alternative pedestrian route treatments. About 33% of the response objected to the proposal.

For the surrounding streets (Kenneth Street, Alexander Street, Sandridge Street, Fletcher Street and Wilga Street), 42% of the respondents support the proposed one-way road system and alternative pedestrian route treatments while 42% opposed the proposal.

Table 1. Feedback from residents on Dellview Street, Gaerloch Avenue and other streets

Street	Options				Total
	Do nothing/ leave as it is	Do something else	Implement the proposed one-way road system and an alternative pedestrian route	Blank	
Dellview	40	8	6	1	55
Gaerloch	5	4	6	0	15
kenneth	5	2	6	0	13
Alexander	3	1	6	0	10
Sandridge	2	2	3	0	7
Fletcher	7	4	6	0	17
Wilga	6	0	2	0	8
Others	54	23	35	4	116
Total	122	44	70	5	241

Taken into consideration the feedback from directly affected residents and the overall response it is recommended that a one way road system is not implemented in Dellview Street and Gaerloch Avenue.

4. Financial Information for Council's Consideration

The works considered in this report are currently unfunded.

5. Attachments

Nil

REPORT
TC/CV.02/21.03

Subject: Flood Street and Evans Street linemarking - Resheeting and Linemarking

TRIM No: A18/0579

Author: Hamoon Bahari, Professional Engineer, Traffic and Transport
Calum Hutcheson, Service Manager, Traffic and Transport

Director: Dan Joannides, Executive Manager, Infrastructure Services

COUNCIL OFFICER'S PROPOSAL:

That Council:

1. Approves the line-marking of the pavement within:
 - (a) Flood Street, Bondi (Attachment 1).
 - (b) Evans Street, Bronte (Attachment 2).
2. Delegates authority to the Executive Manager, Infrastructure Services, to modify the designs should on-site circumstances warrant changes.

1. Executive Summary

Council will be conducting re-sheeting works at Flood Street, Bondi and Evans Street, Bronte to improve traffic flows and enhance vehicular safety. The line-marking works will introduce E1 edge line to differentiate the travel lane from the parking lane on both streets. An S1 centreline will be installed only at Evans Street as per the design.

It is recommended that the line-marking works are supported. The locations of the works are shown below in Figure 1.



Figure 1. Evans Street and Flood Street.

2. Introduction/Background

Council will be conducting re-sheeting works at Flood Street, Bondi from the intersection of Old South Head Road to the intersection at Bondi Road. Similarly, re-sheeting work will be carried out at Evans Street between Gardyne Street and Macpherson Street. This re-sheeting work gives council the opportunity to propose new line-marking to go ahead and improve vehicular and pedestrian safety.

Shown below in Figures 2 and 3 are examples of E1 and S1 line-marking.

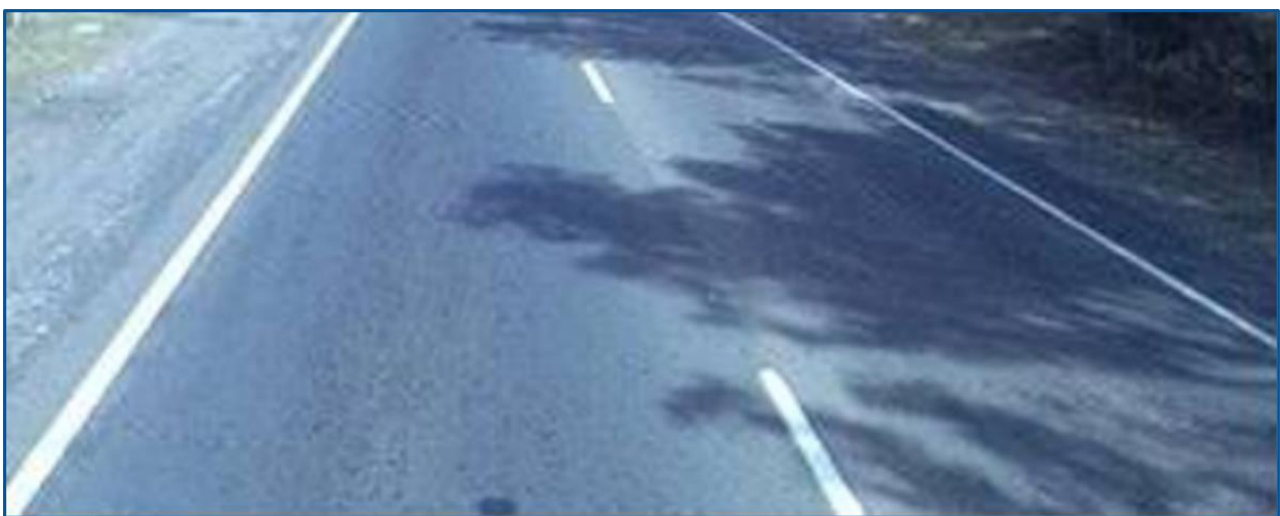


Figure 2. E1 Edge line marking on general purpose roads.



Figure 3. S1 dividing (separation) line on general purpose roads.

Section 4 – Delineation, Longitudinal Markings of the RMS/TfNSW Delineation Guide specifies the requirements for line-marking.

E1 Edge Lines are lines painted on the left hand edge of the travel lane to delineate the outer edges of the shoulder lane and the travel lane. It is primarily used on general purpose roads, dual carriageways and divided carriageways. They also provide a continuous guide and boundaries for motorists to stay inside.

S1 Dividing lines are lines painted to separate the opposing traffic movement on undivided two way roads. Overtaking and turning movement across the lines may be made in both directions.

Flood Street

Flood Street, Bondi is a north to south street with parking on both sides. The new line-marking will be installed taking into consideration the existing signage and traffic restrictions. The proposal introduces an E1 Edge line separating the parking lane from the through travel lane on both sides of the street

Evans Street

Evans Street, Bronte is a north to south street with parking on both sides. The new line-marking will be installed taking into consideration the existing signage and traffic restrictions. The proposal introduces:

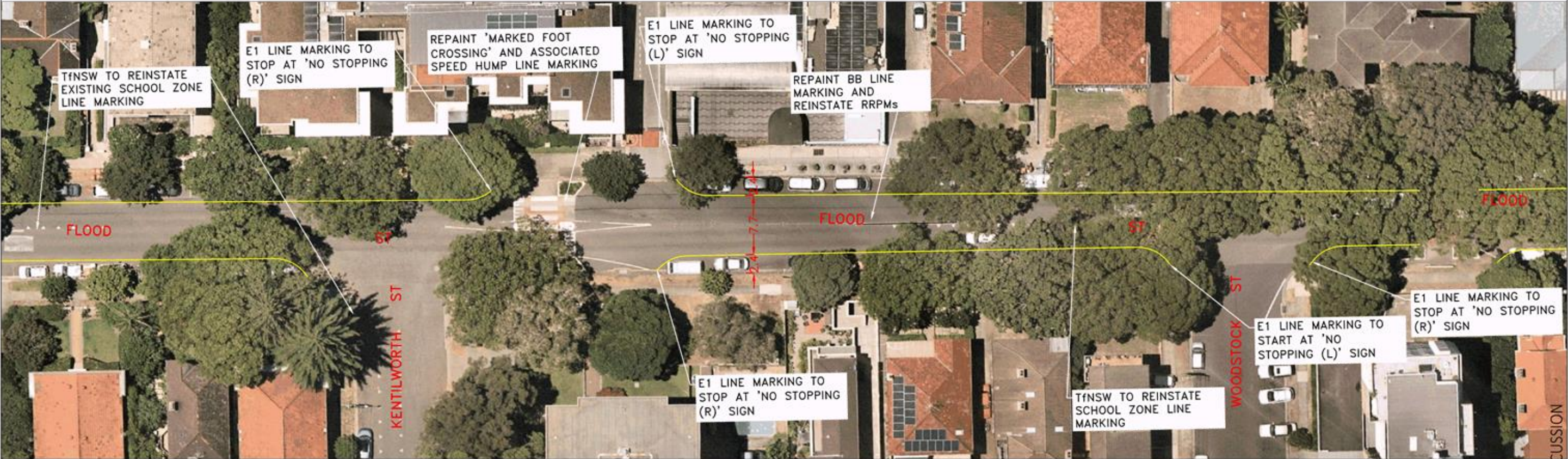
1. An E1 Edge line separating the parking lane from the through travel lane on both sides of the street
2. A S1 broken dividing centreline from Gardyne Street to Macpherson Street

3. Financial Information for Council's Consideration

The line-marking works will be funded under existing road re-sheeting Council budgets C0894, C0902.

4. Attachments

1. Flood Street Linemarking Plan [↓](#)
2. Evans Street Linemarking Plan [↓](#)



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